

STUDIO CITY/CAHUENGA PASS

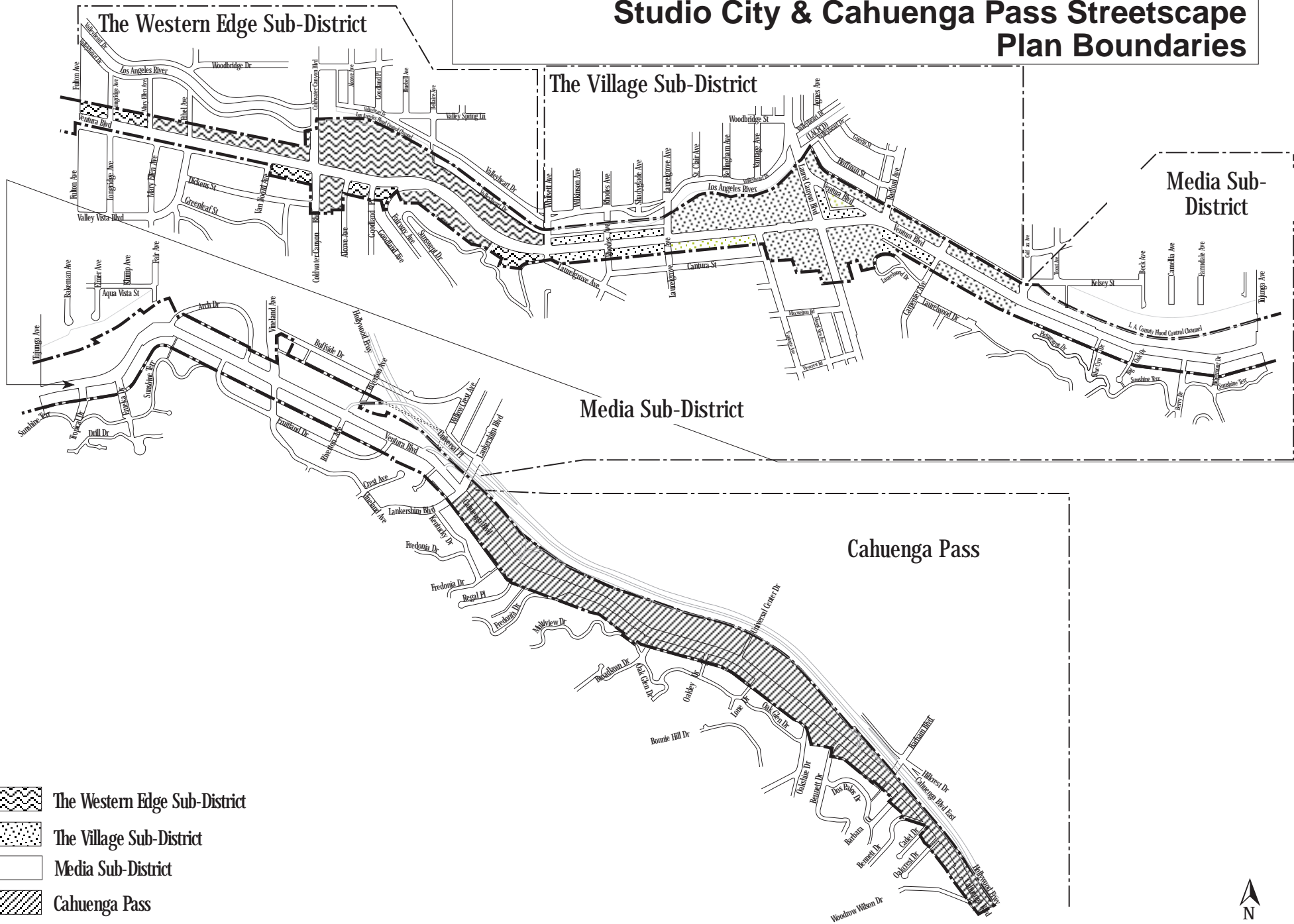
Streetscape Plan and Design Guidelines







TABLE OF CONTENTS

Section 1.	Introduction
Section 2.	Studio City and Cahuenga Pass Communities' Streetscape Plan
Section 3.	Cahuenga Pass (Woodrow Wilson Drive to Lankershim Boulevard)
Section 4.	Studio City (Lankershim Boulevard to Fulton Avenue)
Section 5.	Streetscape Elements and Improvements
Section 6.	Document Submittal-Projects on Public-Right-of-Way
Section 7.	On-Site Improvement Standards - Design Guidelines
Section 8.	Buffers
Section 9.	Maintenance
Section 10	Document Submittal Guidelines for On-site Projects
Section 11	Review Process
Section 12	Acknowledgments
Section 13	Appendix

