



DEPARTMENT OF CITY PLANNING RECOMMENDATION REPORT



City Planning Commission

Date: December 10, 2015
Time: After 8:30 a.m.
Place: City Hall
Public Works Board Room
200 N. Spring Street, Room 350
Los Angeles, CA 90012

Public Hearing: Held October 26, 2015
Appeal Status: Zone Change appealable to City Council by applicant if disapproved in whole or in part by the CPC

Expiration Date: December 15, 2015

Case No.: CPC-2014-2847-VZC-DB
CEQA No.: ENV-2014-2848-MND
Incidental Cases: None
Related Cases: None
Council No.: 11 – Bonin
Plan Area: West Los Angeles
Certified NC: West Los Angeles
GPLU: Neighborhood Commercial
Zone: C2-1VL
Applicant/ Representative: Rexford Elegant Apartments LLC/Jay Nayssan

PROJECT LOCATION: 1650 – 1654 S Sawtelle Boulevard

PROPOSED PROJECT: The proposed project includes the construction of an approximately 42,152 square-foot apartment building that contains 48 dwelling units including four (4) units for very low income households. The proposed five-story building will be approximately 56 feet in height. The project will include a garage with three levels of below-grade parking to provide up to 77 parking spaces. The project will provide a minimum of 5,588 square feet of open space. The project site consists of two lots that comprise approximately 12,610 square feet. The site is located in the West Los Angeles Community Plan Area and is zoned C2-1VL with General Plan Designation of Neighborhood Commercial.

- REQUESTED ACTIONS:**
1. Pursuant to Section 21082.1(c)(3) of the California Public Resources Code, consideration of the **Mitigated Negative Declaration** (ENV-2014-2848-MND) and the Mitigation Monitoring Program (MMP) for ENV-2014-2848-MND, for the above referenced project;
 2. Pursuant to LAMC Section 12.32.Q, a **Vesting Zone Change** from C2-1VL (Commerical Zone) to (T)(Q)RAS4-1VL (Residential/Accessory Services Zone) to permit the construction, use and maintenance of 48 dwelling units;
 3. Pursuant to LAMC 12.22 A.25, **Density Bonus Affordable Housing Incentives** to permit a 48-unit housing development, with four (4) units (11%) of the base 35 units allowed by right, restricted to Very Low Income Households for a period of 55 years, and the utilization of Parking Option 1 to allow 77 residential parking spaces. The following two on-menu incentives are requested:
 1. **Floor Area Ratio (FAR).** Pursuant to Section LAMC 12.22 A.25(f)(4), an increase in FAR of 35% from 3:1 to 4.05:1 resulting in an increase from 31,224 square feet to 42,152 square feet; and

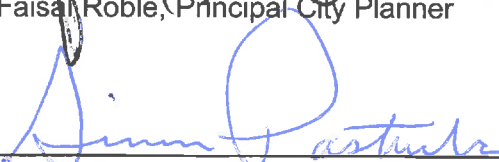
2. **Height.** Pursuant to Section LAMC 12.22 A.25(f)(5), a height increase of 35%, for a maximum of 56 feet, in lieu of the 45-foot maximum height otherwise allowed.

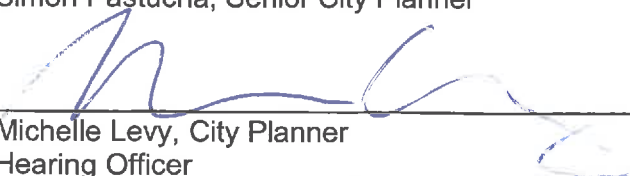
RECOMMENDED ACTIONS:

1. **Adopt** the **Mitigated Negative Declaration** ENV-2014-2848-MND and the Mitigation Monitoring Program for the above referenced project.
2. **Approve and Recommend** that the City Council Adopt a **Vesting Zone Change** from the existing C2-1VL zone to (T)(Q)RAS4-1VL zone, subject to the Conditions of Approval.
3. **Approve Density Bonus Affordable Housing Incentives** to permit a 48-unit rental housing development, with four (4) units (11%) of the base 35 units allowed by right, restricted to Very Low Income Households for a period of 55 years, the utilization of Parking Option 1 to allow up to 77 residential parking spaces. The two on-menu incentives are as follows:
 - a. **Floor Area Ratio (FAR) Increase** of 35% to allow an FAR of 4.05:1 in lieu of 3:1 allowed by the zone; and
 - b. **Height Increase** of 35% to allow a building height of 56 feet in lieu of the 45-foot height otherwise permitted.
4. **Adopt** the attached Findings.
5. **Advise** the applicant that, pursuant to California State Public Resources Code Section 21081.6, the City shall monitor or require evidence that mitigation conditions are implemented and maintained throughout the life of the project and the City may require any necessary fees to cover the cost of such monitoring.
6. **Advise** the applicant that pursuant to State Fish and Game Code Section 711.4, a Fish and Game Fee is now required to be submitted to the County Clerk prior to or concurrent with the Environmental Notice of Determination (NOD) filing.

MICHAEL J. LOGRANDE
Director of Planning


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PROJECT ANALYSIS

Project Summary

This report contains discussions, recommendations, conditions and findings to construct a five-story, 56-foot tall, 42,152 square-foot apartment building consisting of 48 residential units, five (5) of which are proposed to be Joint Live-Work units. The proposed building would be located on the east side of Sawtelle Boulevard between Santa Monica Boulevard and Iowa Avenue. The project site is comprised of two adjacent lots, totaling 12,610 square feet (0.29 acres). The site is currently developed with an eight unit, two-story apartment building and one-story retail store, which are proposed for demolition.

The project includes 5,588 square feet of open space, consisting of 4,888 square feet of common open space on the roof deck and 700 square feet of private open space. A total of 72 automobile parking spaces are proposed on three levels of below-grade parking, as well as 53 bicycle parking spaces. Vehicular access will be provided from the alley to the east (rear) of the building, and pedestrian access will be provided from the Sawtelle Boulevard frontage.

The West Los Angeles Community Plan designates the project site as Neighborhood Commercial land use with a corresponding zone of C2-1VL. The project requests a Vesting Zone Change to RAS4-1VL. In consideration of reserving four units for Very Low Income residents, a Density Bonus is requested for two on-menu Affordable Housing Incentives to permit: 1) A 35 percent increase in the Floor Area Ratio (FAR), to allow 4.05:1 in lieu of 3:1, for a maximum development of up to 42,152 square feet in lieu of 31,224 square feet; and 2) A 35 percent increase in height allowing a maximum of 56 feet in lieu of 45 feet.

The surrounding and abutting properties are developed primarily with commercial and multi-family residential uses, and are zoned R4-1, C2-1VL, and [Q]RAS4-1VL to the south across Iowa Avenue. Nora Sterry Elementary School south of the property, on the west side of Sawtelle Boulevard, is zoned PF-1XL. To the north of the site along Santa Monica Boulevard, the zoning is C2-1VL. The surrounding commercial and multi-family residential buildings range from one to four stories.

General Plan Land Use

The project site is located in the West Los Angeles Community Plan area, which is one of 35 community plans that comprise the Land Use Element of the City's General Plan. The West Los Angeles Community Plan General Plan Land Use Map designates the site for Community Commercial land use with corresponding zones of CR, C2, C4, P, PB, RAS3, RAS4, P, and PB. The Framework Element locates the project site in a Mixed Use Boulevard, which is intended to connect the city's neighborhood districts and community, regional and Downtown centers. Mixed Use development is encouraged along these boulevards, with the scale, density and height of development compatible with the surrounding areas. Generally, different types of Mixed Use Boulevards will fall within a range of floor area ratios from 1.5:1 up to 4.0:1 and generally characterized by 1- to 2-story commercial structures, and up to 3- to 6-story for mixed use buildings between centers and higher buildings within centers. Mixed Use Boulevards are served by a variety of transportation facilities.

Zoning, FAR, and Density

The subject property is comprised of two contiguous parcels zoned C2-1. A Vesting Zone Change to RAS4 is requested in order to increase the allowed FAR from 3:1 to 4.05:1. With the approval of the Vesting Zone Change and Density Bonus, the proposed project will be built to an FAR of 4.05:1, comprising approximately 42,152 square feet of floor area. The RAS4 zone permits a density of 400 square feet of lot area per dwelling unit. Based on the lot size (12,610 square feet

and 14,030 including alleys), a total of 35 dwelling units could be constructed on the site. The project proposes a total of 48 dwelling units.

The Applicant proposes to utilize Los Angeles Municipal Code (LAMC) Section 12.22 A. 25 (Density Bonus) to set aside four units (11 percent) for Very Low Income Households for a period of 55 years. The Density Bonus Ordinance grants an increase in the permitted density in exchange for the Applicant setting aside a portion of their by-right dwelling units.

The C2 zoning of the project site allows for 35 residential units based on a lot square footage of 14,030. The Applicant proposes a density bonus of 35 percent, which permits 13 dwelling units over the maximum density limit of 35 dwelling units on the property, or a total of 48 units. The Applicant is requesting two “on-menu” incentives in order to facilitate the provision of affordable housing at the site.

Site Design

The site is located east of Sawtelle Boulevard with a 15-foot wide alley to the east and a ten-foot wide alley to the north, between Santa Monica Boulevard and Iowa Avenue. The building is oriented towards Sawtelle Boulevard with pedestrian access to the residential lobbies on the Sawtelle- and north alley-facing facades. Vehicular access is provided via one dedicated driveway off the alley, southeast (rear) of the site. All five floors will be dedicated to residential units with 4,888 square feet of common open space on the roof deck. Residential parking will be housed over three levels of below-grade parking.

The building will be built to 56 feet, allowed by Height District 1VL, plus 11 feet per the 35 percent density bonus request. The proposed building height is compatible with development along this portion of Sawtelle Boulevard. The site design complements the residential character of this street and blends in with the surrounding commercial uses. The proposed maximum height of 56 feet is appropriate, given the four- and five-story residential building to the south, the four-story residential building to the southeast, and the four-story commercial building and elementary school to the west of the project along Sawtelle Boulevard.

Open Space

The proposed multi-family residential project is required to provide a minimum of 5,525 square feet of open space pursuant to Section 12.21 G.2 of the Los Angeles Municipal Code. However, more open space is being provided. The open space areas consist of a 4,888 square-foot roof deck which will include furniture and fire pits surrounded by planters filled with a variety of landscaping; and 16 private balconies that provide a total of 700 square feet throughout the project site. Collectively, these areas account for a total of 5,588 square feet, which is a little more than the minimum 5,525 square feet required.

Parking

By setting aside four of the units as Very Low Income affordable units, the project may utilize reduced vehicle parking requirements under LAMC Section 12.22 A.25 Parking Option 1, which permits one on-site parking space for each residential unit with one or fewer bedrooms; two on-site parking spaces for each residential unit with two to three bedrooms; and two-and-one-half parking spaces for each residential unit with four or more bedrooms. The unit make-up includes five efficiency units, 14 one-bedroom units, and 29 two-bedroom units. Based on the number of number and type of dwelling units proposed, 77 parking spaces are required per the Density Bonus Ordinance.

Bicycle Parking

The project has proposed bicycle parking per the City’s Bicycle Ordinance (No. 182386), which became effective as of March 13, 2013, to encourage safe and secure bicycle parking and expand the bicycle parking requirements to include residential developments. The project has provided

long-term and short-term bicycle parking spaces per Table 12.21A. 16(a)(2) of the Ordinance for a new development. This includes 48 long-term and five short-term spaces for residential use. Additionally, pursuant to LAMC Section 12.21.A4, up to 11 (15 percent) of the required automobile parking may be replaced with bicycle parking at a ratio of one automobile parking space for every four bicycle parking spaces provided. With the utilization of the replacement bicycle parking provision, the project will provide a minimum of 66 vehicle parking spaces, as required.

Mitigated Negative Declaration

A Mitigated Negative Declaration, No. ENV-2014-2848-MND, was prepared and circulated for public review on July 16, 2015. The comment period ended on August 5, 2015. The MND found that the proposed project would result in potential impacts to aesthetics, biological resources, and land use planning; however, these potential impacts can be mitigated to a less than significant level with standard mitigation measures and through compliance with regulatory control measures. The Environmental Clearance is discussed in detail in the Findings section (See CEQA Findings, F-11) of this report.

PROJECT SETTING

Site Description

The project site consists of two lots (Lots 15 and 16) totaling 12,610 square feet in area. The project site is currently developed with an existing eight-unit, two-story apartment building, one-story dwelling unit, and one-story retail store in the C2-1VL zone with a land use designation of Neighborhood Commercial. The applicant has requested a Vesting Zone Change from C2-1VL to RAS4-1VL. The project site is located one block north of a five-story 94-unit condominium development in the (Q)RAS4-1VL and C2-1VL zone, with a land use designation of Neighborhood Commercial. The majority of the buildings along Sawtelle Boulevard (south of Santa Monica Boulevard) within this Neighborhood Commercial are multi-family or commercial buildings ranging between one- and five-stories in height.

The site is approximately 12,610 gross square feet and is located on the east side of Sawtelle Boulevard, on the south side of a ten-foot alley, and west side of a 15-foot alley between Santa Monica Boulevard to the north and Iowa Avenue to the south. The site is currently developed with an eight-unit, two-story apartment building, one-story dwelling unit, and one-story retail store.

Surrounding Zones and Uses

Property surrounding the site is zoned C2-1VL and R4-1. Surrounding uses to the north, west, and south of the project site include one-story commercial retail uses, theatre uses, office use, restaurant uses, auto repair use, and a five-story hotel, and a two-story multifamily building in the C2-1VL zone. East (rear) of the project site, the area is improved with a surface parking, and multi-family residential buildings ranging between one- and two-stories in the R4-1 zone. It is important to note, the project site is within 200 feet of the 405 Freeway to the east.

Streets and Circulation

The project site is oriented toward Sawtelle Boulevard and is bounded by two alleys to the north and the east.

Sawtelle Boulevard is a designated Collector in the recently adopted Mobility Element, dedicated with a right-of-way width of 64 feet, improved with a curb, gutter and 12-foot wide sidewalk. Sawtelle Boulevard is also designated as a Comprehensive Transit Enhanced Street in the Mobility Element (Map B), which renders it a candidate for improved transit service and streetscape improvements in the future.

Vehicular access is serviced by a ten-foot wide alley abutting the north of the project site. The site abuts a 15-foot wide alley to the east.

The project site is served by Big Blue Bus Line No. 4; the nearest stop at Nebraska and Sawtelle Boulevard is located less than one-quarter mile from the project site. The project is also served by several local and regional bus lines along Santa Monica Boulevard.

Requested Entitlements

Vesting Zone Change

Pursuant to Section 12.32Q, the project proposes a Vesting Zone Change from C2-1VL to (T)(Q)RAS4-1VL, which would allow a maximum FAR of 3:1, and a project density of one (1) unit per each 400 square feet of lot area pursuant to LAMC Section 12.11.5.

Density Bonus, On-Menu Incentives

The project is eligible for two on-menu incentives to accommodate the proposed development of 48 units. The two incentives are permitted because the project will restrict 11 percent of the base units for Very Low Income Households and the incentives are required in order to facilitate the provision of affordable housing on the site. The list of on-menu incentives in Section 12.22 A.25 (f) was pre-evaluated at the time the Density Bonus Ordinance was adopted to include types of relief that minimize restrictions on the size of the project. Density Bonus Incentives are normally necessary to allow adjustments to the zoning regulations in order to accommodate the additional density bonus units that will be built on the site. Typically, this means an increase in height, an increase in floor area, and a reduction in required yards - all for the purpose of increasing the size of the zoning envelope to make room for the additional units. The additional market-rate units then subsidize the rents for the affordable units, so that the project is financially feasible. The Applicant has requested two on-menu Density Bonus Incentives for the following:

Increase of Floor Area Ratio: A Density Bonus project is permitted up to a 35% increase in the allowable Floor Area Ratio (FAR) equal to the percentage of Density Bonus pursuant to LAMC 12.22 A.25(f)(4i). The Applicant has requested a 35 percent increase in the FAR from 3:1 to 4.05:1 permitting up to a 42,152 square foot residential building in lieu of a 31,224 square foot residential building.

Increase of Height: A Density Bonus project is permitted up to a 35 percent increase in the height requirement in feet equal to the percentage of Density Bonus requested pursuant to LAMC 12.22 A.25(f)(5). The Applicant has requested a 35 percent increase in the building height to allow a 56 foot high residential building in lieu of a 45 foot high (50 feet in the RAS4-1VL Zone) residential building.

Pursuant to LAMC Section 12.22 A.25 (e)(2), in order to be eligible for any on-menu incentives, a Housing Development Project (other than an Adaptive Reuse Project) shall comply with the following criteria, which the proposed project does:

- a. *The façade of any portion of a building that abuts a street shall be articulated with a change of material or a break in plane, so that the façade is not a flat surface.*

The proposed building fronts along Sawtelle Boulevard. As shown in Exhibit B, the façade will have articulation in the form of varying horizontal and vertical planes, and through the placement of windows and projecting balconies. The Sawtelle Boulevard façade is further articulated with a defined glass building entryway in the center of the building, and the use of zinc panels on selected building surfaces to emphasize changes in bulk and plane.

- b. *All buildings must be oriented to the street by providing entrances, windows architectural features and/or balconies on the front and along any street facing elevation.*

The proposed building's primary entrance is located on Sawtelle Boulevard. The front entry is set apart from the façade of the building by a double-door glass entry. In addition, all units facing the street and two alleyways have aluminum framed doors and windows and semi-private patios at the ground level, connecting the building features to the street. Along the rear and north facades, low planters with landscaping define the side and rear yards. Lastly, balconies are located along three of four (excluding the rear) building façades.

- c. *The Housing Development Project shall not involve a contributing structure in a designated Historic Preservation Overlay Zone (HPOZ) and shall not involve a structure that is a City of Los Angeles designated Historic-Cultural Monument (HCM).*

The proposed project is not located within a designated Historic Preservation Overlay Zone, nor does it involve a property that is designated as a City Historic-Cultural Monument.

- d. *The Housing Development Project shall not be located on a substandard street in a Hillside Area or in a Very High Fire Hazard Severity Zone as established in Section 57.25.01 of the LAMC.*

The project is not located in a Hillside Area, nor is it located in a Very High Fire Hazard Severity Zone.

ISSUES

The following section includes a list of issues related to the project. These issues were either identified during the design review by the City's Urban Design Studio Professional Volunteer Program, at the public hearing held on October 26, 2015, or in discussions with the applicant.

Concerns about Displacement of Residents and Existing Commercial Businesses

During the public hearing, several speakers raised concerns regarding the loss of existing businesses and the potential displacement of residents living in the existing multi-family property on the site. In regards to the loss of the retail business, comments were mixed: some were concerned about the project setting a precedent for the replacement of neighborhood-serving businesses with residential-only development, while others expressed support for a residential project that would not generate as many vehicle trips as a commercial project. The applicants responded to this issue at the public hearing, provided background on initial meetings they had had with Neighborhood Council representatives during the schematic design phase. The Neighborhood Council representatives had at that time expressed a strong preference for a residential-only project in order to reduce potential traffic impacts. In an effort to resolve these concerns, the applicants have redesigned their project since the public hearing to include five ground floor units reserved for Joint Live-Work, which would allow home/office occupations while avoiding additional traffic impacts in an already congested area.

Impacts of Freeway Adjacency

The project is located within 200 feet of the 405 Freeway, to the east of the project site; therefore, it is subject to Zoning Information File (Z.I.) No. 2427, known as the Freeway Adjacent Advisory Notice For Sensitive Uses. A Mitigated Negative Declaration was prepared for the project, which identified potential impacts to future residents resulting from the siting of the proposed housing development in close proximity to a freeway. The MND states that with the incorporation of mitigation measures to require a MERV level 13 air filtration system, impacts will be reduced a

less than significant level. Comments were received from SCAQMD citing potential negative impacts of freeway proximity on future residents of the project and advising that a site-specific health risk assessment be conducted. At the time of the writing of this staff report, an Air Quality Assessment is being prepared by a qualified consultant to address the concerns of SCAQMD.

Urban Design Studio - Professional Volunteer Program (PVP)

The Project was reviewed by the Planning Department's Urban Design Studio Professional Volunteer Program (PVP) for an architectural evaluation. The concerns expressed were primarily focused on providing a pedestrian connection within and around the project by creating an opportunity for open space at the ground level; activating the area along the two alleys (north and east) and Sawtelle Boulevard; and varying the project's mix of unit typologies.

In particular, the PVP felt that the project would benefit from opening up the area between the building and the two alleyways by taking advantage of the unique site condition and creating outdoor spaces for residents and other community members to use. They recommended paving the alleys, installing light fixtures on or around the building facades, and orienting the pedestrian entrances, patios, balconies, porches, landscaping, and streetscape to both Sawtelle Boulevard and to the north alley. In addition, they suggested softening the building façade by simplifying the various materials to one or two design elements, as well as considering replacing the metal main entryway with a tempered glass or wood entry to improve transparency and improve the relationship between the building and the street. Lastly, they suggested varying the housing typology by offering joint/live work units on the ground floor below the typical standard dwelling units. This would create a transition between the neighboring commercial retail uses to the north and render a connection between the residents in the live/work units and nearby amenities.

In response to the PVP recommendation, the applicant has revised their design to emphasize the main entrance by replacing the existing aluminum framed entrance door with a clear tempered glass entrance door and by adding a second lobby and pedestrian entrance door on the north alley façade. The stormwater filtration system was redesigned to relocate stormwater capture to the rooftop deck in order to reduce the number of raised planters at the sidewalk level, thereby remove visual barriers at the street level. This had the added benefit of providing a green roof on the roof deck, as an open space amenity for the residents. Additionally, the north alleyway will be improved with special stamped asphalt paving and street lamps with planters filled with landscaping, which will encourage foot traffic and improve safety for pedestrians. The building design and materials were simplified to two design elements – the zinc panels and plaster -- helping to bring consistency throughout the building design and render an attractive building. Lastly, in response to the PVP's recommendation of varying the housing typology, the applicant has now included five live/work units with individual entrances and patios on the ground floor.

Residential Citywide Design Guidelines

The design was reviewed according to the General Plan Framework, Residential Citywide Design Guidelines and the West Los Angeles Community Plan Chapter 5, Urban Design Guidelines. These Guidelines provide performance goals for new residential developments. As proposed and conditioned, the project will achieve a significant number of these Guidelines. The incorporation of these Guidelines will achieve improvements to the design of building façade, connection with pedestrians and the neighboring commercial retail uses, activation of the alleyways, and low level (3 ½ feet high) planters lining the building perimeter. A full analysis of the Residential Citywide Design Guidelines can be found in the General Plan Findings, under General Plan Framework.

Conclusion

The Department recommends the project as conditioned in this report. The Vesting Zone Change to (T)(Q)RAS4-1VL is appropriate given the context of Sawtelle Boulevard as a designated

Mixed-Use Boulevard in the City's adopted plans. The project provides for a residential project with Live-Work units in a job- and transit-rich area that complements the mixed-use, pedestrian oriented character of Sawtelle Boulevard and is compatible in scale with other commercial and multifamily uses in the immediate area. The proposed maximum height of 56 feet is an appropriate height limit given the four- and five-story condominium development located south of the project along Sawtelle Boulevard and other projects of similar building heights along Beloit Avenue. The project is within walking distance of neighboring amenities, which include a public library, a senior center, several restaurants and cafes, theaters, a gymnasium, art gallery, and several high-rise office buildings east of the 405 freeway. Lastly, residents of the project will benefit from its proximity to bus lines along Santa Monica Boulevard, and the soon-to-be opened Metro Expo Line light rail to the south of the project along Exposition Boulevard.

With the incorporation of the design elements recommended and discussed above, the project advances several General Plan objectives, including creating more mixed-income housing and locating new development near public transit, services, and jobs. The project, at the recommended floor area, height, scale, design and layout, will be a compatible addition to the local neighborhood and will locate additional housing on a major thoroughfare that is close to a concentration of employment. The project provides residential uses in an appropriate residential zone and is consistent with the General Plan, and is also in conformity with public necessity, convenience, general welfare and good zoning practice.

(Q) QUALIFIED CONDITIONS OF APPROVAL

Pursuant to Section 12.32 G of the Municipal Code, the following limitations are hereby imposed upon the use of the subject property, subject to the "Q" Qualified classification.

A. Entitlement Conditions

1. **Site Plan.** The use and development of the property shall be in substantial conformance with the Plot plan and elevations submitted, with the application and marked **Exhibit B**, dated **November 20, 2015**, and attached to the administrative file. Prior to the issuance of building permits, revised, detailed development plans that show compliance with all conditions of approval, including complete landscape and irrigation plans, shall be submitted to the City Planning Department for review.
2. **Use.** Use of the subject property shall be limited to the residential uses permitted in the RAS4 zone. The property shall observe the area restrictions of the RAS4 zone, and of the 48 dwelling units proposed, five (5) shall be Joint Live-Work units, located on the ground floor of the buildings.
3. **Height.** The building shall be limited to a height of 56 feet, as defined by Los Angeles Municipal Code (LAMC) Section 12.03 and allowed per LAMC Section 12.21.1. Any structures on the roof, such as air condition units and other equipment, shall be fully screened from view of any abutting properties.
4. **Floor Area Ratio (FAR).** The total floor area of the structure on the property shall not exceed 4.05:1 times the buildable area of the lot.
5. **Yards/Setback.** The project shall provide a 10-foot front yard setback along Sawtelle Boulevard and a minimum of five feet for the required side and rear yards or as provided pursuant to LAMC 12.11.5.C, whichever is greater.
6. **Residential Density.** The project shall be limited to a maximum density of 48 residential units including Density Bonus Units.
7. **Affordable Units.** A minimum of four (4) units, that is, 11% of the 35 base dwelling units, shall be reserved as Very Low Income affordable units, as defined by the State Density Bonus Law 65915 (C)(2).
8. **Change in Restricted Units.** Deviations that increase the number of restricted affordable units or that change the composition of units or change parking numbers shall be consistent with LAMC Section 12.22 A.25 (9a-d).
9. **Housing Requirements.** Prior to issuance of a building permit, the owner shall execute a covenant to the satisfaction of the Los Angeles Housing and Community Investment Department (HCIDLA) to make four (4) units available to Very-Low Income Households, for sale or rental as determined to be affordable to such households by HCIDLA for a period of 55 years. Enforcement of the terms of said covenant shall be the responsibility of HCIDLA. The applicant will present a copy of the recorded covenant to the Department of City Planning for inclusion in this file. The project shall comply with any monitoring requirements established by the HCIDLA. Refer to the Density Bonus Legislation Background section of this determination.

10. **Automobile Parking.** Vehicle parking shall be provided consistent with LAMC 12.22 A.25, Parking Option 1, which permits one on-site parking space for each residential unit with one or fewer bedrooms; two on-site parking spaces for each residential unit with two to three bedrooms; and two-and-one-half parking spaces for each residential unit with four or more bedrooms. Based upon the number and type of dwelling units proposed, a minimum of 77 automobile parking spaces shall be required.

Pursuant to LAMC Section 12.21.A4, up to 11 (15 percent) of the required automobile parking may be replaced with bicycle parking at a ratio of one automobile parking space for every four bicycle parking spaces provided. With the replacement bicycle parking, a minimum of 66 parking spaces shall be required. Should the applicant choose to replace any of the required automobile parking with bicycle parking, and no other Condition of Approval is affected, then no modification of this determination shall be necessary, and the number of parking spaces shall be re-calculated by the Department of Building and Safety based upon the ratios set forth above. **Note:** This is in addition to the required bicycle parking outlined in Condition 12.

11. **Adjustment of Parking.** In the event that the number of Restricted Affordable Units should change, or the composition of such units should change (i.e. the number of bedrooms, or the number of units made available to Senior Citizens and/or Disabled Persons), and no other Condition of Approval is affected, then no modification of this determination shall be necessary, and the number of parking spaces shall be re-calculated by the Department of Building and Safety based upon the ratios set forth above.
12. **Bicycle Parking.** Bicycle parking shall be provided consistent with LAMC 12.21 A.16. Long-term parking shall be provided at a rate of one per dwelling unit or guest room. Additionally, short-term bicycle parking shall be provided at a rate of one per ten dwelling units or guest rooms, with a minimum of two short-term bicycle parking spaces. Based upon the number of dwelling units, a minimum of 48 long-term and 5 short-term bicycle parking spaces shall be provided onsite, for a total of 53 bicycle parking spaces.
13. **Open Space.** The project shall provide 5,588 square feet of common open space areas as follows:
- a. **Porches.** Live-work units on the ground floor will have entries at-grade and semi-private porch spaces along the north façade. A total of eight (8) porches will be provided at the ground floor.
 - b. **Roof Deck.** The project shall provide a roof deck on the fifth floor that is a minimum of 4,888 square feet in area. Amenities such as outdoor furniture, fire pits, barbeques, and landscaped planters shall be provided.
 - c. **Private Balconies.** The project shall provide approximately 16 balconies that each measure 50 square feet, for a minimum total of 700 square feet.
14. **Rooftop Open Space.** Rooftop open space shall include amenities for residents including outdoor furniture, fire pits, shade trees and/or hedges. 4,888 square feet of rooftop open space will be dedicated to a landscaped area, which shall also serve to meet the project's stormwater requirements.
15. **Alley Improvements.** The alley south of Santa Monica Boulevard (north edge of the project) shall be improved with:

1. Stamped asphalt paving; and
2. Five (5) pedestrian street lights installed along the alley-adjacent north façade.

Improvements will be noted on B-permit plans submitted to the Bureau of Engineering, Department of Public Works, subject to the agency's approval and conditions.

16. **Front Yard Landscaping.** As shown on the Landscape Plan (Exhibit B, Sheet L-1), front yard landscaping will include two (2) 36-inch box Australian Willow trees and one 36-inch box Mexican Palo Verde tree.
17. **Landscape Planters.** Landscape planters on the north (alley) frontage shall be limited in height to 3.5 feet above grade so as not to obscure the architectural features of the building.
18. **Other Landscaping.** All open areas not used for buildings, driveways, parking areas, recreational facilities or walks shall be attractively landscaped, including an automatic irrigation system, and maintained in accordance with a landscape plan prepared by a licensed landscape architect or licensed architect, and submitted for approval to the Department of City Planning. The landscape plan shall indicate landscape points for the project equivalent to 10% more than otherwise required by LAMC 12.40 and Landscape Ordinance Guidelines "O".
19. **Building Entrances.** Two lobby entrances shall be provided; one along the Sawtelle Boulevard frontage and one along the alley frontage on the north side of the building. Entry doors shall be made of clear, unobstructed, tempered glass.
20. **Ground Floor Transparency.** Ground floor units shall maintain transparent windows and openings as shown on Elevations in Exhibit B.
21. **Mechanical Equipment.** All mechanical equipment on the roof shall be screened from view. The transformer, if located in the front yard, shall be screened with landscaping

B. Environmental Conditions

22. **Aesthetics (Light)** Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties or the public right-of-way, nor from above.
23. **Aesthetics (Glare).** The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat.
24. **Tree Removal (Public Right-of-Way).**
 - Removal of trees in the public right-of-way requires approval by the Board of Public Works.
 - The required Tree Report shall include the location, size, type, and condition of all existing trees in the adjacent public right-of-way and shall be submitted for review and approval by the Urban Forestry Division of the Bureau of Street Services, Department of Public Works (213-847-3077).
 - All trees in the public right-of-way shall be provided per the current Urban Forestry Division standards.

25. Land Use/Planning.

- An air filtration system shall be installed and maintained with filters meeting or exceeding the ASHRAE Standard 52.2 Minimum Efficiency Reporting Value (MERV) of 13, to the satisfaction of the Department of Building and Safety.

CONDITIONS FOR EFFECTUATING (T) TENTATIVE CLASSIFICATION REMOVAL

Pursuant to Section 12.32 G of the Municipal Code, the (T) or [T] Tentative Classification shall be removed by the recordation of a final parcel or tract map or by posting of guarantees through the B-permit process of the City Engineer to secure the following without expense to the City of Los Angeles, with copies of any approval or guarantees provided to the Department of City Planning for attachment to the subject planning case file.

Dedication(s) and Improvement(s): Prior to the issuance of any building permits, except demolition, excavation, or foundation permits, public improvements and dedications for streets and other rights of way adjoining the subject property shall be guaranteed to the satisfaction of the Department of Public Works, Bureau of Engineering, Fire Department (and other responsible City, regional, and Federal government agencies, as may be necessary).

1. **Responsibilities/Guarantees.**
 - a. As part of early consultation, plan review, and/or project permit review, the applicant/developer shall contact the responsible agencies to ensure that any necessary dedications and improvements are specifically acknowledged by the applicant/developer.
 - b. Prior to issuance of sign-offs for final site plan approval and/or project permits by the Department of City Planning, the applicant/developer shall provide written verification to the Department of City Planning from the responsible agency acknowledging the agency's consultation with the applicant/developer. The required dedications and improvements may necessitate redesign of the project. Any changes to the project design required by a public agency shall be documented in writing and submitted for review by the Department of City Planning.
2. **Transportation Improvements.** The applicant shall consult with the Bureau of Engineering (BOE) and the Department of Transportation (DOT) for any required improvements. These requirements must be guaranteed before the issuance of any building permit through the B-permit process of the Bureau of Engineering, department of Public Works. They must be constructed and completed prior to issuance of any Certificate of Occupancy to the satisfaction of DOT and the Bureau of Engineering. Note: Street Improvement requirements are based on BOE's Letter received on September 24, 2014, prior to the adoption of the Mobility Plan 2035. The following street improvements reflect the prior (Transportation Plan's) street designations and standard roadway dimensions.

The improvements shall include the following:

a. **Dedication Required:**

Sawtelle Boulevard (Collector Street) – A 2-foot wide strip of land along property frontage to complete a 32-foot half right-of-way in accordance with Collector Street standards.

Alley South of Santa Monica Boulevard – A 5-foot wide strip of land along the alley frontage to complete a 10-foot half right-of-way in accordance with

Alley standards, including a 10-foot by 10-foot corner cut at the alley to alley intersection.

Alley East of Sawtelle Boulevard – A 2.5-foot wide strip of land along the alley frontage to complete a 10-foot half right-of-way in accordance with Alley standards.

b. **Improvements Required:**

Sawtelle Boulevard – Construct a new 12-foot concrete sidewalk and repair or replace all broken, off-grade or bad order concrete curb, gutter and asphalt pavement adjacent to the property frontage.

Alley South of Santa Monica Boulevard – Construct a new 15-foot wide alley with asphalt concrete pavement including in the area of alley to alley corner cut. Repair the 2-foot longitudinal concrete gutter and reconstruct and widen the alley intersection at Sawtelle Boulevard to City Standards.

Alley East of Sawtelle Boulevard – Construct a new 10-foot wide alley with asphalt pavement and repair or replace the existing asphalt pavement and 2-foot longitudinal concrete gutter adjacent to the property.

Install tree wells with root barriers and plant street trees satisfactory to the City Engineer and the Urban Forestry Division of the Bureau of Street Services. Some tree removal in conjunction with the street improvement project may require Board of Public Works approval. The applicant should contact the Urban Forestry Division for further information (213) 847-3077.

Notes: Street lighting may be required satisfactory to the Bureau of Street Lighting (213) 847-1551.

Department of Transportation may have additional requirements for dedications and improvements.

1. Drain the roof and site to the public right-of-way.
2. Sewers. Sewer lines exist in Sawtelle Boulevard. All Sewerage Facilities Charges and Bonded Sewer Fees are to be paid prior to obtaining a building permit.
3. An investigation by the Bureau of Engineering West Los Angeles District Office Sewer Counter may be necessary to determine the capacity of the existing public sewers to accommodate the proposed development. Submit a request to the WLA Office of Bureau of Engineering and the Department of Transportation for review and approval.
4. Submit a parking area and driveway plan to the Central District Office of the Bureau of Engineering and the Department of Transportation for review and approval.

ADMINISTRATIVE CONDITIONS OF APPROVAL

1. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review or approval, plans etc., as may be required by the subject conditions, shall be provided to the Department of City Planning for placement in the subject file.
2. **Code Compliance.** Use, area, height, and yard regulations of the zone classification of the subject property shall be complied with, except where granted conditions differ herein.
3. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning and any designated agency or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.
4. **Building Plans.** All the Conditions of Approval, and any other written modifications, shall be printed on the final building plans / drawings submitted to the Department of City Planning and the Department of Building and Safety.
5. **Corrective Conditions.** The authorized use shall be conducted at all times with due regard for the character of the surrounding district, and the right is reserved to the City Planning Commission, or the Director pursuant to Section 12.27.1 of the Municipal Code, to impose additional corrective conditions if, in the Commission's or Director's opinion, such conditions are proven necessary for the protection of persons in the neighborhood or occupants of adjacent property.
6. **Final Plans.** Prior to the issuance of any building permits for the project by the Department of Building and Safety, the applicant shall submit all final construction plans that are awaiting issuance of a building permit by the Department of Building and Safety for final review and approval by the Department of City Planning. All plans that are awaiting issuance of a building permit by the Department of Building and Safety shall be stamped by Department of City Planning staff "Final Plans". A copy of the Final Plans, supplied by the applicant, shall be retained in the subject case file.
7. **Department of Building and Safety.** The granting of this Determination by the Director of Planning does not in any way indicate compliance with applicable provisions of the Los Angeles Municipal Code (LAMC). Any corrections and/or modifications to plans made subsequent to this determination by a Department of Building and Safety Plan Check Engineer that affect the uses, or any part of the exterior design or appearance of the project as approved by the Director, and which are deemed necessary by the Department of Building and Safety for Building Code compliance, shall require a referral of the revised plans back to the Department of City Planning for additional review and sign-off prior to the issuance of any permit in connection with those plans.
8. **Expiration.** In the event that this grant is not utilized within three years of its effective date (the day following the last day that an appeal may be filed), the grant shall be considered null and void. Issuance of a building permit, and the initiation of, and diligent continuation of, construction activity shall constitute utilization for the purposes of this grant.
9. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these Conditions of Approval shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent owners, heirs or assigns. Further, the agreement must be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy

bearing the Recorder's number and date must be given to the Department of City Planning for attachment to the subject file.

10. Indemnification and Reimbursement of Litigation Costs. Applicant shall do all of the following:

- (i) Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- (ii) Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
- (iii) Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$25,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- (iv) Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- (v) If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions include actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

FINDINGS

General Plan Findings

The General Plan defines the foundation for all land use decisions. The City of Los Angeles' General Plan consists of the Framework Element, seven required Elements that are mandated by State law including Land Use, Mobility, Housing, Conservation, Noise, Safety, and Open Space, and optional Elements including Air Quality, Health, and Service Systems. Thirty-five individual community plans comprise the Land Use Element for the City of Los Angeles. This section provides relevant goals, objectives, policies, and programs that are established in the General Plan that form the basis for the Staff's recommended actions for the proposed project.

