

Streets

Chapter 3





3.1 Streets

A. Purposes

These zoning regulations are intended to:

1. Connect the area to its neighboring communities, the City of Los Angeles, and the greater Los Angeles region through a safe, efficient and accessible circulation network that embraces pedestrians, bicyclists, transit, truck traffic, and automobiles.
2. Recognize the shared use of streets not only for moving traffic, but also as the front door to businesses, which are the economic and fiscal foundation of the City, and as public outdoor space for residents and workers.
3. Develop an efficient yet balanced circulation system that defines different types of streets based on their transportation function and community role.
4. Provide residents, employees, and visitors with a variety of transportation alternatives that result in a more efficient use of transportation resources.
5. Encourage a vibrant pedestrian-oriented environment with activity centered along property edges at the interface between buildings and streets.
6. Design streets and sidewalks so that pedestrians, bicyclists, transit riders, transit vehicles, trucks and automobile traffic can coexist safely.
7. Build linkages to the neighboring Chinatown, Lincoln Heights, Cypress Park, Elysian and Heritage Square neighborhoods to nearby regional park amenities such as Elysian Park, Debs Park, El Rio de Los Angeles State Park, and to the Arroyo Seco and to Los Angeles River Greenways.
8. Promote a multi-modal street network.
9. Establish recommended standards for modified cross sections.
10. Illustrate modified street standards.
11. Establish street assumptions and criteria.

B. Street Definitions

Stormwater Best Management Practices (BMPs).

A type of water pollution control that includes both structural or engineered control devices and systems (e.g. retention ponds) to treat polluted stormwater, as well as operational or procedural practices (e.g. minimizing use of chemical fertilizers and pesticides).

Collector Modified Streets. Collector Modified Streets emphasize multi-modal neighborhood travel and serve as a “Main Street” for Urban Villages and Urban Centers. Collector Modified Streets contain one vehicle lane for each traffic direction. Typical features include wide sidewalks, exclusive bicycle lanes, on-street parking, and street trees.

Local Modified Streets. Local Modified Streets emphasize access to individual properties and serve living or work spaces. Local Modified Streets allow for one lane in each direction and are not designed to accommodate regular bus or truck traffic. Typical features include relatively narrow cross sections, on-street parking, sidewalks, and street trees.

Local Industrial Modified Streets. Local Industrial Modified Streets emphasize truck access to industrial properties. Local Industrial Modified Streets allow for one lane in each direction and include a bicycle lane. Typical features include limited on-street parking, sidewalks, and street trees.

Modified Alleys. Modified Alleys emphasize access to individual properties, and accommodate parking access and service functions as an alternative to other streets and provide the opportunity to incorporate stormwater Best Management Practices (BMPs).

Pedestrian Street Lights. Provide ornamentation to supplement the required illumination level. Pedestrian street lights contribute to the pedestrian scale of the area by adding a soft flow of light on the sidewalk and by

enhancing pedestrian safety.

Roadway Lights. Provide roadway illumination.

Secondary Modified Streets. Secondary Modified Streets emphasize intra-city, multi-modal travel and connect urban activity centers. Secondary Modified Streets have two lanes in each direction and carry a mix of local and regional traffic. Typical features include on-street parking, exclusive bicycle lanes, wide sidewalks, and street trees.

Sidewalks. A sidewalk is that portion of the public-right-of-way that is typically raised above the street surface and is physically defined as the area between the property line and the curb. A sidewalk can be divided into three separate zones: parkway, pedestrian, and furniture zone.

Stormwater Greenway. A non-motorized public access road that mimics a dry creek or arroyo ecosystem and supports a biological community that filters and further cleans stormwater runoff.