Studio City & Cahuenga Pass Streetscape Plan Boundaries



-  The Western Edge Sub-District
-  The Village Sub-District
-  Media Sub-District
-  Cahuenga Pass



Not to Scale

TABLE OF CONTENTS

		<u>Page No.</u>
1.0	INTRODUCTION	1
	1.1 How to Use this Manual	1
	1.2 Background	1
	1.3 Goals	1
2.0	STUDIO CITY & CAHUENGA PASS COMMUNITIES' STREETScape PLAN	2
	2.1 Objectives	2
	2.2 General Themes	3
	2.2.1 The Predominant Street Tree	4
	2.3 Streetscape Districts	4
3.0	CAHUENGA PASS	4
	3.1 The Predominant Flowering Tree	4
	3.2 Planters	5
	3.3 Entry Signage	5
	3.4 Street Furniture	6
	3.5 Planted Medians	6
	3.6 The Mulholland Highway Bridge	7
	3.7 The Barham Bridge	7
4.0	STUDIO CITY	7
	4.1.1 The Predominant Ornamental Tree	7
	4.1.2 Planters	8
	4.1.3 Entry Signage	8
	4.1.4 Street Furniture	9
	4.1.5 Planted Medians	9
	4.2 The Media Sub-District	9
	4.2.1 The Predominant Flowering Tree	9
	4.2.2 Planted Medians	9
	4.2.3 Business Unification	10
	4.2.4 MetroRail Station	10
	4.2.5 Other Opportunities	10
	4.3 The Village Sub-District	11
	4.3.1 The Predominant Flowering Tree	13
	4.3.2 Planted Medians	13
	4.3.3 Other Opportunities	13
	4.4 The Western Edge Sub-District	14
	4.4.1 Planted Medians	14
	4.5 Off-Boulevard/Extended Streetscape Areas	14
	4.5.1 Major Cross Streets and Bridges	14
	4.5.2 The Los Angeles River	16

5.0	STREETSCAPE ELEMENTS AND IMPROVEMENTS	17
5.1	Landscaping	17
5.1.1	Soil Preparation	17
5.1.2	Trees	18
5.2	Lighting	18
5.2.1	Pedestrian Lighting	19
5.2.2	Roadway Lighting	19
5.2.3	Maintenance	20
5.3	Street Fixtures, Furniture and Equipment	20
5.3.1	Ash Urns	21
5.3.2	Benches	21
5.3.3	Bicycle Racks	22
5.3.4	Bollards	22
5.3.5	Bus Shelters and Bus Benches	22
5.3.6	Fountains	22
5.3.7	Kiosks and Monuments	22
5.3.8	Neighborhood Logos	22
5.3.9	Sculptures, Artwork, and Interpretive Monuments	23
5.3.10	Newspaper Racks	23
5.3.11	Pots/Planters	23
5.3.12	Telephones	24
5.3.13	Trash Receptacles	24
5.3.14	Tree Well Covers	25
5.3.15	Tree Wells	26
5.3.16	Vaults (Aboveground) and Utility Boxes	26
5.4	Flatwork Improvements	26
5.4.1	Interlocking Pavers	26
5.4.2	Curbs and Gutters	26
5.4.3	Flatwork, Concrete	26
5.4.4	Flatwork, Other	27
5.4.5	Sidewalks	27
5.4.6	Story Pavers	27
5.5	Infrastructure Improvements	28
5.5.1	Medians, Planted	28
5.6	Intersection Improvements	29
5.6.1	Major Intersections	29
5.6.2	Secondary Intersections	30
5.7	Street Banners and Flags	30
5.8	Medallions	30
6.0	DOCUMENT SUBMITTAL GUIDELINES - PUBLIC RIGHT-OF-WAY	31
6.1	Site Plans	32
6.2	Photographs	32
7.0	ON-SITE IMPROVEMENT STANDARDS - DESIGN GUIDELINES	33
7.1	Paint Colors/Surface Treatments	33
7.2	Detail and Ornamentation	33
7.3	Application of Glass	33