1. General Plan Land Use Designation

The subject site is located within the area covered by the West Los Angeles Community Plan updated and adopted by the City Council on July 27, 1999. The Plan Map designates the subject property for Neighborhood Commercial with the corresponding zones of C1, C1.5, C2, C4, RAS3, RAS4, P.

2. General Plan Text

General Plan Framework Element

The Citywide General Plan Framework is a guide for communities to implement growth and development policies by providing a comprehensive long-range view of the City as a whole. The General Plan Framework establishes categories of land use including Single-Family Residential and Multi-Family Residential that are broadly described by ranges of intensity/density, heights, and lists of typical uses. The definitions reflect a range of land use possibilities found in the City's already diverse urban, suburban, and rural land use patterns.

The Citywide General Plan Framework defines policies related to growth and includes policies for land use, housing, urban form/neighborhood design, open space/conservation, economic development, transportation, and infrastructure/public services. The proposed project would be in conformance with several goals of the General Plan Framework as described below.

Policy 3.2.4. Provide for the siting and design of new development that maintains the prevailing scale and character of the City's stable residential neighborhoods and enhance the character of commercial and industrial districts.

Policy 3.7.1. Accommodate the development of multi-family residential units in areas designated in the community plans in accordance with Table 3-1 and Zoning Ordinance densities indicated in Table 3-3, with the density permitted for each parcel to be identified in the community plans.

The project is a new residential development located along Sawtelle Boulevard, between Santa Monica Boulevard to the north and Iowa Avenue to the south, which offers a mixture of commercial and multifamily residential uses, in varying scale with the residential uses housed in one-, two-, four- and five-story buildings, and the commercial uses housed in one- and two-story buildings. The mixture of commercial and multifamily uses along Sawtelle Boulevard encourages local residents to walk to the nearby commercial

amenities, such as retail, restaurants, a theater, and other small businesses. Public facilities such as Nora Sterry Elementary School and the West Los Angeles Civic Center are in close proximity. The new residential development will fit in with the scale and character of the existing residential neighborhood and commercial district along Sawtelle Boulevard as it will be limited to five-stories in height which is consistent with the scale of existing multifamily residential development to the south along Sawtelle Boulevard, and to the east along Beloit Avenue. The new development will enhance the local commercial district as it will create new housing opportunities that will inevitably generate additional pedestrian activity in an active commercial district. By designating five (5) units for Joint Live-Work occupancy, the project will strike a balance by providing additional dwelling units and retain a commercial component that will continue to serve the community.

Moreover, the General Plan Framework Long Range Land Use Diagram designates Sawtelle Boulevard as a Mixed Use Boulevard. The Framework Element describes Mixed Use Boulevards as falling “within a range of floor area ratios from 1.5:1 up to 4.0:1” and “generally characterized by 1- to 2-story commercial structures, up to 3- to 6-story mixed use buildings between centers and higher buildings within centers.” The project consists of 48 residential units, which is allowed through a 35 percent Density Bonus in the RAS4 zone. The RAS4 zoning is a Mixed Use Zone consistent with the land use designation of Neighborhood Commercial on the West Los Angeles Community Plan land use map and the Framework Element designation of Sawtelle Boulevard as a Mixed Use Boulevard.

Lastly, under the General Plan Framework Element, the project is subject to the Residential Citywide Design Guidelines. As designed and conditioned, the project complies with the following relevant Guidelines and is therefore compatible with existing and future development on neighboring projects.

Objective 1: Consider Neighborhood Context & Linkages in Building & Site Design

- *In dense neighborhoods, incorporates passageways or paseos into midblock developments, particularly on through blocks, to facilitate pedestrian access to commercial amenities nearby, such that pedestrians will not need to walk the perimeter of a block in order to access the middle of the next parallel street or alley.*
- *Activates mid-block passageways or paseos using water features, pedestrian-level lighting, artwork, benches, landscaping; or special paving so that they are safe and visually interesting spaces.*
- *Incorporates transitions such as landscaping, paving, porches, stoops, and canopies at individual entrances, and from the sidewalk to the front door. These methods should not protrude into required yards or negatively impact the overall street wall.*
- *Design apartment buildings to ensure that all street-fronting units have a primary entrance facing the street.*
- *Incorporate transitions such as landscaping, paving, porches, stoops, and canopies at individual entrances to residences, and from the sidewalk to the front door. These methods should not protrude into required yards or negatively impact the overall street wall.*

As designed and conditioned, the project will conform to the Guidelines. As proposed, the project provides two pedestrian entrances, one facing Sawtelle Boulevard and the other

facing the north alleyway. Planters filled with landscaping will line each building facade. Such planters will be limited to 3.5 feet in height to allow for a visible connection between the project and pedestrians at the street level. In addition, the ground floor units will have porches facing both Sawtelle Boulevard and the alley to the north. The north and east alley will be improved with special paving referred to as a “stamped asphalt” and pedestrian-scaled street lights thereby activating the alleyway. Lastly, the units on the ground floor will be accented by clear tempered glass windows and doors. The pedestrian entrances, landscaped planters, articulated building facades, and street improvements will foster a safe and walkable environment consistent with the Residential Design Guidelines, as well as be aesthetically pleasing for pedestrians and future residents.

Objective 2: Employ Distinguishable and Attractive Building Design

- *Add architectural details to enhance scale and interest on the building façade by breaking it up into distinct planes that are offset from the main building façade. Porches and stoops can be used to orient housing towards the street and promote active and interesting neighborhood streetscapes.*
- *Alternate different textures, colors, materials, and distinctive architectural treatments to add visual interest while avoiding dull and repetitive facades.*
- *Utilize windows and doors as character-defining features to reflect an architectural style or theme consistent with other façade elements. Windows should project or be inset from the exterior building wall and incorporate well-designed trims and details.*
- *Orient windows on street facing units toward public streets, rather than inward, to contribute to neighborhood safety and provide design interest.*

Objective 3: Provide Pedestrian Connections Within and Around the Project

- *Utilize pedestrian lighting, seating areas, special paving, or landscaping. Ensure that new developments adjacent to transit stops invest in pedestrians amenities such as trash receptacles and sheltered benches or seating areas for pedestrian that do not intrude into the accessible route.*

The proposed project has a contemporary aesthetic. The façade is characterized by horizontal planes at each floor level offering a break in the building mass. One large vertical plane pronounces the building’s center adding to the variations in plane along the front facades. Substantial glazing and balconies with glass railings at each façade provide a strong orientation toward the street and alleyways.

The colors and materials are also contemporary which include: a smooth sand finish, cement plaster in two alternating colors of off-white or dark gray; dark gray zinc panels, frame-less glass clamp railing system for each of the balconies; double door glass entrances; and, aluminum framed doors and windows. The proposed architectural style, articulation in the building material, mass and color will render an attractive building design.

As mentioned above, pedestrian lighting, special paving and landscaping will enhance the alleyways offering a pedestrian connection around the project to nearby commercial corridors.

Objective 4: Minimize the Appearance of Driveways and Parking Areas

- *Prioritize pedestrian access first and automobile access second. Orient parking and driveways toward the rear or side of buildings and away from the public right-of-way.*

Vehicular access will be located to the rear (east alley) of the project site, away from foot traffic along Sawtelle Boulevard. In located the parking garage ingress and egress to the rear, the project will improve the pedestrian experience and reduce potential for pedestrian (or bicyclist) and vehicular conflicts.

The project proposes two pedestrian entryways: one fronting on Sawtelle Boulevard and the second along the north alleyway. This was intentionally designed to activate the alleyway and improve connectivity between the alley and commercial uses along Sawtelle Boulevard and Santa Monica Boulevard. The alley will be clearly marked with special paving and street lights.

Objective 5: Utilize Open Areas and Landscape Opportunities to their Full Potential

- *Design landscaping to be architecturally integrated with the building and suitable to the functions of the space while selecting plant materials that complement the architectural style and form of the building.*

Objective 6: Improve the Streetscape Experience by Reducing Visual Clutter

- *Use ornamental low-level lighting to highlight and provide security for pedestrian paths and entrances. Ensure that all parking areas and pedestrian walkways are illuminated.*

As Conditioned, the project will include mature Mexican Palo Verde and Australian Willow trees placed in the front yard areas to enhance and architecturally integrate into the building design. Along the alley, fixed landscape planters will help to define the transition between the building's side yard and the alley. Ornamental street lights will offer low-level lighting; this will ensure that all areas surrounding each building façade are safe and inviting for pedestrians at all times of the night.

Housing Element

Policy 2.1.4. Enhance livability of neighborhoods by upgrading the quality of development and improving the quality of the public realm, including streets, streetscape and landscaping to provide shade and scale.

The project is a new multi-family residential building that will include 48 residential units. This project will help to generate pedestrian activity during and after normal commercial business hours and create a linkage to the more active commercial blocks of Sawtelle Boulevard to the south. The development will replace an existing one-story bookstore, with a rear surface parking lot and dwelling unit, and a two-story multifamily apartment building, with a well-designed project that relates to Sawtelle Boulevard and activates the adjoining alley to the north of the site. By improving the alley with special paving and lighting, adding a second building lobby off of the alley, providing wider sidewalks through the required street dedication, placing mature trees in the front yard, and creating semi-

private patio areas for the ground floor units, the project will upgrade the public realm and improve pedestrian comfort and safety. The proposed project will incorporate a design that is appropriate for the development pattern adjacent to this portion of Sawtelle Boulevard, while being sensitive to the adjacent moderate density neighborhoods through the architectural design and site layout.

Mobility Element

Policy 3.1. Recognize all modes of travel, including pedestrian, bicycle, transit, and vehicular modes- including goods movement – as integral components of the City's transportation system.

Policy 3.3. Promote equitable land use decisions that result in fewer vehicle trips by providing greater proximity and access to jobs, destinations, and other neighborhood services.

The proposed housing development will conform to the objectives and policies of the **Mobility Element** of the General Plan as it applies. The project fronts the east side of Sawtelle Boulevard, which is classified as a Collector Street and is dedicated to a width of 60 feet. It is one block south of Santa Monica Boulevard, classified as a Boulevard II (Major Highway Class II) and is dedicated to a width of 100 feet. The Bureau of Engineering is requiring a 2-foot strip of land along the property frontage to complete a 32-foot half right-of-way in accordance with Collector Street standards.

The project is a new multifamily residential project located one block south of a major transit and commercial corridor, Santa Monica Boulevard. The project will provide 48 market-rate housing units of which, four will be reserved as Very Low Income housing units. The project will provide new housing opportunities accessible to nearby neighborhood destinations: commercial businesses, employment centers, cultural amenities, public facilities, and the like. Adding on, the location of the project to such destinations will enable residents to pursue alternative modes of travel such as walking, bicycling, and transit-riding instead of driving. This convenience to many local amenities will, in turn, reduce the number of vehicle trips otherwise generated by a housing development with 48-units.

As conditioned, the project will be designed to enhance pedestrian circulation along Sawtelle Boulevard. The dedication along Sawtelle Boulevard will allow for the construction of a 12-foot concrete sidewalk and repair and replacement of all broken, off-grade or bad order concrete curbs, gutters and asphalt pavement adjacent to the property. The project has been designed so that there is no vehicular access to or from Sawtelle Boulevard. The alley will be used for vehicle ingress and egress and only one driveway is proposed at the rear of the project, along the alley (east of Sawtelle Boulevard). This will maintain the Sawtelle Boulevard façade for pedestrian access and avoid unnecessary curb cuts along the street, which will help to reduce vehicle/pedestrian conflicts. In addition, a newly constructed 12-wide sidewalk along Sawtelle Boulevard, will render safer sidewalks that will encourage pedestrian activity and alternative modes of travel, instead of driving.

In the transportation analysis dated November 25, 2014, the Department of Transportation has determined that, based on LADOT's traffic impact criteria, the proposed development is not expected to result in any significant traffic impacts based on estimated trips generated for the proposed uses. The project has been found by LADOT to result in an increase in the number of trips generated directly from the site by over 154 - due to the net increase of 39 dwelling units.

Land Use Element - West Los Angeles Community Plan

While broader planning issues, goals, objectives and policies are provided by the Citywide General Plan through its Framework, the West Los Angeles Community Plan sets forth planning goals, objectives, policies, and programs that pertain to the West Los Angeles Community. The Community Plans further refine the General Plan, and are intended to promote an arrangement of land uses, streets and services which will encourage and contribute to the economic, social and physical health, safety, welfare and convenience of the people who live and work in the community. The proposed project would be in conformance with several goals of the West Los Angeles Community Plan as indicated below.

GOAL 1: A SAFE, SECURE, AND HIGH QUALITY RESIDENTIAL ENVIRONMENT FOR ALL ECONOMIC, AGE, AND ETHNIC SEGMENTS OF THE COMMUNITY.

Policy 1-1.2. Promote neighborhood preservation in all residential neighborhoods.

Policy 1-1.3. Provide for adequate Multiple Family residential development.

Policy 1-2.1. Locate higher residential densities near commercial centers and major bus routes where public service facilities and infrastructure will support this development.

Objective 1-3. Preserve and enhance the varied and distinct residential character and integrity of existing residential neighborhoods.

Policy 1-3.1. Promote architectural compatibility and landscaping for new Multiple Family residential development to protect the character and scale of existing residential neighborhoods.

Objective 1-4. Provide affordable housing and increased accessibility to more population segments, especially students, the handicapped and senior citizens.

Policy 1-4.2. Ensure that new housing opportunities minimize displacement of residents.

The surrounding properties along Sawtelle Boulevard are designated Neighborhood Commercial with corresponding zones of C1, C1.5, C2, C4, RAS3, RAS4, P. Properties to the east and west of the site (on adjoining block faces) along Corinth Avenue and Beloit Avenue are designated High Medium Residential with a corresponding zone of R4. The properties north, west and south are zoned C2-1VL and have a General Plan Land Use Designation of Neighborhood Commercial. The land uses intensify south of Iowa Avenue, consisting of a 94 condominium development zoned (Q)RAS4-1VL with a Neighborhood Commercial General Plan Land Use Designation, and a 40 unit residential development southeast of the project site located in the R4-1 zone with High Medium Residential General Plan Land Use Designation. In addition, the land uses intensify north of Santa Monica Boulevard, consisting of a five-story hotel and one-story commercial retail centers in the C2-1VL zone with Neighborhood Commercial General Plan Land Use Designation.

The project provides 48 new residential units, four of which are affordable, in close proximity to existing jobs and services, as it is located south of the Santa Monica Boulevard Corridor and west of the 405 Freeway. The above objectives seek to concentrate higher density development in existing commercial corridors and in areas that are able to support such

development which are in close proximity to rail and bus transit stations. The West side of Los Angeles is home to UCLA and the Veterans Administration, both major employers, and other regional employment centers such as Century City, Westwood and Santa Monica, government services and other job-generating uses that may employ prospective residents.

There are multiple transportation opportunities in the subject site's immediate area. Currently, the Los Angeles Metropolitan Transportation Authority (Metro) routes a number of transit lines in close proximity to the project site. The Metro Purple Line Wilshire/Westwood Station, the Metro Expo Line Exposition/Sepulveda Station, and three bus lines are available within walking distance. The Metro Purple Line subway currently ends at Wilshire Boulevard and Western Avenue and the future approved alignment of the Purple Line is currently under construction which will expand the transit network eventually to Westwood Village from its current terminus. Similarly, the Metro Expo Line subway currently ends at Exposition in Culver City, but is being expanded to run to the City of Santa Monica – providing an accessible and effective transit network within walking distance of the project site. Consequently, reduced vehicular trips are anticipated due to the project site's location adjacent to these significant public transportation opportunities.

According to the West Los Angeles Community Plan, new development should provide for adequate multi-family residential development that is in close proximity to commercial centers and public transit. As the project is located near a major highway corridor and near major transit/bus lines, it achieves many of the same goals and objectives such as locating economically diverse housing opportunities on a site that is located near a transit corridor.

3. Vesting Zone Change Findings. *Pursuant to Section 12.32 of the Municipal Code, and based on these findings, the recommended action to rezone the property from C2-1VL to (T)(Q)RAS4-1VL is deemed consistent with public necessity, convenience, general welfare and good zoning practice.*

The proposed project, located at 1650–1654 South Sawtelle Boulevard between Santa Monica Boulevard and Iowa Avenue, is a 42,152 square-foot multi-family residential building containing 48 residential units. The building would be built to a maximum height of 56 feet.

The recommended zone change to (T)(Q)RAS4-1VL is consistent with the General Plan Land Use Designation of Neighborhood Commercial. The RAS4 (Mixed Use) zone is one of the corresponding zones of the Neighborhood Commercial designation. The proposed project and the proposed zone change are not only consistent with the existing planned land use but also the surrounding land use development pattern. The block in which the site is located currently has a mix of low- and mid-rise residential uses, ranging from one to five stories in height. Higher residential density development furthers several General Plan objectives, including encouraging in-fill projects, providing economically diverse housing options, and locating higher density development near public transit and shopping, services and employment. The proposed project meets those criteria. There are also many employment opportunities located nearby the site. Encouraging the location of higher density residential near several transit lines, the 405 Freeway, and Santa Monica Boulevard a major east-west arterial, promotes another General Plan policy, in that it will help to reduce vehicle trips, traffic congestion and air pollution.

Public Necessity

The West Los Angeles Community Plan, a part of the General Plan for the City of Los Angeles, designates the Project Site for Neighborhood Commercial uses, which corresponds to the C1, C1.5, C2, C4, RAS3, RAS4, and P zones. In analyzing the land use patterns in the immediate area, the RAS4 Zone with the Neighborhood Commercial General Plan Land Use designation that surrounds the project site to the north, west and south is appropriate for the project site.

The commercial land uses are directed north and west of the project site and the dense multifamily residential uses are directed to the south, east and southeast of the site as the land uses intensify. The Neighborhood Commercial designation for the Project Suite allows the RAS4 zone; therefore, the proposed RAS4 zoning classification is consistent with the City's General Plan.

The RAS4 zone designation would provide reduced yard requirements that would make the affordable housing component of this project feasible, allowing the project to provide an adequate number of market-rate units to off-set the four Very Low Income Units. A smaller project would have a lower development potential, yielding a small ratio of affordable units. At the same time, the project is providing at least five (5) Joint Live-Work units on the ground level in order to provide professional goods and services to the community, as was intended by the mixed-use zone.

As conditioned, the multi-family residential project conforms to the requirements of the RAS4 Residential/Accessory Services zone. The purpose of the RAS4 zone is to provide a mechanism to increase housing opportunities, enhance neighborhoods, and revitalize older commercial corridors. The RAS4 Zone is intended to provide a tool to accommodate projected population growth in mixed use and residential projects that is compatible with existing residential neighborhoods.

The rezoning of the site to accommodate the conditioned project will be consistent with public necessity as it will increase the housing opportunities in the West Los Angeles Community of Los Angeles by providing new housing options. There is a growing housing shortage in the City of Los Angeles. The proposed RAS4-1VL zoning will allow development of a viable and much needed land use in the neighborhood. In order to provide a range of unit types typical of modern apartment buildings, it is necessary to have a higher FAR. As conditioned, the compatible design of the multi-family residential project will enhance the neighborhood and will contribute to the revitalization of the older residential neighborhoods in the West Los Angeles Community Plan area. The West Los Angeles Community Plan encourages more diverse housing options and higher density multiple family residential developments in close proximity to commercial centers and major transit routes/stations where public service facilities and infrastructure will support this demand. The West Los Angeles Community Plan Objective 1-1 encourages the development of new housing to meet the diverse economic and physical needs of the existing residents and projected population growth for the West Los Angeles area.

Not only will the project provide necessary housing opportunities for the community, but it will also provide an improved pedestrian experience for the neighborhood. The existing residential development has minimal landscaping onsite and a large portion of the existing retail property is comprised of surface parking. The project has been designed so that the street frontage and building façade enhance the pedestrian experience with landscaping, semi-private porches and patios, and views into the building.

Convenience

The objectives of the West Los Angeles Community Plan include providing adequate land for new multiple family residential units while protecting lower density residential uses from incompatible development and designing new development to be compatible with adjacent residential neighborhoods. Changing the existing zone to RAS4 will allow for the development of a multi-family residential project that complements the adjoining residential development. It is also consistent with the objectives of the West Los Angeles Community Plan. The increase in the number of dwelling units in this higher density residential area will encourage the viability of the adjacent Santa Monica Boulevard corridor. The Public Convenience is also

served by centrally locating residential opportunities near a variety of employment and services, including commercial uses and high-rise office buildings along the Santa Monica Boulevard corridor.

General Welfare

The proposed project will promote general welfare of the community by the following:

- Helping to meet local housing needs with mixed-income housing
- Enhancing the sense of community in the area by providing a unique, well-designed, attractive development
- Reducing dependency on the automobile by locating new development, particularly housing, near public transit and shopping, services and employment

Good Zoning Practice

The requested Zone Change from C2-1VL to (T)(Q)RAS4-1VL is in substantial conformance with the purposes, intent and provisions of the General Plan, and is consistent with good zoning practice because it will provide for development of a higher density multi-family residential building that complements the high density character of the surrounding residential neighborhoods and adjacent commercial uses. The proposed Zone Change is consistent with the surrounding uses and development pattern. The parcels abutting the project site to the north, west, south are zoned C2-1VL and the parcels east are zoned R4-1VL. The 94-condominium development further south, across Iowa Avenue is zoned C2-1VL and RAS4-1VL. Surrounding uses located to the north, west, and south of the project site include one- and two-story commercial retail, restaurant, theater, a five-story hotel and multifamily residential uses in the Neighborhood Commercial Land Use. Surrounding uses west of the project site include one- to two-story multi-family residential uses in the High Medium Residential Land Use Designation. Also, further south (approximate 200 feet) of the project site, there is a five-story residential use in the Neighborhood Commercial Land Use Designation. The majority of this block is developed with multi-family buildings. The project design provides a land use transition in scale, density and character to the multi-family areas to the south and east of its location and commercial areas located to the north and west of its location.

Given the Neighborhood Commercial land use designation and the surrounding properties and building heights, the requested Zone Change from the current zoning on the property to (T)(Q)RAS4-1VL allowing for 56 feet in maximum height would reflect good zoning practices. As requested, this zone change would allow for better overall site design. The density and project size, made possible with the proposed 4.05:1 FAR, are compatible and consistent with the density of the multi-family uses in the area. The proposed Zone Change will enhance the pedestrian experience, meet local housing needs, and provide a development compatible with the neighborhood's character.

3. Density Bonus/Affordable Housing Incentives Compliance Findings. Pursuant to Section 12.22 A.25(c) of the LAMC, the Director shall approve a density bonus and requested incentive(s) unless the director finds that:

- a. *The incentives are not required to provide for affordable housing costs as defined in California Health and Safety Code Section 50052.5 or Section 50053 for rents for the affordable units.*

The record does not contain substantial evidence that would allow the Director to make a finding that the requested incentives are not necessary to provide for affordable housing costs per State Law. The California Health & Safety Code Sections 50052.5

and 50053 define formulas for calculating affordable housing costs for very low, low, and moderate income households. Section 50052.5 addresses owner-occupied housing and Section 50053 addresses rental households. Affordable housing costs are a calculation of residential rent or ownership pricing not to exceed 25 percent gross income based on area median income thresholds dependent on affordability levels.

The list of on-menu incentives in 12.22 A.25 was pre-evaluated at the time the Density Bonus Ordinance was adopted to include types of relief that minimize restrictions on the size of the project. As such, the Director will always arrive at the conclusion that the density bonus on-menu incentives are required to provide for affordable housing costs because the incentives by their nature increase the scale of the project.

The requested incentives, an increase in the building height and floor area ratio (FAR), are expressed in the Menu of Incentives per LAMC 12.22 A.25(f) and, as such, permit exceptions to zoning requirements that result in building design or construction efficiencies that provide for affordable housing costs. The requested incentives allow the developer to expand the building envelope so the additional four (4) restricted affordable units can be constructed and the overall space dedicated to residential uses is increased. These incentives support the applicant's decision to set aside four (4) Very Low Income dwelling units for 55 years.

Floor Area Ratio: The requested incentive allows for a 35 percent increase in the allowable Floor Area Ratio (FAR), which is 4.05:1 FAR or 42,152 square feet of building area. The Project would otherwise be limited to 3:1 FAR or 31,224 square feet of area. The proposed Project is providing a 4.05:1 FAR at 42,152 square feet of building area. This requested increase in the FAR allows for an expanded building envelope.

Building Height: The requested incentive allows for a 35 percent increase in the allowable building height. With the requested Vesting Zone Change, the allowable building height would be 50 feet by-right. If effectuated, the RAS4 zone, would allow up to 61 feet in height with the density bonus height incentive. Nevertheless, the proposed Project is requesting a 56-foot building height. This requested increase in the building height allows for an expanded building envelope, which is necessary for providing the four (4) units set aside for Very Low Income residents.

- b. *The Incentive will have a specific adverse impact upon public health and safety or the physical environment, or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse Impact without rendering the development unaffordable to Very Low, Low and Moderate Income households. Inconsistency with the zoning ordinance or the general plan land use designation shall not constitute a specific, adverse impact upon the public health or safety.*

There is no evidence that the proposed incentive will have a specific adverse impact. A "specific adverse impact" is defined as, "a significant, quantifiable, direct and unavoidable impact, based on objective, identified written public health or safety standards, policies, or conditions as they existed on the date the application was deemed complete" (LAMC Section 12.22.A.25(b)). The proposed Project and potential impacts were analyzed in accordance with the City's California Environmental Quality Act (CEQA) Guidelines and the L.A. CEQA Thresholds Guide. These two documents establish guidelines and thresholds of significant impact, and provide the data for determining whether or not the impacts of a proposed Project reach or exceed those thresholds. Analysis of the proposed Project involved the preparation of a Mitigated

Negative Declaration (MND) (ENV-2014-2848-MND), and it was determined that the proposed Project may have an impact on the following environmental factors: aesthetics; biology; and land use planning. However, mitigation measures will reduce impacts to less than significant, and are imposed as Conditions of Approval herein (Qualified (Q) Conditions 22 through 25). Therefore, there is no substantial evidence that the proposed Project will have a specific adverse impact on the physical environment, on public health and safety, and on property listed in the California Register of Historic Resources.

4. CEQA Findings

A Mitigated Negative Declaration (ENV-2014-2848-MND) and corresponding Mitigation Monitoring Program (MMP) were prepared for the proposed project. The Mitigation Monitoring Program (MMP) is a document that is separate from the MND and is prepared and adopted as part of the project's approval. Section 21081.6 of the Public Resources Code requires a Lead Agency to adopt a "reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment." The MND was circulated for public review from July 16 through August 5, 2015 for a 20-day period. The Planning Department received two comment letters, one from a concerned resident and one from the South Coast Air Quality Management District which addressed issues related to the sites location within a methane zone, potential air quality issues related to freeway adjacency, watershed quality and water resources, public safety with regard to fire protection services, and the City's General Plan policies related to infrastructure monitoring and circulation. Below are staff responses to the comment letters:

- As defined in Ordinance No. 175,790 and LAMC Section 91.7102, prior to the issuance of a building permit, the applicant is required to hire an engineer to analyze the project site within a methane zone and implement the engineer's design recommendations subject to DOGGR, LADBS, and LAFD plan review and approval. The project is also subject to the LADBS, LAFD, and DOT's review for compliance with fire, infrastructure, and safety requirements including emergency access, pedestrian safety, and the availability of personnel and equipment.
- The applicant is required to submit a Low Impact Development Plan and/or Standard Urban Stormwater Mitigation Plan (LID/SUSMP) to the City of Los Angeles Bureau of Sanitation Watershed Protection Division, which addresses the watershed and pollution control. Water supply and drought are addressed through compliance with the LA Green Building Code and the Water Management Ordinance No. 170,978, which would reduce the project's water use and impose water conservation measures.
- The City Council recently adopted the Mobility Plan 2035 on August 11, 2015, which is the City of Los Angeles' Circulation Element of the General Plan, providing the policy foundation and goals for achieving a transportation system that balances the needs of all roads used. The Mobility Plan also incorporates complete street principles and policy foundation, as required by AB 1358, adopted by the California State Legislature.
- The City's population, infrastructure and service capacities to accommodate the proposed project are addressed in the published MND in Sections X Land Use Planning, XII Population and Housing, XIV Public Services, XV recreation, XVI Transportation/Traffic, and XVII Utilities and Service Systems. As such, the project's environmental impacts have been analyzed in the MND and addressed through existing regulations. With mitigation, the project would have less-than-significant impacts.

- The site is located within 200 feet of the 405 Freeway. For this reason, the MND identified the inclusion of high efficiency “MERV13” air filters as an appropriate mitigation measure to reduce fine particulate matter and impacts to future residents. Minimum Efficiency Reporting Value (MERV) is a standard that rates the overall effectiveness of air filters. Higher value MERV rating equates to finer filtration, meaning fewer dust particles and other airborne contaminants can pass through the filter. Additionally, the building has been designed to orient balconies and outdoor areas toward the Sawtelle Boulevard and north and south facades, away from the freeway to the fullest extent possible.

The concerns expressed in the comment letter are addressed in the Initial Study and Mitigated Negative Declaration (ENV-2014-2848-MND). Any impacts identified by the Initial Study would be mitigated to less-than-significant levels through the implementation of the mitigation measures, which are imposed as Conditions of Approval, and existing Regulatory Compliance Measures (RCMs). The Mitigation Monitoring Program (MMP) is a document that is separate from the MND and is prepared and adopted as part of the project's approval. Section 21081.6 of the Public Resources Code requires a Lead Agency to adopt a “reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment.” In addition to the mitigation measures required of the project and any proposed project design features, the applicant is required to adhere to applicable RCMs required by law.

On the basis of the whole of the record before the lead agency including any comments received, the lead agency finds that, with imposition of the mitigation measures described in the MND, there is no substantial evidence that the proposed project will have a significant effect on the environment. The attached Mitigated Negative Declaration reflects the lead agency's independent judgment and analysis. The records upon which this decision is based are with the Environmental Review Section of the Planning Department in Room 750, 200 North Spring Street.

DENSITY BONUS LEGISLATION BACKGROUND

The California State Legislature has declared that “[t]he availability of housing is of vital statewide importance,” and has determined that state and local governments have a responsibility to “make adequate provision for the housing needs of all economic segments of the community.” Section §65580, subds. (a), (d). Section 65915 further provides that an applicant must agree to, and the municipality must ensure, the “continued affordability of all low and very low income units that qualified the applicant” for the density bonus.

California State Assembly Bill 2222 went into effect January 1, 2015. It introduces rental dwelling unit replacement requirements, which pertain to cases filed (not issued) as of January 1, 2015. This determination letter does not reflect replacement requirements because the case application was submitted to the Department of City Planning on August 5, 2014 prior to the effective date of the amended Law. The new state law also increases covenant restrictions from 30 to 55 years for cases issued (not just filed) as of January 1, 2015. This determination letter does reflect 55 year covenant restrictions, given that the case decision, or approval, as noted on the front page, is being issued after January 1, 2015.

With Senate Bill 1818 (2004), state law created a requirement that local jurisdictions approve a density bonus and up to three “concessions or incentives” for projects that include defined levels of affordable housing in their projects. In response to this requirement, the City created an ordinance that includes a menu of incentives (referred to as “on-menu” incentives) comprised of eight zoning adjustments that meet the definition of concessions or incentives in state law (California Government Code Section 65915). The eight on-menu incentives allow for: 1) reducing setbacks; 2) reducing lot coverage; 3) reducing lot width, 4) increasing floor area ratio (FAR); 5)

increasing height; 6) reducing required open space; 7) allowing for an alternative density calculation that includes streets/alley dedications; and 8) allowing for “averaging” of FAR, density, parking or open space. In order to grant approval of an on-menu incentive, the City utilizes the same findings contained in state law for the approval of incentives or concessions.

Under Government Code Section § 65915(a), § 65915(d)(2)(C) and § 65915(d)(3) the City of Los Angeles complies with the State Density Bonus law by adopting density bonus regulations and procedures as codified in Section 12.22 A.25 of the Los Angeles Municipal Code. Section 12.22 A.25 creates a procedure to waive or modify zoning code standards which may prevent, preclude or interfere with the effect of the density bonus by which the incentive or concession is granted, including legislative body review. The Ordinance must apply equally to all new residential development.

In exchange for setting aside a defined number of affordable dwelling units within a development, applicants may request up to three incentives in addition to the density bonus and parking relief which are permitted by right. The incentives are deviations from the City’s development standards, thus providing greater relief from regulatory constraints. Utilization of the Density Bonus/Affordable Housing Incentives Program supersedes requirements of the Los Angeles Municipal Code and underlying ordinances relative to density, number of units, parking, and other requirements relative to incentives, if requested.

For the purpose of clarifying the Covenant Subordination Agreement between the City of Los Angeles and the United States Department of Housing and Urban Development (HUD) note that the covenant required in the Conditions of Approval herein shall prevail unless pre-empted by State or Federal law.

FINANCIAL ANALYSIS/PRO-FORMA

Pursuant to the Affordable Housing Incentive Density Bonus provisions of the LAMC (Section 12.22 A.25), proposed projects that involve on-menu incentives are required to complete the Department’s Master Land Use Permit Application form, and no supplemental financial data is required. The City typically has the discretion to request additional information when it is needed to help make required findings. However, the City has determined that the level of detail provided in a pro forma is not necessary to make the findings for on-menu incentives. This is primarily because each of the City’s eight on-menu incentives provides additional buildable area, which, if requested by a developer, can be assumed to provide additional project income and therefore provide for affordable housing costs. When the menu of incentives was adopted by ordinance, the impacts of each were assessed in proportion to the benefits gained with a set-aside of affordable housing units. Therefore, a pro-forma illustrating construction costs and operating income and expenses is not a submittal requirement when filing a request for on-menu incentives. The City’s Density Bonus Ordinance requires “a pro forma or other documentation” with requests for off-menu incentives but has no such requirement for on-menu requests.

PUBLIC HEARING AND COMMUNICATIONS

The Public Hearing on this matter was held at West Los Angeles Municipal Building, 1645 Corinth Avenue, 2nd Floor Hearing Room, Los Angeles, CA 90012 on Monday, October 26, 2015 at 2 p.m.

Summary of Public Hearing

1. Present: 17 people signed in at the public hearing.
2. Public Speakers: 14 people spoke at the hearing, not inclusive of the applicant team. 6 people spoke in opposition to the project; 4 people spoke in support of the project.
3. The applicant, the applicant's representative and the project architect spoke at the hearing and described the project design and entitlement requests.
4. Public Hearing Testimony Notes

Speaker Comments Support the Project

- Location – The proposed 48-unit housing development will clean up and activate the area by connecting residents to the local and nearby commercial businesses, employment center, amenities and public facilities as it is one block south of Santa Monica Boulevard, which is a transit corridor, and home to commercial uses and employment. It will encourage residents to walk or use alternative modes of transportation other than driving.
- Height – The project's height is not a problem– the height is appropriate at this location.
- Design – The Contemporary architectural design is attractive and will complement the other Contemporary buildings on the block.
- Current Condition – The project site as existing is under-utilized and poorly maintained. The 48-unit housing development will enhance the area and clean up Sawtelle Boulevard and the two alleyways. The new development will keep away transients and keep the area safe.
- Traffic – The area already has lots of vehicular traffic. However, the shortage of vehicle parking spaces will encourage residents to use alternative modes of travel, such as walking, riding transit, or bicycling.

Speaker Comments Opposing the Project

- Height – The area was zoned to limit building height to four stories or 45 feet in height.
- Tenant Rights – What are the rights of the tenants living in the existing housing units at the project site?
- Gentrification – The new housing project will displace residents living in the existing dwelling units on the project site. It will change the character of the neighborhood from middle/working class to upper class.

- **Traffic** – There will be a net increase of housing thereby increasing the traffic in the area, thereby adding to the traffic problem. There is no cross-walk at the corner of Sawtelle Boulevard and Idaho Avenue and one should be added.
- **Existing Uses** – The existing bookstore will be forced out of business, as it will not be able to afford rent in such a pedestrian oriented neighborhood as its current location. It is local businesses such as the local bookstore, which contribute the neighborhood's character. Although the new development will not change the character of the neighborhood, it will serve as a catalyst of change as more sites are demolished and developed.

Summary of Written Comments

1. Six (6) people submitted comments in opposition to the project; two (2) people submitted written comments in support of the project.
2. Written Comments

Written Comments Supporting the Project

- **Location** – The housing project will be a great addition to the neighborhood considering it is one block south of Santa Monica Boulevard, which is a major transit corridor. Its location will also connect residents to local commercial businesses, employment centers and public facilities.
- **Density** – There is a substantial housing shortage in the City of Los Angeles, especially affordable housing. The new housing development will assist in this shortage and
- **Height** – The project height is appropriate to the area. There are many tall buildings in the area and Target fits in. It's near bus lines and the Metro stop and will serve an underserved neighborhood with new retail. It may be blocking some people's view, but the Target store is more important than someone's view.

Written Comments Opposing the Project

- **Height** – The local zoning restricts building height to 45 feet in height or four stories. Any height above this limit will hurt the character of this community.
- **Density** – The proposed density is too much for the area.
- **Traffic** – The increase in density will increase traffic in an area already congested with traffic – Sawtelle Boulevard and Santa Monica Boulevard, and the 405 Freeway. Due to the site's proximity to these congested streets, vehicles use these local streets as a major thoroughfare.
- **Air Quality during Construction** – Demolishing the existing bookstore and two-story apartment building onsite will create air pollution for the nearby residents and pedestrians.
- **Loss of Character** – The new housing development comes at a loss of existing uses, such as the bookstore on the project site. This development and the 94-condominium development to the south are contributing to a slow change in the neighborhood's character.

- Lack of Amenities – Rezoning this area will add to the lack of services to residents residing in this neighborhood. A mixed-used project that offers ground floor retail with residential above would benefit the local community than the proposed housing development.
- Gentrification – Housing projects such as the proposed 48 housing development and the newly constructed 94-condominium unit housing project down the street are displacing residents who wouldn't be able afford to live anywhere else in the City of Los Angeles with such amenities within walking distance. The new units will not be offered at the same rent as the units currently on-site.
- Easements – It is important that all existing easements housed in front of the subject property remain.