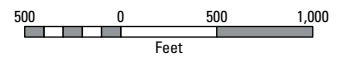
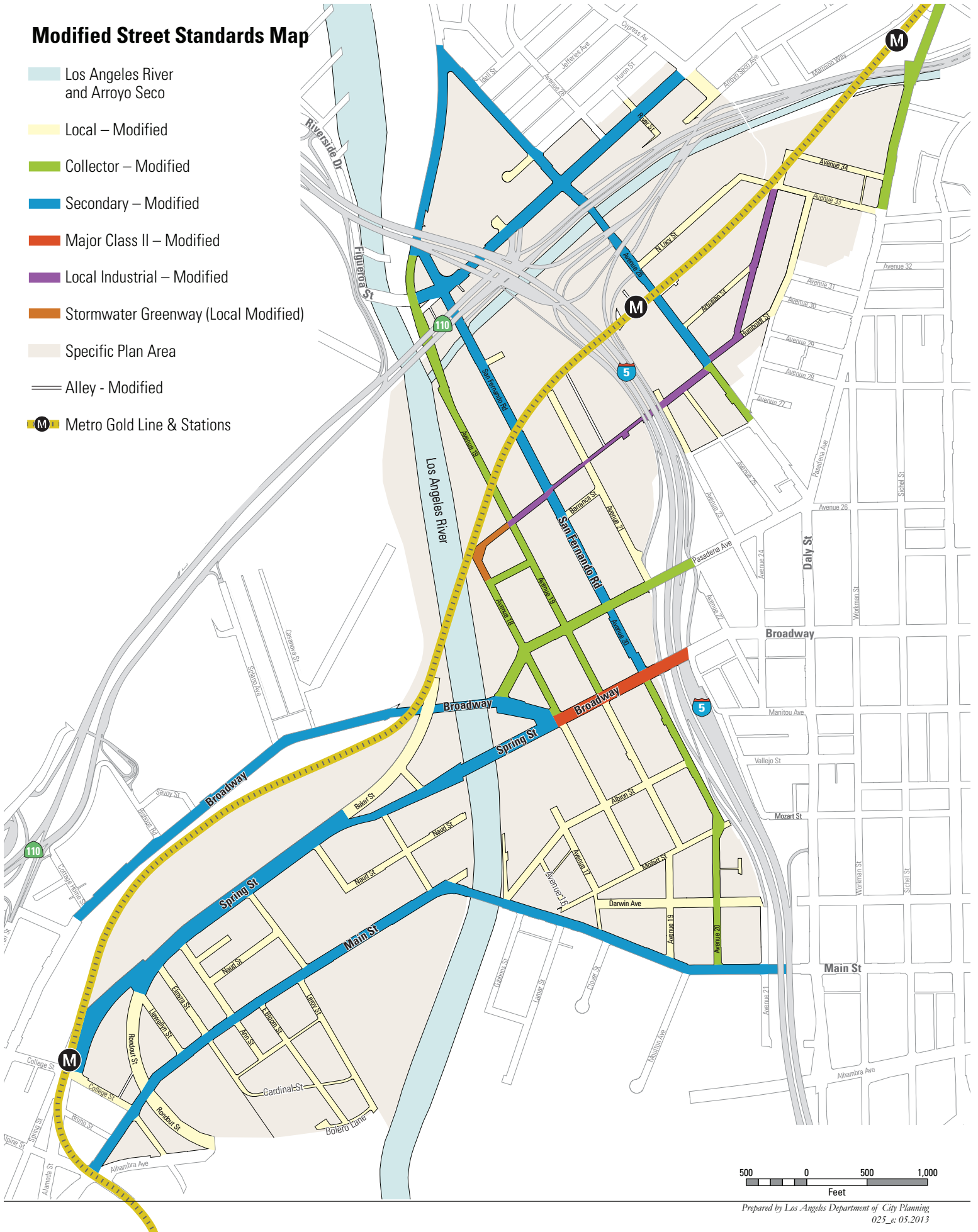
Parkway Zone. The sidewalk area adjacent to the curb is typically referred to as the Parkway zone and depending upon the level of activity may include landscaping, trees, transit infrastructure, signage, lighting, benches, fire hydrants, and vending machines.

Pedestrian Zone. The Pedestrian Zone is the portion of the sidewalk that shall be maintained clear of obstructions for the safe and accessible passage of pedestrians.

Furniture Zone. The sidewalk area immediately abutting the property line is typically referred to as the Furniture Zone. The width of the Furniture Zone will vary throughout the Plan area depending upon the overall width of the sidewalk area.

Modified Street Standards Map

- Los Angeles River and Arroyo Seco
- Local – Modified
- Collector – Modified
- Secondary – Modified
- Major Class II – Modified
- Local Industrial – Modified
- Stormwater Greenway (Local Modified)
- Specific Plan Area
- Alley - Modified
- M Metro Gold Line & Stations



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C. Street Modification Summary

For the purposes of this Subsection, the regulations and procedures contained in Section 12.37 of the LAMC shall be followed. Notwithstanding Section 12.37 H, the modified highway and street improvement standards illustrated in Appendix 4 of this Plan, and summarized in the Street Modification Table, the Modified Street Standards Map, and the other street maps located in this section shall be utilized, to the extent physically feasible, for any street improvements required in the Specific Plan area.

STREET MODIFICATION TABLE

Street	Current Designation	New Designation
Albion	Local Street	Local - Modified
Ann (Spring to Main)	Collector Street	Local – Modified
Ann (South of Main)	Local Street	Local – Modified
Artesian	Local Street	Local – Modified Industrial
Artesian Place	Local Street	Local-Modified
Aurora	Local Street	Local – Modified
Ave 16	Local Street	Local – Modified
Ave 17	Local Street	Local – Modified
Ave 18	Local Street	Local/Collector – Modified
Ave 19	Local Street	Local/Collector- Modified
Ave 20 (South of Broadway)	Collector Street	Collector- Modified
Ave 20 (North of Broadway)	Secondary Hwy	Secondary- Modified
Ave 21	Local Street	Local- Modified
Ave 22	Local Street	Local- Modified
Ave 23	Local Street	Local - Modified
Ave 25	Local Street	Local - Modified
Ave 26	Secondary Hwy	Collector- Modified
Ave 33	Local Street	Local - Modified
Baker – to Aurora	Local Street	Local - Modified
Barranca	Local Street	Local - Modified

STREET MODIFICATION TABLE

Street	Current Designation	New Designation
Bloom	Local Street	Local - Modified
Bolero	Local Street	Local - Modified
Broadway	Major Hwy Class II	Secondary - Modified
Cardinal	Local Street	Local - Modified
College	Local Street	Local – Modified
Darwin	Local Street	Local – Modified
Elmyra (North of Main)	Collector Street	Local – Modified
Elmyra (South of Main)	Local Street	Local – Modified
Figueroa	Major Hwy Class II	Secondary- Modified
Humboldt	Local Street	Local – Modified Industrial/ Local – Modified (Stormwater Greenway)
Lacy	Local Street	Local – Modified
Leroy	Local Street	Local – Modified
Llewellyn	Local Street	Local - Modified
Magdalena	Local Street	Local – Modified
Main	Secondary Hwy	Collector – Modified
Mesnager	Collector Street	Local – Modified
Mozart	Local Street	Local – Modified
Naud	Collector Street	Local – Modified
Pasadena	Secondary Hwy	Collector- Modified
Rondout	Local Street	Local – Modified
San Fernando	Secondary Hwy	Secondary – Modified
Sotello	Collector Street	Local – Modified
Spring	Major Hwy Class II	Secondary – Modified
Weyse	Collector Street	Local – Modified
Wilhardt	Collector Street	Local – Modified

D. Sidewalk Regulations

The Project applicant shall provide a site plan that includes all abutting public rights of way and indicate the location and design specifications of all curb and gutter, parkway, crosswalk, sidewalk, pedestrian, bicycle, and transit improvements.