7.4	Screening Rooftop Utilities	34
7.5	Street Level Design	34
7.6	Multi-tenant Buildings	34
7.7	Arrangement and Form of Buildings	35
7.8	On-Site Signage	35
7.9	Flatwork Materials	36
7.10	Landscaping - On-site Trees	36
7.11	Lighting	36
7.12	Parking Lot/Structure Design	37
7.13	Utility Undergrounding	41
8.0	BUFFERS	41
8.1	Buffers Between Vehicular Use Areas and Residential Areas	41
8.2	Buffers Between Outdoor Uses	41
9.0	MAINTENANCE	42
9.1	Maintenance Guidelines	42
9.2	Cleaning	42
9.3	Irrigation	42
9.4	Street Tree Management	43
9.5	Pruning and Trimming	43
9.6	Repairs and Replacements	43
10.0	DOCUMENT SUBMITTAL GUIDELINES - ON-SITE PROJECTS	44
10.1	Site Plans	44
10.2	Floor Plans, Elevations and Sections	45
10.3	Photographs	45
10.4	Material Boards	45
10.5	Project Thresholds	45
11.0	REVIEW PROCESS	45
11.1	Shop Inspection	46
12.0	ACKNOWLEDGMENTS	46
13.0	APPENDIX	47
13.1	Other Relevant City Requirements	47
13.2	Background - Ventura/Cahuenga Boulevard Corridor Specific Plan	47
13.3	History	48

STUDIO CITY/CAHUENGA PASS STREETSCAPE PLAN AND DESIGN GUIDELINES

Section 1.

INTRODUCTION

1.1 How to use this manual

This Streetscape Plan conforms to the proviso in Section 14 of the Ventura/Cahuenga Boulevard Corridor Specific Plan for Studio City/Cahuenga Pass. City Departments and Applicants with streetscape projects should review the pertinent provisions of the Specific Plan and consult with the planner from the Department of City Planning who administers the Specific Plan.

The Streetscape Plan serves as a design guide for the Studio City and Cahuenga Pass communities, and goes on to identify the planting and hardscape features desired for the particular district in which a proposed project is to be located, as well as design guidelines for adjacent private projects.

1.2 Background

The Ventura/Cahuenga Boulevard Corridor Specific Plan contains a proviso for the establishment of streetscape and design guidelines that define and express the character of Studio City and the Cahuenga Pass portions of Ventura and Cahuenga Boulevards in a unified and attractive manner. Such guidelines are to be in the form of a Streetscape Plan and Design Guidelines which reflect the history of the communities and integrate well with the existing building inventory. This Streetscape Plan reflects the input from several public hearings, the contributions of volunteer architects and the advice of the Studio City and Cahuenga Pass Design Advisory Committees.

1.3 Goals of the Specific Plan Guiding the Streetscape Plan and Design Guidelines

- ? To promote the integration of signage, landscaping, and architectural design at the conceptual stage of all proposed private and public projects.
- ? To promote awareness that parking facilities are part of the commercial environment and to integrate their appearance with streetscape components.
- ? To provide building and site design guidelines which promote attractive and harmonious commercial and multi-family development.
- ? To preserve and enhance community aesthetics by establishing coordinated and comprehensive standards

for street hardware, signs, buffers, setbacks, lot coverage, and landscaping.

- ? To enhance landscaping by providing guidelines and a process for a coordinated landscaping program on public and private property.
- ? To promote an attractive pedestrian environment which will encourage pedestrian activity and reduce traffic congestion by regulating the placement of buildings and structures to accommodate outdoor dining and other ground level retail activity, as well as provide for attractive landscaping.
- ? To promote and enhance the distinct character of Studio City by establishing design guidelines and community development limitations.
- ? To improve the visual environment and insure compatibility with adjacent residential areas.
- ? To promote design components such as trees, planted median strips, street furniture and paving that give streets an identity.

Section 2.