EXHIBIT A2

RADIUS MAP
CPC-2014-2847-VZC-DB




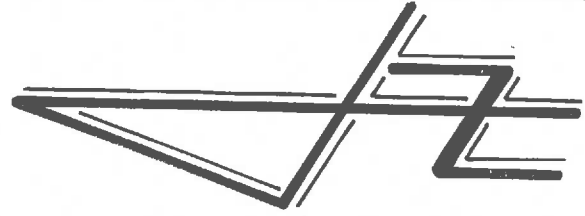
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LOTS 15 & 16
SEE APPLICATION

☐ REQUEST: C2-IVL TO RAS4

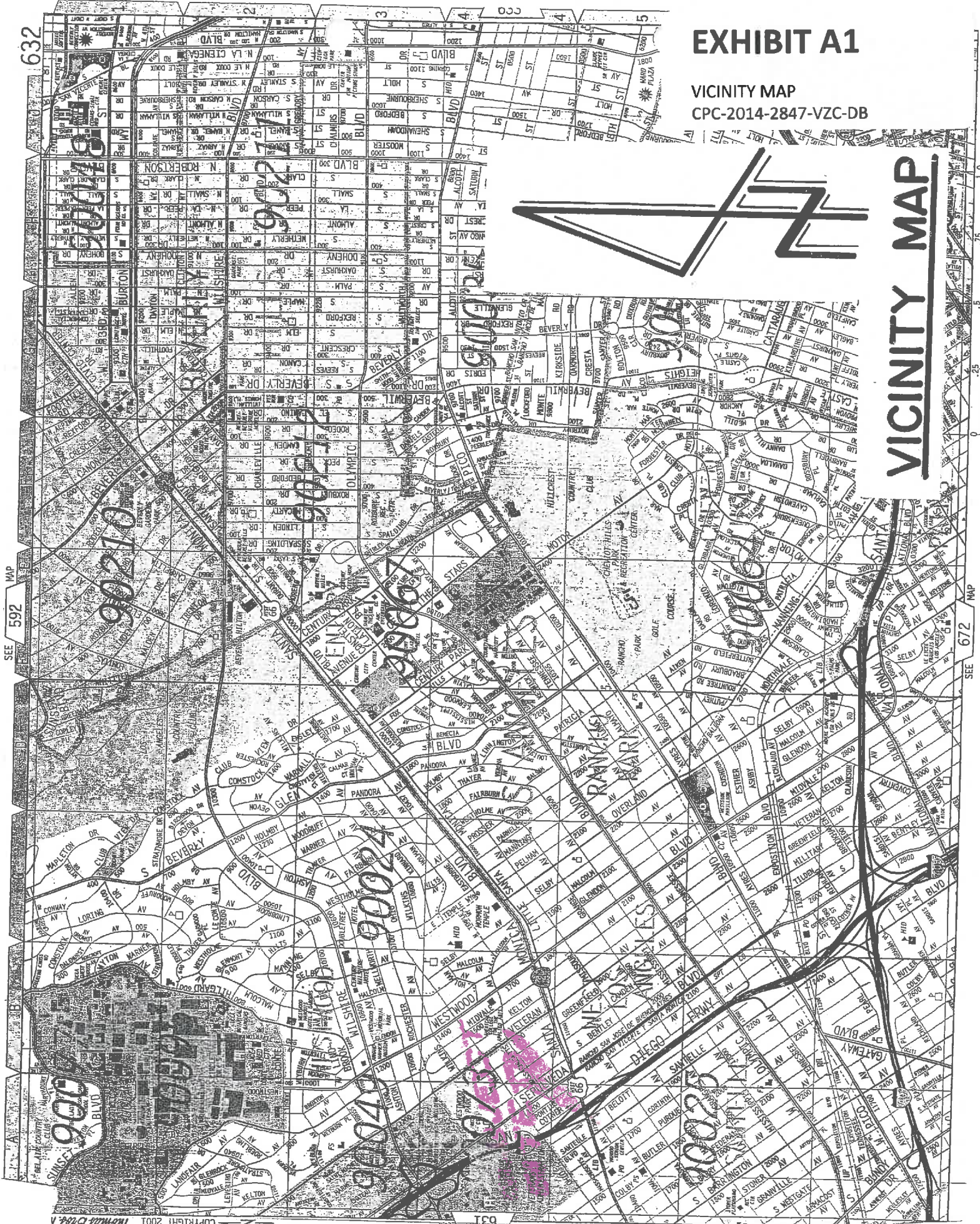
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<p>THOMAS BROS. PAGE: 632 , GRID: 5/A</p>	<p>ZONE CHANGE</p> <p> CONTINENTAL MAPPING SERVICE 6315 Van Nuys Boulevard, Van Nuys, CA 91401 (818) 787-1663</p>	<p>CASE NO.: DATE: 8-4-14 SCALE: 1" = 100' USES: FIELD D.M.: 129B149, 126B149</p>
<p>C.D. 11 C.T. 2673 P.A. 270</p>	<p>W.D. CWS 14-6588</p>	



VICINITY MAP





WEST RENDERING

No.	Revision	date

ARCHITECT

CHARLES HEFNER

No. C-23963

RENEWAL DATE

STATE OF CALIFORNIA

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PROJECT

SAWTELLE TERRACES
1650-1652 SAWTELLE BLVD.
LOS ANGELES, CA 90025

DRAWING TITLE

WEST RENDERING

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drawn: S.G.S.S.

job: 2014-14A

sheet:

A0.1

of sheets



NORTH RENDERING

No.	Revision	date

LICENSED ARCHITECT

CHARLES HEFNER

No. C-23863

RENEWAL DATE

STATE OF CALIFORNIA

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©WHLR

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LOS ANGELES, CA 90025

DRAWING TITLE

NORTH RENDERING

date: 07/16/2014

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drawn: S.G.S.S.

job: 2014-14A

sheet:

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of sheets



SOUTH RENDERING



EAST RENDERING

No.	Revision	date

LICENSED ARCHITECT

CHARLES HEFNER

No. C-23963

RENEW DATE

STATE OF CALIFORNIA

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LOS ANGELES, CA 90025

DRAWING TITLE

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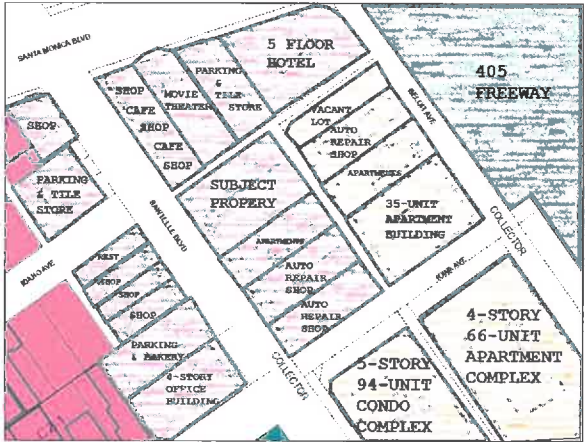
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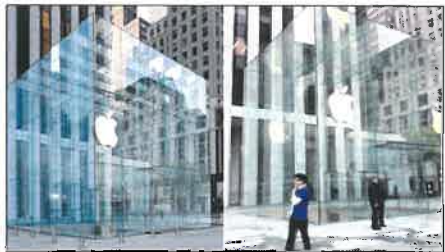
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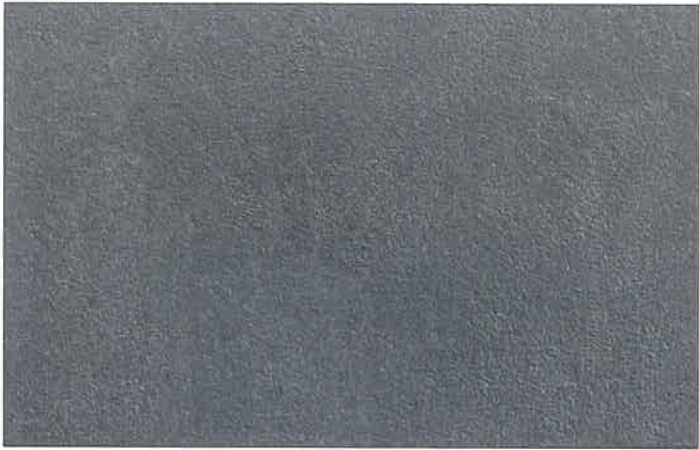
NEIGHBORHOOD VICINITY MAP



NEIGHBORHOOD VICINITY MAP W/ NEIGHBORS BUILDING LINES



4.1 DOUBLE DOOR GLASS ENTRY



2 SMOOTH SAND FINISH, CEMENT PLASTER, COLOR 8261 'NEW UNIFORM', BY VISTA PAINT (DARK GRAY)



8 42" HI. TEMPERED GLASS RAILING



13 PATTERNED - COLORED PAVING



1 SMOOTH SAND FINISH, CEMENT PLASTER, COLOR OW_107 'BRIDAL BANQUET', BY VISTA PAINT (OFF WHITE)



3 DOUBLE SIDE LANDSCAPE/STREET LIGHT



12 DARK GRAY ZINC PANELS

No.	Revision	date

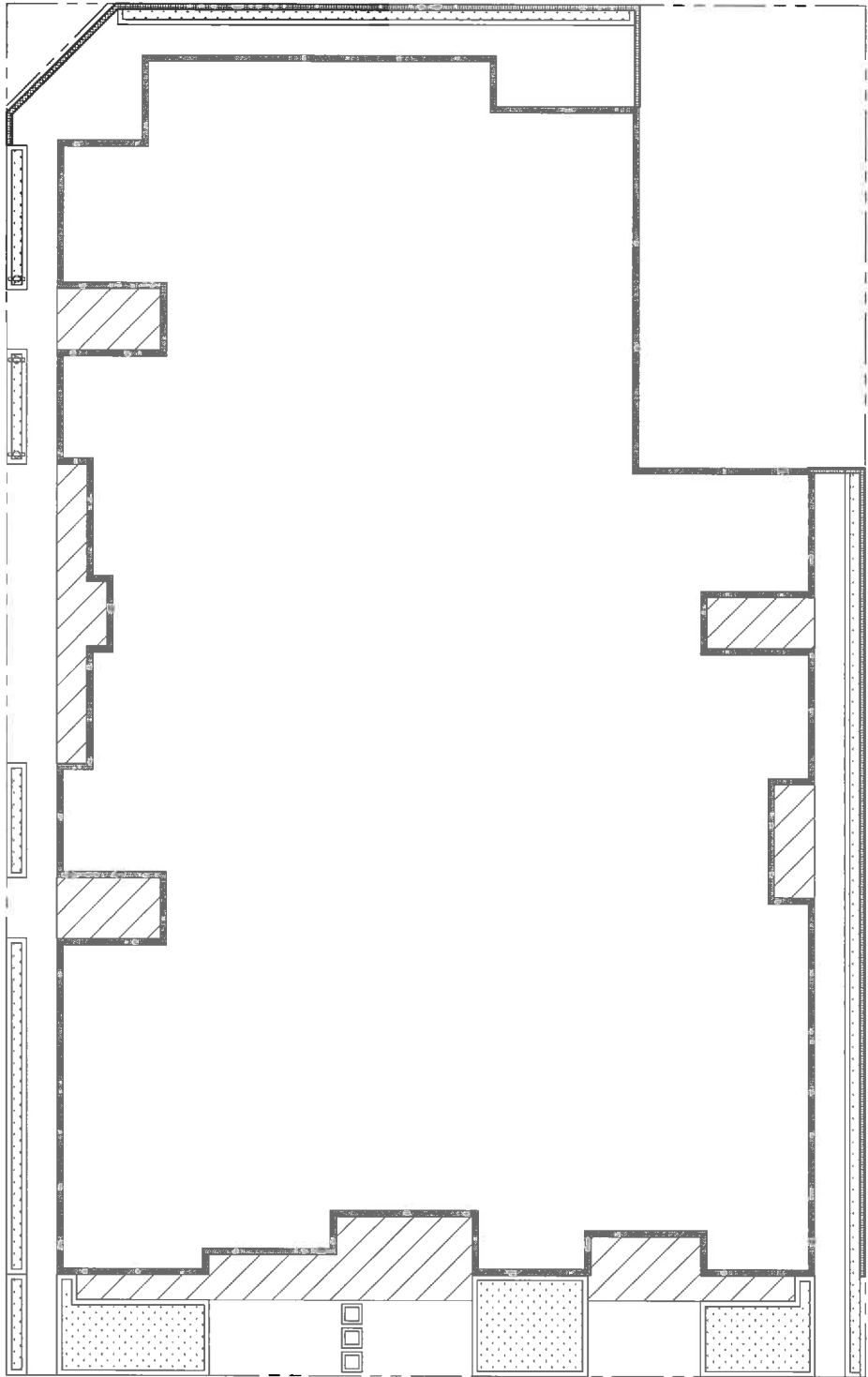


OWNER
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PROJECT
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1650-1652 SAWTELLE BLVD.
LOS ANGELES, CA 90025

DRAWING TITLE
MATERIAL AND VICINITY MAP

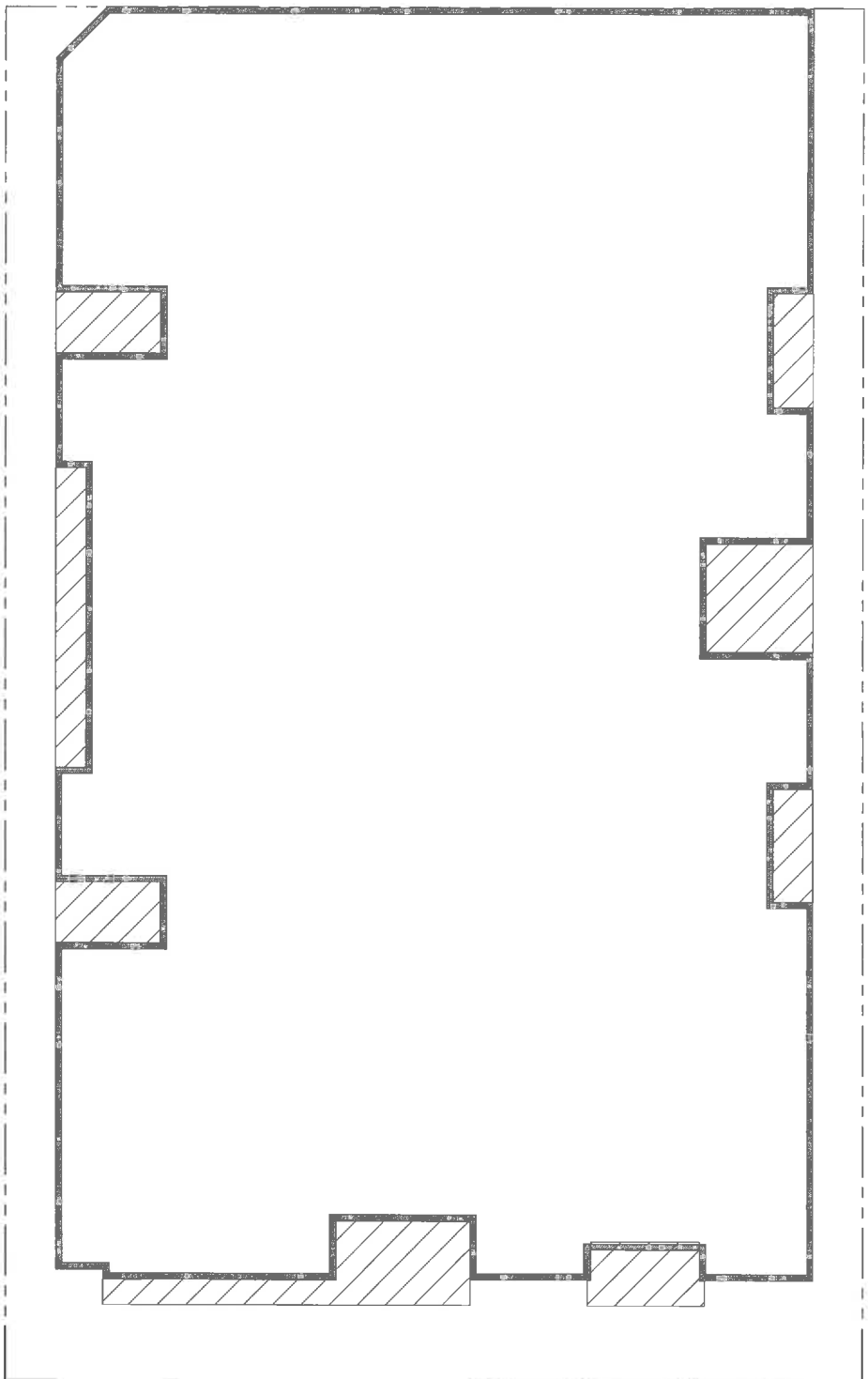
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FIRST FLOOR ARTICULATION PLAN

SCALE: 1/8" = 1'-0"

ARTICULATION / OPEN AREA



UPPER FLOOR ARTICULATION PLAN

ARTICULATION / OPEN AREA

No.	Revision	date

LICENSED ARCHITECT

CHARLES HEFNER

No. C-238963

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PROJECT

SAWTELLE TERRACES

1650-1652 SAWTELLE BLVD.

LOS ANGELES, CA 90025

DRAWING TITLE

ARTICULATION PLANS

date: 11/12/2015

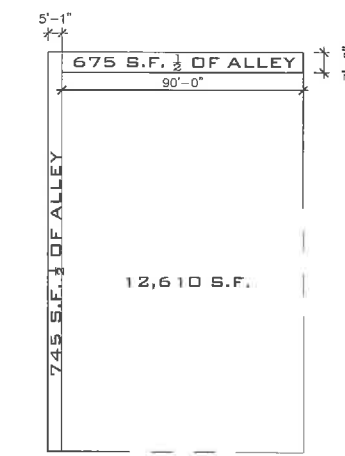
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drawn: S G S S

job: 2014-14A

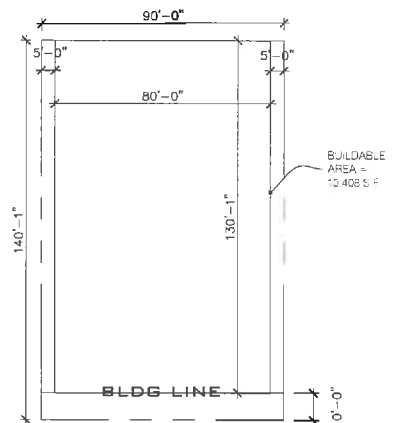
sheet: A1.2

of sheets



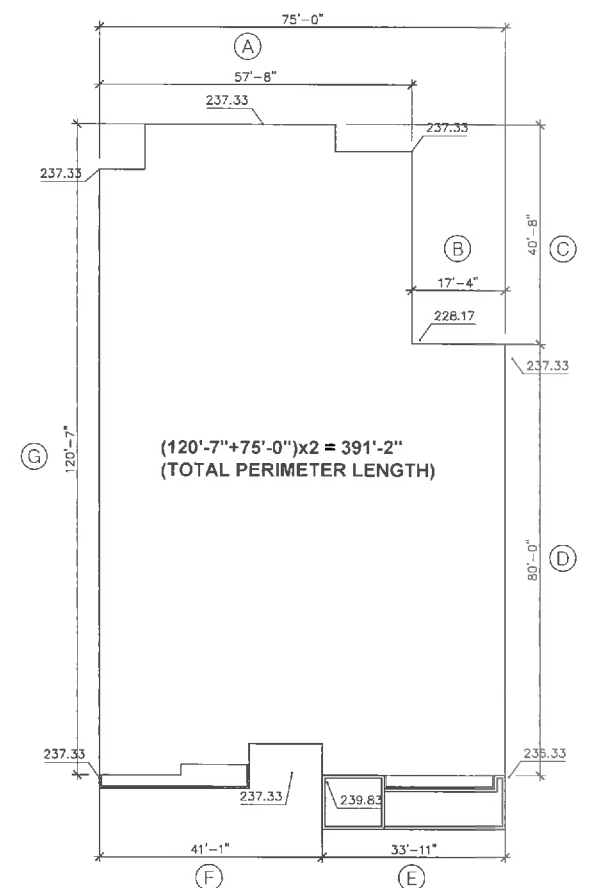
DENSITY CALC
SCALE: 1/32" = 1'-0"

12,610 ÷ 675 = 18.83
14,030 ÷ 400 (PER RAS4) = 35 UNITS



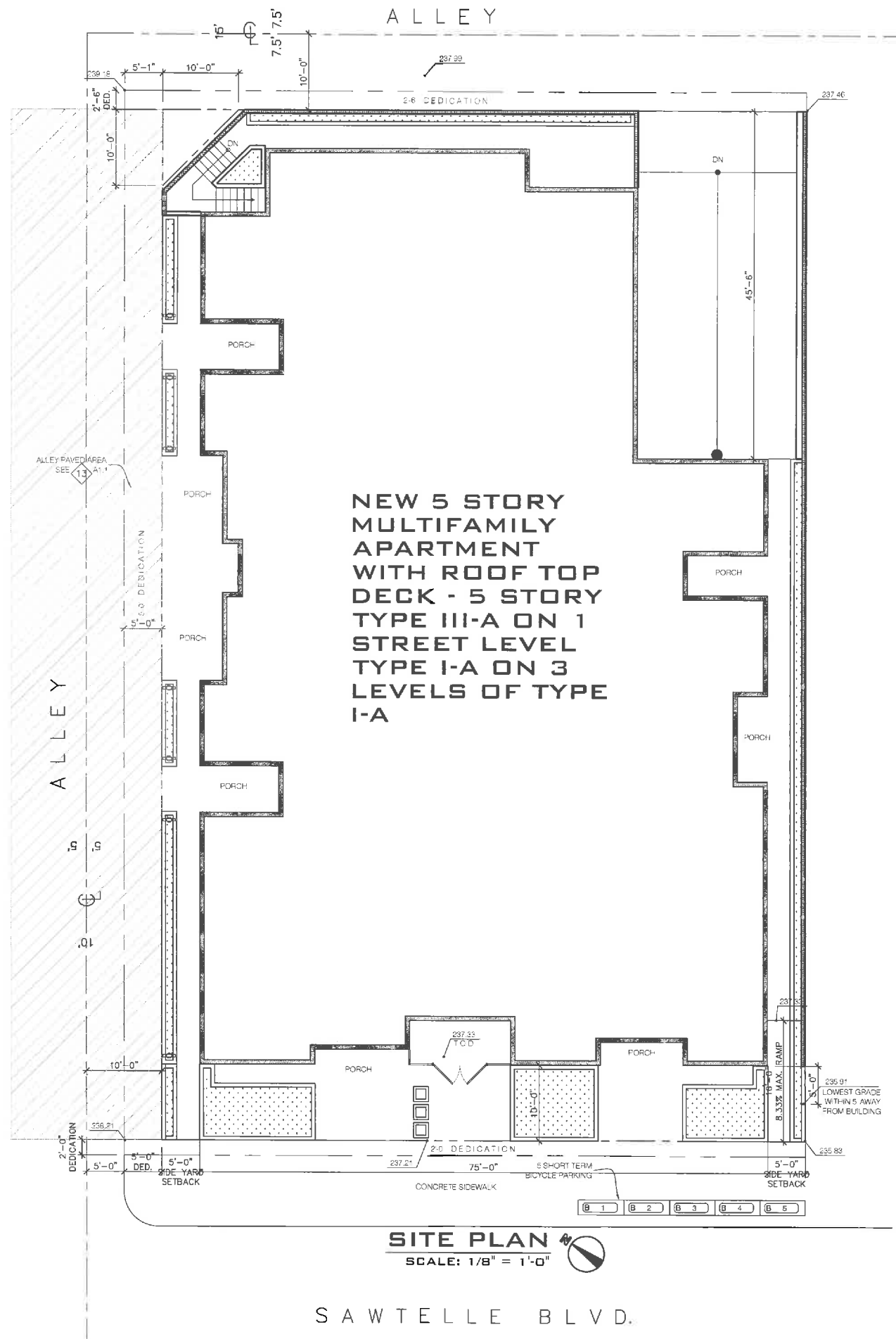
FAR CALC
SCALE: 1/32" = 1'-0"

FAR = 3X BUILDABLE PER RAS4
12,408 x 3 = 37,224 S.F.



GRADE PLANE CALC.
SCALE: 1/16" = 1'-0"

TAG	Length (foot)	Elevation	Elevation	Average	
A	57.66	237.33	237.33	237.33	13684.4478
B	17.33	228.17	237.33	232.75	4033.5575
C	40.66	237.33	228.17	232.75	9463.615
D	80	237.33	236.33	236.83	18946.4
E	33.91	237.33	239.83	238.58	8090.2478
F	41.08	237.33	237.33	237.33	9749.5164
G	120.58	237.33	237.33	237.33	28617.2514
	391.16				92585.0359
SUB Total					
TOTAL					92585.0359
Grade Plane (Total / 391.16 Feet)					236.693516



SITE PLAN
SCALE: 1/8" = 1'-0"

SAWTELLE BLVD.

No.	Revision	date
1		

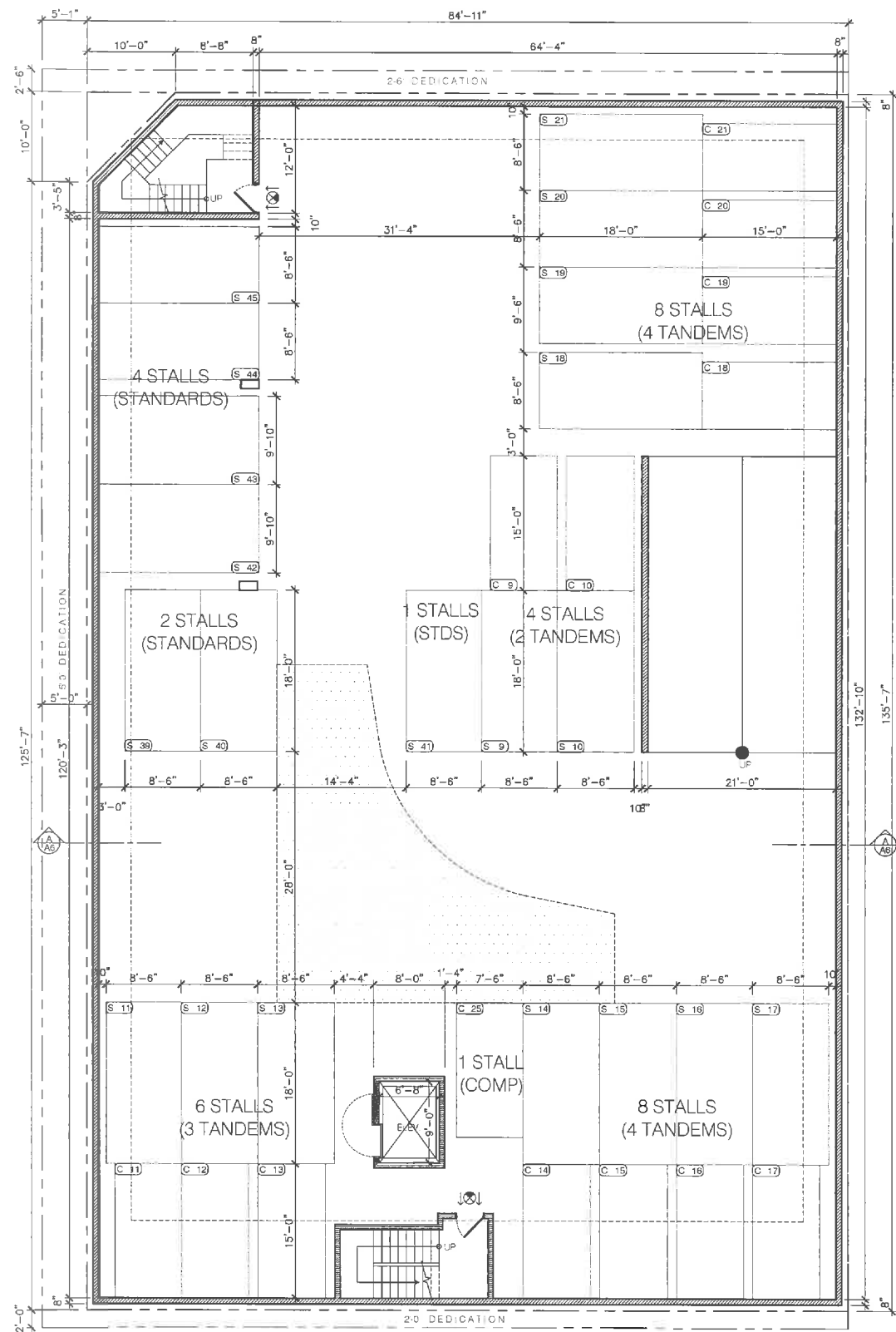
LICENSED ARCHITECT
CHARLES HEFNER
No. C-23963
PENNSYLVANIA
REGISTERED
SAM GHANOUNI
DESIGNER
9049 ALCOFF STREET #205
LOS ANGELES, CA 90035
TEL: 310-450-1976
e-mail: samghanouni@msa.com

OWNER
REXFORD ELEGANT
APARTMENTS, LLC
10350 SANTA MONICA BLVD, STE 190
LOS ANGELES, CA 90025

PROJECT
SAWTELLE TERRACES
1650-1652 SAWTELLE BLVD.
LOS ANGELES, CA 90025

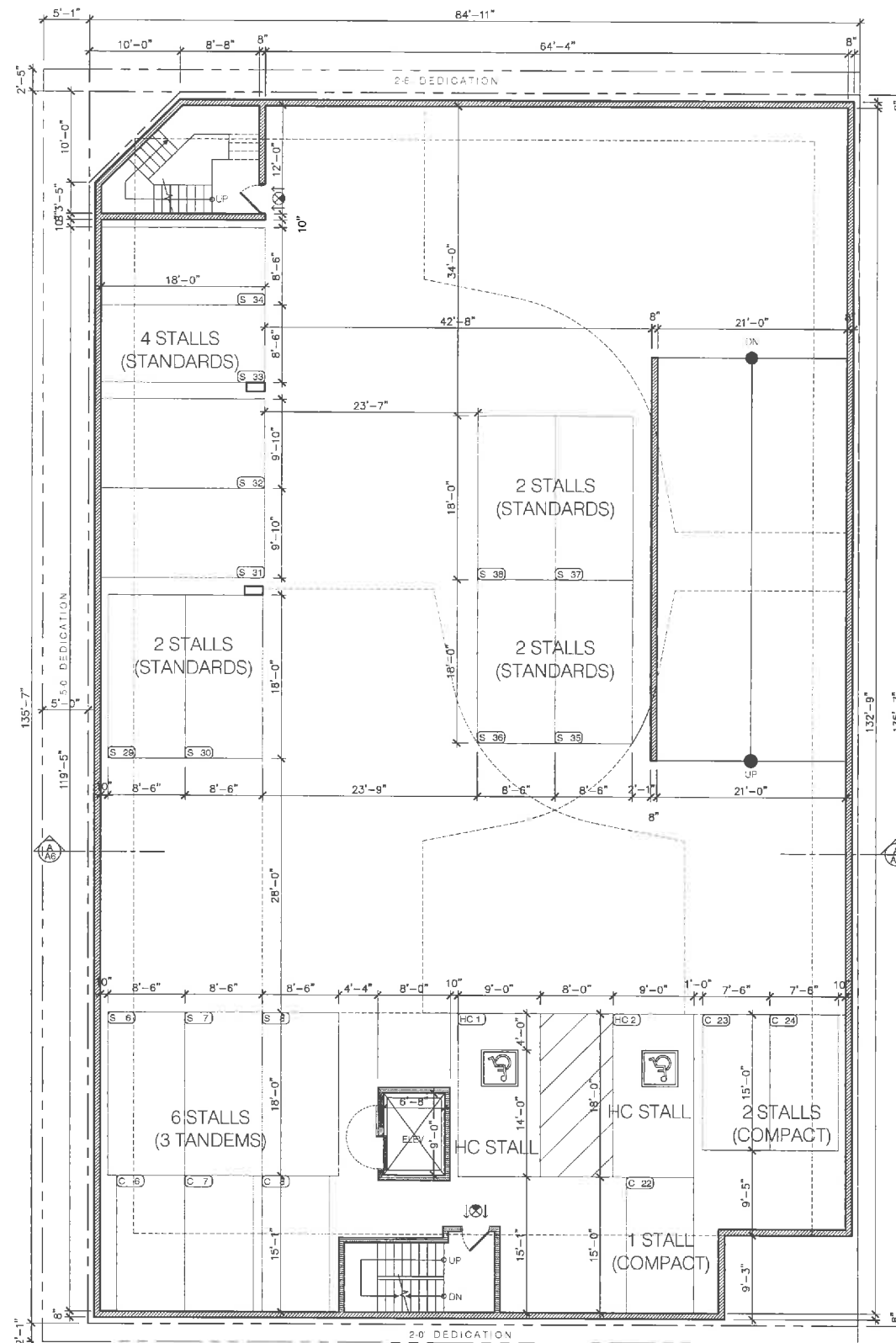
DRAWING TITLE
SITE PLAN

date: 07/16/2014
scale: 1/8" = 1'-0"
drawn: S G S
job: 2014-14A
sheet:
A1
of sheets



PARKING PLAN LEVEL 3

SCALE: 1/8" = 1'-0"



PARKING PLAN LEVEL 2

SCALE: 1/8" = 1'-0"

WALL TYPE LEGENDS:

- INTERIOR 1 HR. WOOD STUD WALL - SEE 2/D1
- INTERIOR 1 HR. WOOD STUD WALL-STC 50 - SEE 3/D1
- INTERIOR 1 HR. METAL STUD WALL - SEE 11a/D1
- INTERIOR 1 HR. WOOD STUD WALL-STC 50 - SEE 3/D1
- EXTERIOR TWO HOUR WOOD STUD WALL - SEE 9/D1
- INTERIOR TWO HOUR WOOD STUD WALL - STC 50 - SEE 7/D1
- INTERIOR TWO HOUR WOOD STUD WALL - SEE 6/D1
- 2x6 STUD PLUMBING WALL
- 2x4 STUD PLUMBING WALL
- 8" THK. CONC. WALL - SEE STRUCT.
- 6" THK. CONC. BLK WALL - SEE STRUCT.

