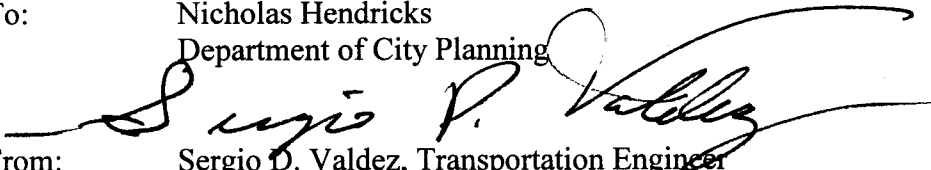


I-210 Freeway & La Tuna Cyn Road  
SFV03-009

Date: August 17, 2004

To: Nicholas Hendricks  
Department of City Planning

  
From: Sergio D. Valdez, Transportation Engineer  
Department of Transportation

**Subject: CLARIFICATION OF RECOMMENDED TRAFFIC IMPROVEMENTS FOR THE PROPOSED 280 SINGLE FAMILY DWELLING UNITS (CANYON HILLS) AND AN EQUESTRIAN PARK AT I-210 FREEWAY AND LA TUNA CANYON ROAD. (ENV-2002-2481-EIR)**

The Department of Transportation (DOT) has conducted further review of its recommended traffic improvements for the proposed Canyon Hills project. DOT has determined that the prior recommendation contained in our December 31, 2003 letter to Maya Zaitzevsky (as well as in prior DOT correspondences to the Department of City Planning on this project) for a center left-turn lane on segments of La Tuna Canyon Road along the project frontage that are not in the vicinity of a project access point is not required because such a lane is not needed for traffic capacity since no left turn movements in either the eastbound or westbound direction can be made in these segments. Instead, DOT will require that adequate improvements be constructed on La Tuna Canyon Road to provide two through travel lanes in each direction along the project frontage, with separate left-turn lanes provided at the project access points for Development Areas A and B, to the satisfaction of DOT. In addition, DOT reserves the right to consider separate right-turn deceleration lanes to the satisfaction of DOT at the vehicular access points to Development Areas A and B. These improvements will modify the existing two-lane segment of La Tuna Canyon Road (i.e., one lane in each direction) that is along the project frontage (approximately one half-mile in length located approximately one mile west of the I-210 interchange) to provide two lanes in each direction.

It is noted that a second two-lane segment of La Tuna Canyon Road currently exists west of the project site (also approximately one half-mile in length). Unlike the easterly two-lane segment, this westerly two-lane segment provides a center two-way left-turn lane (in addition to the one through travel lane in each direction) to facilitate left-turn access to the residential properties fronting La Tuna Canyon Road in this area. The westerly two-lane segment is not adjacent to the project frontage, although a point of access to the proposed Equestrian Park will occur in this area of La Tuna Canyon Road opposite Wildwood Fire Road. DOT agrees with the findings provided in the traffic study that the Canyon Hills project will not significantly impact operations on the westerly two-lane

segment of La Tuna Canyon Road. Therefore, DOT will not recommend street improvements on the westerly "off-site" two-lane segment of La Tuna Canyon Road.

Further, in addition to our prior recommendation to install a traffic signal at the La Tuna Canyon Road intersection with the Development Area access point opposite the I-210 westbound on/off ramps, DOT has received a request from the developer's traffic engineer to consider installation of traffic signal control along La Tuna Canyon Road at one of the entrances to the Development Area B portion of the project site, as well as at the Equestrian Park.

The following shall replace the section titled "PROJECT REQUIREMENTS" contained in our December 31, 2003 letter to the Department of City Planning. However, the other mitigation measures previously mentioned, such as the traffic signal previously mentioned are not precluded.

### **PROJECT REQUIREMENTS**

#### **A. Highway Dedication and Improvements**

La Tuna Canyon Road is a designated Secondary Highway in the Streets and Highways Element of the City's General Plan. La Tuna Canyon Road currently consists of a variable width roadway with predominantly unimproved sidewalk. Standard Plan S-470-0, effective November 10, 1999, dictates that the standard cross section for a Secondary Highway is a 35-foot half roadway on a 45-foot half right-of-way. The developer shall dedicate along the entire frontage on La Tuna Canyon Road to bring the half right-of-way up to the standard required by the General Plan. Further, as outlined below, the developer shall widen La Tuna Canyon Road within the project frontage to the satisfaction of DOT to provide two through travel lanes in each direction, plus separate left-turn lanes at the vehicular access points to Development Areas A and B. Relocate and modify any street lights, curbs and gutters, trees, utilities, etc. as required. With the provision for the dedication to be provided by the developer, DOT or others may further improve La Tuna Canyon Road to the General Plan half roadway standard if ever deemed required by DOT in the future.

The applicant should contact the Bureau of Engineering, Department of Public Works (BOE) to ensure compliance with requirements of the municipal code. Additional street improvements may be required by BOE.

#### **B. Equestrian Park**

Activities, including but not limited to competitions and shows, where mass gathering of spectators and participants are anticipated, shall be prohibited as land use for the equestrian park.

#### **C. Site Access and Internal Circulation**

Adverse traffic impacts could occur due to access and circulation issues.

1. The access to Development Area A on La Tuna Canyon Road at I-210 westbound ramps shall be aligned as the north leg of this proposed signalized intersection.
2. To avoid vehicles encroaching onto the public right-of-way, a minimum 40-foot reservoir space (distance between property line and first parking stall and/or gate) shall be provided at each driveway.
3. The proposed driveways at Development Area B south of the I-210 Freeway shall be located away from any blind curve along La Tuna Canyon Road. Queuing and merging area shall be provided for ingress and egress vehicles respectively. DOT recommends that a minimal number of driveways be designed to serve Development Area B, but the number of driveways shall be consistent with the requirement(s) of other City departments.
4. Backing in and out onto arterial highways or collector streets is not permitted; therefore, the path and location of all trucks and vehicles with horse trailers shall be indicated on the site plan.
5. A minimum of two lanes in each direction shall be provided along the project frontage on La Tuna Canyon Road. Separate left-turn lanes to the satisfaction of DOT shall be provided at the vehicular access points to Development Areas A and B. The existing lane configuration on La Tuna Canyon Road at the proposed access to the Equestrian Park opposite Wildwood Fire Road consisting of one travel lane in each direction plus a center left-turn lane is satisfactory to DOT.
6. The developer shall install a traffic signal at the La Tuna Canyon Road intersection with the Development Area A access opposite the I-210 Freeway westbound on/off ramps.
7. DOT reserves the right to consider requesting separate right-turn deceleration lanes to the satisfaction of DOT at the vehicular access points to Development Areas A and B.

If you have any further questions, you may contact Mr. Ray Lau at (818) 374-4699.

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