STUDIO CITY & CAHUENGA PASS COMMUNITIES' STREETScape PLAN

2.1 Objectives

The objective of this Streetscape Plan is to establish guidelines that define and express the character of this portion of Ventura/Cahuenga Boulevard in a unified and attractive manner. The Plan recommends indigenous drought resistant riparian, and canyon landscaping materials. It references native Indian settlements, Spanish exploration, early dryland ranching, and later citrus and nut groves. The original 1930's decorative street lights are reintroduced, and Studio City's Mexican Fan Palms, planted in the 1970's through a special assessment, are reinforced and maintained as the forerunners to this Streetscape Plan. Various flowering trees will mark the different zones along this portion of the Boulevard. Site amenities will underscore the heritage of the area with planted terra-cotta colored pots, graceful benches, and bus shelter enclosures. Paving details and materials may vary to illustrate more specific ideas and intentions, as needed. Markings in the pavement, through the use of Story Pavers, can be used to describe where a pedestrian is standing along this incredible 5-mile portion of the Boulevard.

A variety of design features are to be combined to create of an environment attractive to pedestrian activity. Design features included in this Streetscape Plan are:

Street Trees. To provide shade and to create space that is designed to human scale, comfort and function. Trees have been selected which, with maturity, will grow tall enough to not interfere with commercial signage and which will need minimal pruning.

Lighting. To provide safety and security for pedestrians, to create community character, and to enhance community identity.

Street Furniture, Benches and Bus Shelters. To provide pedestrians, especially the disabled and elderly, with amenities that encourage window shopping and browsing in comfort, and to encourage more frequent and longer visits to the area. *This includes all structures and temporary elements placed by public agencies, non-profit agencies, and private parties on the public right-of-way.*

Color. To enhance the aesthetic appearance of street amenities by designating colors which produce a “greening” effect on the street environment. Streetscape projects funded by public monies shall be painted, to the extent possible, “Spring Street Green”. Streetscape projects acquired through private funding sources, shall be painted, to the extent possible, “Ivy Green.”

2.2 General Themes

? *The Cahuenga Pass neighborhood is a rustic hillside, primarily residential, community that extends along Cahuenga Boulevard from Woodrow Wilson Drive to Lankershim Boulevard.* The residents of this historic area wish to express the character of this portion of the Boulevard without creating an obvious theme. The existing landscape will be enhanced to help unify and reinforce the Boulevard.

? *Studio City is a hillside and flatland community nestled at the base of the Santa Monica Mountains.* The residents see Studio City as primarily a suburban community with country village-like qualities. They are aware of the rich studio history of this area and wish to see it reflected.

The Ventura/Cahuenga Boulevard "main street" is to be unified throughout this area by extending the existing Mexican Fan Palms, alternating them with a flowering/canopy tree, along the entire length. The Palms are emblems of the City and of the southwest. They will not only provide a visual presence, but will function as the principal means of unifying the Boulevard throughout Studio City and Cahuenga Pass.

2.2.1 The Predominant Street Tree

Common/Botanical Name	Features	Planting
Mexican Fan Palm <i>Washingtonia robusta</i>	Tall vertical tree to 100 feet	5 feet x 5 feet minimum cut out

2.3 Streetscape Districts

The Cahuenga Pass and Studio City communities along Ventura/Cahuenga Boulevard have been separated into two Districts, each of which is to have its own character, while maintaining an overall sense of unity. In addition, Studio City has been divided into three sub-districts:

Cahuenga Pass

? Cahuenga Pass -- Mixed Use Boulevard from Woodrow Wilson Drive at the east end, to Lankershim Boulevard at the west end.

Studio City- 3 Sub-Districts

? The Media Sub-District -- Mixed Use Boulevard, Regional Center, Transit Station, from Lankershim Boulevard on the east, to Colfax Avenue on the west.

? The Village Sub-District -- Community Commercial Center from Colfax Avenue on the east, to Whitsett Avenue on the west.

? The Western Edge Sub-District-- Mixed Use Boulevard from Whitsett Avenue on the east, to Fulton Avenue on the west.

Section 3.

CAHUENGA PASS (Woodrow Wilson Drive to Lankershim)

The Cahuenga Pass District, especially the Barham Bridge, is perceived as being the gateway from downtown Los Angeles to the San Fernando Valley.

3.1 The Predominant Flowering Tree

Common/Botanical Name	Features/Height	Planting
Pink Trumpet Tree <i>Tabebuia impetiginosa</i>	Compound leaves, semideciduous; pink late winter and spring flowers	24 inch box minimum; 5' x 5' minimum root area

Pink Trumpet Tree
Tabebuia impetiginosa



Pink Trumpets are of sufficient height to provide a sense of scale for the Boulevard. In addition, their pink flowers will be visually striking in the late winter, when the branches are bare, and spring.