No.	Revision	date



SAM GHANOUNI
DESIGNER

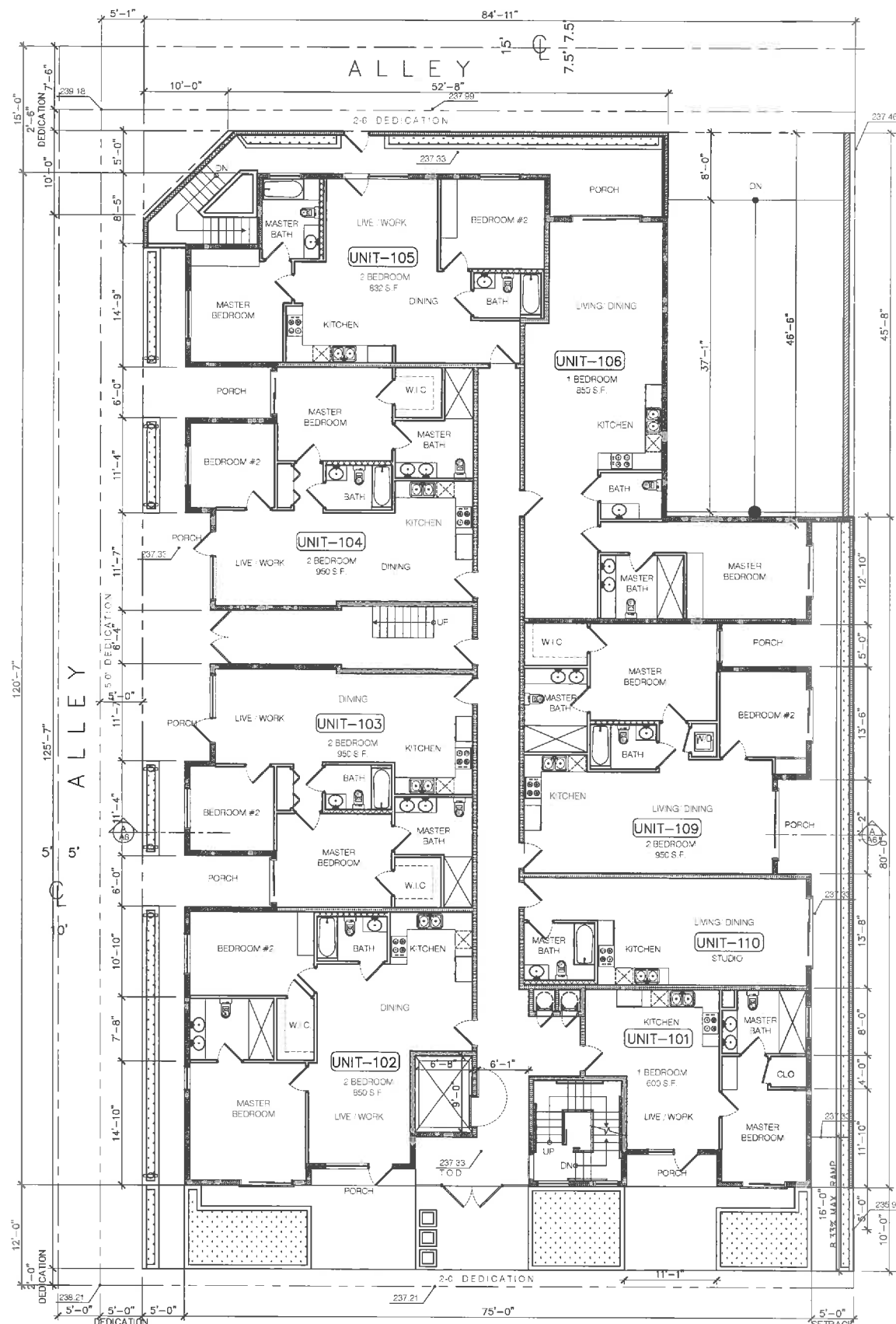
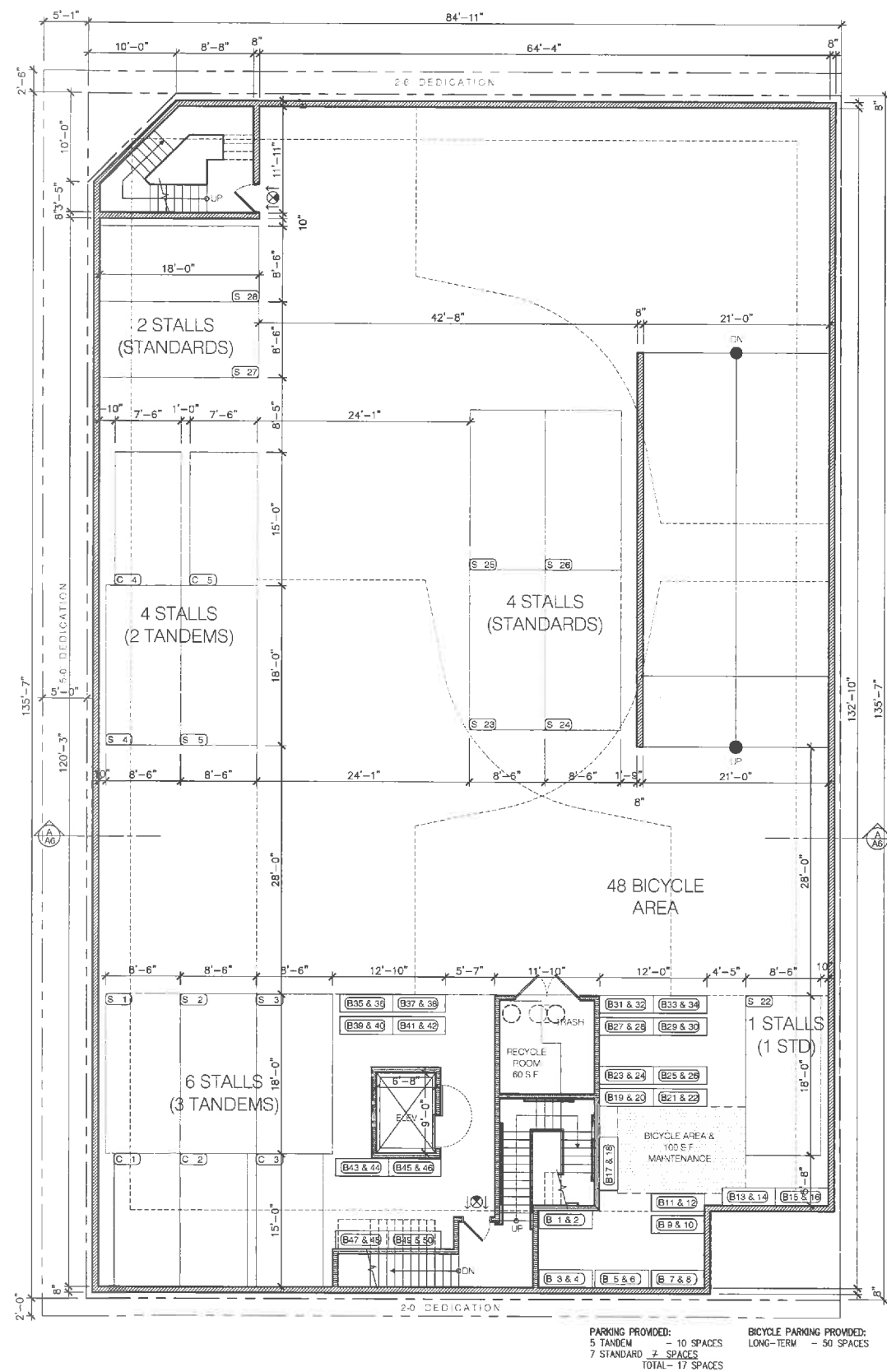
9046 ALDOTT STREET #206
LOS ANGELES, CA 90035
TEL: 310-430-1978
e-mail: samghanouni@msa.com












OWNER
 REXFORD ELEGANT
 APARTMENTS, LLC
 10350 SANTA MONICA BLVD, STE 190
 LOS ANGELES, CA 90025

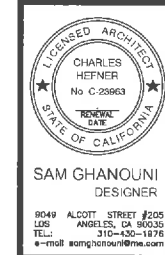
PROJECT
 SAWTELLE TERRACES
 1650-1652 SAWTELLE BLVD.
 LOS ANGELES, CA 90025

DRAWING TITLE
 PARKING PLAN LEVEL 2 & 3

date: 07/16/2014
 scale: 1/8" = 1'-0"
 drawn: S.G.S.S.
 job: 2014-14A
 sheet:
 A2
 of sheets



- ### WALL TYPE LEGEND:
- | | |
|---|--|
|  | INTERIOR 1 HR. WOOD STUD WALL- SEE 2/D1 |
|  | INTERIOR 1 HR. WOOD STUD WALL-STC 50 - SEE 3/D1 |
|  | INTERIOR 1 HR. METAL STUD WALL - SEE 11c/D1 |
|  | INTERIOR 1 HR. WOOD STUD WALL-STC 50 - SEE 3/D1 |
|  | EXTERIOR TWO HOUR WOOD STUD WALL - SEE 9/D1 |
|  | INTERIOR TWO HOUR WOOD STUD WALL - STC 50 - SEE 7/D1 |
|  | INTERIOR TWO HOUR WOOD STUD WALL - SEE 6/D1 |
|  | 2x6 STUD PLUMBING WALL |
|  | 2x4 STUD PLUMBING WALL |
|  | 8" THK. CONC. WALL - SEE STRUCT. |
|  | 6" THK. CONC. BLK WALL - SEE STRUCT. |

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QWINTER
REXFORD ELEGANT
APARTMENTS, LLC
10350 SANTA MONICA BLVD, STE 190
LOS ANGELES, CA 90025

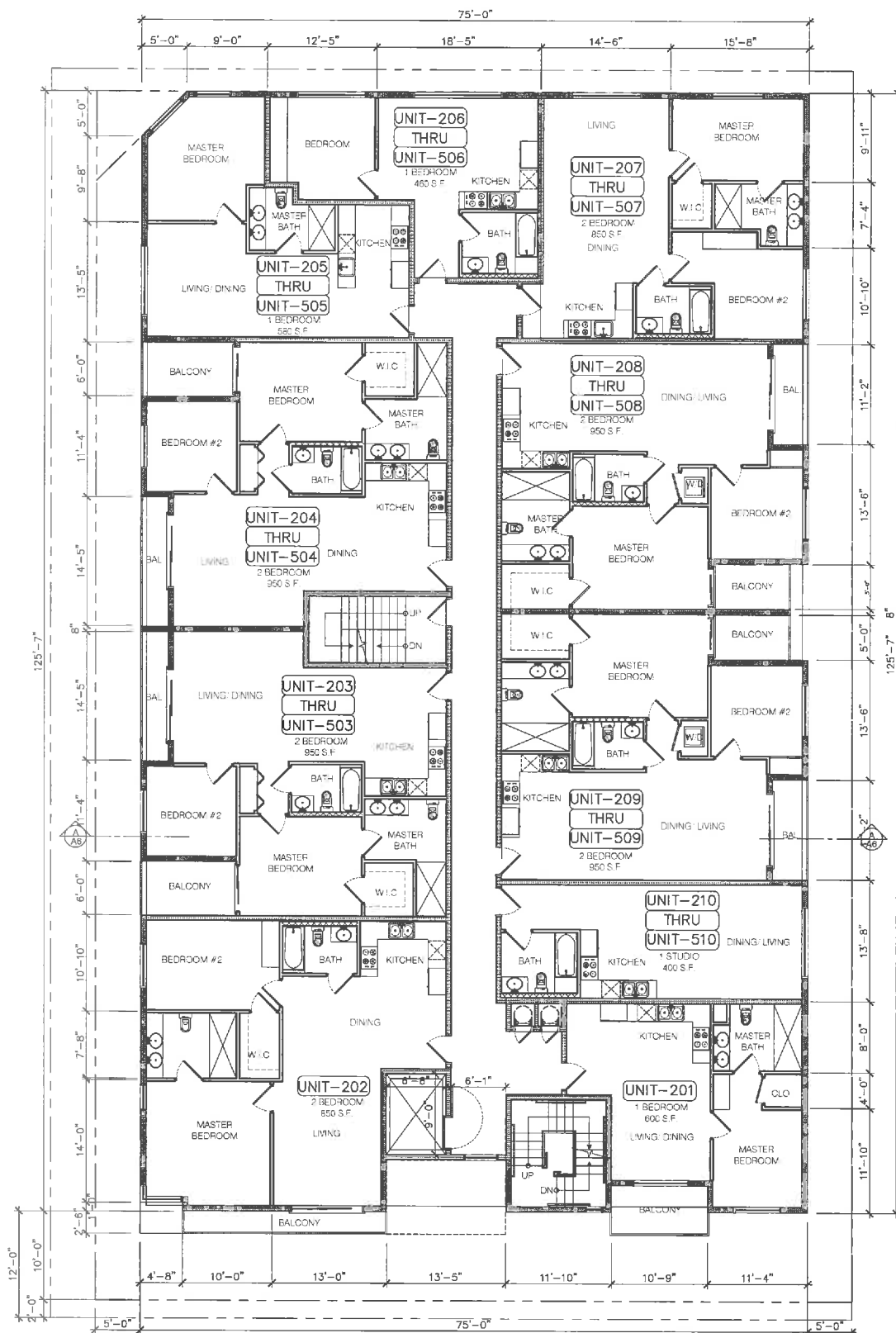
PROJECT

SAWTELLE TERRACES
1650-1652 SAWTELLE BLVD.
LOS ANGELES, CA 90025

DRAWING TITLE

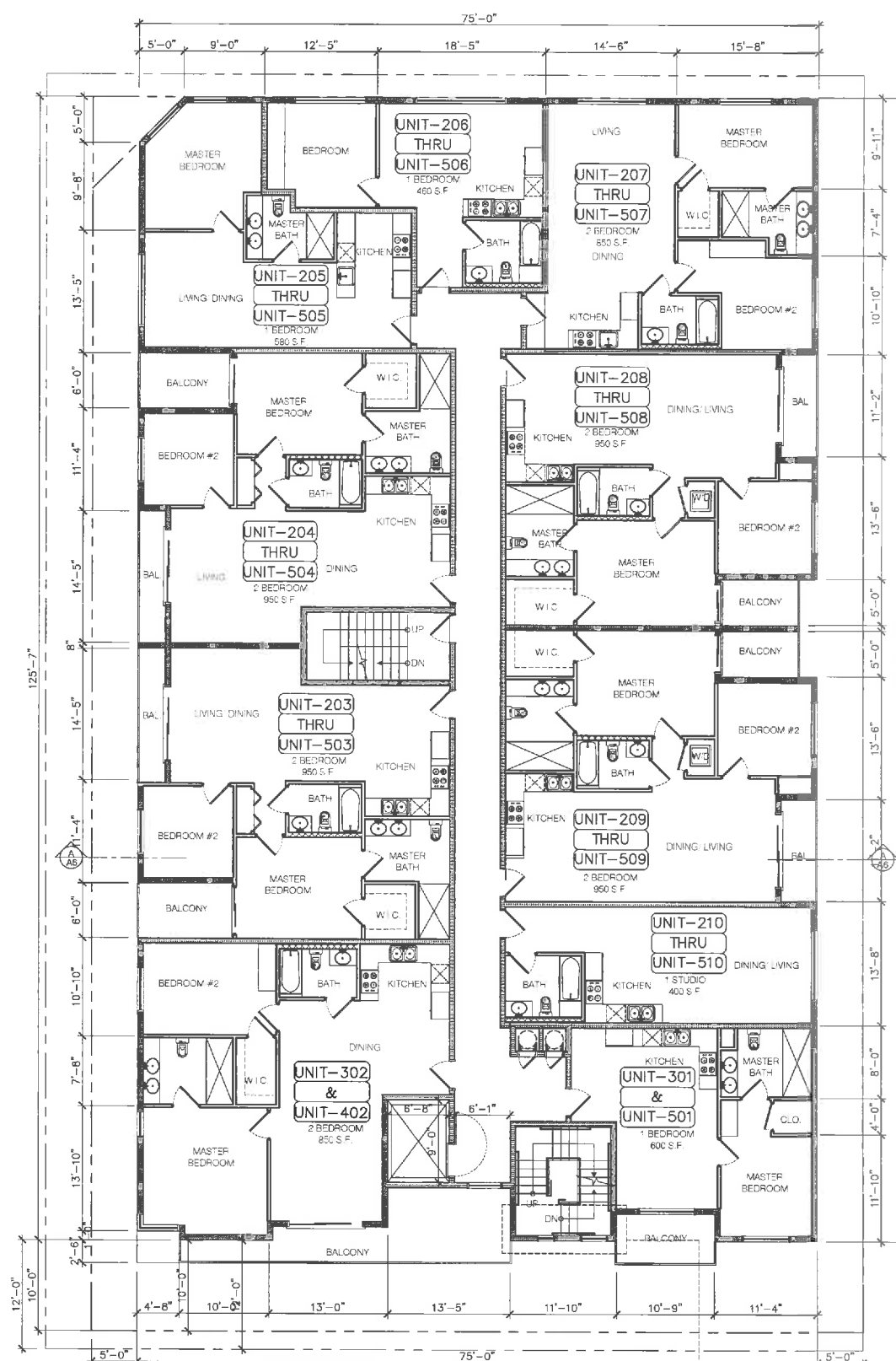
PARKING PLAN LEVEL 1
FIRST FLOOR PLAN

date: 07/16/2014
scale: 1/8" = 1'-0"
drawn: S.G.S.S.
job: 2014-14A
sheet:
A3
of sheets



2ND FLOOR PLAN
SCALE: 1/8" = 1'-0"

9,100 S.F. GROSS
8,100 S.F. NET



3RD-4TH FLOOR PLAN
SCALE: 1/8" = 1'-0"

9,100 S.F. GROSS
8,100 S.F. NET

WALL TYPE LEGENDS:

- INTERIOR 1 HR. WOOD STUD WALL— SEE 2/D1
- INTERIOR 1 HR. WOOD STUD WALL-STC 50 — SEE 3/D1
- INTERIOR 1 HR. METAL STUD WALL — SEE 11a/D1
- INTERIOR 1 HR. WOOD STUD WALL-STC 50 — SEE 3/D1
- EXTERIOR TWO HOUR WOOD STUD WALL — SEE 9/D1
- INTERIOR TWO HOUR WOOD STUD WALL — STC 50 — SEE 7/D1
- INTERIOR TWO HOUR WOOD STUD WALL — SEE 6/D1
- 2x6 STUD PLUMBING WALL
- 2x4 STUD PLUMBING WALL
-

No.	Revision	date

LICENSED ARCHITECT
CHARLES HEFNER
No. C-23963
RENEWAL DATE
STATE OF CALIFORNIA

SAM GHANOUNI
DESIGNER

9049 ALCOFF STREET #205
LOS ANGELES, CA 90035
TEL: 310-450-1976
e-mail: samghanouni@msa.com

PROJECT

REXFORD ELEGANT
APARTMENTS, LLC
10350 SANTA MONICA BLVD, STE 190
LOS ANGELES, CA 90025

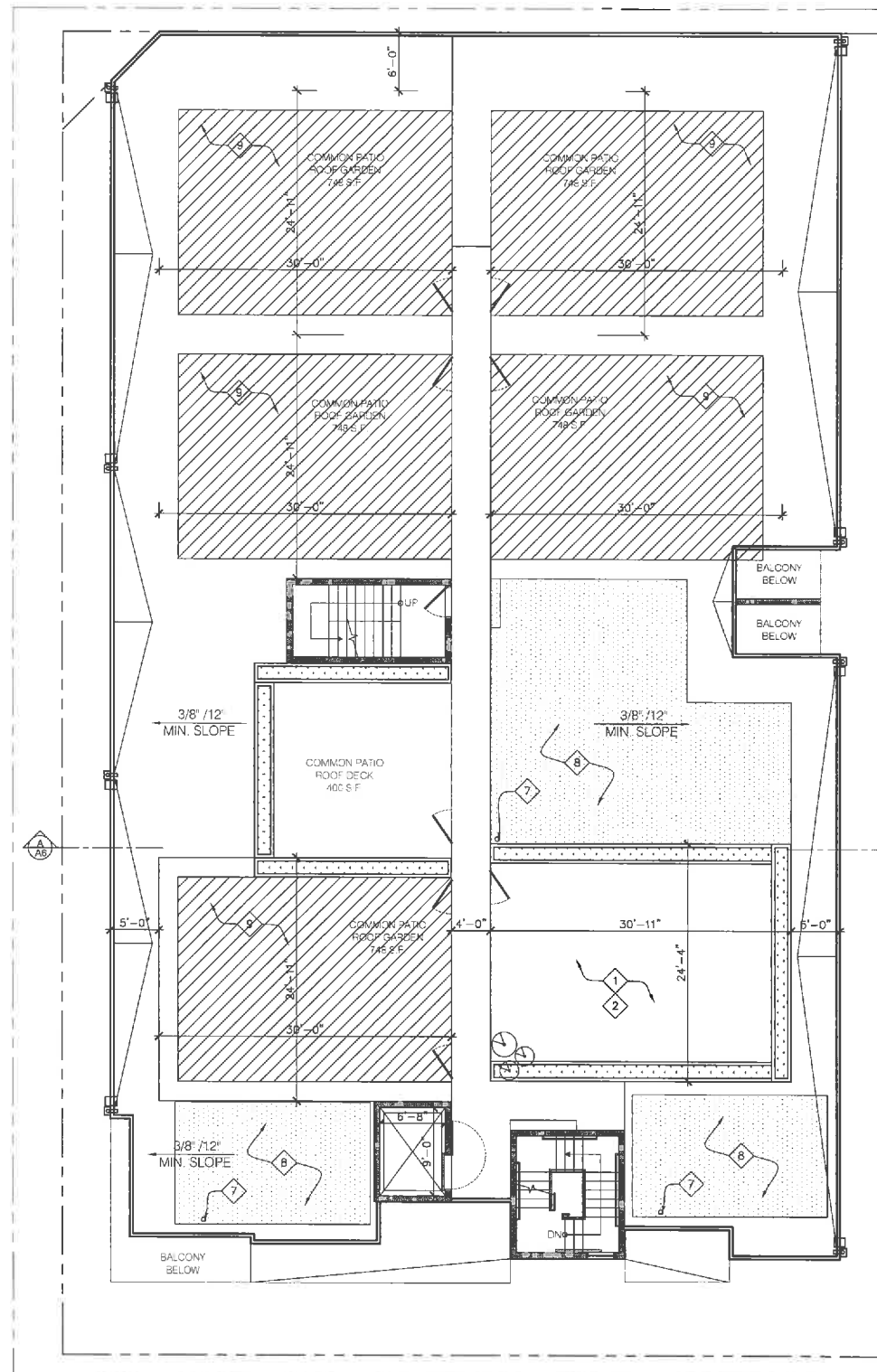
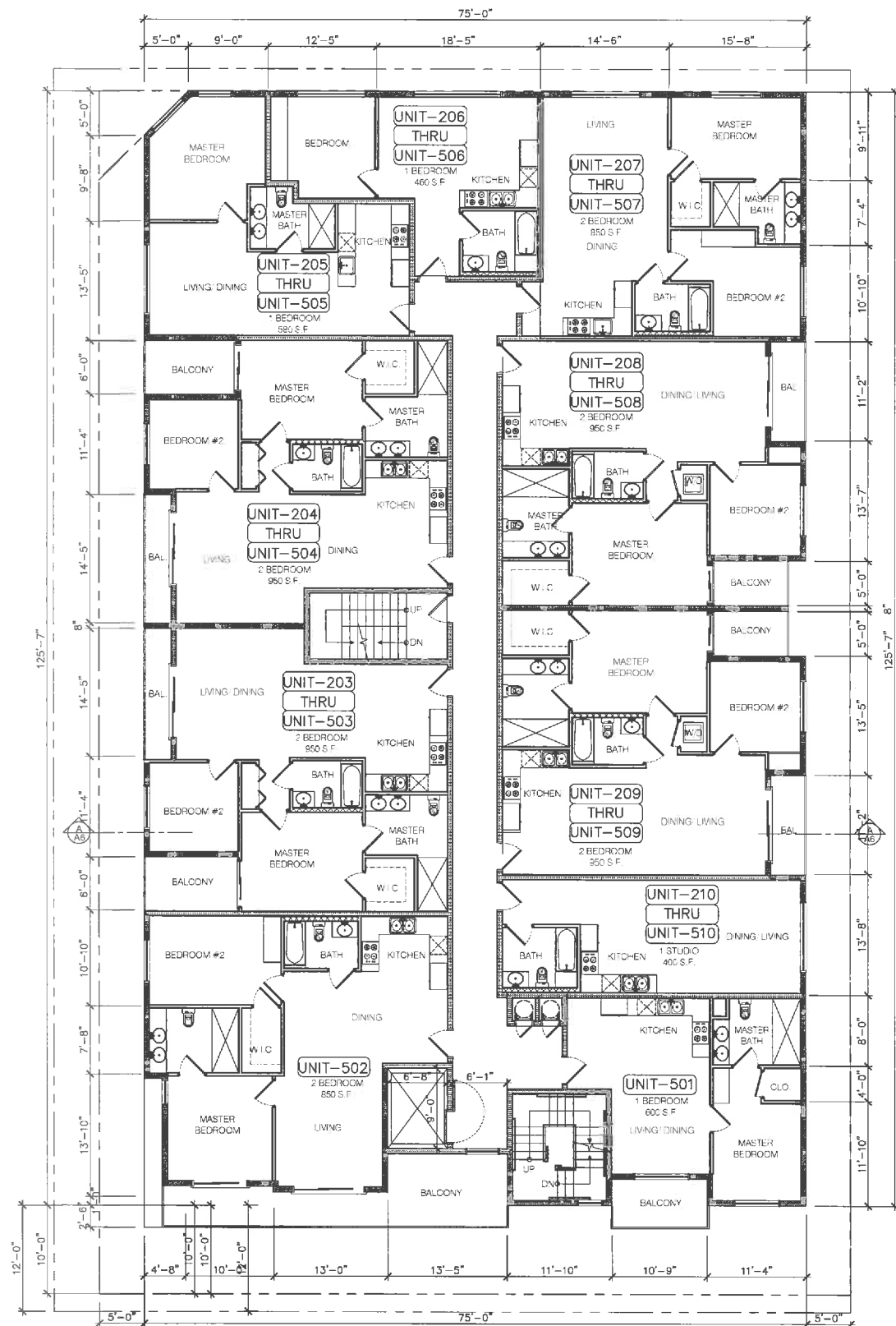
DRAWING TITLE

SAWTELLE TERRACES
1650-1652 SAWTELLE BLVD.
LOS ANGELES, CA 90025

DRAWING TITLE

2ND - 4TH FLOOR PLAN

date: 07/16/2014
scale: 1/8" = 1'-0"
drawn: S.G.S.
job: 2014-14A
sheet:
A4
of sheets



ROOFING MATERIAL SPECIFICATIONS :

- 1 WATERPROOFING MEMBRANE: SHALL BE "AVM SYSTEM 700" AS MANUFACTURED BY 'AVM INDUSTRIES, LLC, TEL. 818-888-0050, AVM SYSTEM CERTIFIED TO MEET ANSI A118.10 SPECS-ICC ESR-2662, LARR# 25430
- 2 UL LISTED, CLASS A, GS ROOFING PRODUCTS, (800)955-0811, SPECS. NO. "GTA-FR-N-B2" SHALL BE MODIFIED BITUMEN MINERAL SURFACE FOOF MATERIAL SYSTEM, ONE PLY OF GLASBASE BASE SHEET & ONE PLY OF "FLINKASTIC GTA-FR", DRIFT GREY COLOR PER ARCHITECT'S APPROVAL OF SAMPLE. THE ROOFING CONTRACTOR SHALL APPLY THE ROOFING ACCORDING TO WRITTEN INSTRUCTION OF THE MANUFACTURER AND SHALL:
 - 1- BLIND NAIL THE "GTA" PLY TO THE SUBSTRATE &
 - 2- RETORCH THE JOINTS OF THE PRODUCT OVERLAP TO MAINTAIN A SMOOTH AND EVEN SURFACE, ESR-2505
- 3 UL LISTED, CLASS A, GS ROOFING PRODUCTS, (800)955-0811, SPECS. NO. "G-N-B3" SHALL BE BUILT-UP ROOFING ON NAILABLE DECK W/ APPROPRIATE GS APPROVED BASE FLASHING AND COUNTERFLASHING, SURFACING SHALL BE OPAQUE, CLEAN, MOISTURE NOT TO EXCEED 2% AND CONFORM TO ASTM D-1863, ESR-2505
- 4 42" HI GAURDRAIL / SCREEN
- 5 42" WIDE, 42" HI GATE
- 6 CRICKET
- 7 1" MIN. FOR FUTURE SOLAR PANEL - VERIFY LOCATION IN FIELD
- 8 CONTIGUOUS UNOBSTRUCTED AREA FOR FUTURE INSTALLATION OF ELECTRICAL SOLAR PANELS
- 9 GREEN ROOF AREA FOR STORM WATER FILTRATION

WALL TYPE LEGENDS:

- INTERIOR 1 HR. WOOD STUD WALL- SEE 2/D1
- INTERIOR 1 HR. WOOD STUD WALL-STC 50 - SEE 3/D1
- INTERIOR 1 HR. METAL STUD WALL - SEE 11a/D1
- INTERIOR 1 HR. WOOD STUD WALL-STC 50 - SEE 3/D1
- EXTERIOR TWO HOUR WOOD STUD WALL - SEE 9/D1
- INTERIOR TWO HOUR WOOD STUD WALL - STC 50 - SEE 7/D1
- INTERIOR TWO HOUR WOOD STUD WALL - SEE 6/D1
- 2x6 STUD PLUMBING WALL
- 2x4 STUD PLUMBING WALL
- 8" THK. CONC. WALL - SEE STRUCT.
- 6" THK. CONC. BLK WALL - SEE STRUCT.

No.	Revision	date

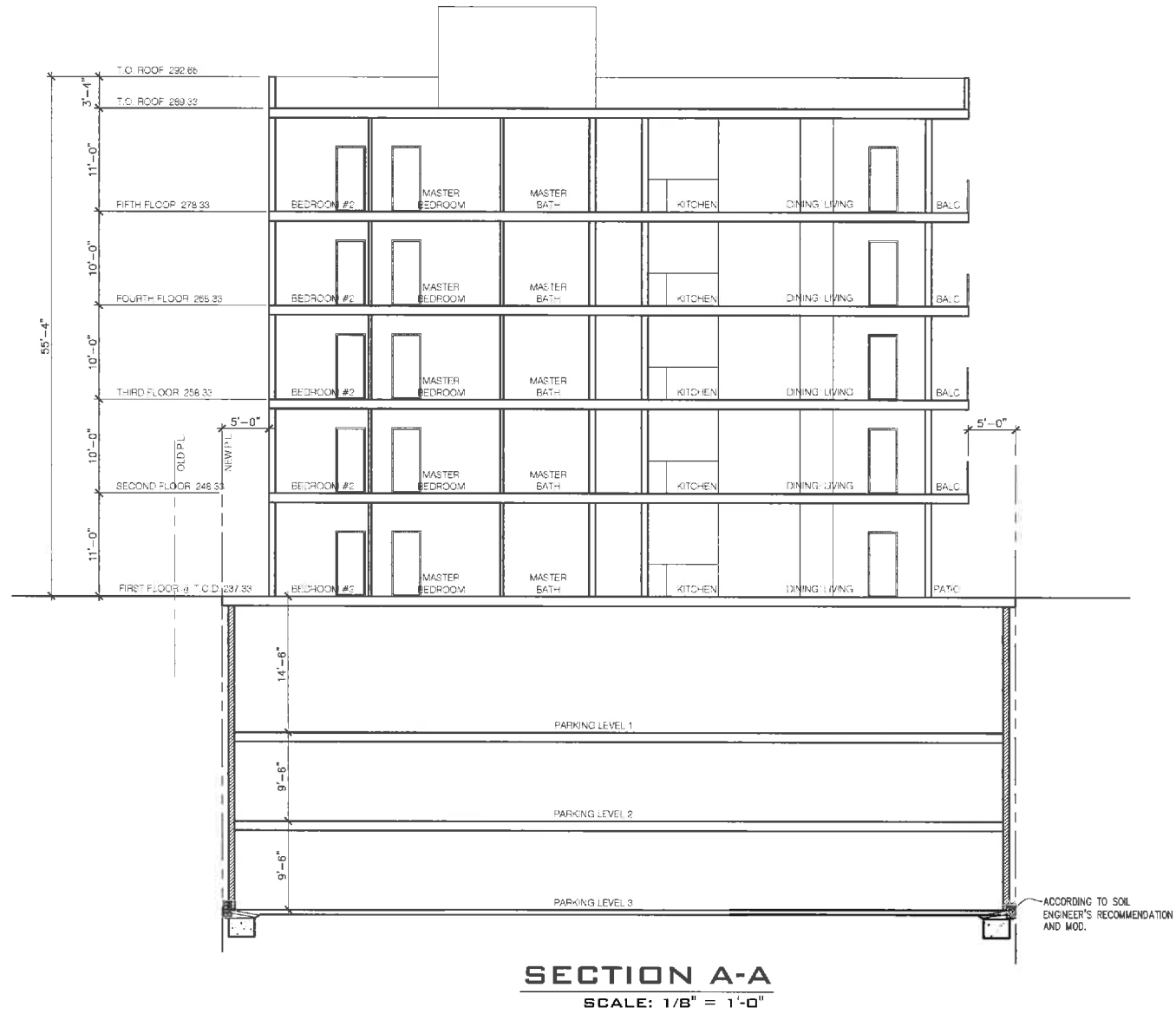


OWNER
REXFORD ELEGANT
APARTMENTS, LLC
10350 SANTA MONICA BLVD, STE 190
LOS ANGELES, CA 90025

PROJECT
SAWTELLE TERRACES
1650-1652 SAWTELLE BLVD.
LOS ANGELES, CA 90025

DRAWING TITLE
5TH FLOOR PLAN
ROOF PLAN

date: 07/16/2014
scale: 1/8" = 1'-0"
drawn: S.G.S.
job: 2014-14A
sheet:
A5
of sheets



No.	Revision	date

REGISTERED ARCHITECT

CHARLES HEFNER

No. C-23963

RENEWAL DATE

STATE OF CALIFORNIA

SAM GHANOUNI

DESIGNER

9049 ALCOTT STREET #203

LOS ANGELES, CA 90033

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OWNER

REXFORD ELEGANT APARTMENTS, LLC

10350 SANTA MONICA BLVD, STE 190

LOS ANGELES, CA 90025

PROJECT

SAWTELLE TERRACES

1650-1652 SAWTELLE BLVD.

LOS ANGELES, CA 90025

DRAWING TITLE

SECTION A-A

date: 07/16/2014

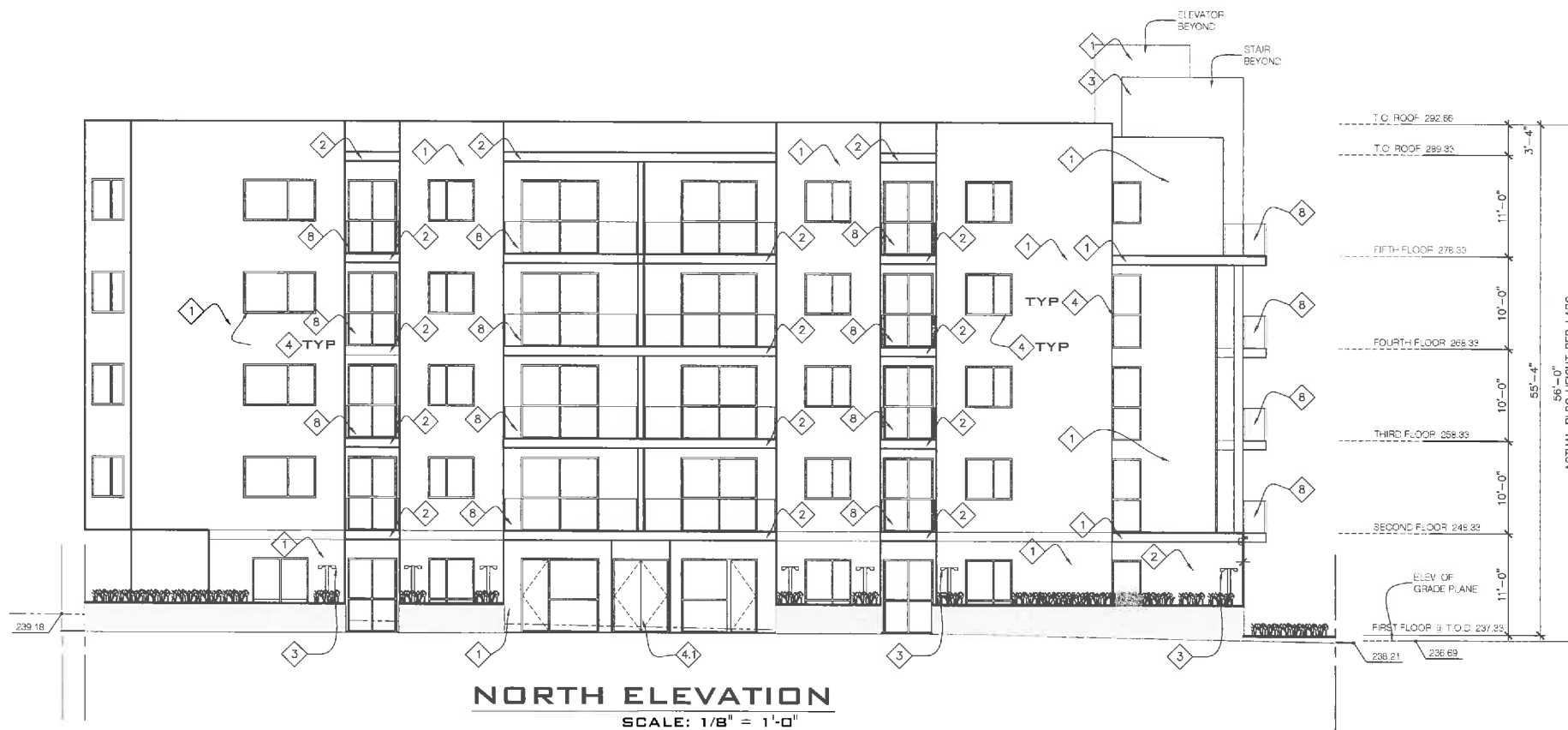
scale: 1/8" = 1'-0"

drawn: S.G.S.S

job: 2014-14A

sheet: A6

of sheets



PROCEDURE FOR ESTABLISHMENT OF ELEVATION POINTS FOR APARTMENT HOUSE CONSTRUCTION

Elevation monuments shall be established by a Registered Land Surveyor or Civil Engineer and a contour map provided for each building site on which an apartment house is to be erected.

A minimum of four monuments shall be shown on the approved building plans as main reference points for the building and shall be established before approval of excavation of footings is given.

The elevation monuments shall be identified and marked showing the grades from which all required building elevations may be verified and shall be located along each property line not more than ten feet from the building or structure to be erected. Additional monuments may be required if the contour of the grade or the complexity of the building so warrants. The monuments shall be of durable construction and accurately hold their position until the Certificate of Occupancy for the building or structure is issued.

The Department may waive these requirements when sufficient data is submitted establishing the grades and elevations for the building site.

Inspection staff will ensure that the approved plans indicate a point from where the final elevation will be measured. If the approved plan does not specify the height measurement location inspection staff shall refer the property representative to the Plan Check Division to include where the measurement is to be taken. A Licensed Survey Report will be provided at the time of excavation, at the first above grade structural floor levels, at the time of the final frame inspection and at the time of issuance of a Temporary Certificate of Occupancy (TCO) or Certificate of Occupancy. The final framing inspection will not be approved until the Licensed Survey Report is provided. All Survey Reports will be signed by the Licensed Surveyor/Civil Engineer of record, the final amendment to our procedure is that prior to the issuance of a Certificate of Occupancy a letter signed by the Surveyor/Civil Engineer of record stating that the building height is in conformance with the approved set of plans shall be provided.

ELEVATION MATERIAL:

- 1 SMOOTH SAND FINISH, CEMENT PLASTER, COLOR OW-107 'BRIDAL BANQUET', BY VISTA PAINT (OFF WHITE)
- 2 SMOOTH SAND FINISH, CEMENT PLASTER, COLOR 8261 'NEW UNIFORM', E VISTA PAINT (DARK GRAY)
- 3 DOUBLE SIDE LANDSCAPE/STREET LIGHT
- 4 ALUM. FRAME DOOR/WINDOW
- 4.1 DOUBLE DOOR GLASS ENTRY
- 5 GALV. PIPE DOWNSPOT & LEADERHEAD
- 6 BRUSHED STAINLESS BUILDING ADDRESS 12"x4" NUMBERS
- 7 STUCCO CONTROL JOINTS
- 8 42" H. TEMPERED GLASS RAILING
- 9 PAINTED METAL RAILING BEYOND
- 10 PAINTED METAL ROLL-UP GARAGE DOOR, COLOR GRAY
- 11 3'-0"x6'-0" EXTERIOR IRON GATE WITH 4'-6" FIXED FENCE
- 12 DARK GRAY ZINC PANELS

NOTE:

1- PROVIDE APPROVED ANTI-GRAFFITI COATING OVER THE NINE-FOOT BOTTOM PORTION OF ALL WALLS, COATING AS MANUF. BY 'GENESIS COATING INC', ULTRASHIELD CLEAR, RESEARCH REPORT NO. RR25042

No.	Revision	date



SAM GHANOUNI
DESIGNER

8048 ALCOTT STREET #208
LOS ANGELES, CA 90033
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e-mail: samghanouni@me.com

OWNER
REXFORD ELEGANT
APARTMENTS, LLC
10350 SANTA MONICA BLVD., STE 190
LOS ANGELES, CA 90025

PROJECT
SAWTELLE TERRACES
1650-1652 SAWTELLE BLVD.
LOS ANGELES, CA 90025

DRAWING TITLE
NORTH & WEST ELEVATION

date: 07/18/2014
scale: 1/8" = 1'-0"
drawn: S.G.S.
job: 2014-14A
sheet:
A7
of sheets



Geijera parviflora / Australian Willow Laurus nobilis / Sweet Bay



Podocarpus henkelii / Henkel's Yellowwood Parkinsonia aculeata / Mexican Palo Verde



Carpenteria c. / Bush Anemone Phormium hyb. 'Dazzler' / New Zealand Flax Phormium hyb. 'Dazzler' / New Zealand Flax



Sedum r. 'Angelina' / Angelina Stonecrop Senecio serpens / Blue Chalks Sticks Parthenocissus tricuspidata / Boston Ivy Carex divulsa / Berkeley Sedge

Carpenteria californica / Bush Anemone Phormium hyb. 'Dazzler' / New Zealand Flax

Geijera parviflora / Australian Willow Sedum r. 'Angelina' / Angelina Stonecrop

Parthenocissus tricuspidata / Boston Ivy Parkinsonia aculeata / Mexican Palo Verde

Senecio serpens / Blue Chalks Sticks

ALLEY

2'-6" DEDICATION

OPEN SPACE REQUIRED: 5,625 S.F.
33 UNITS (2 BEDROOM) x 125 S.F. +
15 UNITS (1 BEDROOMS) x 100 S.F. = 5,625 S.F.
OPEN SPACE PROVIDED: 5,838 S.F.
ROOF DECK 4,888 S.F.
19 BALCONIES 950 S.F.
REQ. OPEN SPACE TO BE LANDSCAPED: 1,222 S.F.
ON ROOF DECK
PROVIDED OPEN SPACE LANDSCAPED: 1,296 S.F.