1. Sidewalks.
 - a. Required minimum sidewalk widths for all streets in the plan area are included in Appendix 4.
 - b. Street furniture, trees and similar amenities shall be located outside of the Pedestrian Zone.
 - c. A minimum 6 foot wide barrier-free continuous path of travel shall be provided in all Pedestrian Zones.
 - d. Project applicants installing a paving pattern or using non-standard materials shall obtain prior approvals from the Department of Public Works.
 - e. Sidewalks shall be maintained by the adjacent property owner.
 2. Street Lighting.
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The Project applicant shall provide a site plan that indicates the location of street lighting and the distance between each light pole.

- a. Roadway lights shall be spaced 90 to 110 feet apart and designed to illuminate both the roadways and sidewalks to the levels required by the Bureau of Street Lighting for safety and security.
- b. Trees shall be spaced from other elements, as specified by the Urban Forestry Division, except that trees may be positioned within 10 feet of pedestrian lights. The adjacent property owner shall maintain any tree planted within 10 feet of a pedestrian light so that the lights are accessible for maintenance purposes.
- c. Energy-efficient Pedestrian Style Lighting Fixtures shall be installed midway between two street lights and no less than every 100 feet or as determined by the Bureau of Street Lighting.
- d. Once the Bureau of Street Lighting selects a Pedestrian Style Lighting Fixture for a particular block the entirety of that block shall be designated with that lighting fixture type.

Subarea 1 Street Map

- Los Angeles River and Arroyo Seco
- Specific Plan Area
- Continental Striping for Existing Crosswalks
- Continental Striping for Proposed Crosswalks
- Proposed Traffic Signal
- Existing Traffic Signal
- Existing Stop Sign
- Proposed Street Extension
- Sidewalk Extension
- M Metro Gold Line & Stations





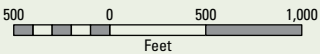
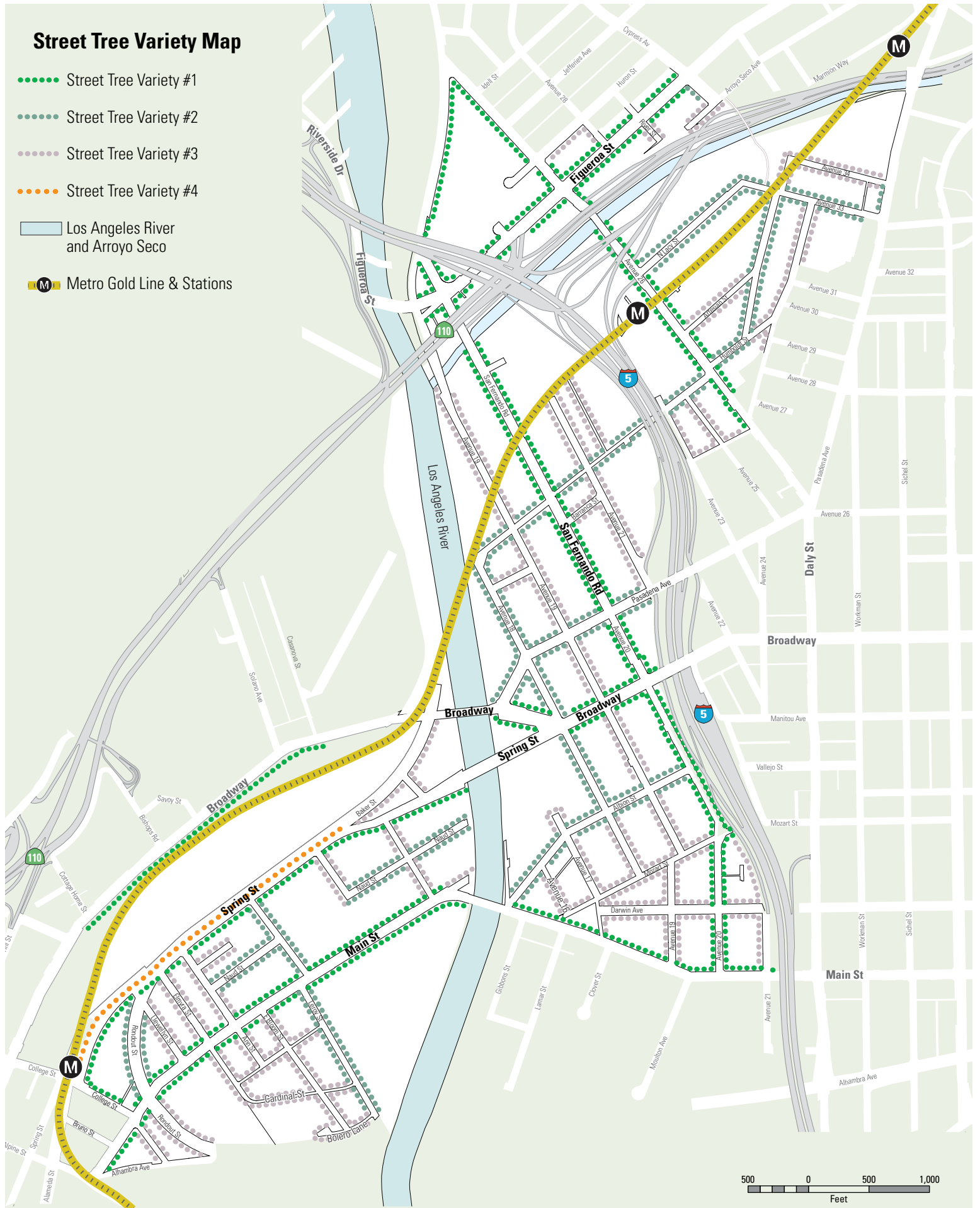
- e. All light poles shall be located adjacent to the curb as required by the Bureau of Street Lighting.
 - f. If the streets are widened or narrowed, the existing street lights shall be moved along with the new curb line to maintain the existing street light and tree spacing pattern.
 - g. Maintenance of street lighting shall be provided by the Bureau of Street Lighting, and shall be funded through the assessment district process. Any additions or changes to the assessment rates must be approved by all affected property owners pursuant to California law.
3. **Special Lighting.** Special lighting that adds to the Area's sense of place is permitted within the public right-of-way, provided that it does not interfere with pedestrian movement, vehicular safety, the approved street light/street tree spacing pattern, or other required streetscape elements.
- a. Examples of special lighting include accent lighting of landscape and architectural features, and seasonal light displays celebrating holidays or special events.
 - b. Special lighting may be installed with a revocable permit. The infrastructure for this lighting shall be maintained by the permit holder and not the Bureau of Street Lighting.
4. Street Trees.