Pink Trumpets will be alternated with the existing and new Mexican Fan Palms—one Palm, one Pink Trumpet planted at approximately twenty-five feet on center from each other. See Section 9.2 for detailed irrigation instructions.

3.2 Planters

Planters are to be provided. See Section 5.3.11 for information on planters. When Pink Trumpet Trees on the freeway ramps are installed, they should be placed in appropriately sized planters to match those specified in Section 5.3.11 with an appropriate drip irrigation system installed.

3.3 Entry Signage

A signage monument announcing entry into “Cahuenga Pass” should be placed at or near the intersection of Barham and Cahuenga Boulevards. There should also be another one at or near the intersection of Lankershim Boulevard and Ventura Boulevard marking the departure from Cahuenga Pass into

Studio City. The design of these signs is to be determined by the Design Advisory Committee with input from the community. Tall plants should not be planted in such a way as to overshadow or block these entry signs.

3.4 Street Furniture

Street furniture is to be provided. See Section 5.3 for information on street furniture.

3.5 Planted Medians

Planted medians are to be provided on the Boulevard in this district from Barham Boulevard to Lankershim Boulevard.

Medians are crucial to establishing a pedestrian-friendly, mixed-use Boulevard with a reasonable sense of scale. Medians will be placed in portions of both communities. The medians will be planted primarily with a selection of ornamental grasses that will blow gently in the breeze suggesting movement. These grasses will be augmented with compatible flowering accents and Pink Trumpet Trees, using California natives and Mediterranean plants wherever possible. The details and accents of the medians may vary to help highlight a special feature or opportunity zone along the Boulevard.

The *Accent/Flowering Plants* and *Tree* are:

Common Name	Botanical Name	Feature	Height
Fortnight Lily	<i>Dietes vegeta</i>	white	3' to 4'
Agapanthus	<i>Agapanthus orientalis</i>	blue-violet flower	2' to 3'
Pink Trumpet Tree	<i>Tabebuia impetiginosa</i>	Compound leaves, semideciduous; pink late winter and spring flowers	To 30 feet

The *Common/Consistent Plants* are:

Common Name	Botanical Name	Feature	Height
Agave	<i>Agave attenuata</i>	blue/grey-green	2' to 5'
Blue Oat Grass	<i>Helictotrichon</i>	bright blue leaf	3'
Deer Grass	<i>Muhlenbergia rigens</i>	grey-green leaf	3'

3.6 The Mulholland Highway Bridge

Under this bridge, on the west side of the freeway, is an excellent opportunity for a mural to match or compliment the existing one on the east side of the freeway.

3.7 The Barham Bridge

The Barham Bridge is the gateway from downtown Los Angeles to the San Fernando Valley. This stately 1940's bridge, which has been determined eligible for the National Register of Historic Places, is from the Public Works Administration era and could become a grand entrance into this area.

The Pink Trumpet Trees specified as street trees for this district could be grown in large planters along the length of the two off-ramps bordering the east and west sides of the Hollywood Freeway. The effect will be a series of tree heads marching up the incline. This should be coordinated with Caltrans.

One of the 1940's light standards on the bridge is missing and should be replaced with one of the same style to match the others on the bridge. The concrete railings along the freeway south of the bridge should be continued to replace chain link fencing now in place. The existing railing should be restored to its original design.

Section 4.

STUDIO CITY

Studio City is characterized by small retail shops, restaurants and extensive studio support businesses. Studio City is a mix of fine established residential neighborhoods: a "village" scale commercial area and movie/television industry dating back to the 1900's. The commercial area on Ventura Boulevard is divided into three sub-districts, but has a central streetscape theme for the entire community.

4.1.1 The Predominant Ornamental Tree

Common Name	Botanical Name	Features	Height	Planting
Chinese Pistache	<i>Pistache chinensis</i>	deciduous tree with red-orange autumn leaves	up to 40 feet	24" box minimum; 5' x 5' minimum root area



The Chinese Pistache Tree is a striking sight in the fall when its leaves change to red-orange, and gold. These trees will be interspersed among the existing and new Mexican Fan Palms. See Section 9.3 for detailed irrigation instructions.