TREE LEGEND

SYM.	BOTANICAL NAME	COMMON NAME	SIZE	QTY.	REMARKS
⊗	*Laurus nobilis hedge	Sweet Bay	24"box	3	low water
⊙	*Geijera parviflora	Australian Willow	36"box	2	low water
⊙	*Parkinsonia aculeata	Mexican Palo Verde	36"box	1	low water
⊙	Podocarpus henkelii	Henkel's Yellowwood	36"box	1	

SHRUBS AND GROUND COVER LEGEND

SYM.	BOTANICAL NAME	COMMON NAME	SIZE	QTY.	REMARKS
⊙	*Agave 'Blue Flame'		5-gal	2	low water
⊙	*Carex divulsa	Berkeley Sedge	5gal	103	low water
⊙	*Carpenteria californica	Bush Anemone	5gal	10	low water
⊙	Parthenocissus tricuspidata	Boston Ivy	5gal	40	
⊙	Phormium hyb. 'Dazzler'	New Zealand Flax	5-gal	6	
⊙	Sedum r. 'Angelina'	Angelina Stonecrop	1-gal	12"oc	low water
⊙	Senecio serpens	Blue Chalks Sticks	1-gal	18"oc	low water

*Points Claimed

All trees to be planted with commercial root barriers.
2" deep shredded Cedar bark to spread between plants.

NOTE:

All groundcover areas where plants are 4' or greater to have 2 layers of geotextile fabric in 2 different directions geotextile fabric installed 3" below finished grade w/ 3" shredded bark above to eliminate weed growth.

NOTE:

Waterproofing and drains in planters by others.

Landscape Points

Total square footage	12,610.00 sf
Total number of points required for site	15

Detail of points

Continuously planted parkway	Points Claimed	Reference
	55	L-1

TOTAL POINTS

55

Water Management Points

Total square footage of site	12,610.00 sf
Total number of points required for site	200

Detail Of Points

Points 2 per plant 272 plants	Points Claimed	Reference
	544	L-1

TOTAL POINTS

544

PLANTING NOTES

- DRAWING IS DIAGRAMMATIC. CONTRACTOR TO VERIFY ALL LOCATIONS AND CONDITIONS ON SITE. COUNT ALL PLANT MATERIAL BEFORE BIDDING.
- CONTRACTOR TO INSPECT ALL EXISTING CONDITIONS ON SITE AND LOCATE ALL EXISTING UTILITIES BEFORE CONSTRUCTION BEGINS.
- CONTRACTOR TO REPAIR AT HIS OWN EXPENSE ALL PROPERTY DAMAGE WHICH OCCURS DURING PROJECT INSTALLATION.
- NOTE ADDITIONAL REMARKS ON SPECIFIC PLANTS IN PLANT LIST.
- ALL EXISTING PLANT MATERIAL TO BE REMOVED EXCEPT WHERE NOTED ON PLAN.
- CONTRACTOR TO GUARANTEE ALL PLANT MATERIAL FOR 90 DAYS FROM THE DATE OF ACCEPTANCE BY OWNER. PALM TO BE GUARANTEED FOR THE PERIOD OF 1 YEAR.
- FINISH GRADE TO BE 3" BELOW ALL WALKS, CURBS, AND PAVING.
- ALL PLANTED AREAS SHALL RECEIVE THE FOLLOWING AMENDMENTS PER 1,000 SQ. FT. OF SURFACE AREA. ROTO-TILL AMENDMENTS TO A DEPTH OF 6"
 - *150 LBS. GRO-POWER
 - *3 CU YDS NITROGENIZED, MINERALIZED FIR BARK
 - *ADD 8 LBS OF GRO-POWER CONTROLLED RELEASE 12-8-8 PER CU YD OF MIX.

- PLANT HOLE TO BE TWICE AS WIDE AND DEEP AS THE PLANT ROOT BALL. BACKFILL AND COMPACT TO 80 % SOIL OF SITE AND 20 % FIR BARK AS DEFINED IN #8. PROVIDE GRO-POWER PLANT TABLETS AT THE FOLLOWING RATES:

5 GAL	6-9
24" box	14-16

PLACE RECOMMENDED TABLETS BETWEEN THE BOTTOM AND THE TOP OF THE ROOT BALL BUT NO HIGHER THAN 1/3 OF THE WAY UP TO THE TOP OF THE ROOT BALL. SPACE TABLETS EQUALLY AROUND THE PERIMETER OF THE ROOT BALL APPROXIMATELY 2" FROM THE ROOT TIPS. PALM TREES ARE NOT TO RECEIVE TABLETS.

- ALL PROPOSED SHRUBS AND GROUND COVER AREAS ARE TO BE TREATED WITH A PRE-EMERGENT WEED KILLER (EPTAM / RONSTAR). APPLY PER MANUFACTURER'S SPECIFICATIONS: A) IMMEDIATELY AFTER PLANTING, B) AT THE BEGINNING OF THE MAINTENANCE PERIOD, AND C) AT THE END OF THE MAINTENANCE PERIOD.
- CONTRACTOR TO INSTALL AND MAINTAIN LANDSCAPE PLANTING IN ACCORDANCE WITH THE GOVERNING AGENCY'S GUIDELINES AND SPECIFICATIONS UNLESS NOTED OTHERWISE IN THESE NOTES OR ON THE PLANS.
- SOIL SAMPLES TAKEN FROM VARIOUS LOCATIONS IN THE PLANTING AREAS WILL BE SENT TO A SOIL LAB FOR PROFESSIONAL ANALYSIS AND RECOMMENDATIONS FOR SOIL IMPROVEMENT. CONTRACTOR TO FOLLOW SOIL TESTING RECOMMENDATIONS.

REVISIONS	DATE
1.	2.09.15
2.	10.22.15
3.	11.18.15
4.	
5.	
6.	
7.	
8.	
9.	

Yael
ASLA
Yael Lir Landscape Architects
1010 Sycamore Ave. Suite 313
South Pasadena, CA 91030
Tel 323.258.5222
Fax 323.258.5333
yael@yaelir.com

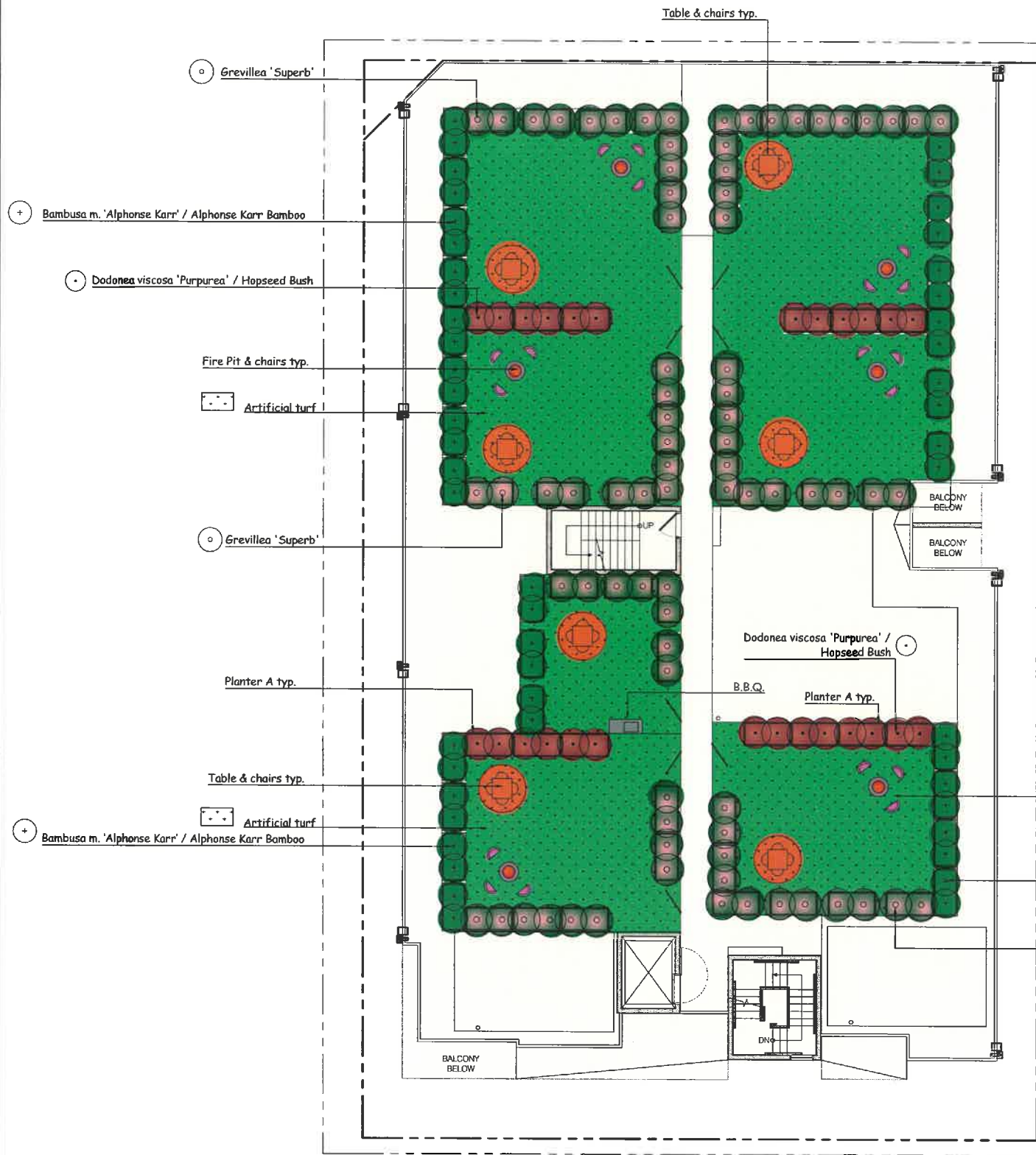
SAWTELLE TERRACES
1650-52 SAWTELLE BLVD.
LOS ANGELES, CA 90025

FIRST FLOOR PLANTING PLAN



DATE: JULY 31, 2014
SCALE: 1/8" = 1'-0"
JOB NUMBER: 162314
DRAWN BY:

L-1



TREE LEGEND

SYM.	BOTANICAL NAME	COMMON NAME	SIZE	QTY.	REMARKS
○	* Dodonea viscosa 'Purpurea'	Hopseed Bush	15-gal	26	

SHRUBS AND GROUND COVER LEGEND

SYM.	BOTANICAL NAME	COMMON NAME	SIZE	QTY.	REMARKS
○	* Grevillea 'Superb'	Alphonse Karr Bamboo	5-gal	74	
○	* Bambusa m. 'Alphonse Karr'	Alphonse Karr Bamboo	5-gal	50	w/ root barrier

* Points Claimed

2" deep shredded Cedar bark to spread between plants.

Artificial turf

NOTE:
Waterproofing and drains in planters by others.

Landscape Form items		
Item	Model	Color
Table	Cheap Chic square top	Flambe Orange
Chairs	Catena	Flambe Orange
Trash	Parc Vue	Stainless Steel

tel: 800.521.2546

Architectural Pottery planters		
Item	Model	Color
Planter	Geo F66RECT2x36x24	Beechwood
Fire Pit	Geo Round Fire Pit	Beechwood

tel: 714.895.3359



Bambusa m. 'Alphonse Karr' /
Alphonse Karr Bamboo



Dodonea v. 'Purpurea' /
Hopseed Bush



Grevillea 'Superb'



Chair



Table



Trash



Planter



Fire Pit

REVISIONS	DATE
1.	2.09.15
2.	10.22.15
3.	11.12.15
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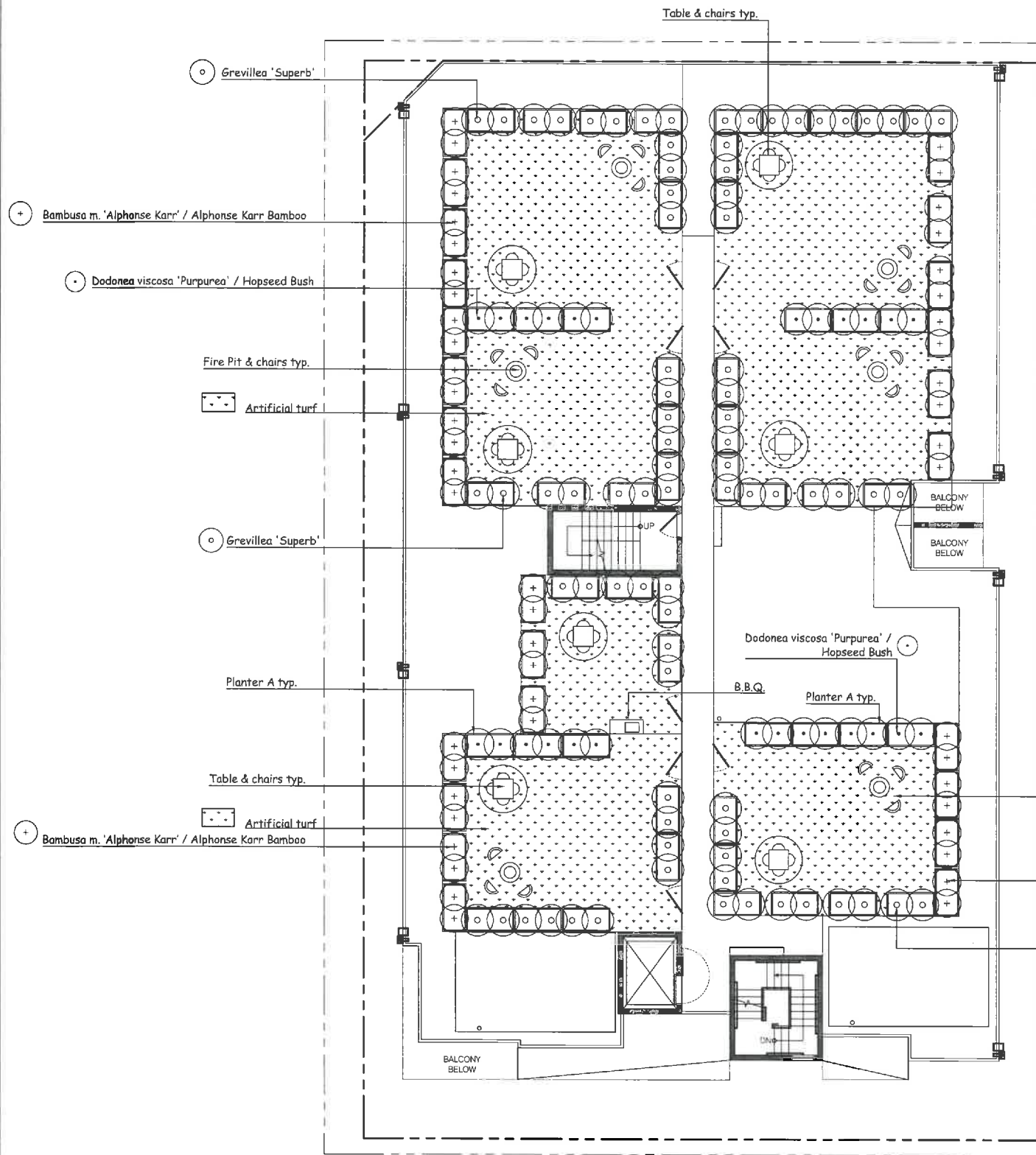
Yael Lir Landscape Architects
1010 Sycamore Ave. Suite 313
South Pasadena, CA 91030
Tel 323.258.5222
Fax 323.258.5333
yael@yaellir.com

SAWTELLE TERRACES
1650-52 SAWTELLE BLVD.
LOS ANGELES, CA 90025

ROOF PLANTING PLAN



DATE: JULY 31, 2014
SCALE: 1/8"=1'-0"
JOB NUMBER: 162314
DRAWN BY:



TREE LEGEND

SYM.	BOTANICAL NAME	COMMON NAME	SIZE	QTY.	REMARKS
⊙	* Dodonea viscosa 'Purpurea'	Hopseed Bush	15-gal	26	

SHRUBS AND GROUND COVER LEGEND

SYM.	BOTANICAL NAME	COMMON NAME	SIZE	QTY.	REMARKS
⊙	* Grevillea 'Superb'	Alphonse Karr Bamboo	5-gal	74	
⊙	* Bambusa m. 'Alphonse Karr'	Alphonse Karr Bamboo	5-gal	50	w/ root barrier

* Points Claimed

2" deep shredded Cedar bark to spread between plants.

Artificial turf

NOTE:
Waterproofing and drains in planters by others.

Landscape Form items		
Item	Model	Color
Table	Cheap Chic square top	Flambe Orange
Chairs	Catena	Flambe Orange
Trash	Parc Vue	Stainless Steel

tel: 800.521.2546

Architectural Pottery planters		
Item	Model	Color
Planter	Geo F6GREG72x36x24	Beechwood
Fire Pit	Geo Round Fire Pit	Beechwood

tel: 714.895.3359



Bambusa m. 'Alphonse Karr' /
Alphonse Karr Bamboo



Dodonea v. 'Purpurea' /
Hopseed Bush



Grevillea 'Superb'



Chair



Table



Trash



Planter



Fire Pit

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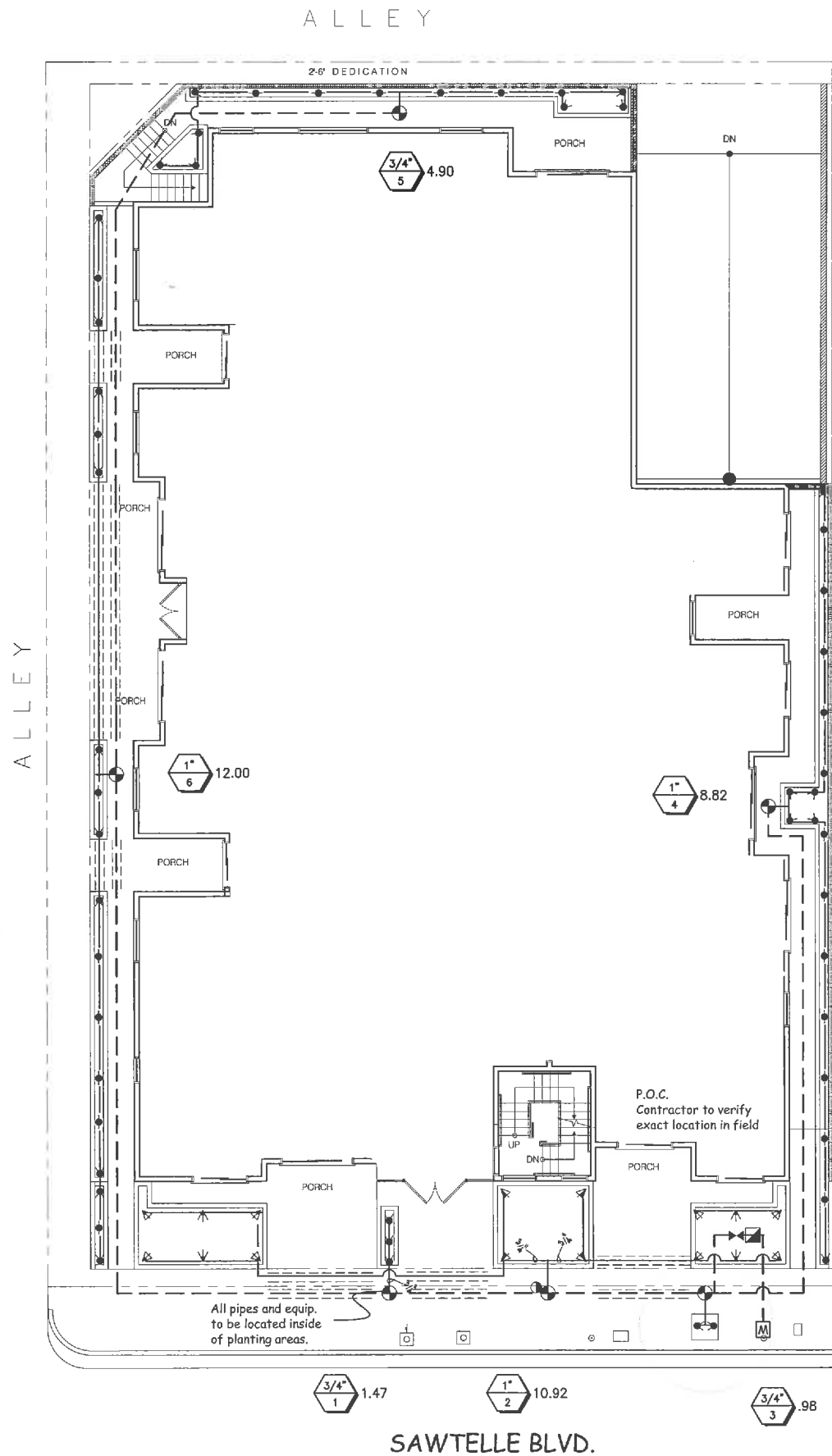
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LOS ANGELES, CA 90025

ROOF
PLANTING PLAN



DATE: JULY 31, 2014
SCALE: 1/8"=1'-0"
JOB NUMBER: 162314
DRAWN BY:



IRRIGATION LEGEND				
DESCRIPTION	SYM.	P.S.I.	RAD.	G.P.M.
'RAINBIRD' POP-UP 1804-PRS -8 Q, H	↓	30	8'	.26 .52
'RAINBIRD' POP-UP 1806 PRS -8 Q, H	↓	30	8'	.26 .52
'RAINBIRD' POP-UP 1806 PRS -12 Q, H	↓	30	12'	.65 1.30
'TORO' SB-90 BUBBLER	•	10	7'	.49
'TORO' SB-2-180 BUBBLER	•	10	7'	.49
'RAINBIRD' CONTROL VALVE 68 SERIES	⊗	W/WYE STRAINER (BFP TO BE PAINTED DARK GREEN)		
'NIBCO' GATE VALVE T-113	⊗			
'BROOKS' VALVE BOX SERIES 1419	⊗			
'RAINBIRD' QUICK COUPLER 44 LRC 1"	⊗			
'WILKINS' BACKFLOW PREVENTER 375	⊗			
'HUNTER' I-CORE	⊗	LOCATION BY OWNER LOCATED ON EAVE OF ROOF TWICE LINE SIZE (MIN.) 1" SEE PLAN FOR SIZE VERIFY LOCATION ON SITE		
'HUNTER' SOLAR SYNC ET	⊗			
SLEEVING SCH. 80 P.V.C.	---			
PRESSURE LINE CLASS 315 PVC	---			
NON-PRESSURE LINE SCH. 40 P.V.C.	---			
POINT OF CONNECTION	P.O.C.			

SIZE
NO. GPM

NOTE:
ALL HEADS TO BE 4" POP-UP FOR TURF AREAS
AND 6" POP-UP FOR SHRUB AND GROUND COVER AREAS.

MAXIMUM APPLIED WATER ALLOWANCE (MAWA)

(50.1) (.62) [(7 x 1,181)] = 25,678 GALLONS

ESTIMATED TOTAL WATER USE (ETWU)

HYDROZONE	PLANT WATER USE TYPE(S)	PLANT FACTOR (PF)	HYDROZONE AREA (HA) square feet	PF x HA (square feet)
1	LOW	0.2	6	1.2
2	LOW	0.2	390	78
3	LOW	0.2	10	2
4	LOW	0.2	105	21
5	LOW	0.2	80	16
6	LOW	0.2	153	30.6
7	LOW	0.2	78	15.6
8	LOW	0.2	40	8
9	LOW	0.2	78	15.6
10	LOW	0.2	50	10
11	LOW	0.2	50	10
12	LOW	0.2	73	14.6
13	LOW	0.2	68	13.6
SUM				220.2

(220.2 / .71) (.62) (50.1) = 9,633 GALLONS

IRRIGATION NOTES

- THIS DESIGN IS DIAGRAMMATIC. ALL PIPING, VALVES, ETC. SHOWN WITHIN PAVED AREAS ARE FOR DESIGN CLARIFICATION ONLY AND SHALL BE INSTALLED IN PLANTING AREAS WHEREVER POSSIBLE.
- SET ALL VALVES AND QUICK COUPLERS NEXT TO WALKS OR PAVED SURFACES.
- ALL SPRINKLER HEADS ARE TO HAVE TRIPLE SWING JOINTS (EXCEPT WHERE NOTED ON PLANS).
- PIPE SIZES SHALL CONFORM TO THOSE SHOWN ON THE DRAWINGS. NO SUBSTITUTIONS OF SMALLER PIPE SIZES SHALL BE PERMITTED, BUT SUBSTITUTIONS OF LARGER SIZES MAY BE APPROVED. ALL DAMAGED AND REJECTED PIPE SHALL BE REMOVED FROM THE SITE AT THE TIME OF THE SAID REJECTION.
- FINAL LOCATION OF THE AUTOMATIC CONTROLLER SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT AND OWNER.
- 120VAC ELECTRICAL POWER SOURCE AT CONTROLLER LOCATION SHALL BE PROVIDED BY OTHERS.
- BEFORE COMMENCING ANY EXCAVATION, THE CONTRACTOR SHALL OBTAIN AN UNDERGROUND SERVICE ALERT I.D. NUMBER BY CALLING 1-800-422-4133. TWO (2) WORKING DAYS SHALL BE ALLOWED AFTER THE I.D. NUMBER IS OBTAINED AND BEFORE THE EXCAVATION WORK IS STARTED SO THAT UTILITY OWNERS CAN BE NOTIFIED.
- ALL SPRINKLER HEADS SHALL BE SET PERPENDICULAR TO FINISH GRADE UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR SHALL FLUSH AND ADJUST ALL SPRINKLER HEADS AND VALVES FOR OPTIMUM COVERAGE WITH MINIMAL OVER SPRAY ONTO WALKS, STREETS, ETC.
- IT IS THE RESPONSIBILITY OF THE IRRIGATION CONTRACTOR TO FAMILIARIZE HIMSELF WITH THE GRADE DIFFERENCES, LOCATION OF WALLS, AND UTILITIES. THE IRRIGATION CONTRACTOR SHALL REPAIR OR REPLACE ALL ITEMS DAMAGED BY HIS WORK. HE SHALL COORDINATE HIS WORK WITH OTHER CONTRACTORS FOR THE LOCATION AND INSTALLATION OF PIPE SLEEVES AND LATERALS UNDER ROADWAYS AND PAVING, ETC.
- THE SPRINKLER SYSTEM DESIGN IS BASED ON A MINIMUM OPERATING PRESSURE OF 80 P.S.I. AND A MAXIMUM FLOW DEMAND OF 25 G.P.M. THE CONTRACTOR SHALL VERIFY WATER PRESSURES PRIOR TO CONSTRUCTION. REPORT ANY DIFFERENCE BETWEEN WATER PRESSURE INDICATED ON THE DRAWINGS AND THE ACTUAL PRESSURE READING AT THE IRRIGATION POINT OF CONNECTION TO THE ARCHITECT.
- DO NOT WILLFULLY INSTALL THE SPRINKLER SYSTEM AS SHOWN ON THE DRAWINGS WHEN IT IS OBVIOUS IN THE FIELD THAT THERE ARE UNKNOWN OBSTRUCTIONS OR GRADE DIFFERENCES IN THE AREA. DIMENSIONS EXIST THAT MIGHT NOT HAVE BEEN CONSIDERED IN THE ENGINEERING. SUCH OBSTRUCTIONS OR DIFFERENCES SHOULD BE BROUGHT TO THE ATTENTION OF THE ARCHITECT. IN THE EVENT THAT THIS NOTIFICATION IS NOT GIVEN, THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY FOR ANY NECESSARY REVISIONS.
- ALL SPRINKLER EQUIPMENT NOT OTHERWISE DETAILED OR SPECIFIED SHALL BE INSTALLED AS PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS.
- THE INTENT OF THE CONTRACTOR IS TO PROVIDE 100% COVERAGE TO ALL PLANTING AREAS. AS PART OF THE SCOPE OF WORK, PROVIDE ANY ADDITIONAL HEADS, SPECIAL NOZZLES, OR PATTERNS TO ACHIEVE PROPER COVERAGE WITH A MINIMUM OF OVER SPRAY AT NO ADDITIONAL COST TO THE OWNER.
- INSTALLATION FOR THE CONTROL WIRES SHALL FOLLOW MAINLINE ROUTING.
- PROVIDE SLEEVES AS SHOWN ON DRAWING OR AS NEEDED. USE SIZE DIAMETER MIN. SCH. 80 P.V.C. MIN. DEPTH TO TOP OF LINE.
- LOCATE VALVE CHART IN CONTROLLER - REDUCE AND ENCASE IN PLASTIC (AS BUILT).
- GUARANTEE: THE INSTALLED SPRINKLER SYSTEM SHALL BE GUARANTEED FOR A PERIOD OF ONE (1) YEAR FROM THE DATE OF ACCEPTANCE OF THE WORK. SHOULD ANY TROUBLE DEVELOP WITHIN THE TIME SPECIFIED DUE TO INFERIOR OR FAULTY MATERIAL OR WORKMANSHIP, THE TROUBLE SHALL BE CORRECTED BY THE CONTRACTOR WITHOUT EXPENSE TO THE OWNER.
- REFER TO GENERAL NOTES FOR ADDITIONAL INFORMATION REGARDING THIS SECTION OF WORK.

REVISIONS	DATE
1.	2.09.15
2.	10.22.15
3.	11.18.15
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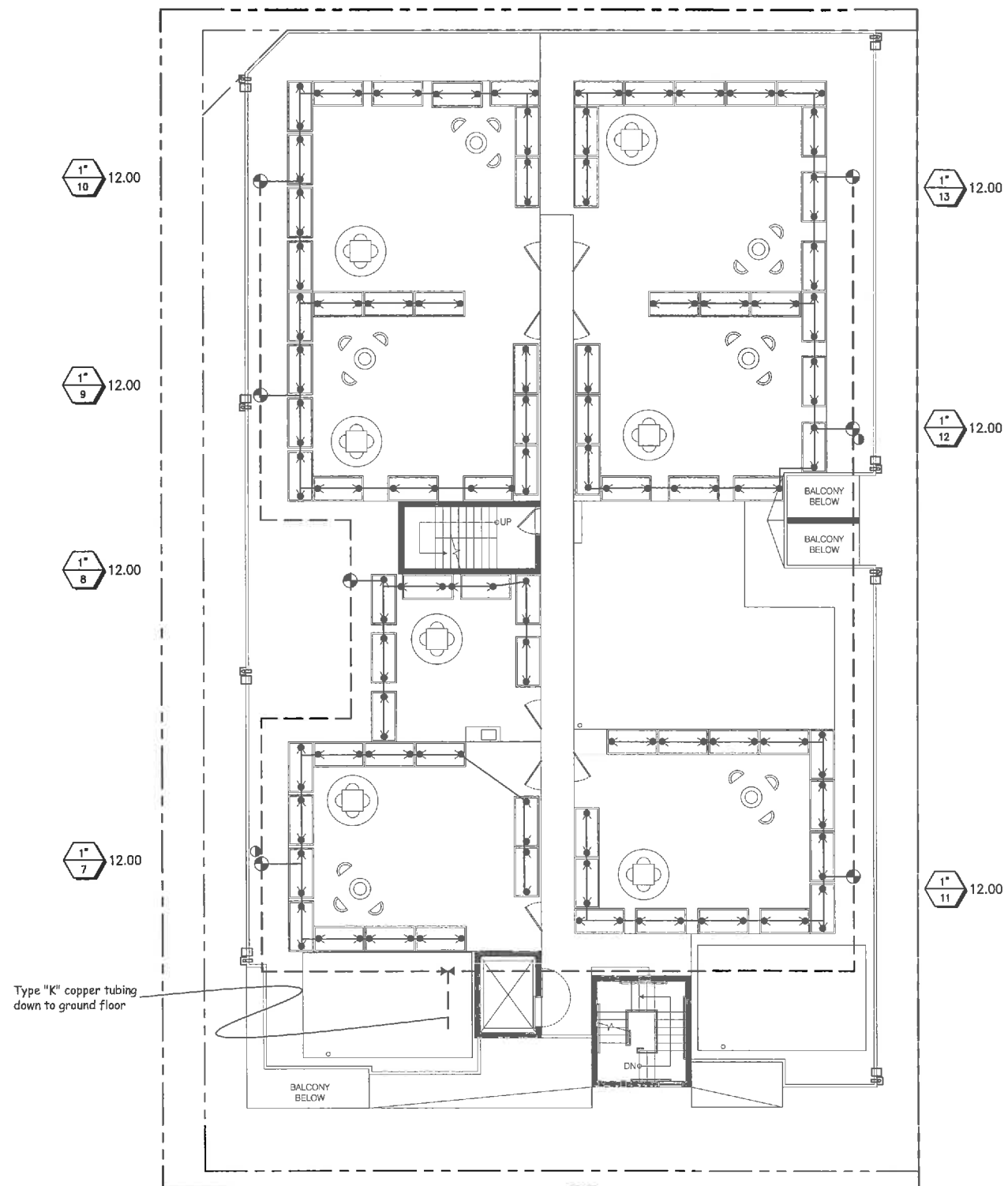
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1650-52 SAWTELLE BLVD.
LOS ANGELES, CA 90025

IRRIGATION PLAN



DATE: JULY 31, 2014
SCALE: 1/8" = 1'-0"
JOB NUMBER: 162314
DRAWN BY:



IRRIGATION LEGEND				
DESCRIPTION	SYM.	P.S.I.	RAD.	G.P.M.
'RAINBIRD' POP-UP 1804-PRS -8 Q, H	↓	30	8'	.26, .52
'RAINBIRD' POP-UP 1806 PRS -8 Q, H	↓	30	8'	.26, .52
'RAINBIRD' POP-UP 1806 PRS -12 Q, H	↓	30	12'	.65, 1.30
'TORO' SB-90 BUBBLER	•	10	7'	.49
'TORO' SB-2-180 BUBBLER	•	10	7'	.49
'RAINBIRD' CONTROL VALVE GB SERIES	⊗			
'NIBCO' GATE VALVE T-113	⊗			
'BROOKS' VALVE BOX SERIES 1419	⊗			
'RAINBIRD' QUICK COUPLER 44 LRC 1"	⊗			
'WILKINS' BACKFLOW PREVENTER 375	⊗			
'HUNTER' I-CORE	⊗			
'HUNTER' SOLAR SYNC ET	⊗			
SLEEVING SCH. 80 P.V.C.	----			
PRESSURE LINE CLASS 315 PVC	----			
NON-PRESSURE LINE SCH. 40 P.V.C.	----			
POINT OF CONNECTION	P.O.C.			
		W/WYE STRAINER (BFP TO BE PAINTED DARK GREEN)		
		LOCATION BY OWNER		
		LOCATED ON EAVE OF ROOF		
		TWICE LINE SIZE (MIN.)		
		1"		
		SEE PLAN FOR SIZE		
		VERIFY LOCATION ON SITE		

SIZE
NO. GPM

NOTE:
ALL HEADS TO BE 4" POP-UP FOR TURF AREAS
AND 6" POP-UP FOR SHRUB AND GROUND COVER AREAS.