The Project applicant shall provide a landscape plan that indicates the location, caliper at planting, and radial distance at maturity of each tree, the size of tree wells and the material and porosity of the surface area under the tree.

- a. The Parkway Zone shall be planted with a tree selected from the Street Tree Table at the designated spacing for the selected tree. Street trees shall be spaced from 20 to 40 feet on center. If there are no existing trees within the block, the applicant may select the tree species from the list, with the approval of the Urban Forestry Division. Once a tree is selected and planted for a particular block that same tree species shall be planted for subsequent Projects that develop within the same block.
- b. Where existing street trees must be removed as a result of required street widening, or other improvements, they shall be relocated or replaced, as approved by the Urban Forestry Division.
- c. Trees shall be planted using minimum 24 inch box trees.

Street Tree Variety Map

- Street Tree Variety #1
- Street Tree Variety #2
- Street Tree Variety #3
- Street Tree Variety #4
-  Los Angeles River and Arroyo Seco
-  Metro Gold Line & Stations



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- d. Trees shall be planted with 4 x 8 foot tree wells with a 3 inch decomposed granite on the tree well surface, compacted to no more than 80%.
- e. Structural soil to a depth of three feet shall be installed under the entire width of a sidewalk within 25 feet of all new or relocated street trees.
- f. Street trees shall not be planted without first obtaining approval from the Department of Public Works-Urban Forestry Division.
- g. The adjacent property owner shall be responsible for regular pruning, staking, and supplemental irrigation of trees for the first three years as needed.



Sidewalk Dining

STREET TREE TABLE

STREET TREE VARIETY 1	STREET TREE VARIETY 2	STREET TREE VARIETY 3	MEDIAN TREATMENT
African Sumac	African Sumac	Brazilian Rosewood	American Sweetgum
American Sweetgum	American Sweetgum	California Sycamore	Brazilian Rosewood
Brazilian Rosewood	Australian Willow	California White Oak	California Black Walnut
California Sycamore	Brazilian Rosewood	Chinese Pistachio	California White Oak
California White Oak	California Sycamore	Coast Live Oak	Jacaranda
Chinese Elm	California White Oak	Honey Locust	Los Angeles Beautiful Floss Silk Tree
Chinese Pistachio	Chinese Elm	Purple Orchid Tree	
Coast Live Oak	Chinese Pistachio	Los Angeles Beautiful Floss Silk Tree	
Honey Locust	Coast Live Oak		
	Honey Locust		
	Rainbow Bark		

5. **Sidewalk Dining Facilities.** Just as sidewalk dining contributes to street life, the physical facilities associated with it should contribute to the quality of the street environment and the Project. While sidewalk dining is not required, Projects that elect to include sidewalk dining shall comply with the following regulations:

- a. All dining facilities located on the sidewalk shall be freestanding, shall not be attached to the sidewalk, and shall be removed when the dining facilities are closed for business.

Subarea 2 Street Map

- Los Angeles River and Arroyo Seco
- Specific Plan Area
- Continental Striping for Existing Crosswalks
- Continental Striping for Proposed Crosswalks
- Proposed Traffic Signal
- Existing Traffic Signal
- Existing Stop Sign
- M Metro Gold Line & Stations



- b. Enclosures are required only where alcohol is served, but may be provided elsewhere to create a sense of security. Enclosures shall not exceed 42 inches in height and shall be constructed of durable materials that are in the same family as, or compatible with, the Project's architectural materials.
- c. A revocable permit, from the Department of Public Works (DPW) is required for outdoor dining facilities.