The Chinese Pistache Tree will be alternated with the existing and new Mexican Fan Palms—one Palm, one Chinese Pistache Tree, and one Palm - planted approximately twenty-five feet on center.

4.1.2 Planters

Planters are to be provided in parts of certain districts, in accordance with Section 5.3.11.

4.1.3 Entry Signage

There should be a monument sign at or near the intersection of Lankershim Boulevard and Ventura Boulevard marking entry into "Studio City" and departure from Cahuenga Pass. The design of these signs is to be determined by the Design Advisory Committee with input from the community. One will be placed on the eastern-most median of this district. Tall plants should not be planted in such a way as to overshadow or block this sign.

4.1.4 Street Furniture

Street furniture is to be provided. See Section 5.3 for information on street furniture.

4.1.5 Planted Medians

Planted medians are to be provided within the individual sub-districts.

4.2 The Media Sub-District (Lankershim Boulevard to Colfax Avenue)

The theme for the Media Sub-District should reference the movie and television studios.

4.2.1 The Predominant Flowering Tree

The predominant flowering tree for the Media Sub-District is the Chinese Pistache (See Studio City - Section 4.1).

4.2.2 Planted Medians

Planted medians are to be provided on the Boulevard in this entire district.

The *Accent/Flowering Plants* and *Tree* are:

Common Name	Botanical Name	Feature	Height
Star Jasmine	<i>Trachelospermum jasminoides</i>	white, sweet scented flowers	1-2'
Orchid Rockrose	<i>Cistus purpureus</i>	orchid	4'
Germander	<i>Teucrium chamaedrys</i>	dark green leaves	12 "
Golden Trumpet	<i>Tabebuia chrysotricha</i>	yellow flower	40'

The *Common/Consistent Plants* are:

Common Name	Botanical Name	Future Leaf Color	Height
Agave	Agave attenuata	blue/grey-green	2'-5'
Blue Oat Grass	Helictotrichon sempervirens	bright blue	3'
Deer Grass	Muhlenbergia rigens	grey-green	3'

4.2.3 Business Unification

The various businesses in this district neither relate visually to each other nor are they sufficiently landscaped. Stark concrete walls, devoid of planted setbacks, abut the sidewalk. Many parking lots have no shade trees. Portions of this district lack sidewalks, and cars park where people would otherwise walk. This area needs sidewalks, street trees, and other amenities.

Businesses could create transitions from the sidewalk to their respective sites by introducing new/additional landscaping through planted setbacks and planter boxes. These transitions would help integrate these businesses with their surroundings, give them an air of permanence, and create a sense of welcome to shoppers.

Free-standing concrete walls in the area could be re-faced with glazed, ceramic tile, or could merely be cleaned and re-painted. Flowering or evergreen vines could be planted to cover the walls to help soften their austerity. Vines such as *Parthenocissus tricuspidata* (Boston Ivy) and *Ficus repens* (Creeping Fig) are recommended. Graffiti will not be a concern once these vines have matured.

4.2.4 MetroRail Station

The artist contracted by MTA to work on this station has chosen to portray themes of old California as the subject for decorative tiles and other accents. We hope to extend this work above ground and to tie it to the Boulevard. Details are to be determined and coordinated with MTA.

The freeway underpass at Lankershim adjacent to the MetroRail station is another opportunity for a mural.