REVISIONS	DATE
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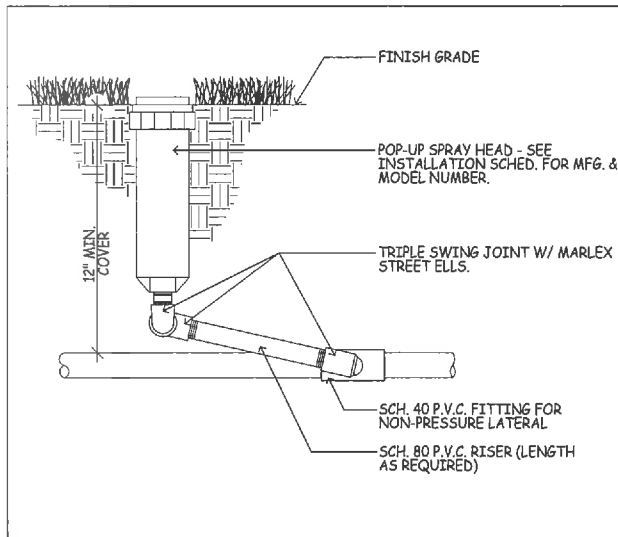
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ROOF
IRRIGATION PLAN

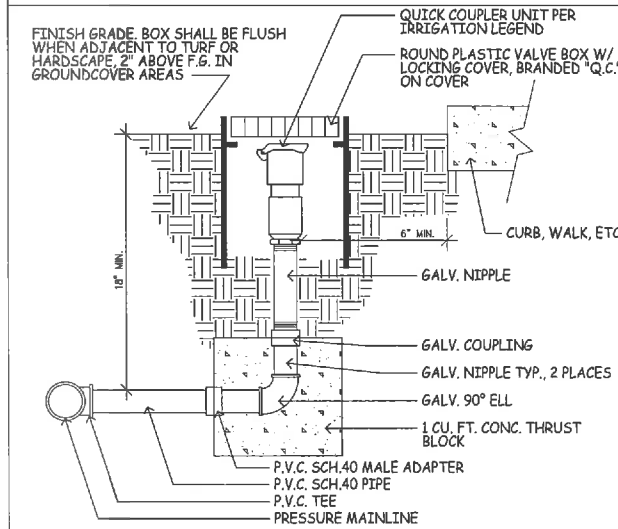


DATE: JULY 31, 2014
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JOB NUMBER: 162314
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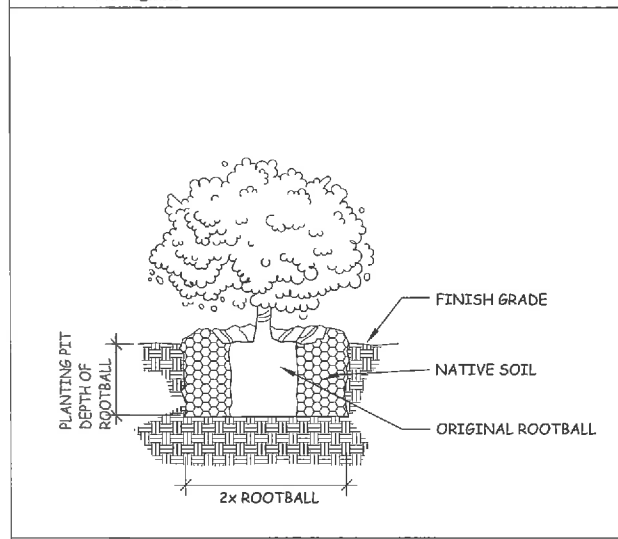
POP-UP DETAIL

SCALE: N.T.S.
FILE: D:\RR001



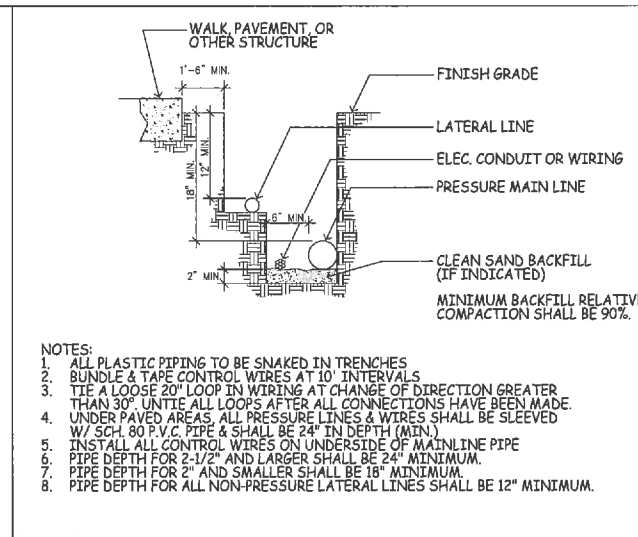
QUICK COUPLER

SCALE: N.T.S.
FILE: D:\RR005



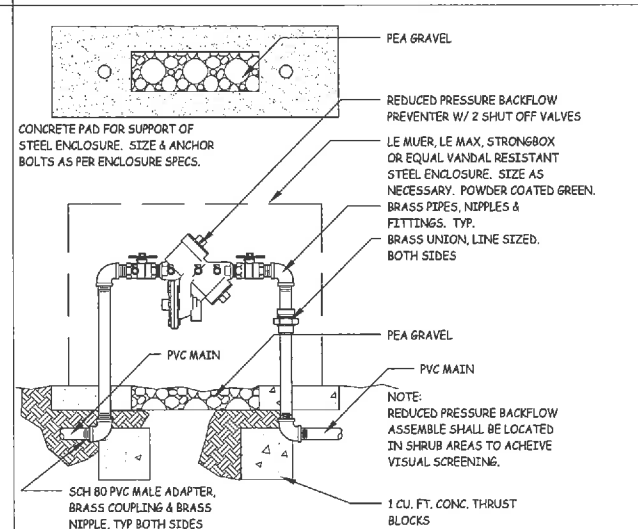
SHRUB PLANTING

SCALE: N.T.S.
FILE: D:\PLA001



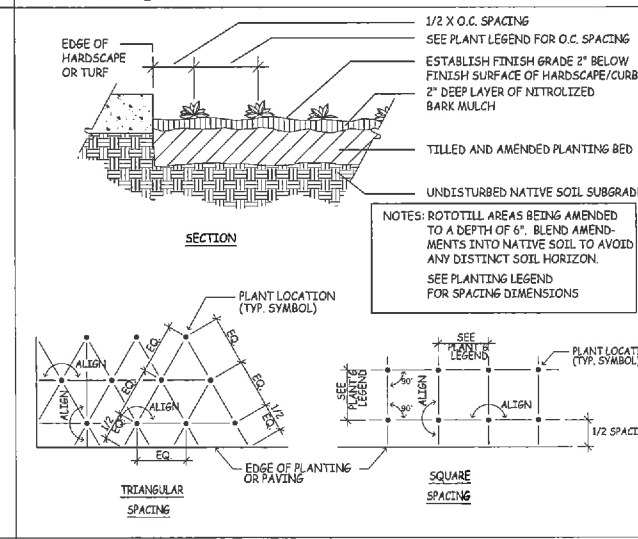
TRENCHING DETAIL

SCALE: N.T.S.
FILE: D:\RR002



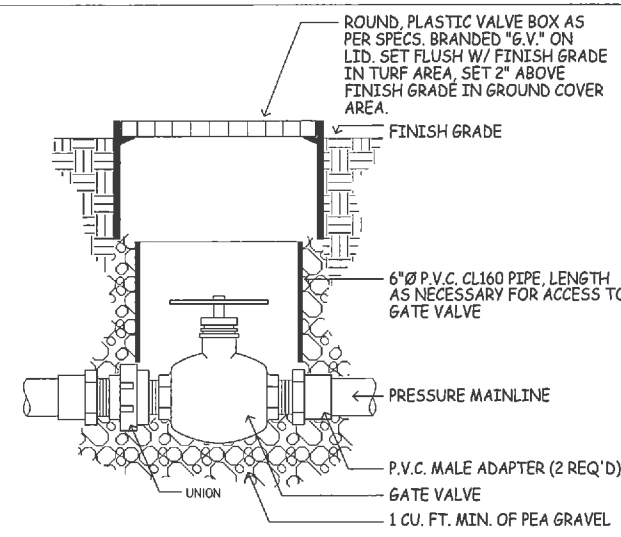
BACKFLOW PREVENTER

SCALE: N.T.S.
FILE: D:\RR006



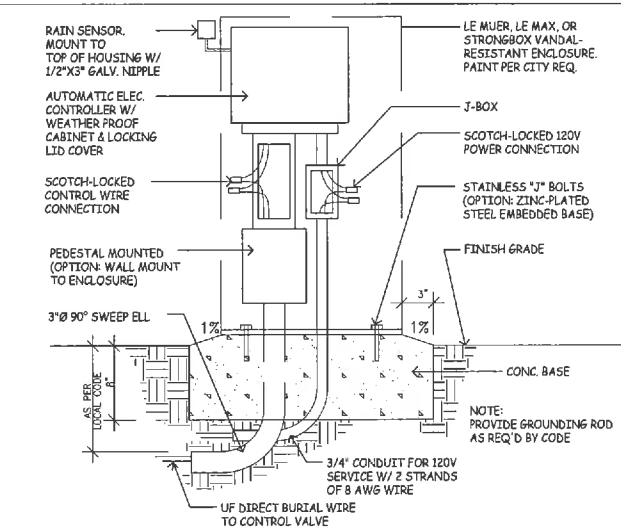
GROUNDCOVER PLANTING

SCALE: N.T.S.
FILE: D:\PLA002



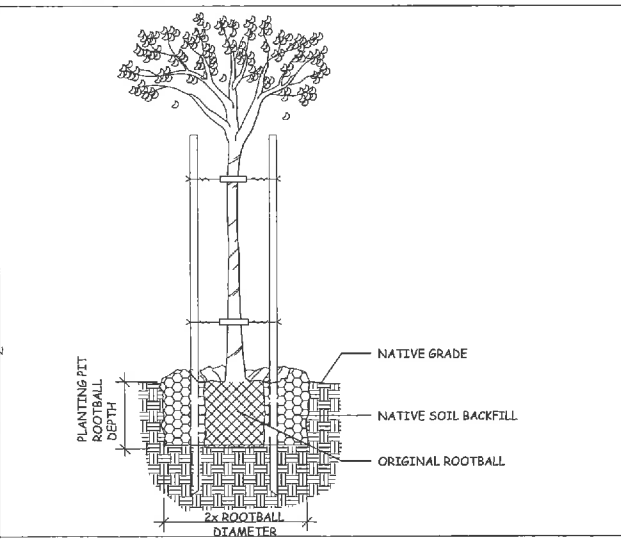
GATE VALVE

SCALE: N.T.S.
FILE: D:\RR003



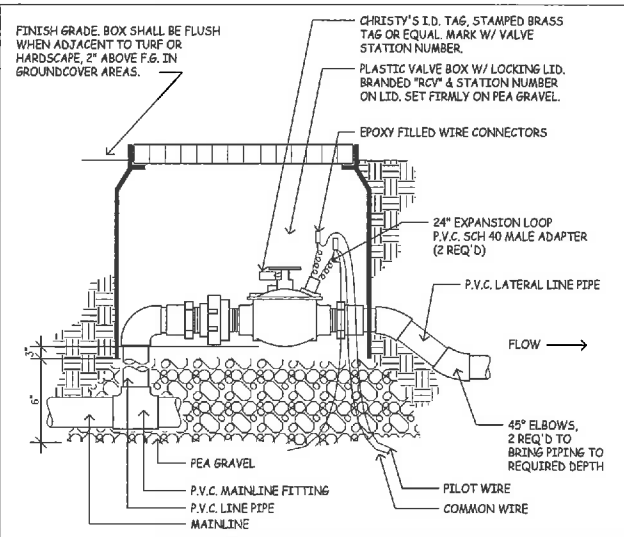
CONTROLLER

SCALE: N.T.S.
FILE: D:\RR007



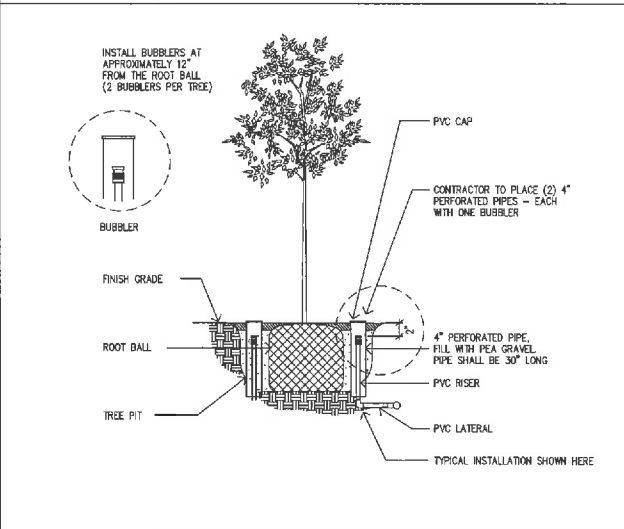
TREE STAKING

SCALE: N.T.S.
FILE: D:\PLA003



CONTROL VALVE

SCALE: N.T.S.
FILE: D:\RR004



TREE BUBBLER

SCALE: N.T.S.
FILE: D:\BUBBLER

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DETAIL SHEET



DATE: JULY 31, 2014
SCALE: 1/8"=1'-0"
JOB NUMBER: 162314
DRAWN BY:

EXHIBIT CEnvironmental Clearance
ENV-2014-2848-MND

CITY OF LOS ANGELES
OFFICE OF THE CITY CLERK
ROOM 395, CITY HALL
LOS ANGELES, CALIFORNIA 90012
CALIFORNIA ENVIRONMENTAL QUALITY ACT
PROPOSED MITIGATED NEGATIVE DECLARATION

LEAD CITY AGENCY City of Los Angeles	COUNCIL DISTRICT CD 11 - MIKE BONIN
PROJECT TITLE ENV-2014-2848-MND	CASE NO. CPC-2014-2847-ZC-DB
PROJECT LOCATION 1650-1654 S. SAWTELLE BLVD	
PROJECT DESCRIPTION Demolition of a multi-family building and a retail store building and construction of a new five-story, approximately 55-foot tall, 42,095 square feet building containing 48 apartment units over three subterranean levels of parking with 66 parking spaces and 53 bicycle parking spaces, all on an approximately 12, 610 square feet of two lots. Vehicular ingress/egress will be via a driveway on the back alley. The unit mix includes 5 efficiency units, 14 one-bedroom units, and 29 two-bedroom dwelling units. The subject site is designated Neighborhood Commercial on the West Los Angeles Community Plan land use map and zoned C2-1VL. The project requests a zone change from C2-1 VL to RAS4-1VL. In consideration of reserving four units for very low-income tenants, a Density Bonus is requested for two on-menu Affordable Housing Incentives to permit a 35 percent increase in the Floor Area Ratio (FAR), 4.0:1 in lieu of 3.0:1, permitting 42,152 square-foot in lieu of 31,224 square feet; and 35 percent increase in height permitting max 56 feet in lieu of 45 feet.	
NAME AND ADDRESS OF APPLICANT IF OTHER THAN CITY AGENCY Rexford Elegant Apartments, LLC Jay Nayssan 10350 Santa Monica Blvd. Suite 190 Los Angeles, CA 90025	
FINDING: The City Planning Department of the City of Los Angeles has Proposed that a mitigated negative declaration be adopted for this project because the mitigation measure(s) outlined on the attached page(s) will reduce any potential significant adverse effects to a level of insignificance <p style="text-align: center;">(CONTINUED ON PAGE 2)</p> SEE ATTACHED SHEET(S) FOR ANY MITIGATION MEASURES IMPOSED. Any written comments received during the public review period are attached together with the response of the Lead City Agency. The project decision-maker may adopt the mitigated negative declaration, amend it, or require preparation of an EIR. Any changes made should be supported by substantial evidence in the record and appropriate findings made.	
THE INITIAL STUDY PREPARED FOR THIS PROJECT IS ATTACHED.	
NAME OF PERSON PREPARING THIS FORM NAOMI GUTH	TITLE City Planner TELEPHONE NUMBER (213) 978-1171
ADDRESS 200 N. SPRING STREET, 7th FLOOR LOS ANGELES, CA. 90012	SIGNATURE (Official)  DATE AUGUST 5, 2015

I-120. Aesthetics (Light)

- Environmental impacts to the adjacent residential properties may result due to excessive illumination on the project site. However, the potential impacts will be mitigated to a less than significant level by the following measure:
- Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties, the public right-of-way, nor from above.

I-130. Aesthetics (Glare)

- Environmental impacts to adjacent residential properties may result from glare from the proposed project. However, the potential impacts will be mitigated to a less than significant level by the following measure:
- The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat.

IV-90. Tree Removal (Public Right-of-Way)

-
- Removal of trees in the public right-of-way requires approval by the Board of Public Works.
- The required Tree Report shall include the location, size, type, and condition of all existing trees in the adjacent public right-of-way and shall be submitted for review and approval by the Urban Forestry Division of the Bureau of Street Services, Department of Public Works (213-847-3077).
- All trees in the public right-of-way shall be provided per the current Urban Forestry Division standards.

X-60. Land Use/Planning

- The project will result in land use and/or planning impact(s). However, the impact(s) can be reduced to a less than significant level through compliance with the following measure(s):
- An air filtration system shall be installed and maintained with filters meeting or exceeding the ASHRAE Standard 52.2 Minimum Efficiency Reporting Value (MERV) of 11, to the satisfaction of the Department of Building and Safety.
- An air filtration system shall be installed and maintained with filters meeting or exceeding the ASHRAE Standard 52.2 Minimum Efficiency Reporting Value (MERV) of 12, to the satisfaction of the Department of Building and Safety.
- An air filtration system shall be installed and maintained with filters meeting or exceeding the ASHRAE Standard 52.2 Minimum Efficiency Reporting Value (MERV) of 13, to the satisfaction of the Department of Building and Safety.

CITY OF LOS ANGELES
OFFICE OF THE CITY CLERK
ROOM 395, CITY HALL
LOS ANGELES, CALIFORNIA 90012
CALIFORNIA ENVIRONMENTAL QUALITY ACT
INITIAL STUDY
and CHECKLIST
(CEQA Guidelines Section 15063)

LEAD CITY AGENCY: City of Los Angeles		COUNCIL DISTRICT: CD 11 - MIKE BONIN	DATE:
RESPONSIBLE AGENCIES: Department of City Planning			
ENVIRONMENTAL CASE: ENV-2014-2848-MND	RELATED CASES: CPC-2014-2847-ZC-DB		
PREVIOUS ACTIONS CASE NO.:	<input type="checkbox"/> Does have significant changes from previous actions. <input checked="" type="checkbox"/> Does NOT have significant changes from previous actions.		
PROJECT DESCRIPTION: ZONE CHANGE AND DENSITY BONUS FOR DEVELOPMENT OF 48 UNIT APARTMENT BUILDING.			
ENV PROJECT DESCRIPTION: Demolition of a multi-family building and a retail store building and construction of a new five-story, approximately 55-foot tall, 42,095 square feet building containing 48 apartment units over three subterranean levels of parking with 66 parking spaces and 53 bicycle parking spaces, all on an approximately 12,610 square feet of two lots. Vehicular ingress/egress will be via a driveway on the back alley. The unit mix includes 5 efficiency units, 14 one-bedroom units, and 29 two-bedroom dwelling units. The subject site is designated Neighborhood Commercial on the West Los Angeles Community Plan land use map and zoned C2-1VL. The project requests a zone change from C2-1 VL to RAS4-1VL. In consideration of reserving four units for very low-income tenants, a Density Bonus is requested for two on-menu Affordable Housing Incentives to permit a 35 percent increase in the Floor Area Ratio (FAR), 4.0:1 in lieu of 3.0:1, permitting 42,152 square-foot in lieu of 31,224 square feet; and 35 percent increase in height permitting max 56 feet in lieu of 45 feet.			
ENVIRONMENTAL SETTINGS: The property, consisting of two contiguous lots, is a level, rectangular-shaped, corner lot, approximately 12,610 square-foot parcel of land with an approximately 140-foot depth and 90-foot frontage on the east side of Sawtelle Blvd. The property is fully developed with a multi-family building from 1954 and a retail store building constructed circa 1936. Vehicular access to the site will be taken from an alley on the east side of the property. The property has pedestrian access to another alley along the north side of the lot and Sawtelle Blvd. on the west side which is designated a Secondary Collector Street. The subject site is designated Neighborhood Commercial on the West Los Angeles Plan land use and is currently zoned C2-1VL. The east side adjoining properties, fronting on Beloit Avenue, are zoned R4-1 and High Medium Residential. All other abutting properties are zoned C2-1VL and Neighborhood Commercial. The property is located within the Methane Buffer Zone of a Methane Hazard Site and is approximately 460 feet from the nearest known fault, the Santa Monica Fault.			
PROJECT LOCATION: 1650-1654 S. SAWTELLE BLVD			
COMMUNITY PLAN AREA: WEST LOS ANGELES STATUS: <input checked="" type="checkbox"/> Does Conform to Plan <input type="checkbox"/> Does NOT Conform to Plan	AREA PLANNING COMMISSION: WEST LOS ANGELES		CERTIFIED NEIGHBORHOOD COUNCIL: WEST LOS ANGELES
EXISTING ZONING: C2-1VL	MAX. DENSITY/INTENSITY ALLOWED BY ZONING: 1.5:1 FAR and 1 D.U./400 sq. ft. (LAMC)		

GENERAL PLAN LAND USE: Neighborhood Commercial	MAX. DENSITY/INTENSITY ALLOWED BY PLAN DESIGNATION: 1.5:1 FAR and 1 D.U./400 sq. ft.	LA River Adjacent:
	PROPOSED PROJECT DENSITY: 3:1 FAR and 1 D.U./400 sq. ft.	

Determination (To Be Completed By Lead Agency)

On the basis of this initial evaluation:

- ☐ I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- ☒ I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions on the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- ☐ I find the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- ☐ I find the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- ☐ I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



City Planner

(213) 978-1171

Signature

Title

Phone

Evaluation Of Environmental Impacts:

1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
4. "Negative Declaration: Less Than Significant With Mitigation Incorporated" applies where the incorporation of a mitigation measure has reduced an effect from "Potentially Significant Impact" to "Less Than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from "Earlier Analyses," as described in (5) below, may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR, or negative declaration. Section 15063 (c)(3)(D). In this case, a brief discussion should identify the following:
 - a. Earlier Analysis Used. Identify and state where they are available for review.
 - b. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
7. Supporting Information Sources: A sources list should be attached, and other sources used or individuals contacted should be cited in the discussion.
8. This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.
9. The explanation of each issue should identify:
 - a. The significance criteria or threshold, if any, used to evaluate each question; and
 - b. The mitigation measure identified, if any, to reduce the impact to less than significance.

Environmental Factors Potentially Affected:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

<input checked="" type="checkbox"/> AESTHETICS	<input type="checkbox"/> GREEN HOUSE GAS EMISSIONS	<input type="checkbox"/> POPULATION AND HOUSING
<input type="checkbox"/> AGRICULTURE AND FOREST RESOURCES	<input type="checkbox"/> HAZARDS AND HAZARDOUS MATERIALS	<input type="checkbox"/> PUBLIC SERVICES
<input type="checkbox"/> AIR QUALITY	<input type="checkbox"/> HYDROLOGY AND WATER QUALITY	<input type="checkbox"/> RECREATION
<input checked="" type="checkbox"/> BIOLOGICAL RESOURCES	<input checked="" type="checkbox"/> LAND USE AND PLANNING	<input type="checkbox"/> TRANSPORTATION/TRAFFIC
<input type="checkbox"/> CULTURAL RESOURCES	<input type="checkbox"/> MINERAL RESOURCES	<input type="checkbox"/> UTILITIES AND SERVICE SYSTEMS
<input type="checkbox"/> GEOLOGY AND SOILS	<input type="checkbox"/> NOISE	<input type="checkbox"/> MANDATORY FINDINGS OF SIGNIFICANCE

INITIAL STUDY CHECKLIST (To be completed by the Lead City Agency)

Background

PROPONENT NAME:

Rexford Elegant Apartments, LLC
Jay Nayssan

PHONE NUMBER:

(310) 999-3060

APPLICANT ADDRESS:

10350 Santa Monica Blvd. Suite 190
Los Angeles, CA 90025

AGENCY REQUIRING CHECKLIST:

Department of City Planning

DATE SUBMITTED:

08/05/2014

PROPOSAL NAME (if Applicable):

Potentially significant impact	Potentially significant unless mitigation incorporated	Less than significant impact	No impact
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I. AESTHETICS

a.	Have a substantial adverse effect on a scenic vista?				✓
b.	Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?				✓
c.	Substantially degrade the existing visual character or quality of the site and its surroundings?			✓	
d.	Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?		✓		

II. AGRICULTURE AND FOREST RESOURCES

a.	Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use?				✓
b.	Conflict with existing zoning for agricultural use, or a Williamson Act contract?				✓
c.	Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?				✓
d.	Result in the loss of forest land or conversion of forest land to non-forest use?				✓
e.	Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?				✓

III. AIR QUALITY

a.	Conflict with or obstruct implementation of the applicable air quality plan?			✓	
b.	Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			✓	
c.	Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?			✓	
d.	Expose sensitive receptors to substantial pollutant concentrations?			✓	
e.	Create objectionable odors affecting a substantial number of people?				✓

IV. BIOLOGICAL RESOURCES

a.	Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?				✓
b.	Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?				✓
c.	Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?				✓
d.	Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				✓
e.	Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?		✓		
f.	Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?				✓

V. CULTURAL RESOURCES

Potentially significant impact	Potentially significant unless mitigation incorporated	Less than significant impact	No impact
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a.	Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5?			✓
b.	Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5?		✓	
c.	Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?		✓	
d.	Disturb any human remains, including those interred outside of formal cemeteries?		✓	
VI. GEOLOGY AND SOILS				
a.	Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.		✓	
b.	Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: Strong seismic ground shaking?		✓	
c.	Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: Seismic-related ground failure, including liquefaction?		✓	
d.	Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: Landslides?			✓
e.	Result in substantial soil erosion or the loss of topsoil?		✓	
f.	Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?		✓	
g.	Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?		✓	
h.	Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?			✓
VII. GREEN HOUSE GAS EMISSIONS				
a.	Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?		✓	
b.	Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?		✓	
VIII. HAZARDS AND HAZARDOUS MATERIALS				
a.	Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?		✓	
b.	Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?		✓	
c.	Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?			✓
d.	Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?			✓
e.	For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?			✓
f.	For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?			✓
g.	Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?		✓	

Potentially significant impact	Potentially significant unless mitigation incorporated	Less than significant impact	No impact
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h.	Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?				✓
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IX. HYDROLOGY AND WATER QUALITY

a.	Violate any water quality standards or waste discharge requirements?			✓	
b.	Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of preexisting nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?			✓	
c.	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?			✓	
d.	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?			✓	
e.	Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?			✓	
f.	Otherwise substantially degrade water quality?				✓
g.	Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?				✓
h.	Place within a 100-year flood hazard area structures which would impede or redirect flood flows?				✓
i.	Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?				✓
j.	Inundation by seiche, tsunami, or mudflow?				✓

X. LAND USE AND PLANNING

a.	Physically divide an established community?				✓
b.	Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?		✓		
c.	Conflict with any applicable habitat conservation plan or natural community conservation plan?				✓

XI. MINERAL RESOURCES

a.	Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?				✓
b.	Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?				✓

XII. NOISE

a.	Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?			✓	
b.	Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?			✓	
c.	A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?			✓	
d.	A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?			✓	

Potentially significant Impact	Potentially significant unless mitigation incorporated	Less than significant impact	No impact
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e.	For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?				✓
f.	For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?				✓

XIII. POPULATION AND HOUSING

a.	Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?			✓	
b.	Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?			✓	
c.	Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?			✓	

XIV. PUBLIC SERVICES

a.	Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Fire protection?			✓	
b.	Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Police protection?			✓	
c.	Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Schools?			✓	
d.	Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Parks?			✓	
e.	Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: Other public facilities?			✓	

XV. RECREATION

a.	Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?			✓	
b.	Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?			✓	

XVI. TRANSPORTATION/TRAFFIC

a.	Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?			✓	
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Potentially significant impact	Potentially significant unless mitigation incorporated	Less than significant impact	No impact
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b.	Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?			✓	
c.	Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?				✓
d.	Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?			✓	
e.	Result in inadequate emergency access?				✓
f.	Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities supporting alternative transportation (e.g., bus turnouts, bicycle racks)?				✓

XVII. UTILITIES AND SERVICE SYSTEMS

a.	Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?			✓	
b.	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			✓	
c.	Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			✓	
d.	Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?			✓	
e.	Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?			✓	
f.	Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?			✓	
g.	Comply with federal, state, and local statutes and regulations related to solid waste?				✓

XVIII. MANDATORY FINDINGS OF SIGNIFICANCE

a.	Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?			✓	
b.	Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?			✓	
c.	Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?			✓	

Note: Authority cited: Sections 21083, 21083.05, Public Resources Code. Reference: Section 65088.4, Gov. Code; Sections 21080, 21083.05, 21095, Pub. Resources Code; *Eureka Citizens for Responsible Govt. v. City of Eureka* (2007) 147 Cal.App.4th 357; *Protect the Historic Amador Waterways v. Amador Water Agency* (2004) 116 Cal.App.4th at 1109; *San Franciscans Upholding the Downtown Plan v. City and County of San Francisco* (2002) 102 Cal.App.4th 656.

DISCUSSION OF THE ENVIRONMENTAL EVALUATION (Attach additional sheets if necessary)

The Environmental Impact Assessment includes the use of official City of Los Angeles and other government source reference materials related to various environmental impact categories (e.g., Hydrology, Air Quality, Biology, Cultural Resources, etc.). The State of California, Department of Conservation, Division of Mines and Geology - Seismic Hazard Maps and reports, are used to identify potential future significant seismic events; including probable magnitudes, liquefaction, and landslide hazards. Based on applicant information provided in the Master Land Use Application and Environmental Assessment Form, impact evaluations were based on stated facts contained therein, including but not limited to, reference materials indicated above, field investigation of the project site, and any other reliable reference materials known at the time.

Project specific impacts were evaluated based on all relevant facts indicated in the Environmental Assessment Form and expressed through the applicant's project description and supportive materials. Both the Initial Study Checklist and Checklist Explanations, in conjunction with the City of Los Angeles's Adopted Thresholds Guide and CEQA Guidelines, were used to reach reasonable conclusions on environmental impacts as mandated under the California Environmental Quality Act (CEQA).

The project as identified in the project description may cause potentially significant impacts on the environment without mitigation. Therefore, this environmental analysis concludes that a Mitigated Negative Declaration shall be issued to avoid and mitigate all potential adverse impacts on the environment by the imposition of mitigation measures and/or conditions contained and expressed in this document; the environmental case file known as **ENV-2014-2848-MND** and the associated case(s), **CPC-2014-2847-ZC-DB**. Finally, based on the fact that these impacts can be feasibly mitigated to less than significant, and based on the findings and thresholds for Mandatory Findings of Significance as described in the California Environmental Quality Act, section 15065, the overall project impact(s) on the environment (after mitigation) will not:

- Substantially degrade environmental quality.
- Substantially reduce fish or wildlife habitat.
- Cause a fish or wildlife habitat to drop below self sustaining levels.
- Threaten to eliminate a plant or animal community.
- Reduce number, or restrict range of a rare, threatened, or endangered species.
- Eliminate important examples of major periods of California history or prehistory.
- Achieve short-term goals to the disadvantage of long-term goals.
- Result in environmental effects that are individually limited but cumulatively considerable.
- Result in environmental effects that will cause substantial adverse effects on human beings.

ADDITIONAL INFORMATION:

All supporting documents and references are contained in the Environmental Case File referenced above and may be viewed in the EIR Unit, Room 763, City Hall.

For City information, addresses and phone numbers: visit the City's website at <http://www.lacity.org> ; City Planning - and Zoning Information Mapping Automated System (ZIMAS) cityplanning.lacity.org/ or EIR Unit, City Hall, 200 N Spring Street, Room 763.

Seismic Hazard Maps - <http://gmw.consrv.ca.gov/shmp/>

Engineering/Infrastructure/Topographic Maps/Parcel Information - <http://boemaps.eng.ci.la.ca.us/index01.htm> or

City's main website under the heading "Navigate LA".

PREPARED BY:	TITLE:	TELEPHONE NO.:	DATE:
NAOMI GUTH	City Planner	(213) 978-1171	07/01/2015

Impact?	Explanation	Mitigation Measures
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APPENDIX A: ENVIRONMENTAL IMPACTS EXPLANATION TABLE

I. AESTHETICS		
a.	NO IMPACT	<p>A significant impact would occur if the proposed project would have a substantial adverse effect on a scenic vista. A scenic vista refers to views of focal points or panoramic views of broader geographic areas that have visual interest. A focal point view would consist of a view of a notable object, building, or setting. A panoramic view would be generally wide and extend into the distance.</p> <p>Diminishment of a scenic vista would occur if the bulk or design of a building or development contrasts enough with a visually interesting view, so that the quality of the view is permanently affected. The project site is located south of the Santa Monica Blvd. (approximately 180 feet) and west of the 405 Freeway (approximately 185 feet). Santa Monica Blvd. has a pattern of low-rise neighborhood commercial uses in this part. The project proposed replacement of a retail store and a multi-family building with a five-story, over three levels of subterranean parking (56 feet tall), approximately 42,152 square-foot, residential building, containing 48 dwelling units. The project is not located in the vicinity of any scenic vistas. Therefore, although the proposed project would substantially increase the height and massing of development on the project site, project implementation would not partially or entirely obstruct any views of unique scenic vistas or focal points. Accordingly, impacts related to scenic vistas would be less than significant.</p>
b.	NO IMPACT	<p>A significant impact would occur if the proposed project would substantially damage scenic resources within a State Scenic Highway. The project is located in a heavily developed region and is not in the vicinity of any known scenic resources, nor is it located within a city or state designated scenic highway. Therefore, the project will have no impact on scenic resources.</p>

Impact?	Explanation	Mitigation Measures
c. LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the proposed project would substantially degrade the existing visual character or quality of the project site and its surroundings. Significant impacts to the visual character of a site and its surroundings are generally based on the removal of features with aesthetic value, the introduction of contrasting urban features into a local area, and the degree to which the elements of the proposed project detract from the visual character of an area. The project area is currently developed with a mix of land uses, including residential, neighborhood commercial, and institutional. The Nora Sterry Elementary School campus is located to the south of the project site across Sawtelle Blvd. Immediately all around the project site are low-rise residential and community commercial buildings. The proposed project will be a 5-story building over three levels of subterranean parking and the building height will be at 56 feet, which is under the 60 feet CEQA threshold of light-blocking structures. On the other hand, the proposed building may cast limited shadows over adjacent buildings at north, northwest, or northeast of the project site, however, the shadow will be cast on the roof of these building which are commercial buildings and not shadow-sensitive uses. Shading of the proposed building on northwest buildings will be limited due to the distance across Sawtelle Blvd. from the subject site. The project would result in less than significant impacts related to shade and shadow. The proposed project would include landscaping and streetscape improvements to enhance the visual quality of the area. Finally, the project must comply with the City of Los Angeles Residential Citywide Design Guidelines which require activation of all open areas not used for buildings, driveways, parking, recreational facilities, or pedestrian amenities with landscaping. The proposed project would include landscaping and streetscape improvements to enhance the visual quality of the area. Accordingly, the proposed project would not degrade the existing visual character or quality of the project site and its surroundings as the project would enhance rather than detract</p>	

Impact?	Explanation	Mitigation Measures
	<p>from the visual character of an area. Therefore, the proposed project would result in a less-than-significant impact on visual quality.</p>	
<p>d. POTENTIALLY SIGNIFICANT UNLESS MITIGATION INCORPORATED</p>	<p>A significant impact would occur if light and glare substantially altered the character of off-site areas surrounding the site or interfered with the performance of an off-site activity. Light impacts are typically associated with the use of artificial light during the evening and night-time hours. Glare may be a daytime occurrence caused by the reflection of sunlight or artificial light from highly polished surfaces, such as window glass and reflective cladding materials, and may interfere with the safe operation of a motor vehicle on adjacent streets. Daytime glare is common in urban areas and is typically associated with mid- to high-rise buildings with exterior façades largely or entirely comprised of highly reflective glass or mirror-like materials. Nighttime glare is primarily associated with bright point-source lighting that contrasts with existing low ambient light conditions. The project site is located in a densely developed portion of the City of Los Angeles that contains a substantial amount of lighting and sources of glare, such as windows and light-colored building surfaces. Nighttime lighting sources include street lights, vehicle headlights, and interior and exterior building illumination. The project would include a variety of interior and exterior lighting that complies with the Los Angeles Municipal Code (LAMC) to minimize the effect of the new sources of lighting that would be introduced. Also, the project would be required to use non-reflective glass. Therefore, the proposed development project is not likely to contain light sources beyond the lighting typical of residential development. However, the subject site is adjacent to residential land use, which is defined as light-sensitive land use. The project might include lighting that would routinely spillover onto them. Therefore, a significant impact may occur on adjacent residences if new development introduces new sources of substantial light or glare</p>	<p>I-120, I-130</p>

Impact?	Explanation	Mitigation Measures
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which would be incompatible with the existing levels. Mitigation measures will reduce any light and glare impacts to less than significant levels.

II. AGRICULTURE AND FOREST RESOURCES

a.	NO IMPACT	A significant impact would occur if the proposed project would convert valued farmland to non-agricultural uses. The project site is currently developed with a retail store and a multi-family building. No Farmland, agricultural uses, or related operations are present within the project site or surrounding area. Due to its urban setting, the project site and surrounding area are not included in the Farmland Mapping and Monitoring Program of the California Resources Agency. Therefore, the proposed project would not convert any Prime Farmland, Unique Farmland, or Farmland of Statewide Importance to non-agricultural use, and no impact would occur.	
b.	NO IMPACT	A significant impact would occur if the proposed project conflicted with existing agricultural zoning or agricultural parcels enrolled under the Williamson Act. The project site is not zoned for agricultural use or under a Williamson Act. The project site is currently zoned C2-1VL. As the project site and surrounding area do not contain farmland of any type, the proposed project would not conflict with a Williamson Act. Therefore, no impacts would occur.	
c.	NO IMPACT	A significant impact would occur if the proposed project conflicted with existing zoning for, or caused rezoning of forest land or timberland or result in the loss of forest land or in the conversion of forest land to non-forest use. The project site and the surrounding area are not zoned for forest land or timberland. As identified above, the project site is currently zoned C2-1VL. Accordingly, the proposed project would not conflict with forest land or timberland zoning or result in the loss of forest land or conversion of forest land to non-forest use. Therefore, no impact would occur.	
d.	NO IMPACT	The proposed project site is in a neighborhood which is designated for the Neighborhood Commercial use. The existing buildings on the project site were built in 1936 and 1954 and the surrounding area is developed with	

Impact?	Explanation	Mitigation Measures
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		neighborhood commercial and residential land use and buildings, which are mostly developed around the same time. Therefore, the proposed project will not directly or indirectly result in the conversion of any forest land to non-forest use.	
e.	NO IMPACT	A significant impact would occur if the proposed project caused the conversion of farmland to non-agricultural use. The project site does not contain farmland, forestland, or timberland. The proposed project site is within an urbanized region, zoned for neighborhood Commercial use and is surrounded primarily by the same or similar land use zoning and will not directly or indirectly result in the conversion of any farmland to non-agricultural use or forest land to non-forest use. Therefore, no impacts would occur.	