E. Street Intersection Design Regulations

1. Crosswalks.
 - a. Continental crosswalks shall be installed at all intersections that include either a Major Class II, Modified Secondary and/or Collector street as indicated on the five Subarea Street Maps included in this Section.
 - b. The Bureau of Engineering (BOE) shall identify intersections that require crosswalks on Navigate LA.

Project applicants shall provide a site plan that indicates any bicycle friendly streets or bicycle lanes, and that indicates any on-street parking spaces. The Plan shall also indicate whether the space is a car share, bicycle share, or bicycle corral space.

2. Signalized Intersections.
 - a. Traffic signals shall be added to the intersections indicated below and illustrated on the five Subarea Street Maps included in this Section:

Main Street and W. College

Ann Street and N. Spring Street

Sotello Street and N. Spring Street

Messenger and N. Spring Street







Wilhardt Street and N. Main Street

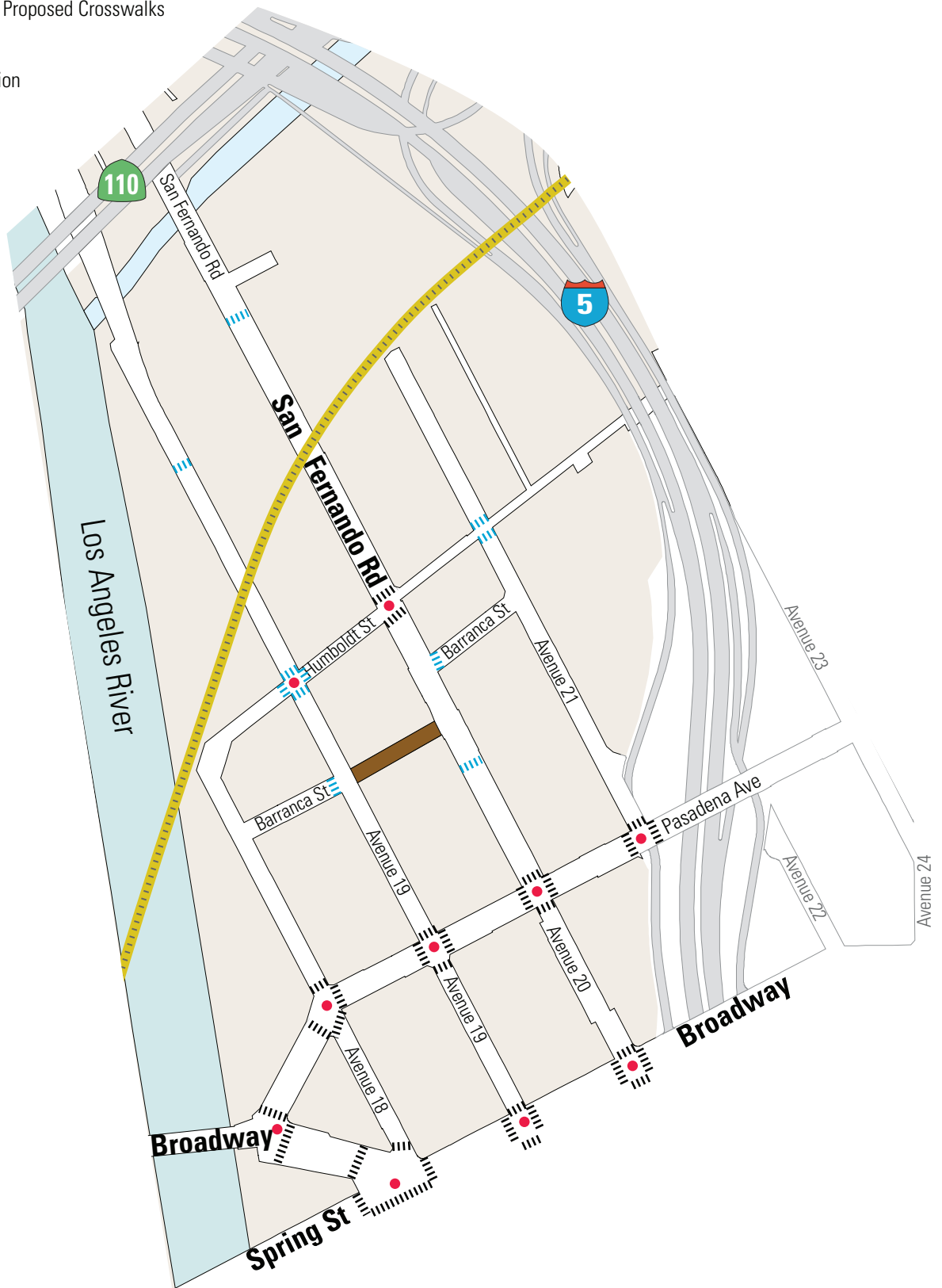
Avenue 21 and N. Main Street

Humboldt and Avenue 26

- b. The Bureau of Engineering (BOE) shall identify intersections that require signals on Navigate LA.