4.2.5 Other Opportunities

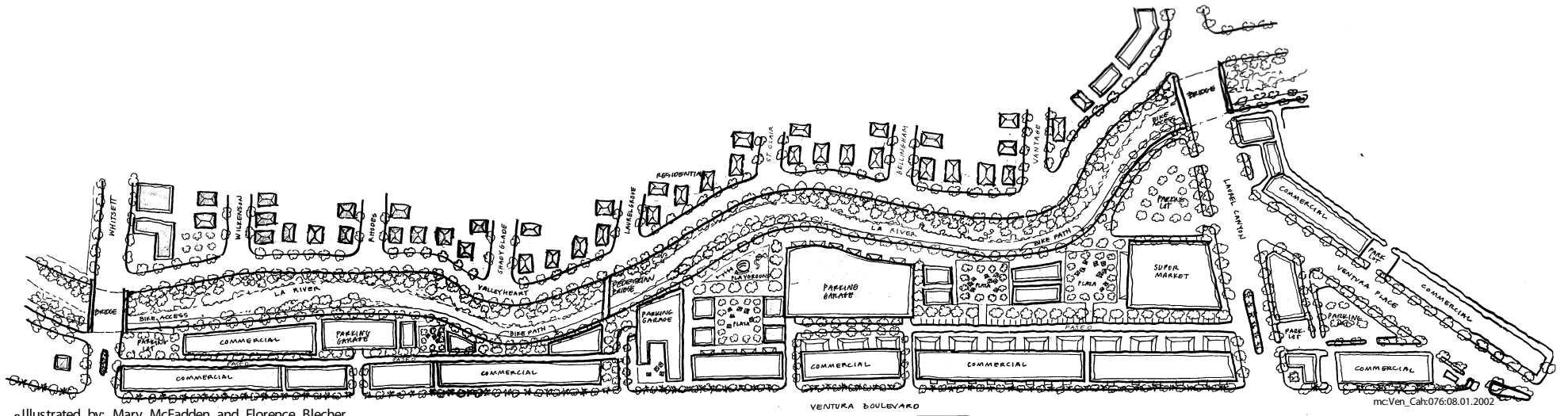
The Media Sub-District is one of the sub-districts in this area with river frontage. This presents an opportunity to create a connection between the Los Angeles River and the commercial edge of the Boulevard. This is also an opportunity to make the parking lots and businesses facing onto the Los Angeles River safer and more attractive. This plan proposes that jogging and bicycle paths, as well as a pedestrian promenade be developed along the south side of the Los Angeles River adjacent to a re-engineered flood control wall. This promenade would have special decorative paving, pedestrian lighting, modest signage, benches, plantings of trees, and vines and flowers.

4.3 The Village Sub-District (Colfax Avenue to Whitsett Avenue)

Studio City's "heart" and commercial core is the Village. The Village in its current state is already pedestrian-oriented and the area's specialness will only improve with the addition of more trees, flowers, paving, and other amenities.

Although many of the existing buildings are built in an Art Deco style, a country village theme is preferred and will be encouraged through the design review process.

Studio City Village



~Illustrated by: Mary McFadden and Florence Blecher

mc:Ven_Cah:076:08.01.2002

4.3.1 The Predominant Flowering Tree

The predominant flowering tree for The Village Sub-District will be: Chinese Pistache (See Studio City-Section 4.1.1).

4.3.2 Planted Medians

Planted medians are recommended in this district only from Radford Avenue east to Colfax Avenue with the following accent/flower plants and Golden Trumpet Trees.

This Plan suggests that the remainder of district bases of the trees in the balance of The Village be planted, and that planters on the sidewalks be filled with the following *Flowering/Accent Plants*:

The *Accent/Flowering Plants and Trees* are:

Common Name	Botanical Name	Feature	Height
Butterfly Lily	<i>Dietes bicolor</i>	yellow	2'
Lavender Starflower Vine	<i>Grewia occidentalis horizontalis</i>	yellow lavender flower	up to 6'
Daylily	<i>Hemerocallis hybrids</i>	yellow	2'-4'
Golden Trumpet	<i>Tabebuia chrysotricha</i>	yellow flower	40'

4.3.3 Other Opportunities

Aside from the Village itself, another opportunity in this sub-district is the Los Angeles River frontage. The Village is one of the sub-districts with public river frontage. This presents an opportunity to create a connection between the Los Angeles River and the commercial edge of the Boulevard. This is also an opportunity to make the parking lots and businesses facing onto the Los Angeles River safer and more attractive. This Plan recommends that a pedestrian promenade be developed along the south side of the Los Angeles River adjacent to a re-engineered flood control wall. This promenade would have special decorative paving, pedestrian lighting, modest signage, benches, plantings of trees, vines and flowers, limited commercial vendors, and cafés. It should have limited hours of operation in consideration for the residents who face onto it on the north side of the channel.

4.4 The Western Edge Sub-District (Whitsett Avenue to Fulton Avenue)

The steep rise of the hills from Whitsett Avenue to Coldwater Canyon Boulevard in this sub-district form a dramatic backdrop to the Boulevard. From Coldwater Canyon Boulevard to Fulton Avenue, She-oaks (*Casuarina cunninghamiana*) form a tall visual buffer for the homes south of the Boulevard. These trees shall be left intact.

4.4.1 Planted Medians

Planted medians are to be provided on the Boulevard.