III. AIR QUALITY

a.	LESS THAN SIGNIFICANT IMPACT	<p>The overall control strategy for the South Coast Air Quality Management District (SCAQMD) 2012 Air Quality Management Plan (AQMP) is designed to meet applicable federal and State requirements, including attainment of ambient air quality standards. The 2012 AQMP provides base year emissions and future baseline emission projections, including the South Coast Air Basin. In doing so, the 2012 AQMP relies upon the most recent zoning and land use designations and the best available information. The proposed project involves construction of a new 5-story residential building. The project site is currently zoned C2-1VL with a Neighborhood Commercial land use designation, per the West Los Angeles Community Plan land use. The project site is currently developed with a retail store and a multi-family building. A project would not conflict with the AQMP if it is consistent with the population, housing and employment assumptions which were used in the development of the AQMP. The project involves a zone change from C2-1VL to RAS4-1VL which is similar to a project in close vicinity and is the same zone as the adjacent property at the rear (east) of the site. The type of land use proposed as part of the project (multi-family residential) is allowed as a "by-right" development at the project site</p>	
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Impact?	Explanation	Mitigation Measures
	in existing and applied zones. Therefore, the proposed project is not expected to conflict with or obstruct the implementation of the AQMP, the South Coast Air Quality Management District (SCAQMD) plans, or Congestion Management Plan (CMP).	
b.	<p>LESS THAN SIGNIFICANT IMPACT</p> <p>A significant impact would occur if the proposed project would violate any air quality standard or contribute substantially to an existing or projected air quality violation. The project site is currently zoned C2-1VL and designated Neighborhood Commercial per the West Los Angeles Community Plan land use map, allowing multi-family dwelling, as proposed. The projects applying for a zone change from C2-1VL to RAS4. The project is not expected to violate any air quality standard or contribute substantially to an existing or projected air quality violation. However, the development of the proposed project will temporarily generate emissions from heavy-duty construction vehicles and construction workers' vehicles. In addition, fugitive dust would be generated by construction activities. It is mandatory for all construction projects in the South Coast Air Basin (Basin) to comply with SCAQMD Rule 403 for Fugitive Dust. Specific Rule 403 control requirements include, but are not limited to, applying water in sufficient quantities to prevent the generation of visible dust plumes, applying soil binders to uncovered areas, reestablishing ground cover as quickly as possible, utilizing a wheel washing system to remove bulk material from tires and vehicle undercarriages before vehicles exit the project site, and maintaining effective cover over exposed areas. Compliance with Rule 403 would reduce regional particulate matter emissions associated with construction activities and air quality impacts due to short-term construction are anticipated to be less than significant.</p>	
c.	<p>LESS THAN SIGNIFICANT IMPACT</p> <p>The project site is located within the South Coast Air Quality Management District, a known non-attainment zone. The project involves new construction of 48-unit multi-family building, and is not expected to result in a cumulatively considerable net increase of any criteria pollutant for which the air basin is</p>	

Impact?	Explanation	Mitigation Measures
	<p>non-attainable under an applicable federal or state ambient air quality standard. Possible project related air quality concerns will derive from the mobile source emissions that will be generated by the motor vehicles traveling to and from the new multi-family residential building. Operational emissions related to traffic generated by the project will be less than significant per the Los Angeles Department of Transportation (LADOT) Referral Form, dated November 25, 2014. In addition to mobile sources from vehicles, general development causes smaller amounts of "area source" air pollution to be generated from on-site energy consumption (natural gas combustion) and from off-site electrical generation. These sources represent a small percentage of the total pollutants. The inclusion of such emissions adds negligibly to the total significant project-related emissions burden generated by the proposed project. Operational and construction regional emissions would not likely exceed the project-level SCAQMD localized significance thresholds for criteria air pollutants. Therefore, the proposed project would not generate a cumulatively considerable increase in emissions of the pollutants for which the Basin is in nonattainment, and operational emission impacts will be at a less than significant level.</p>	
d.	<p>LESS THAN SIGNIFICANT IMPACT</p> <p>Based on the City of Los Angeles CEQA Thresholds Guide, a significant impact may occur if a project were to generate pollutant concentrations to a degree that would significantly affect sensitive receptors. Sensitive receptors are populations that are more susceptible to the effects of air pollution than are the population at large. The South Coast Air Quality Management District (SCAQMD) identifies the following as sensitive receptors: long-term health care facilities, rehabilitation centers, convalescent centers, retirement homes, residences, schools, playgrounds, child care centers, and athletic facilities. The nearest sensitive receptors that could potentially be subject to localized air quality impacts associated with construction of the proposed project include all single-family and multi-family residences around the project site and an elementary school that</p>	

Impact?	Explanation	Mitigation Measures
	<p>is 220 feet distance from the project site. Since the proposed project is a residential building, it will not include any land uses that would involve the use, storage, or processing of carcinogenic or non-carcinogenic toxic air contaminants. In addition, no toxic airborne emissions would typically result from the proposed project implementation. Therefore, the use itself will not result in new sources of pollutant concentrations exposing sensitive receptors. However, emissions from construction activities have the potential to generate localized emissions that may expose sensitive receptors to harmful pollutant concentrations.</p> <p>Compliance with Rule 403 which was explained in III.b section, would reduce regional particulate matter emissions associated with construction activities and air quality impacts due to short-term construction are anticipated to be less than significant. The project site is located within the SCAQM District, a known non-attainment zone. Motor vehicle emissions can influence local air quality through changes in carbon monoxide (CO) concentrations, which are usually highest at busy intersections, parking garages, or other focused areas of vehicle activity. SCAQMD recommends an evaluation of potential localized CO impacts when vehicle-to-capacity (V/C) ratios are increased by two percent or more at intersections with a level of service (LOS) of C or worse, and/or when the LOS for an intersection worsens from C to D or worse. Traffic volumes that meet these criteria have the potential to result in CO "hotspots". Based on the LADOT Referral Form, dated November 25, 2014, project-related traffic volumes is under the threshold to require a traffic study. It means that the project traffic would not meet these criteria at either surrounded intersections. Thus, project traffic would not have the potential to result in CO hotspots. As such, the project would not expose sensitive receptors to substantial pollutant concentrations, and therefore, project impacts related to this issue will be less than significant.</p>	

Impact?	Explanation	Mitigation Measures
e.	<p>NO IMPACT</p> <p>A significant impact may occur if objectionable odors occur which would adversely impact sensitive receptors. Potential sources that may emit odors during construction activities include equipment exhaust and architectural coatings. Odors from these sources would be localized and generally confined to the immediate area surrounding the project site. The proposed project would utilize typical construction techniques, and the odors would be typical of most construction sites and temporary in nature. According to the SCAQMD CEQA Air Quality Handbook, land uses and industrial operations that are associated with odor complaints include agricultural uses, wastewater treatment plants, food processing plants, chemical plants, composting, refineries, landfills, dairies and fiberglass molding. The project includes development of a multi-family residential building on the project site and therefore, involves no elements related to these types of activities and would not result in activities that create objectionable odors. Good housekeeping practices would be sufficient to prevent nuisance odors. No impacts related to this issue would occur.</p>	

IV. BIOLOGICAL RESOURCES

a.	<p>NO IMPACT</p> <p>A project would have a significant biological impact through the loss or destruction of individuals of a species or through the degradation of sensitive habitat. The project site is located in a highly urbanized area, adjacent to the 405 freeway and Santa Monica Blvd. The project site is currently improved with a one-story retail store and a two-story apartment dwelling. There is no tree on the site and vegetation on the project site is limited to a couple of flower bushes. There is only one street tree next to the project site and will likely be removed or disturbed during construction. The project site does not contain any critical habitat or support any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. Therefore, project would have no impact on sensitive biological species or habitat.</p>	
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	Impact?	Explanation	Mitigation Measures
b.	NO IMPACT	A significant impact would occur if any riparian habitat or natural community would be lost or destroyed as a result of urban development. The project site is occupied by a one-story retail store and a two-story apartment building. No riparian or other sensitive natural vegetation communities are located on or adjacent to the project site and water courses necessary to support riparian habitat. Therefore, implementation of the proposed project would not result in any adverse impacts to riparian habitat or other sensitive natural communities.	
c.	NO IMPACT	A significant impact would occur if federally protected wetlands would be modified or removed by a project. The project site is developed and does not contain any federally protected wetlands, wetland resources, or other waters of the United States as defined by Section 404 of the Clean Water Act. The project site is located in a highly urbanized area and developed/previously developed with residential, office, and commercial uses. Therefore, the proposed project would not have any effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means, and no impacts would occur.	
d.	NO IMPACT	A significant impact would occur if the proposed project would interfere with, or remove access to, a migratory wildlife corridor or impede use of native wildlife nursery sites. Due to the highly urbanized nature of the project site and surrounding area, the lack of a major water body and trees, and the proximity to the 405 Freeway, the project site does not support habitat for native resident or migratory species or contain native nurseries. Therefore, the proposed project would not interfere with wildlife movement or impede the use of native wildlife nursery sites, and no impact would occur.	
e.	POTENTIALLY SIGNIFICANT UNLESS MITIGATION INCORPORATED	A significant impact would occur if the proposed project would be inconsistent with local regulations pertaining to biological resources. The project site is located in a highly urbanized area of the City. The site is developed with a one-story retail shop and a two-story apartment building.	IV-90

Impact?	Explanation	Mitigation Measures
	<p>The vegetation on the project site is limited to flowers and bushes. There is one tree on the street in front of the project site, which will likely remain and could be disturbed during construction. The project site does not contain locally-protected biological resources, such as oak trees, Southern California black walnut, western sycamore, and California bay trees. The proposed project would not conflict with any policies or ordinances protecting biological resources, such as the City of Los Angeles Protected Tree Ordinance (No. 177,404). However, removal or planting of any tree in the public right-of-way requires approval of the Board of Public Works. Therefore, the proposed mitigation measures will confirm that the project will not conflict with any local policies or ordinances protecting biological resources, such as tree preservation policy or ordinance.</p>	
f. NO IMPACT	<p>A significant impact would occur if the proposed project would be inconsistent with mapping or policies in any conservation plans of the types cited. According to the Biological Resource Areas Maps (Coastal and Southern Geographical Area) in the Los Angeles CEQA Thresholds Guide (2006), the project site and its vicinity are not part of any draft or adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional or state habitat conservation plan. Therefore, no impact would occur with implementation of the proposed project.</p>	

V. CULTURAL RESOURCES

a. NO IMPACT	<p>A significant impact may occur if the proposed project would disturb historic resources which presently exist within the proposed project site. A one-story retail store and a two-story apartment building currently occupy the project site. The existing buildings are not designated as a local, state or federally listed historic resource, thus no listed historic resources would be impacted by the redevelopment of the project site. The structures are not known to be associated with any important events, historical patterns, or persons to be considered significant resources. Thus, there is no evidence to suggest that any historic resources would</p>	
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Impact?	Explanation	Mitigation Measures
	be impacted by the redevelopment of the project site. Therefore, the proposed project would not cause an adverse change in the significance of an historic resource and no impact would occur.	
b. LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if grading or excavation activities associated with the proposed project would disturb archaeological resources which presently exist within the project site. The project site and immediately surrounding areas do not contain any known archaeological sites or archaeological survey areas (City of Los Angeles, Environmental and Public Facilities Maps 1996, Prehistoric & Historic Archaeological Sites and Survey Areas Map). The proposed project includes subgrade preparation. Thus, the potential exists for the accidental discovery of archaeological materials. Because the presence or absence of such materials cannot be determined until the site is excavated, it will be required to comply with the City's Standard Conditions of Approval, which would require that prior to construction of the project, the prime contractor and any subcontractor(s) shall be advised of the legal and/or regulatory implications of knowingly destroying cultural resources or removing artifacts, human remains, bottles, and other cultural materials from the project site. In addition, in the event that buried archaeological resources are exposed during project construction, work within 50 feet of the find shall stop until a professional archaeologist, meeting the standards of the Secretary of the Interior, can identify and evaluate the significance of the discovery and develop recommendations for treatment, in conformance with California Public Resources Code Section 21083.2. Through compliance with the City's Standard Conditions of Approval, potential project impacts to unknown archaeological resources would be less than significant.</p>	
c. LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if grading or excavation activities associated with the proposed project were to disturb paleontological resources or geologic features which presently exist within the proposed project site. The project site has been previously graded and is currently improved with a one-story retail store and</p>	

Impact?	Explanation	Mitigation Measures
	<p>a two-story apartment building. The project site and immediate surrounding areas do not contain any known vertebrate paleontological resources (City of Los Angeles, Environmental and Public Facilities Maps 1996, Vertebrate Paleontological Resources Map). Although no paleontological resources are known to exist on site, there is a possibility that paleontological resources exist at sub-surface levels on the project site and may be uncovered during subgrade preparation. It would be required to comply with the City's Standard Conditions of Approval, which would require that prior to project construction, the prime contractor and any subcontractor(s) shall be advised of the legal and/or regulatory implications of knowingly destroying paleontological or unique geologic resources or sites from the project site. In addition, in the event that paleontological resources or sites, or unique geologic features are exposed during project construction, work within 50 feet of the find shall stop until a professional paleontologist, can identify and evaluate the significance of the discovery and develop recommendations for treatment. Any paleontological resources or sites, or unique geologic features shall be treated in accordance with State Law. Through compliance with the City's Standard Conditions of Approval, potential project impacts to unknown paleontological resources or sites, or unique geologic features would be less than significant.</p>	
d. LESS THAN SIGNIFICANT IMPACT	<p>A project-related significant adverse effect could occur if grading or excavation activities associated with the proposed project would disturb previously interred human remains. No known human burials have been identified on the project site or its vicinity (City of Los Angeles, Environmental and Public Facilities Maps 1996, Prehistoric & Historic Archaeological Sites and Survey Areas Map). However, it is possible that unknown human remains could occur on the project site, and if proper care is not taken during construction, damage to or destruction of these unknown remains could occur. If human remains are found during excavation, the project will need to follow procedures as detailed in the California Health and Safety Code</p>	

Impact?	Explanation	Mitigation Measures
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Sections 7000 et seq. If human remains of Native American origin are discovered during project construction, compliance with state laws, which fall within the jurisdiction of the Native American Heritage Commission (NAHC) (Public Resource Code Section 5097), relating to the disposition of Native American burials will be adhered to. Therefore, the impact would be less than significant.

VI. GEOLOGY AND SOILS

a.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if a proposed project site is located within a State-designated Alquist-Priolo Zone or other designated fault zone. The project site is not located within an Alquist-Priolo Earthquake Fault Zone and there are no known faults (active, potentially active, or inactive) onsite. The subject site is approximately 460 feet from the Santa Monica Fault Zone (ZIMAS). Since no active faults cross the property, the surface rupture hazard at the project site is very low. However, due to the intense seismic environment of Southern California, there is always a potential for blind thrust faults, or otherwise unmapped faults that do not have a surface trace, to be present. New development will be required to comply with the seismic safety requirements in the California Building Code (CBC) and the California Geological Survey Special Publication 117 (Guidelines for Evaluating and Mitigating Seismic Hazards in California [1997]), which provide guidance for evaluating and mitigating earthquake-related hazards as approved by the Los Angeles Department of Building and Safety. Therefore, impacts will be less than significant.</p>
b.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if a project represents an increased risk to public safety or destruction of property by exposing people, property, or infrastructure to seismically induced ground shaking hazards that are greater than the average risk associated with other locations in Southern California. The subject site is in the vicinity of the Santa Monica Fault Zone, per ZIMAS. Any development that occurs within the geographical boundaries of Southern California has the potential of exposing people and/or structures to potentially substantial adverse effects involving potential blind thrust faults, the rupture of</p>

Impact?	Explanation	Mitigation Measures
	<p>known and/or unknown earthquake faults, or strong seismic ground shaking. New development will be required to comply with the seismic safety requirements in the California Building Code (CBC) and the California Geological Survey Special Publication 117 (Guidelines for Evaluating and Mitigating Seismic Hazards in California [1997]), which provide guidance for evaluating and mitigating earthquake-related hazards as approved by the Los Angeles Department of Building and Safety. Compliance with such requirements would reduce seismic ground shaking impacts to the maximum extent practicable with current engineering practices. Therefore, impacts related to strong seismic ground shaking would be less than significant.</p>	
c. LESS THAN SIGNIFICANT IMPACT	<p>Based upon the criteria established in the City of Los Angeles CEQA Thresholds Guide, a significant impact may occur if a proposed project site is located within a liquefaction zone. Liquefaction is the loss of soil strength or stiffness due to a buildup of pore-water pressure during severe ground shaking. The project site is located in a liquefaction zone, per ZIMAS and the California Department of Conservation's Seismic Hazard Zones Map for the Beverly Hills Quadrangle. Liquefaction is precluded above the high water table, by definition. Since the project site is located in an area that is susceptible to liquefaction, the Department of Building and Safety Grading Division will require a soils study. Complying with the City of Los Angeles Building and Safety requirements will reduce the impact to a less than significant level.</p>	
d. NO IMPACT	<p>A project would have a significant geologic hazard impact if it would cause or accelerate geologic hazards which would result in substantial damage to structures or infrastructure, or expose people to substantial risk of injury. A project-related significant adverse effect may occur if the project is located in a hillside area with soil conditions that would suggest a high potential for sliding. According to the ZIMAS and the California Department of Conservation's Seismic Hazard Zones Map for the Beverly Hills Quadrangle, the project is not located within a seismic hazard zone for</p>	

Impact?	Explanation	Mitigation Measures
	<p>landsliding or faulting. Additionally, per the site Topography Survey, dated March 31, 2013 and NavigateLA, the project site is flat. Due to the subsurface geotechnical characteristics, the relatively flat nature of the site, the potential for lateral spreading is expected to be very low. Therefore the probability of landslides, including seismically induced landslides, is considered to be very low and no impact would occur.</p>	
<p>e. LESS THAN SIGNIFICANT IMPACT</p>	<p>A significant impact would occur if the proposed project would be implemented on a site that would be located in a hillside area with unstable geological conditions or soil types that would be susceptible to failure when saturated. According to the ZIMAS, the project is not located within a seismic hazard zone for landsliding or faulting. Additionally, per the site Topography Survey dated March 31, 2013, and NavigateLA, the project site is flat. Although development of the proposed project has the potential to result in the erosion of soils during site preparation and construction activities, erosion would be reduced by implementation of stringent erosion controls imposed by the City of Los Angeles through grading and building permit regulations. Minor amounts of erosion and siltation could occur during grading. The potential for soil erosion during the ongoing operation of the proposed project is extremely low due to the generally level topography of the project site and the fact that the project site would be mostly paved-over or built upon, so little soil would be exposed. All grading activities require grading permits from the Department of Building and Safety, which include requirements and standards designed to limit potential impacts to acceptable levels. In addition, all onsite grading and site preparation would comply with applicable provisions of Chapter IX, Division 70 of the LAMC, which addresses grading, excavations, and fills. The project does not include grading, clearing, or excavation activities in an area of known or suspected erosion hazard. Proper grading practices during the construction in accordance with City regulations and complying with the Los Angeles Bureau of Sanitation, Wastewater Engineering Services Division (WESD) letter requirements</p>	

Impact?	Explanation	Mitigation Measures
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		dated September 2, 2014, will minimize soil erosion and the loss of topsoil, and will reduce any impacts to a less than significant level.	
f.	LESS THAN SIGNIFICANT IMPACT	<p>A project would have a significant geologic hazard impact if it could cause or accelerate geologic hazards causing substantial damage to structures or infrastructure, or expose people to substantial risk of injury. For the purpose of this specific issue, a significant impact may occur if the proposed project is built in an unstable area without proper site preparation or design features to provide adequate foundations for buildings, thus posing a hazard to life and property. Development of the proposed project would not have the potential to expose people and structures to seismic-related ground failure, including liquefaction and landslide. Subsidence and ground collapse generally occur in areas with active groundwater withdrawal or petroleum production. The extraction of groundwater or petroleum from sedimentary source rocks can cause the permanent collapse of the pore space previously occupied by the removed fluid. The compaction of subsurface sediments by fluid withdrawal will cause subsidence or ground collapse overlying a pumped reservoir. The project site is not identified by the City as being located in an oil field or within an oil drilling area. In addition, there are no tunnels, groundwater wells, covered quarries, or caves that are located beneath the project site. The proposed project would be required to implement standard construction practices that would ensure that the integrity of the project site and the proposed structures is maintained. Therefore, impacts would be less than significant. Construction of the proposed project will be required by the Department of Building and Safety to comply with the City of Los Angeles Uniform Building Code (Building Code) which is designed to assure safe construction and includes building foundation requirements appropriate to site conditions. With the implementation of the Building Code requirements and the Department of Building and Safety's Soils Report Approval Letter when issued, the potential for landslide lateral spreading, subsidence, liquefaction or collapse would</p>	

Impact?	Explanation	Mitigation Measures
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		be less-than-significant.	
g.	LESS THAN SIGNIFICANT IMPACT	<p>A project would have a significant geologic hazard impact if it would cause or accelerate geologic hazards which would result in substantial damage to structures or infrastructure, or expose people to substantial risk of injury. For the purpose of this specific issue, a significant impact may occur if the proposed project is built on expansive soils without proper site preparation or design features to provide adequate foundations for buildings, thus posing a hazard to life and property. Expansive soils contain significant amounts of clay particles that swell considerably when wetted and which shrink when dried. Foundations constructed on these soils are subject to uplifting forces caused by the swelling. According to the Geologic Compilation of Quaternary Surficial Deposits in Southern California, Los Angeles 30' X 60' Quadrangle (2012), the soil in this area is made up of Young alluvium, undivided (late Pleistocene) Unconsolidated, generally friable, stream-deposited silt, sand and gravel on canyon floors; surfaces may show slight to moderate pedogenic soil development. Therefore, it is not likely that the project site is located on expansive soil. The project will be required prepare a geotechnical report and to adhere to the International Building Code and the California Building Code, which includes structural and materials standards as well as foundation design requirements based upon on-site soil conditions that would mitigate effects of adverse soil conditions to less than significant.</p>	
h.	NO IMPACT	<p>This question would apply to the proposed project only if it was located in an area not served by an existing sewer system. The project site is located in a developed area of the City of Los Angeles, which is served by a wastewater collection, conveyance and treatment system operated by the City of Los Angeles. No septic tanks or alternative disposal systems neither are necessary, nor are they proposed. Thus, no impact would occur.</p>	

VII. GREEN HOUSE GAS EMISSIONS

Impact?	Explanation	Mitigation Measures
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a.	LESS THAN SIGNIFICANT IMPACT	<p>Greenhouse gases (GHG) are those gaseous constituents of the atmosphere, both natural and anthropogenic (human generated), that absorb and emit radiation at specific wavelengths within the spectrum of terrestrial radiation emitted by the earth's surface, the atmosphere itself, and by clouds. The greenhouse effect compares the Earth and the atmosphere surrounding it to a greenhouse with glass panes. The glass panes in a greenhouse let heat from sunlight in and reduce the amount of heat that escapes. GHGs, such as carbon dioxide (CO₂), methane (CH₄), and nitrous oxide (N₂O), keep the average surface temperature of the Earth close to 60 degrees Fahrenheit (°F). Without the greenhouse effect, the Earth would be a frozen globe with an average surface temperature of about 5°F. The construction and operation of the proposed project would generate GHG emissions. Construction-related emissions would be generated from off-road equipment and vehicle exhaust. Operational emissions include both direct and indirect sources including mobile sources, water use, solid waste, area sources, natural gas, and electricity use emissions. The City has adopted the LAGreen Plan to provide a citywide plan for achieving the City's GHG emissions targets, for both existing and future generation of GHG emissions. In order to further implement the LA Green Plan's goal of improving energy conservation and efficiency, the Los Angeles City Council has adopted multiple ordinances and updates to establish the current Los Angeles Green Building Code applicable to new development projects. As it relates to new development, the City adopted the LAGreen Building Code, and in some cases, outlines stricter GHG reduction measures available to development projects in the City of Los Angeles. The LAGreen Building Code requires projects to achieve a 20 percent reduction in potable water use and wastewater generation, meet and exceed Title 24 Standards. As the LAGreen Building Code applicable provisions of the CALGreen Code, a new development project that can demonstrate it complies with the LA Green Building Code is considered consistent with statewide GHG reduction goals and policies</p>
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Impact?	Explanation	Mitigation Measures
	<p>including AB32 (California Global Warming Solutions Act of 2006). Through required implementation of the LA Green Building Code, the proposed project would be consistent with local and statewide goals and policies aimed at reducing the generation of GHGs. Therefore, the proposed project's generation of GHG emissions would not make a cumulatively considerable contribution to emissions. Air quality mitigation measures identified in this report such as water use conservation and bicycle parking and additional project-specific mitigation such as complying with LAGreen codes will be required in order to reduce the global cumulative impacts to a less than significant level.</p>	
b. LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the proposed project would conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases. The California legislature passed Senate Bill (SB) 375 to connect regional transportation planning to land use decisions made at a local level. SB 375 requires the metropolitan planning organizations to prepare an SCS in their regional transportation plans to achieve the per capita GHG reduction targets. For the SCAG region, the SCS is contained in the 2012-2035 RTP/SCS. The 2012-2035 RTP/SCS focuses the majority of new housing and job growth in high-quality transit areas and other opportunity areas on existing main streets, in downtowns, and commercial corridors, resulting in an improved jobs-housing balance and more opportunity for transit-oriented development. In addition, SB 743, adopted September 27, 2013, encourages land use and transportation planning decisions and investments that reduce vehicle miles traveled that contribute to GHG emissions, as required by AB 32. As described in Question VII.a, the proposed project would be consistent with local and statewide goals and policies aimed at reducing the generation of GHGs, including CARB's AB 32 Scoping Plan aimed at achieving 1990 GHG emission levels by 2020. The project would provide infill residential development proximate to a major transportation corridor (i.e., Santa Monica</p>	

Impact?	Explanation	Mitigation Measures
	<p>Boulevard) and would not interfere with SCAG's ability to implement the regional strategies outlined in the 2012-2035 RTP/SCS. The proposed project would provide residential units to meet demand for housing in proximity to urban uses, including transportation/transit and would provide a healthy environment by reducing vehicle trips and corresponding GHG emissions. Therefore, the proposed project's generation of GHG emissions would not make a cumulatively considerable contribution to conflicting with an applicable plan, policy or regulation for the purposes of reducing the emissions of greenhouse gasses and project's impact would be less than significant.</p>	

VIII. HAZARDS AND HAZARDOUS MATERIALS

a.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the proposed project would create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials. The proposed project involved the construction and operation of a multi-family residential project. Construction of the proposed project would involve the temporary use of potentially hazardous materials, including vehicle fuels, oils, and transmission fluids. Operation of the project would involve the limited use and storage of common hazardous substances typical of those used in multi-family residential developments, including lubricants, paints, solvents, custodial products (e.g., cleaning supplies), pesticides and other landscaping supplies, and vehicle fuels, oils, and transmission fluids. No industrial uses or activities are proposed that would result in the use or discharge of unregulated hazardous materials and/or substances, or create a public hazard through transport, use, or disposal. As a residential development, the proposed project would not involve large quantities of hazardous materials that would require routine transport, use, or disposal. With compliance to applicable standards and regulations and adherence to manufacturer's instructions related to the transport, use, or disposal of hazardous materials, the proposed project would not create a significant hazard to the public or the environment through the routine</p>	
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Impact?	Explanation	Mitigation Measures
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		transport, use, or disposal of hazardous materials, and impacts would be less than significant.	
b.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if a project utilizes quantities of hazardous materials as part of its routine operations and could potentially pose a hazard to nearby sensitive receptors under accident or upset conditions. The proposed project is not anticipated to result in a substantial release of hazardous materials into the environment. The subject site is not within a hazardous Waste/Border Zone, the project site does not contain any oil or gas wells, but the subject site is identified as within a Methane Buffer Zone (ZIMAS). The project is required to comply with the City Ordinance no 175790 and 180619 that require preparation of a methane assessment prior to redevelopment of a site located within a methane zone and implementation of the appropriate Methane Mitigation Standards, outlined by the City's Department of Building and Safety. Section 91.7102 of the Municipal Code requires all multiple residential buildings shall have adequate ventilation of a gas-detection system installed in the basement or on the lowest floor level on grade, and within the under-floor space in buildings with raised foundations. Compliance with these City standards would ensure that project impacts related to methane would be less than significant. The existing multi-family residential building and retail store on the project site have not been surveyed for asbestos-containing materials (ACMs) and lead-based paint (LBP). Demolition of these buildings would have the potential to release asbestos fibers into the atmosphere if such materials exist and they are not properly stabilized or removed prior to demolition activities. The removal of asbestos is regulated by SCAQMD Rule 1403; therefore, any asbestos found on-site would be required to be removed by a certified asbestos containment contractor in accordance with applicable regulations prior to demolition. Similarly, it is likely that lead-based paint is present in buildings constructed prior to 1979. Compliance with existing State laws regarding removal would be required. With this compliance, the proposed project would result in a less-than-significant impact</p>	

Impact?	Explanation	Mitigation Measures
c.	NO IMPACT	<p>related to asbestos and LBP.</p> <p>Construction activities have the potential to result in the release, emission, handling, and disposal of hazardous materials within one-quarter mile of an existing school. The closest public school to the project site is Nora Sterry Elementary School located at 1730 Corinth Ave. approximately 200 feet across Sawtelle Blvd. west of the project site. The proposed project would provide for a infill development that consists of residential use. This type of use would be expected to use and store very small amounts of hazardous materials, such as paints, solvents, cleaners, pesticides, etc. Nevertheless, all hazardous materials within the project site would be acquired, handled, used, stored, transported, and disposed of in accordance with all applicable federal, State, and local requirements to reduce impacts to less than significant.</p>
d.	NO IMPACT	<p>A significant impact would occur if the project site is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and would create a significant hazard to the public or the environment. The California Department of Toxic Substances Control (DTSC) maintains a database (EnviroStor) that provides access to detailed information on hazardous waste permitted sites and corrective action facilities, as well as existing site cleanup information. EnviroStor also provides information on investigation, cleanup, permitting, and/or corrective actions that are planned, being conducted, or have been completed under DTSC's oversight. The subject site is currently developed with a retail store and a multi-family building. A review of EnviroStor did not identify any records of hazardous waste facilities on the project site. Therefore, the proposed project would not be located on a site that is included on a list of hazardous materials sites or create a significant hazard to the public or the environment, and no impact would occur.</p>

Impact?	Explanation	Mitigation Measures
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e.	NO IMPACT	A significant project-related impact may occur if the proposed project were placed within a public airport land use plan area, or within two miles of a public airport, and subject to a safety hazard. The closest public airport to the project site is the Los Angeles International Airport (LAX) (approximately 6.5 miles). Therefore, the project site is not located within an airport land use plan or within two miles of a public airport or public use airport, no impact would occur.	
f.	NO IMPACT	The subject site is not located within the vicinity of a private airstrip (ZIMAS). The closest private airport is the Santa Monica private airstrip (approximately 1.75 miles). The project will not result in a safety hazard for people residing or working in the project site. The proposed project site is within an urbanized region, and in a neighborhood which is largely designated for residential and commercial uses. Therefore, no impacts are anticipated to occur.	
g.	LESS THAN SIGNIFICANT IMPACT	A project would have a significant impact to hazards and hazardous materials if the project involved possible interference with an emergency response plan or emergency evacuation plan. The proposed project of 48 dwelling units is not located on or near an adopted emergency response or evacuation plan. The nearest Disaster Route to the project site is Santa Monica Blvd. Development of the project site may require temporary and/or partial street closures due to construction activities. Nonetheless, while such closures may cause temporary inconvenience, they would not be expected to substantially interfere with emergency response or evacuation plans. The proposed project would not cause permanent alterations to vehicular circulation routes and patterns impede public access or travel upon public rights-of-way. Additionally, development of the proposed project would not adversely affect access on Santa Monica Boulevard either temporarily during construction or long-term during operation. Further, the proposed project will be required to meet all fire and safety requirements of the Department of Building and Safety, the Los Angeles Police Department, and the Los Angeles Fire Department. In summary, the	

Impact?	Explanation	Mitigation Measures
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		proposed project does not include permanent changes in public through-traffic flow or impair access to a route heavily relied on by emergency services, and the project is not expected to impair emergency evacuation or response. Therefore, impacts will be less than significant.	
h.	NO IMPACT	The subject site is not located in a Very High Fire Hazard Severity zone, Fire District No.1 (ZIMAS), Brush Fire Hazard area, or Inadequate Fire Hydrant Service area (Environmental and Public Facilities Maps 1996). The subject site is located in a developed and urbanized region, and therefore would not expose people and/or structures to a significant loss, injury, or death involving wildland fires. In addition, the proposed structure is not expected to increase the fire hazard in the project area and will be required to meet all fire safety requirements of the Department of Building and Safety and the Los Angeles Fire Department. Therefore, no impacts are anticipated to occur.	

IX. HYDROLOGY AND WATER QUALITY

a.	LESS THAN SIGNIFICANT IMPACT	A significant impact would occur if the proposed project discharges water that does not meet the quality standards of agencies which regulate surface water quality and water discharge into storm water drainage systems. A significant impact would also occur if the proposed project would not comply with all applicable regulations with regard to surface water quality as governed by the Los Angeles Regional Water Quality Control Board (LARWQCB). The proposed project includes development of a 48-unit residential building in an urban area and would not have any point-source discharges. As is typical of most non-industrial urban development, stormwater runoff from the proposed project has the potential to introduce small amounts of pollutants into the stormwater system. There is a potential impact during the proposed project's long term operations due to pollutants contained in water runoff from the site which may be carried into storm drains and discharged into the stormwater runoff control system. Pollutants would be associated with runoff from landscaped areas (pesticides and fertilizers) and paved surfaces (ordinary household	
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Impact?	Explanation	Mitigation Measures
	<p>cleaners). These include: oil, grease, metals, and hydrocarbons from streets, parking lots, and driveways, dirt from unpaved areas, herbicides, pesticides and fertilizer from landscaped areas and animal wastes. Construction-related activities of the proposed project also have the potential to contribute to pollutants in water runoff from the site. The proposed project would be required to comply with the National Pollutant Discharge Elimination System (NPDES) standards and the City's Stormwater and Urban Runoff Pollution Control Ordinance to ensure pollutant loads from the project site are minimized for downstream receiving waters. Since the project site will be covered with more than 80% impervious surfaces, the project falls within the second tier of LID requirements, which states that development projects that involve five or more units intended for residential uses and result in an alteration of at least 50% or more of the impervious surfaces on an existing developed site, the entire site must comply with the standards and requirements of Section 64.7, Article 4.4 of Chapter VI of the LAMC and with the Development Best Management Practices Handbook. The site shall be designed to manage and capture stormwater runoff to the maximum extent feasible utilizing various LID techniques, including but not limited to infiltration, evapotranspiration, capture for use, high efficiency bio-filtration and retention systems BMP (listed in priority order). If partial or complete onsite compliance of any type is technically infeasible, the project site and LID Plan shall be required to comply with all applicable SUSMP requirements in order to maximize onsite compliance. Therefore, operational water quality impacts would be less than significant.</p>	
b.	<p>LESS THAN SIGNIFICANT IMPACT</p> <p>A project would have a significant impact on groundwater level if it would change potable water levels sufficiently to reduce the ability of a water utility to use the groundwater basin for public water supplies, conjunctive use purposes, storage of imported water, summer/winter peaking, or respond to emergencies and drought; or reduce yields of adjacent wells or well fields; or adversely change the rate or direction of flow of groundwater; or</p>	

Impact?	Explanation	Mitigation Measures
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		<p>result in demonstrable and sustained reduction in groundwater recharge capacity. The proposed project involves the construction of a 48-unit residential building in a heavily developed and urbanized region. The subject site does not contain any natural and/or significant drainage features, such as streams or rivers. The proposed project would not require the use of groundwater at the project site. Potable water would be supplied by the Los Angeles Department of Water and Power (LADWP), which draws its water supplies from distant sources for which it conducts its own assessment and mitigation of potential environmental impacts. Therefore, the project would not require direct additions or withdrawals of groundwater. The project site is composed of more than 80% impervious surfaces. As such, surface water runoff from the project site is directed to adjacent storm drains. This area of Los Angeles is not known to have a high groundwater table. Historically and per the Seismic Hazard Zone Report for the Beverly Hills 7.5-Minute Quadrangle, 1998, highest groundwater in this area of Los Angeles is estimated to be more than 30 feet below the ground surface. Because the depth of groundwater is sufficiently lower than the depth of proposed excavation, a permanent dewatering system is not required. Therefore, construction of the proposed project would not deplete groundwater supplies or interfere substantially with groundwater recharge. No impact to the groundwater table would occur.</p>	
c.	LESS THAN SIGNIFICANT IMPACT	<p>A project would have a significant impact on surface water hydrology if it would result in a permanent, adverse change to the movement of surface water sufficient to produce a substantial change in the current or direction of water flow. The project site is located in a highly urbanized area of Los Angeles, and no streams or river courses are located on or within the project vicinity. The project site is mostly impervious with some landscaped areas. Implementation of the proposed project would not increase site runoff or result any changes in the local drainage patterns. Implementation of the SWPPP and LID Plan, however, as required pursuant to Section 64.70 of the LAMC, would reduce the amount of</p>	

Impact?	Explanation	Mitigation Measures
	<p>surface water runoff after storm events, as the proposed project would be required to implement stormwater BMPs to retain or treat the runoff from a storm event producing $\frac{3}{4}$ inch of rainfall in a 24-hour period. Therefore, any potential impacts will be mitigated to a level of insignificance related to surface water hydrology or result in substantial erosion or siltation on- or off-site.</p>	
<p>d. LESS THAN SIGNIFICANT IMPACT</p>	<p>A project would have a significant impact on surface water hydrology if it would result in a permanent, adverse change to the movement of surface water sufficient to produce a substantial change in the current or direction of water flow. The proposed project would not result in a significant increase in site runoff, or any changes in the local drainage patterns. The subject site is in an urbanized area. Water runoff would drain into the existing storm drain system. The project site is currently developed and stormwater runoff is directed to the adjacent stormwater infrastructure serving the greater project area. The project site will be covered with more than 80% impervious surfaces. As such, the rate of surface water runoff under the proposed project would not increase as compared to existing conditions. The proposed project will comply with the City's stormwater management provisions per LAMC. Section 64.70, including implementation of the Best Management Practices therein, and therefore, impacts related to flooding on- and off-site will be reduced to a less-than-significant level.</p>	
<p>e. LESS THAN SIGNIFICANT IMPACT</p>	<p>A project would have a significant impact on surface water quality if discharges associated with the project would create pollution, contamination, or nuisance as defined in Section 13050 of the California Water Code (CWC) or that cause regulatory standards to be violated, as defined in the applicable National Pollution Discharge Elimination System (NPDES) stormwater permit or Water Quality Control Plan for the receiving water body. For the purpose of this specific issue, a significant impact may occur if the volume of storm water runoff from the project site were to increase to a level which exceeds the capacity of the storm drain system serving the project site. A significant adverse effect would</p>	

Impact?	Explanation	Mitigation Measures
	<p>also occur if a project substantially increases the probability that polluted runoff would reach the storm drain system. The project site is covered with more than 80% impervious surfaces. All surface water is directed off site to the adjacent storm drain system on Sawtelle Boulevard, as illustrated on the Figure 1 of the Los Angeles Bureau of Sanitation, Wastewater Engineering Services Division (WESD) letter dated September 2, 2014 . Pursuant to local practice and Section 64.7 of the LAMC, storm water retention will be required as part of the LID/SUSMP implementation features (despite no increased imperviousness of the site). Any contaminants gathered during routine cleaning of construction equipment would be disposed of in compliance with applicable stormwater pollution prevention permits. Further, any pollutants from the parking areas would be subject to the requirements and regulations of the NPDES and applicable Stormwater LID Ordinance. The proposed project will be required to demonstrate compliance with Stormwater LID Ordinance standards and retain or treat the first ¼ inch of rainfall in a 24-hour period, which will reduce the proposed project's impact to the stormwater infrastructure. Therefore, with implementation of the Los Angeles Bureau of Sanitation, Wastewater Engineering Services Division (WESD) letter requirements dated September 2, 2014, short-term construction-related impacts and long-term operational impacts related to the amount of runoff and the capacity of the storm drain system will be reduced to a less-than-significant level.</p>	
f. NO IMPACT	<p>A significant impact may occur if a project includes potential sources of water pollutants that would have the potential to substantially degrade water quality. The proposed project does not include potential sources of contaminants, which could potentially degrade water quality and would comply with all federal, state and local regulations governing stormwater discharge. Therefore, impacts would be less than significant.</p>	

Impact?	Explanation	Mitigation Measures
NO IMPACT	A significant impact may occur if the project was located within a 100-year flood zone, which would impede or redirect flood flows. The project site is not in an area designated as a 100-year flood hazard area. The project site is located in a highly urbanized area and, as no changes to the local drainage pattern would occur with implementation of the proposed project, the proposed project would not have the potential to impede or redirect floodwater flows. No impact would occur.	
NO IMPACT	A significant impact may occur if the project was located within a 100-year flood zone, which would impede or redirect flood flows. The project site is not in an area designated as a 100-year flood hazard area. The project site is located in a highly urbanized area and, as no changes to the local drainage pattern would occur with implementation of the proposed project, the proposed project would not have the potential to impede or redirect floodwater flows. No impact would occur.	
NO IMPACT	A significant impact may occur if the proposed project exposes people or structures to a significant risk of loss or death caused by the failure of a levee or dam, including but not limited to a seismically-induced seiche. Seiches are large waves generated in very large enclosed bodies of water or partially enclosed arms of the sea in response to ground shaking. Tsunamis are waves generated in large bodies of water by fault displacement or major ground movement. Based on the lack of such large enclosed water bodies nearby, seiches and tsunami risks are considered nil. Thus, the proposed project would not expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam and no impact would occur.	
NO IMPACT	A significant impact would occur if the project site is sufficiently close to the ocean or other water body to be potentially at risk of the effects of seismically-induced tidal phenomena (i.e., seiche and tsunami), or if the project site is located adjacent to a hillside area with soil characteristics that would indicate potential susceptibility to mudslides or	

Impact?	Explanation	Mitigation Measures
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mudflows. The proposed project site is not located in a potential seiche or tsunami zone. The project site is relatively flat. There are no adjacent bodies of water, and the site is not located within a State Earthquake Induced Landslide Zone. Therefore, the project site is not subject to slope instability, tsunamis, and seiches. Due to the relatively level topography and developed properties within the project area, the potential for mudflow to impact the project site is relatively low. Therefore, no impact would occur.