Subarea 3 Street Map

-  Los Angeles River and Arroyo Seco
-  Specific Plan Area
-  Continental Striping for Existing Crosswalks
-  Continental Striping for Proposed Crosswalks
-  Existing Traffic Signal
-  Proposed Street Extension
-  Metro Gold Line










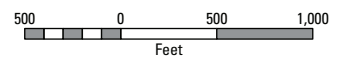
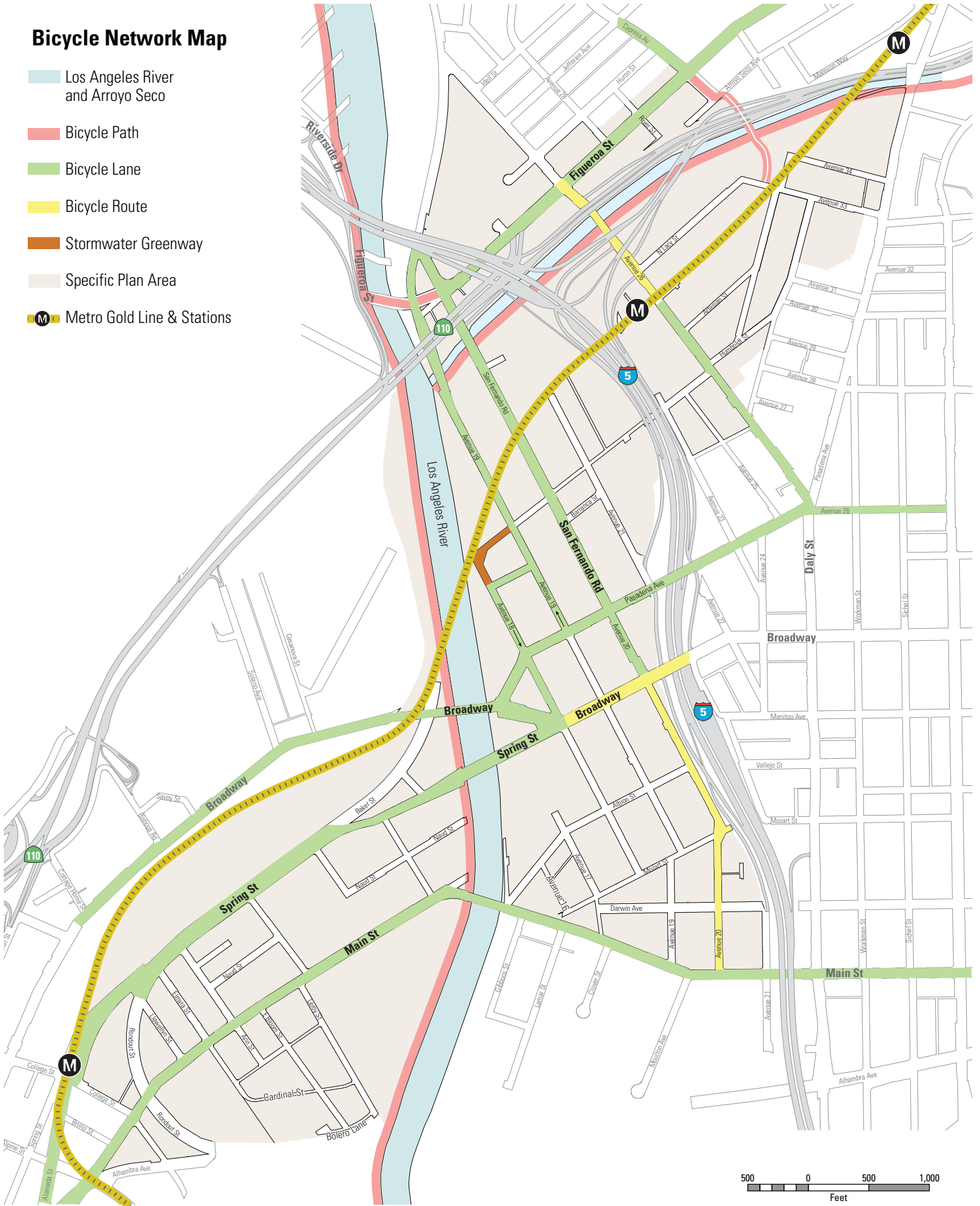
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3. Bicycle, Vehicle, and Parking Lanes

- a. Traffic Lanes. Roadbeds shall be marked with the number of traffic lanes that coincide with the standard plans on Navigate LA.
- b. Bikeways.
 - i. All Bicycle Friendly Streets identified in the 2010 Bicycle Plan shall be improved to include Bicycle Friendly Street improvements as described in the 2010 Bicycle Plan and highlighted in the 2010 Bicycle Plan's Technical Design Handbook.
 - ii. Any landscaped portions of a bicycle friendly feature shall be planted with drought tolerant trees and/or low-maintenance, drought tolerant shrubs and groundcover.
 - iii. Bicycle lanes shall be included on N. Spring, N. Main, Pasadena Avenue, San Fernando Boulevard, Figueroa Street, and a portion of Avenue 26 as illustrated on the cross-section standard plans on Navigate LA, the Bicycle Network Map on the following page and Appendix 4.
 - iv. Bicycle sharrows shall be included on Avenue 26 between the Arroyo Seco (Pasadena) Freeway and the Gold Line Bridge since severe roadway width constraints (i.e. the existence of freeway on and off-ramps) prohibit the addition of bicycle lanes at this location.
 - v. A bicycle lane shall be installed on Avenue 20 between Broadway and Main Street as illustrated in the cross-section standard plans on Navigate LA, the Bicycle Network Map on the following page and Appendix 4.
 - vi. Temporary sharrows shall be installed on Broadway between Avenue 18 and the Golden State Freeway to indicate the presence of bicyclists until such time as a bicycle lane is installed at the location, as described in the 2010 Bicycle Plan.
- c. Parking Lanes.
 - i. Car Share, Bicycle share or bicycle corrals shall be given priority access to on-street parking spaces.
 - ii. Approval for any enhancement or unique design treatments in the parking lane shall be obtained from the Department of Transportation.

Bicycle Network Map

-  Los Angeles River and Arroyo Seco
-  Bicycle Path
-  Bicycle Lane
-  Bicycle Route
-  Stormwater Greenway
-  Specific Plan Area
-  Metro Gold Line & Stations



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iii. If a parking space is reserved for a bicycle corral, the adjacent property owner shall assume maintenance responsibilities beyond normal re-striping and repair, which will continue to be performed by the Department of Transportation.





d. Landscaped Median.

- i. A landscaped median shall be installed along Spring Street between College and Baker Streets. The median improvements shall be interrupted to accommodate left-turn pockets at Ann Street, Sotello and Mesnager Streets.
- ii. The landscaped median shall be approximately 10 feet in width and shall be planted with mature, drought-tolerant, shade canopy trees and low-maintenance, drought-tolerant ground cover and shrubs.
- iii. Approval for the design, plant selection, and irrigation plans for the landscaped median shall be obtained from the Department of Transportation and the Department of Public Works.
- iv. The Bureau of Street Services shall be responsible for regular pruning, weed control, tree and/or plant replacement, and irrigation repair and replacement.

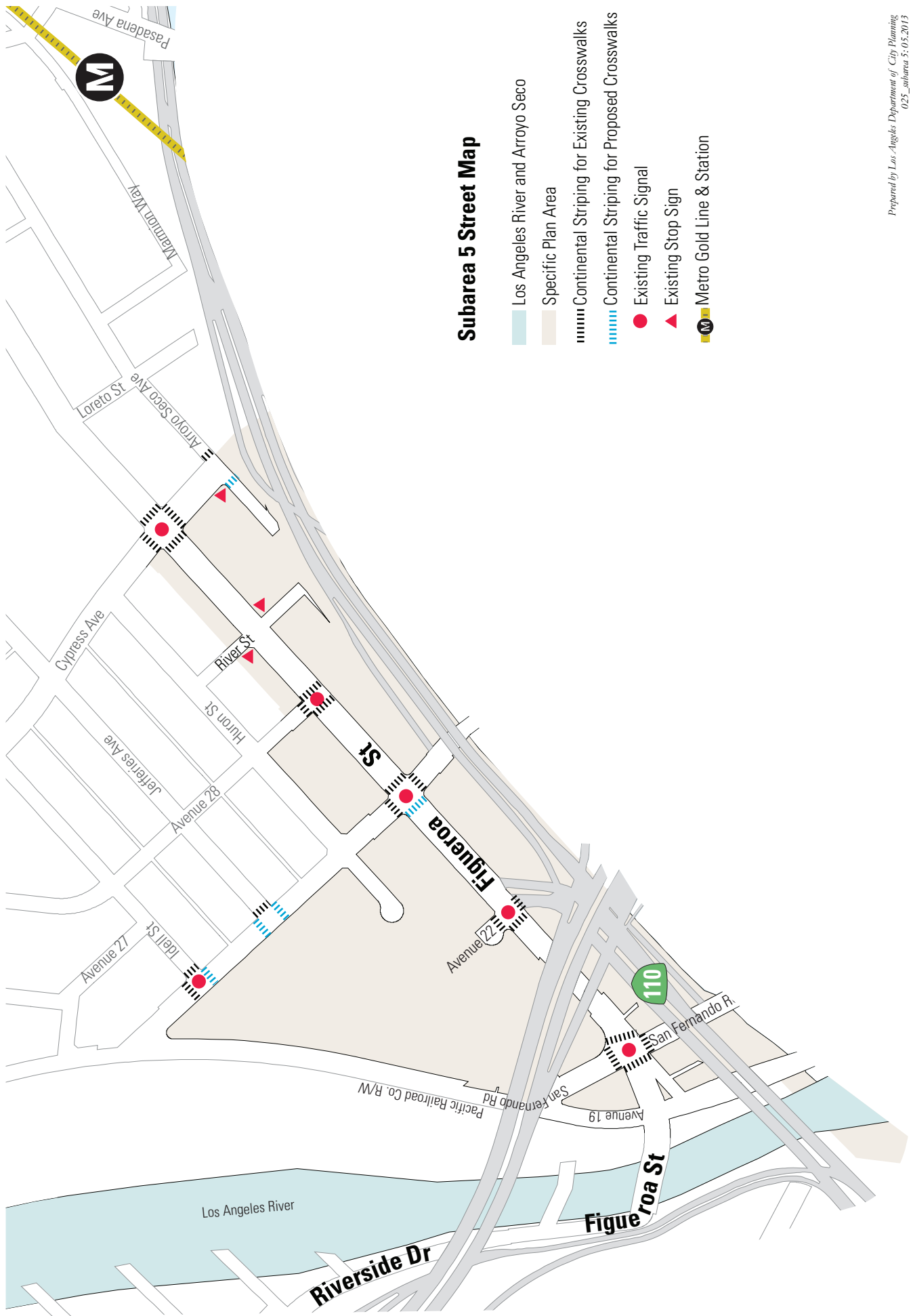
F. Street Standards

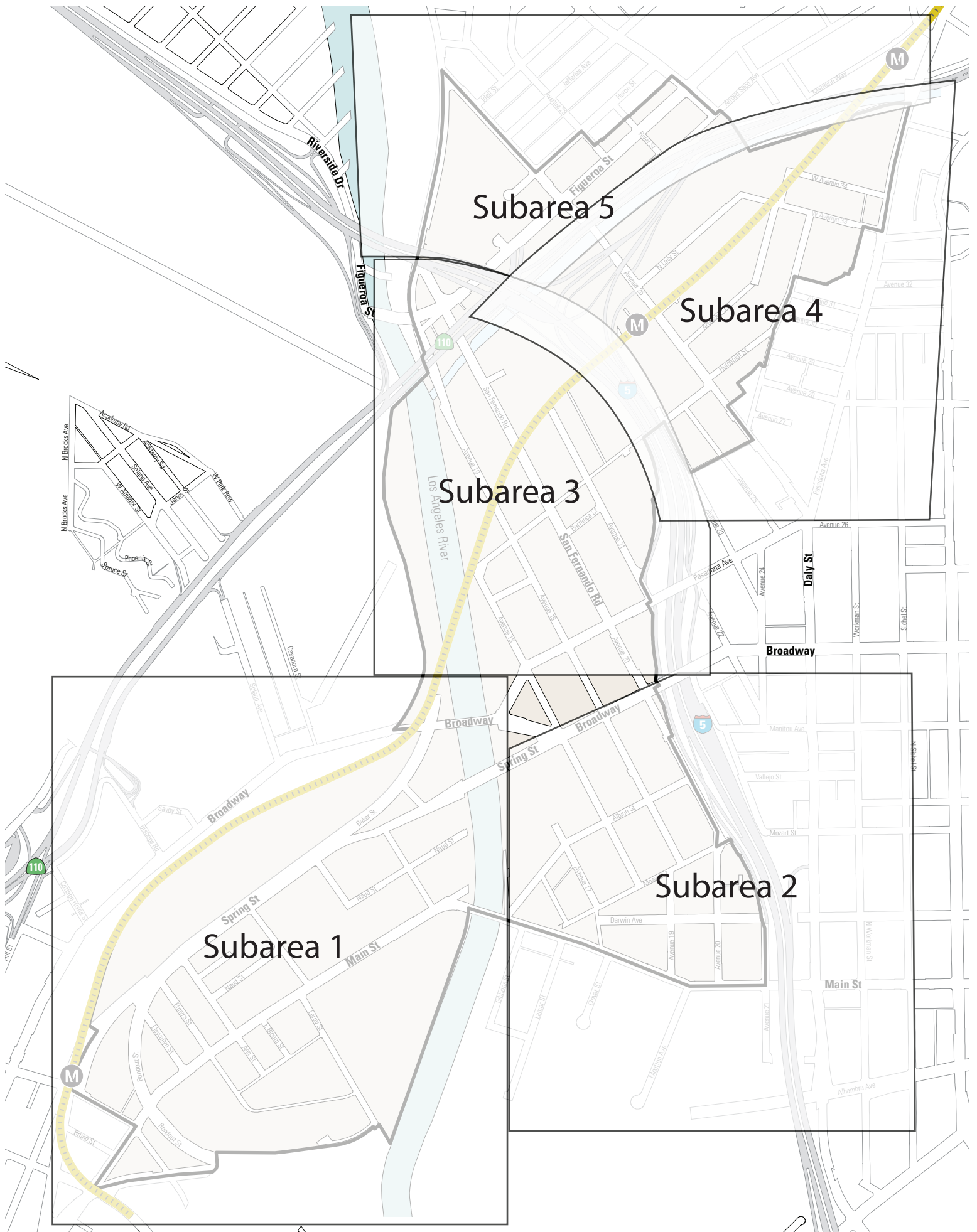