X. LAND USE AND PLANNING

a. NO IMPACT

A significant impact would occur if the proposed project would be sufficiently large or configured in such a way so as to create a physical barrier within an established community. A physical division of an established community is caused by an impediment to through travel or a physical barrier, such as a new freeway with limited access between neighborhoods on either side of the freeway, or major street closures. The proposed project site is located within an urbanized area of the West Los Angeles community and is consistent with the existing physical arrangement of the properties within the vicinity of the site. The proposed project would result in the demolition of a retail store and a multi-family building and the construction of 42 new multi-family residential units. Because the project site is already developed with residential land uses, no separation of land uses or disruption of access between land use types would occur as a result of the proposed project. Accordingly, implementation of the proposed project would not disrupt or divide the physical arrangement of the established community, and no impact would occur. The proposed project would not involve any street vacation or closure or result in development of new thoroughfares or highways. The project involves construction of a 42,152 square foot building with 42 multi-family dwelling units in an area that is already developed with similar uses and the associated infrastructure. The project site is located in a highly urbanized area of the City. The site is surrounded by existing multi-family and single-family residential and

Impact?	Explanation	Mitigation Measures
	commercial uses and roadways. The project will not divide an established community. The Applicant is requesting approval of a zone change from C2-1VL to RAS4. With the approval of the requested zone change, the proposed project still will be consistent with the zoning and general plan designation of the project site and vicinity. Therefore, no impacts related to this issue would occur.	
b. POTENTIALLY SIGNIFICANT UNLESS MITIGATION INCORPORATED	<p>The project site is currently zoned C2-1VL with the land use designation of Neighborhood Commercial per the West Los Angeles Community Plan. The applicant is requesting approval of a zone change to RAS4. The C2-1VL designation indicates that the project site is located in Height District 1VL, which specifies a building height limit of 45 feet above grade, and limits Floor Area Ratio (FAR) on the project site to 1.5:1. As a result of zone change, the allowable building area increases from 15,612 (10,408 x 1.5) square feet to 31,224 (10,408 x 3) square feet. The applied zone RAS4-1VL has the same height restriction as C2-1VL, and both zones allow residential use. Also density is restricted to 400 square feet per dwelling unit in both zones, but the FAR in RAS4-1VL zone increases to 3:1. The proposed project would be comprised of 42 residential dwelling units. The lot area (12,610 square feet), including half of the alleys (1,420 square feet) allows 35 units. In consideration of reserving 11% of the 35 units (four units) for very low-income tenants, which allows for a 35% increase in density to 48 units, two on-menu Affordable Housing Incentives are requested to permit a 35% increase in the floor area ratio (FAR), 4.0:1 in lieu of 3.0:1, permitting 42,152 square feet in lieu of 31224 square feet (after zone change), and increase in height permitting 56' in lieu of 45'. Therefore, the type of land uses proposed as part of the project (multi-family residential) and the number of dwelling units (48) are allowed as a "by-right" development at the project site and under existing zone. As a zone change case, the project would be consistent with the General Plan and be in conformity with</p>	X-60

Impact?	Explanation	Mitigation Measures
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		<p>public necessity, convenience, general welfare and good zoning practice. Vehicle parking shall be provided consistent with LAMC 12.22 A.25. The applicant has selected Density Bonus Parking Option 1, which permits one on-site parking space for each residential unit with one or fewer bedrooms; two on-site parking spaces for each residential unit with two to three bedrooms. Based upon the number and type of dwelling units proposed (74 efficiency units and 12 two-bedroom) at least 98 parking spaces shall be provided. However, per Bicycle Parking Ordinance no 182386, since the project has applied for a density bonus, up to 30% of the required automobile parking may be replaced. The project has provided 77 parking spaces and it has no conflict with applicable policy or regulation. Therefore, the proposed project would conform to the allowable land uses pursuant to the Los Angeles Municipal Code. The project site is within 500 feet of the 405 Freeway. Adverse impacts on future occupants may result due to the existing air pollution levels in the vicinity. Installation of an air filtration system will reduce impacts level to less than significant.</p>	
c.	NO IMPACT	<p>A project-related significant adverse effect could occur if the project site were located within an area governed by a habitat conservation plan or natural community conservation plan. As discussed in Section IV.f above, no such plans presently exist which govern any portion of the project site. Further, the project site is located in an area, which is already fully developed with residential and commercial uses, and is also within a heavily urbanized area of Los Angeles and therefore, no impact would occur.</p>	
XI. MINERAL RESOURCES			
a.	NO IMPACT	<p>A significant impact may occur if the project site is located in an area used or available for extraction of a regionally-important mineral resource, or if the development would convert an existing or future regionally-important mineral extraction use to another use, or if the development would affect access to a site used or potentially available for regionally-important mineral resource</p>	

Impact?	Explanation	Mitigation Measures
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		<p>extraction. The project site is located in an urban developed setting, and is therefore not likely to be a suitable site for mining of any sort, surface or otherwise. There is no knowledge of the presence of mineral resources that would be of value to the region and residents of the State on the project site; subsequently, the project is not expected to result in the loss of availability of said mineral resources. The project site is not located in a known Oil Field (Navigate LA) or in a known area of mineral resources (Environmental and Public Facilities Maps 1996). Therefore, no impact associated with the loss of availability of a known mineral resource would occur.</p>	
b.	NO IMPACT	<p>A significant impact may occur if the project site is located in an area used or available for extraction of a regionally-important mineral resource, or if the development would convert an existing or future regionally-important mineral extraction use to another use, or if the development would affect access to a site used or potentially available for regionally-important mineral resource extraction. The project site is not located within a Mineral Resource Zone 2 (MRZ-2) Area. As stated above, the project site is not located within an Oil Drilling District. Therefore, no impact associated with the loss of availability of a known mineral resource would occur.</p>	

XII. NOISE

a.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if the proposed project would generate excess noise that would cause the ambient noise environment at the project site to exceed noise level standards set forth in the City of Los Angeles General Plan Noise Element (Noise Element) and the City of Los Angeles Noise Ordinance (Noise Ordinance). The City of Los Angeles has established policies and regulations concerning the generation and control of noise that could adversely affect its citizens and noise-sensitive land uses. The noise sensitive uses within 500 feet of the project include residences, Nora Sterry Elementary School, West Los Angeles Regional Library, and Felicia Mahood Senior Citizen Center. The proposed project involves the construction of 48 residential dwelling units. Implementation of the proposed</p>	
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Impact?	Explanation	Mitigation Measures
	<p>project would result in an increase in ambient noise levels during both construction and operation, however, it is unlikely to result in new sources of noise in which would exceed levels established in the General Plan or Municipal Code. On-site operational noise would be generated by heating, ventilation, and air conditioning equipment installed in the new development. However, any on-site stationary sources of noise are required to comply with the LAMC Section 112.02 which prohibits noise from this type of equipment from exceeding the ambient noise level on the premises of other occupied properties by more than five decibels. A temporary increase in noise levels is expected to occur during the construction phase, due to the heavy construction equipment and related construction activity, and could be audible to those sensitive uses closest to the project site. The construction activities on the proposed site are expected to be short-term. Chapter IV, Article 1, Section 41.40 of the LAMC establishes permitted construction/demolition hours to 7 AM – 9 PM, Monday through Friday, 8 AM – 6 PM on Saturdays or national holidays, and no work is permitted on Sundays. Therefore, the corresponding noise will be minimized, and should reduce any potentially significant impacts to less than significant. Impacts due to noise from vehicular traffic would be less-than-significant.</p>	
b. LESS THAN SIGNIFICANT IMPACT	<p>Groundborne vibration and noise levels in residential land uses are lower than those found in commercial or industrial land uses, and are unlikely to exceed existing levels and levels established in the General Plan or LAMC. Construction activities can generate varying degrees of vibration, depending on the construction procedures and the type of construction equipment used. Therefore, the proposed project is expected to create a temporary increase in groundborne vibration and/or groundborne noise during the construction phase, due to the heavy construction equipment and related construction activity, and could be felt/audible to the closest residents and other noise sensitive uses to the project site. Nonetheless, the duration of construction activities on the proposed site is expected to be short-term. The LAMC limits</p>	

Impact?	Explanation	Mitigation Measures
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		construction hours, therefore construction of the project will be typical of residential structures and impacts from excessive groundborne vibration and noise levels are anticipated to be less than significant.	
c.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if the proposed project were to result in a substantial permanent increase in ambient noise levels above existing ambient noise levels without the proposed project. According to the City of Los Angeles, CEQA Thresholds Guide, a project would have a significant impact on noise levels from project operations if the project causes the ambient noise level measured at the property line of the affected uses to increase by three decibels (dBA) in Community Noise Exposure Level (CNEL) to or within the normally "unacceptable" or "clearly unacceptable" category, or any five dBA or greater noise increase. The proposed project involves the construction of 48 residential units and roof-decks. The project will result in a permanent, if minimal increase in ambient noise levels. The project will comply with Noise Ordinance Nos. 144,331 and 161,574, which prohibit the emission or creation of noise beyond certain levels at adjacent uses. New stationary sources of noise, such as rooftop mechanical HVAC equipment would be installed on the proposed development. The design of the equipment will be required to comply with LAMC Section 112.02, which prohibits noise from air conditioning, refrigeration, heating, pumping, and filtering equipment from exceeding the ambient noise level on the premises of other occupied properties by more than five dBA. The anticipated increase does not meet or exceed guidelines or thresholds of significance for adverse impact: 75 or more dwelling units. With implementation of the current regulations that address rooftop mechanical equipment, a substantial permanent increase for nearby sensitive receptors would be reduced to a less than significant level.</p>	
d.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the project resulted in substantial temporary or periodic increase in ambient noise levels. A temporary increase in noise levels is expected to occur during the construction phase, due to the heavy</p>	

Impact?	Explanation	Mitigation Measures
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		<p>construction equipment and related construction activity, and could be audible to the closest residents to the project site. The Los Angeles CEQA Thresholds Guide establishes the threshold for a significant impact of construction noise if construction activities lasting more than one day would increase the ambient noise levels by 10 dBA or more at any off-site noise-sensitive location. In addition, the Los Angeles CEQA Thresholds Guide also states that construction activities lasting more than ten days in a three-month period, which would increase ambient exterior noise levels by 5 dBA or more at a noise sensitive use, would also normally result in a significant impact. As discussed above, impacts are expected to be less than significant for construction noise and vibration, and operational noise and vibration and the duration of construction activities on the proposed site are expected to be short-term. Chapter IV, Article , Section 41.40 of the LAMC establishes permitted construction/demolition hours of 7 AM to 9 PM – Monday through Friday, 8 AM to 6 PM on Saturdays or National Holidays, and no work permitted on Sundays. Noise impacts related to construction will be less than significant.</p>	
e.	NO IMPACT	<p>A significant impact may occur if the proposed project were located within an airport land use plan and would introduce substantial new sources of noise or substantially add to existing sources of noise within or in the vicinity of the project site. There are no airports within a two-mile radius of the project site, and the project site is not within any airport land use plan or airport hazard zone (ZIMAS). The closest airport is the LAX international airport (approximately 6.5 miles). The proposed project would not expose people to excessive noise levels associated with airport uses. No impact would occur.</p>	
f.	NO IMPACT	<p>A significant impact would occur if the proposed project would expose people residing or working in the project area to excessive noise levels from a private airstrip. The proposed project is not within the vicinity of a private airstrip. The closest private airport is the Santa Monica Airport (approximately 1.7 miles).</p>	

Impact?	Explanation	Mitigation Measures
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	Accordingly, the proposed project would not expose people working or residing in the project area to excessive noise levels from a private airstrip. Therefore, no impact would occur.	
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XIII. POPULATION AND HOUSING

a.	LESS THAN SIGNIFICANT IMPACT	<p>A potentially significant impact would occur if the proposed project would induce substantial population growth that would not have otherwise occurred as rapidly or in as great a magnitude. The proposed project would result in the development of 48 residential units. Based on the community's household demographics (e.g., an average of 2.05 persons per household for West Los Angeles Community Plan Area), the construction of 39 additional residential dwelling units would result in an increase of approximately 80 new permanent residents in the area (Los Angeles Department of City Planning/Demographics Research Unit, Effective October 1, 2009). The proposed increase in housing units and population would not be substantial, and would not result in an adverse physical change in the environment; or introduce unplanned infrastructure that was not previously evaluated in the adopted Community Plan or General Plan. Therefore, impacts related to housing would be less than significant.</p>	
b.	LESS THAN SIGNIFICANT IMPACT	<p>A potentially significant impact would occur if the proposed project would result in the displacement of existing housing units, necessitating the construction of replacement housing elsewhere. The proposed project would consist of the development of 48 new apartments units land use on a site that is currently occupied by an approximately 78-year old, 1-story commercial retail building and an approximately 60-year old, two-story nine-unit apartment building. As such, displacement of existing housing would occur with the proposed project. The proposed project would increase the number of apartment units on the site by 39. Therefore, impacts associated with displacement would be less than significant.</p>	

Impact?	Explanation	Mitigation Measures
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c.	LESS THAN SIGNIFICANT IMPACT	A potentially significant impact would occur if the proposed project would result in the displacement of people, necessitating the construction of replacement housing elsewhere. The proposed project would consist of the development of 48 new apartments units on a site that is currently occupied by nine existing apartments units. As such, displacement of existing housing would occur with the proposed project. The proposed project would increase the number of apartment units on the site by 39. Therefore, impacts associated with displacement would be less than significant.	
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XIV. PUBLIC SERVICES

a.	LESS THAN SIGNIFICANT IMPACT	A significant impact would occur if the LAFD could not adequately serve the proposed project, necessitating a new or physically altered station. Pursuant to LAMC Section 57.09.07A, the maximum response distance between residential land uses and a LAFD fire station that houses an engine or truck company is 1.5 miles. The subject site is within the vicinity of Los Angeles Fire Department (LAFD), fire Station 37 (Engine and Truck Company service) located at 1090 Veteran Avenue approximately 1.3 miles northeast of the project site, which is below the 1.5-mile threshold Maximum Response Distance for engine or truck companies for neighborhood land uses identified in the L.A. CEQA Thresholds Guide. The proposed project would result in an increase of 39 units. The increased residential activity associated with the proposed project could increase the number of emergency calls and demand for LAFD fire and emergency services. To maintain the level of fire protection and emergency services at the time of the proposed project's build-out, the LAFD may require additional fire personnel and equipment. However, it is not anticipated that there would be a need to build a new or expand an existing fire station to serve the proposed project and maintain acceptable service ratios, response times, or other performance objectives for fire protection. By analyzing data from previous years and continuously monitoring current data regarding response times, types of incidents, and call frequencies, LAFD can shift	
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Impact?	Explanation	Mitigation Measures
	<p>resources to meet local demands for fire protection. The proposed project would neither create capacity or service level problems nor result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities in order to maintain acceptable service ratios, response times or other performance objectives for fire protection. Therefore, the proposed project would result in a less-than-significant impact.</p>	
<p>b. LESS THAN SIGNIFICANT IMPACT</p>	<p>A significant impact would occur if the Los Angeles Police Department (LAPD) could not adequately serve the proposed project, necessitating a new or physically altered station. The subject site is served by the Los Angeles Police Department, West Los Angeles Area Division located at 1663 Butler Ave. (approximately 0.3 miles west of the project site). The proposed project will be reviewed by the LAPD, and will comply with public safety requirements and policies. The proposed project would result in an increase of 39 units. However, the proposed project would incorporate security features to provide for the safety of on-site residents. These features would include sufficient lighting throughout the project site to ensure safety and visibility. Entryways, lobbies, and parking areas would also be well illuminated and designed to eliminate areas of concealment. In addition, prior to the issuance of a building permit, the LAPD would review the project plans to ensure that the design of the project follows the LAPD's Design Out Crime Program, an initiative that introduces the techniques of Crime Prevention Through Environmental Design (CPTED) to all City departments beyond the LAPD. Through the incorporation of these techniques into the project design, in combination with the safety features already incorporated into the proposed project, the proposed project would neither create capacity/service level problems nor result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities in order to maintain acceptable service ratios, response times or other performance objectives for police protection. Therefore, the proposed project would result in a less-than-significant impact related to</p>	

Impact?	Explanation	Mitigation Measures
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		police protection services.	
c.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the proposed project would include substantial employment or population growth, which could generate a demand for school facilities that would exceed the capacity of the school district. The proposed project would be primarily served by the Los Angeles Unified School District's (LAUSD) Nora Sterry Elementary School, which is located at 1730 Corinth Ave. (approximately 130 feet south of the project site), Emerson Middle School, which is located at 1650 Selby Ave. (approximately 1.1 mile northeast of the project site), and University Senior High School located at 11800 Texas Ave. (approximately 0.9 mile northwest of the project site). The proposed project would add 39 residential units and would generate a total of approximately 10 students, including 5 elementary students, 3 middle school students, and 2 high school students (Los Angeles Unified School District, Student Generation Rate Calculation, February 25, 2008). However, development of the proposed project would be subject to California Government Code Section 65995, which would allow LAUSD to collect impact fees from developers of new residential and commercial space. Conformance to California Government Code Section 65995 is deemed to provide full and complete mitigation of impacts to school facilities. Therefore, the proposed project would result in a less-than-significant impact to public schools.</p>	
d.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the proposed project would exceed the capacity or capability of the local park system to serve the proposed project. The City of Los Angeles Department of Recreation and Parks (RAP) is responsible for the provision, maintenance, and operation of public recreational and park facilities and services in the City. The project site is located within a highly urbanized area of the West Los Angeles community and is currently below the standard ratio. The proposed project would result in an increase of 39 units, which could result in increased demand for parks and recreation facilities. The proposed project would meet the minimum code</p>	

Impact?	Explanation	Mitigation Measures
	<p>requirements for open space as required by the LAMC. The proposed project will provide a minimum of 5,525 square feet of open space areas, including private open space balconies and roof deck. These project features would reduce the demand for park space created by the proposed project. In addition, payment of required impact fees by the proposed residential development within the City of Los Angeles per LAMC Sections 21.10.3(a)(1) (Dwelling Unit Construction Tax) and 12.33 (Dedication of Land or Payment for Park and Recreational Facilities as a Requirement for a Zone Change for a Multiple Residential Use) could offset some of the increased demand by helping fund new facilities, as well as the maintenance and/or expansion of existing facilities. Therefore, the proposed project would not create capacity or service level problems, or result in substantial physical impacts associated with the provision of new or altered parks facilities. Accordingly, the proposed project would result in a less-than-significant impact on park facilities.</p>	
e. LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the proposed project would result in substantial employment or population growth that could generate a demand for other public facilities, including libraries, which exceed the capacity available to serve the project site, necessitating new or physically altered public facilities, the construction of which would cause significant environmental impacts. The LA public library closest to the project site is West Los Angeles Regional Library, located at 11360 Santa Monica Blvd. (approximately 0.2 mi west of the project site). The proposed project would result in an increase of 39 units, which could result in increased demand for library services and resources of the LAPL System. While the increase in population as a result of the proposed project may create a demand for library services, the proposed project would not create substantial capacity or service level problems that would require the provision of new or physically altered library facilities in order to maintain an acceptable level of service for libraries. Therefore, the proposed project would result in a less-than-significant impact on library</p>	

Impact?	Explanation	Mitigation Measures
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services.

XV. RECREATION

a.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if a project includes substantial employment or population growth, which would increase the use of existing neighborhood and regional parks or other recreational facilities, such that substantial physical deterioration of the facility would occur or be accelerated. Based on the City of Los Angeles CEQA Thresholds Guide, the determination of whether the project results in a significant impact on recreation and parks shall be made considering the following factors: (a) the net population increase resulting from the project; (b) the demand for recreation and park services anticipated at the time of project buildout compared to the expected level of service available, considering, as applicable, scheduled improvements to recreation and park services (renovation, expansion, or addition) and the project's proportional contribution to the demand; and (c) whether the project includes features that would reduce the demand for park services (e.g., on-site recreation facilities, land dedication, or direct financial support to the Department of Recreation and Parks). The proposed project will provide a minimum of 5,525 square feet of open space areas, including private open space balconies and roof deck. The availability of these on-site recreation amenities and opportunities would serve to reduce the demand for off-site park services. Notwithstanding the availability of on-site open space areas, it is reasonable to assume that future occupants of the proposed project would utilize recreation and park facilities in the surrounding area. Westwood Recreation Center at 1350 South Sepulveda Blvd. is 0.7 mile northeast of the project site and Stoner Recreation Center, located at 1835 Stoner Ave. is approximately 0.8 mile southwest of the project site. The proposed project would not substantially increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated. Under the City's mandatory Dwelling Unit Construction Tax, which is collected prior to a</p>
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Impact?	Explanation	Mitigation Measures
	certificate of occupancy for residential land uses, the proposed project's impact upon parks and recreational facilities would be reduced to a less than significant level.	
b. LESS THAN SIGNIFICANT IMPACT	A significant impact would occur if the proposed project would necessitate construction of new recreational facilities, which would adversely impact the environment, or require the expansion or development of parks or other recreational facilities in order to maintain acceptable service ratios, or other performance objectives for parks. The proposed project would include approximately 5,588 square feet of project amenities, including private open space balconies and roof deck. As previously discussed above, the proposed project would not require the construction or expansion of recreational facilities beyond the limits of the project site. Although the proposed project would place some additional demands on park facilities, the increase in demand would be met through a combination of on-site amenities and existing parks in the project area. The proposed project's increased demands upon recreational facilities would not in and of itself result in the construction of a new park, which might have an adverse physical effect on the environment. Thus, impacts to park and recreational facilities would be less than significant.	

XVI. TRANSPORTATION/TRAFFIC

a. LESS THAN SIGNIFICANT IMPACT	A significant impact could occur if a project were to result in substantial increases in traffic volumes in the vicinity of the project such that the existing street capacity experiences a decrease in the existing volume to capacity ratios, or experiences increased traffic congestion exceeding the Los Angeles Department of Transportation's (LADOT) recommended level of service. The project would develop 48 dwelling units on the project site. According to the Los Angeles Department of Transportation (LADOT) Referral Form, dated November 25, 2014, the proposed 48 residential units project is expected to generate a net increase of 154 daily trips, a net increase of 16 A.M. peak hour trips, and a net increase of 7 P.M. peak hour trips. The LADOT has determined that the proposed project will	
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Impact?	Explanation	Mitigation Measures
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		<p>not have significant conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system. Therefore, although the proposed project will result in an increase in traffic, the increase is expected to have a less than significant impact on the existing load and capacity of the street system.</p>	
b.	LESS THAN SIGNIFICANT IMPACT	<p>The Congestion Management program (CMP) is a State-mandated program that serves as the monitoring and analytical basis for transportation funding decisions in the County made through the Regional Transportation Improvement Program (RTIP) and State Transportation Improvement Program (STIP) processes. The CMP requires that a Traffic Impact Analysis (TIA) be performed for all CMP arterial monitoring intersections where a project would add 50 or more trips during either the morning or afternoon weekday peak hours and all mainline freeway monitoring locations where a project would add 150 or more trips (in either direction) during the morning or afternoon weekday peak hours. According to the LADOT referral Form dated November 25 2014, 2014, the proposed 48 residential units project is expected to generate a net increase of 154 daily trips, a net increase of 16 A.M. peak hour trips, and a net increase of 7 P.M. peak hour trips. therefore, the project will not add 50 or more trips to the CPM intersections. The project peak hour trips generation is also below the freeway threshold of 150 directional trips. Therefore, no CMP analysis is required and the proposed project impacts on congestion management programs is less than significant.</p>	
c.	NO IMPACT	<p>A significant impact may occur if the proposed project involved an aviation-related use or would influence changes to existing flight paths. The proposed project is not located within an airport hazard zone (ZIMAS), nor an airport land use plan, or within two miles of a public airport, or public use airport. The closest public airport is Los Angeles International Airport (approximately 6.5 miles). The proposed project does not include any aviation-related uses and would have no airport impact. It would also not require any modification of flight</p>	

Impact?	Explanation	Mitigation Measures
	paths for the existing airports in the Los Angeles Basin. Therefore, no impact would occur.	
d. LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if the proposed project includes new roadway design or introduces a new land use or features into an area with specific transportation requirements and characteristics that have not been previously experienced in that area, or if a project site access or other features were designed in such a way as to create hazard condition. The proposed project would not include unusual or hazardous design features. Access to the project's parking garage would be provided via a two-way driveway located at eastside alley along the southeast property corner. The applicant is required to consult with DOT for driveway location and specifications prior to the commencement of any architectural plans and final DOT approval shall be obtained prior to issuance of any building permits. With proper site planning and implementation of the infrastructural deficiencies adjustments of 2-foot dedication/improvement to Sawtelle Blvd., 5-foot dedication to the north alley and 2.5-foot dedication to the east alley, per Bureau of Engineering letter, dated September 24, 2014, associated impacts will be mitigated to a less than significant level.</p>	
e. NO IMPACT	<p>A significant impact may occur if the project design would not provide emergency access meeting the requirements of the Los Angeles Fire Department (LAFD), or in any other way threatened the ability of emergency vehicles to access and serve the project site or adjacent uses. The proposed project is not located on or near an adopted emergency response or evacuation plan. The proposed project involves development of a 48 dwelling units. The subject site is on Sawtelle Blvd., which is designated in the Streets and Highways Element of the City's General Plan and the West Los Angeles Community Plan, as a Collector Street and has not been adopted as an emergency response or evacuation plan. Santa Monica Blvd. at the north and Olympic Blvd. at the south of the project site are both designated as Disaster</p>	

Impact?	Explanation	Mitigation Measures
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		<p>Routes. The 405 Freeway at the east of the project site is also designated as the Freeway Disaster Route. Sawtelle Blvd. as a designated Collector Street, carries a half right-of-way 32-foot and two alleys in north and east of the project site a 10-foot half right-of-way. The project is required to improves all portions of Sawtelle Blvd., alleys, and alleys' intersection cut along the project frontage that currently do not meet this standard, according to the Bureau of Engineering letter dated September 24, 2014. Development of the project site may require temporary and/or partial street closures due to construction activities. Any such closures would be temporary in nature and would be coordinated with the Departments of Transportation, Building and Safety, and Public Works. Nonetheless, while such closures may cause temporary inconvenience, they would not be expected to substantially interfere with emergency response or evacuation plans. Therefore, the proposed project would not cause permanent alterations to vehicular circulation routes and patterns, impede public access, or travel upon public rights-of-way. The proposed project would be subject to the plan review requirements of the LAFD and the Los Angeles Police Department (LAPD) to ensure that all access roads, driveways and parking areas would remain accessible to emergency service vehicles. The proposed project would satisfy the emergency response requirements of the LAFD. Furthermore, as discussed above, there are no hazardous design features included in the access design or site plan for the proposed project that could impede emergency access. Therefore, impacts to emergency access will be less than significant level.</p>	
f.	NO IMPACT	<p>A significant impact may occur if the proposed project would conflict with adopted policies or involve modification of existing alternative transportation facilities located on- or off-site. The proposed project would not require the disruption of public transportation routes. Furthermore, the proposed project would not interfere with any class I or class II bikeway systems. Since the proposed project would not modify or conflict with any alternative transportation policies, plans or programs, there will be no impacts.</p>	

Impact?	Explanation	Mitigation Measures
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XVII. UTILITIES AND SERVICE SYSTEMS

a.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact would occur if the proposed project would exceed wastewater treatment requirements of the (Los Angeles Regional Water Quality Control Board). A significant impact would also occur if the proposed project would increase water consumption or wastewater generation to such a degree that the capacity of facilities currently serving the project site would be exceeded. The project site is located within the service area of the Hyperion Treatment Plant (HTP), which experiences an average daily flow of 362 mgd, below a capacity of 450 mgd. According to the Los Angeles Bureau of Sanitation, Wastewater Engineering Services Division (WESD) letter dated September 2, 2014, the proposed project would generate approximately 5,610 gallons (or 0.00561 mgd) of wastewater per day. As a proportion of total average daily flow experienced by the HTP, the wastewater generation of the proposed project would account for a small percentage of average daily wastewater flow. This increase in wastewater flow would not jeopardize the HTP to operate within its established wastewater treatment requirements. WESD has evaluated the local sewer condition at the project area and has determined that the sewer system might be able to accommodate the total flow for the proposed project. WESD will determine the sewer connection point as part of the permit process, and the project will build sewer lines to a point in the sewer system with sufficient capacity. Therefore, with implementation of the WESD requirements, the proposed project would result in a less-than-significant impact related to wastewater treatment requirements.</p>	
b.	LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if a project would increase water consumption or wastewater generation to such a degree that the capacity of facilities currently serving the project site would be exceeded. Prior to any construction activities, the project applicant would be required to coordinate with the City of Los Angeles Bureau of Sanitation (BOS) to determine the exact wastewater conveyance requirements of the proposed</p>	

Impact?	Explanation	Mitigation Measures
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	<p>project, and any upgrades to the wastewater lines in the vicinity of the project site that are needed to adequately serve the proposed project would be undertaken as part of the project. Therefore, impacts related to wastewater treatment would be less than significant. The proposed project would use up to approximately 5,544 gpd of water more than existing buildings. The estimated water demand for the proposed project is conservative and provides a worst-case scenario, since it does not take into account reductions from inclusion of these water conservation features. Features, such as drought tolerant landscaping, high-efficiency toilets, and "smart" irrigation controllers could result in a reduction in potable water consumption by at least 20 percent and landscaping water demand by at least 50 percent. LADWP conducts water planning based on forecast population growth. Accordingly, the increase in residential population resulting from the proposed project would not be considered substantial in consideration of anticipated growth. The new 80 residents as a result of the proposed project would be consistent with projected Citywide growth, and, therefore, the project demand for water is not anticipated to require new water supply entitlements and/or require the expansion of existing or construction of new water treatment facilities beyond those already considered in the 2010 Urban Water Management Plan. Thus, it is anticipated that the proposed project would not create any water system capacity issues, and there would be sufficient reliable water supplies available to meet project demands. Therefore, the proposed project would have a less-than-significant operational impact related to water supply and infrastructure.</p>	
c.	<p>LESS THAN SIGNIFICANT IMPACT</p> <p>A significant impact would occur if the proposed project would increase surface water runoff, resulting in the need for expanded off-site storm water drainage facilities. Development of the proposed project would maintain existing drainage patterns; site-generated surface water runoff would continue to flow to the City's storm drain system. Since the project site is almost entirely impervious, impermeable surfaces resulting from the</p>	

Impact?	Explanation	Mitigation Measures
	<p>development of the project would not significantly change the volume of storm water runoff. Accordingly, since the volume of runoff from the site would not measurably increase over existing conditions, water runoff after development would not exceed the capacity of existing or planned drainage systems. With implementation of the Los Angeles Bureau of Sanitation, Wastewater Engineering Services Division (WESD) letter requirements, dated September 2, 2014, the proposed project would not create or contribute runoff water that would exacerbate any existing deficiencies in the storm drain system or provide substantial additional sources of polluted runoff. Therefore, the proposed project would result in a less-than-significant impact related to existing storm drain capacities.</p>	
d. LESS THAN SIGNIFICANT IMPACT	<p>A significant impact may occur if a project would increase water consumption to such a degree that new water sources would need to be identified. Based on the City of Los Angeles CEQA Thresholds Guide, the determination of whether the project results in a significant impact on water shall be made considering the following factors; (a) the total estimated water demand for the project; (b) whether sufficient capacity exists in the water infrastructure that would serve the project, taking into account the anticipated conditions at project build-out; (c) the amount by which the project would cause the projected growth in population, housing or employment for the Community Plan area to be exceeded in the year of the project completion; and (d) the degree to which scheduled water infrastructure improvements or project design features would reduce or offset service impacts. The proposed project is the development of 48 dwelling units and the water demand would be approximately 6,732 gallons per day and comparing to existing buildings 5,544 gallons extra per day (water consumption is assumed to be 120% of waste water generation). As concluded above, the proposed project would have a less-than-significant impact on water demand. In addition, the project is required to comply with LAGreen code that requires 20 percent reduction in indoor the building water use and 2014</p>	

Impact?	Explanation	Mitigation Measures
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		amendment that requires 20 percent reduction in wastewater generation, and would further reduce impacts associated with this issue to a level that is less than significant. Impacts to water supplies will be less than significant.	
e.	LESS THAN SIGNIFICANT IMPACT	Based upon the criteria established in the City of Los Angeles CEQA Thresholds Guide, a project would normally have a significant wastewater impact if: (a) the project would cause a measureable increase in wastewater flows to a point where, and a time when, a sewer's capacity is already constrained or that would cause a sewer's capacity to become constrained; or (b) the project's additional wastewater flows would substantially or incrementally exceed the future scheduled capacity of any one treatment plant by generating flows greater than those anticipated in the Wastewater Facilities Plan or General Plan and its Elements. As stated above in XVII b., the sewage flow will ultimately be conveyed to the Hyperion Treatment Plant, which has sufficient capacity for the proposed project. Also WESD, per the letter dated September 2, 2014, determines the local sewer system capacity to accommodate the total flow for the proposed project. Therefore, impacts would be less than significant.	
f.	LESS THAN SIGNIFICANT IMPACT	A significant impact may occur if a project were to increase solid waste generation to a degree such that the existing and projected landfill capacity would be insufficient to accommodate the additional solid waste. Within the City of Los Angeles, the Sunshine Canyon Landfill and the Chiquita Canyon Landfill serve existing land uses within the City. Both landfills accept residential, commercial, and construction waste. The proposed project's net generation during the life of the proposed project would be approximately 587 pounds per day (48 units x 12.23 lbs/ households/ day) according to the City of Los Angeles Bureau of Sanitation, Solid Waste Generation, 1981. This estimate is conservative, as it does not factor in any recycling or waste diversion programs. The proposed project's solid waste would be handled by private waste collection services. The amount of waste is minimal compared to daily capacities of nearby	

Impact?	Explanation	Mitigation Measures
	<p>recycling or disposal facilities and transfer stations and these modest amounts would be further reduced through source reduction and recycling programs (i.e., AB 939 and AB 341), further reducing the amount of solid waste to be disposed of at the landfills described above. Further, the proposed project would not conflict with solid waste policies or objectives that are required by law, statute, or regulation. Therefore, with implementation of recommendations contained in the WESD letter dated September 2, 2014, on Solid Resource Requirements, the impact would be less than significant.</p>	
g. NO IMPACT	<p>A significant impact may occur if a project would generate solid waste that was not disposed of in accordance with applicable regulations. Solid waste generated on-site by the proposed project would be disposed of in accordance with all applicable federal, state, and local regulations related to solid waste, such as the California Integrated Solid Waste Management (ISWM) Act (also known as AB 939). The amount of project-related waste disposed of at area landfills would be reduced through recycling and waste diversion programs implemented by the City, in compliance with the City's Solid Waste Management Policy Plan (CiSWMPP), which is the long-range solid waste management policy for the City, and the Source Reduction and Recycling Element (SRRE), which is the strategic action policy plan for diverting solid waste from landfills. The project would also comply with applicable regulatory measures, including the provisions of City of Los Angeles Ordinance No. 171,687 with regard to all new construction; the provision of permanent, clearly marked, durable, source sorted bins to facilitate the separation and deposit of recyclable materials; implementation of a demolition and construction debris recycling plan, with the explicit intent of requiring recycling during all phases of site preparation and building construction. Finally, the project should comply with the solid waste requirements in the Bureau of Sanitation letter, dated September 2, 2014. With the implementation of the regulatory measures, waste generated by the project would not significantly alter the projected timeline for landfills within the region to reach capacity. Therefore, since</p>	

Impact?	Explanation	Mitigation Measures
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the proposed project would comply with local, state, and federal regulations no impact with respect to these regulations would occur.

XVIII. MANDATORY FINDINGS OF SIGNIFICANCE

a.	LESS THAN SIGNIFICANT IMPACT	<p>The project proposes the construction of a 48 units multi-family dwelling. The project site is located in an urbanized region that is mostly segmented and lacks the continuity, which is consistent with those known to support any non-avian candidate, sensitive, or special-status species. Moreover, the subject site has not been identified as being a Significant Ecological Area (City of Los Angeles, Environmental and Public Facilities Map 1996). The subject site lacks any riparian habitat or other sensitive natural community, and does not contain any wetlands. The subject is not identified as being a site or an area of historical significance, therefore it is unlikely that the proposed project will have impacts on important examples of the major periods of California history. Also, the subject site is not in the vicinity of an Archaeological Survey Area and an Archaeological Site (City of Los Angeles, Environmental and Public Facilities Maps 1996, Prehistoric & Historic Archaeological Sites and Survey Areas Map). However, there may be a possibility for the discovery of unrecorded archaeological resources during the proposed grading activity. The mitigation measures proposed in Section V of this document are expected to reduce any potential impacts to a less than significant level. The subject site and vicinity are not identified as being located in a Vertebrate Paleontological Area (City of Los Angeles, Environmental and Public Facilities Maps 1996, Vertebrate Paleontological Resources Map). It is possible that site excavation could uncover paleontological resources. Compliance with the City's Standard Conditions of Approval will ensure that if any previously unknown paleontological resources are discovered during the excavation period of construction, such resources will be handled properly and reduce any potential impacts to a level that is less than significant.</p>
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Impact?	Explanation	Mitigation Measures
b.	<p>LESS THAN SIGNIFICANT IMPACT</p> <p>A significant impact may occur if a project, in conjunction with other related projects in the area of the project site, would result in impacts that are less than significant when viewed separately, but would be significant when viewed cumulatively. The project site is located in an urban setting which is developed primarily with multi-family, single-family and commercial buildings. The proposed project involves new construction of a 48-unit residential building. Any development activity which may occur is most likely to comply with all applicable federal, State, and City Regulations that would preclude significant cumulative impacts with regard to geology and soils, cultural resources, hazards and hazardous materials, hydrology and water quality, and transportation and traffic. Compliance with City regulations and mitigation measures contained herein would ensure that any cumulative impacts related to aesthetics and land use would be less than significant. Furthermore, an increase in area population resulting from the proposed project and other development activity in the area are anticipated to be within City and SCAG forecasts; therefore, less than significant cumulative impacts to population and housing are anticipated. Similarly, the demands on public services such as fire and police protection, schools, parks, recreation, and solid waste generation resulting from the proposed project and other development activity in the area are anticipated to be less than significant, as described herein. As service providers conduct on-going evaluations to ensure that facilities are adequate to service the forecasted growth of the community, cumulative impacts on utilities are concluded to be less than significant with the application of mitigation measures proposed in this report.</p>	
c.	<p>LESS THAN SIGNIFICANT IMPACT</p> <p>A significant impact may occur if the proposed project has the potential to result in significant impacts, as discussed in the preceding sections. All potential impacts of the proposed project have been identified, and mitigation measures have been prescribed, where applicable, to reduce all potential impacts to less-than-significant levels. Upon implementation of mitigation measures identified, the proposed project would not</p>	

Impact?	Explanation	Mitigation Measures
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	have the potential to result in substantial adverse impacts on human beings either directly or indirectly.	
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