1. This Plan's Street Standards are modifications of the existing street designations and apply to the Plan's street segments illustrated in the Cross-Sections in Appendix 4 Modified Streets. The cross-sections show the typical midblock conditions. Intersections are not shown. For each street, the existing street designation and existing cross sections by segment are shown in the left column. The proposed cross-sections for those same segments are shown in the right column.
2. The proposed Plan Street Standard illustrated in Appendix 4 for each street segment includes:
 - a. **Right-of-way width** (ROW).
 - b. **Roadway width** (curb to curb).
 - c. **Sidewalk width within the ROW.** The designated sidewalk width cannot be reduced. In other words, the roadway width cannot be widened at the expense of the sidewalk.
3. Upon final approval of these standards the Bureau of Engineering shall add a layer to its Navigate LA website to inform all developers of the future block-by-block requirements for streets and sidewalk widths.

Subarea 4 Street Map

-  Los Angeles River and Arroyo Seco
-  Specific Plan Area
-  Continental Striping for Existing Crosswalks
-  Continental Striping for Proposed Crosswalks
-  Proposed Traffic Signal
-  Existing Traffic Signal
-  Metro Gold Line & Stations

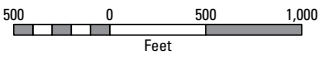









Street Label Map

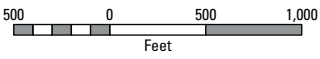
- Los Angeles River and Arroyo Seco
- Specific Plan Area



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Block Numbers Map

-  Los Angeles River and Arroyo Seco
-  Specific Plan Area
-  Metro Gold Line & Stations



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