The Accent/Flowering Plants and Tree will be:

Common Name	Botanical Name	Feature	Height
Kangaroo Paw	<i>Anigozanthos flavidus</i>	yellow, orange, burgundy flower	3'-6'
Coreopsis	<i>Coreopsis grandiflora</i>	yellow-gold	1'-2'
Red Hot Poker	<i>Kniphofia uvaria</i>	yellow-orange	2'-3'
Golden Trumpet	<i>Tabebuia chrysotricha</i>	yellow flower	40'

The Common/Consistent Plants will be:

Common Name	Botanical Name	Feature	Height
Agave	<i>Agave attenuata</i>	blue/grey-green	2'-5'
Blue Oat Grass	<i>Helictotrichon sempervirens</i>	bright blue leaf	3'
Deer Grass	<i>Muhlenbergia rigens</i>	grey-green leaf	3'

4.5 Off-Boulevard/Extended Streetscape Areas

4.5.1 Major Cross Streets and Bridges

It is important to extend the Boulevard, its plantings and improvements, beyond the strict limits of the Boulevard wherever possible and appropriate. These intersections should be planted in a manner similar to but distinct from the Boulevard itself. The distance or extent of these improvements will be determined on a case-by-case basis and vary by site, as will the specific plant materials. However, the selected design elements should extend a minimum of one half block beyond the intersection, generally to

the limits of the boundaries set out in the Ventura/Cahuenga Boulevard Corridor Specific Plan. The Studio City and Cahuenga Pass communities have ten off-boulevard areas. Landscaping should be extended or continued as indicated:

Barham Boulevard/Bridge	Across the Barham Bridge in planters as possible
Lankershim Boulevard/MetroRail Station	Northeast from the freeway underpass to the LA River including the MetroRail station
Vineland Avenue	North to the freeway
Tujunga Avenue	North at least to the river, preferably to Moorpark Ave.
Colfax Avenue	North to at least CBS's back gate, preferably to Moorpark Ave.
Radford Avenue/Ventura Place	Including all of Ventura Place, Radford Ave. north to the dead end
Laurel Canyon Boulevard	North to the freeway, south at least 1 block
Whitsett Avenue	North to Moorpark Ave.
Coldwater Canyon Boulevard	North to the freeway, south at least 1 block
Fulton Avenue	North to Moorpark Ave., south at least 1 block

Off-boulevard areas are streets crossing or abutting Ventura/Cahuenga Boulevard. The trees selected for each of these streets should be as follows:

Barham Boulevard/Bridge	Pink Trumpet Tree/ <i>Tabebuia Impetiginosa</i>
Lankershim Boulevard/ MetroRail Station	Canary Island Pine [<i>Pinus canariensis</i>] (from freeway underpass to one (1) block west of the Boulevard) Pink Trumpet Tree [<i>Tabebuia Impetiginosa</i>]
Vineland Avenue	London Plane Tree [<i>Platanus acerifolia</i> 'Bloodgood']
Tujunga Avenue	Southern Live Oak [<i>virginiana</i>]
Colfax Avenue	Chinese Flame Tree [<i>Koelreuteria bipinnata</i>]
Radford Avenue	Maidenhair Tree [<i>Ginkgo biloba</i> 'Fairmount'] 'male trees only'
Ventura Place	Chinese Flame Tree [<i>Koelreuteria bipinnata</i>]
Laurel Canyon Boulevard	London Plane Tree [<i>Platanus acerifolia</i> 'Bloodgood']
Whitsett Avenue	Magnolia [<i>Magnolia grandiflora</i> 'St. Mary']
Barham Boulevard/Bridge	Pink Trumpet Tree/ <i>Tabebuia Impetiginosa</i>
Coldwater Canyon Boulevard	Canary Island Pine [<i>Pinus canariensis</i>]
Fulton Avenue	Magnolia [<i>Magnolia grandiflora</i> 'St. Mary']

These trees should be planted thirty (30') feet on center, or as determined by the Street Tree Division. See Section 9.3 for detailed irrigation instructions.

4.5.2 The Los Angeles River

There is currently a local movement to return the Los Angeles River from a channelized flood control ditch, back into a public amenity and wildlife corridor. The Los Angeles River runs parallel to much of Ventura Boulevard in this area and abuts the Ventura/Cahuenga Boulevard Corridor Specific Plan boundaries in Studio City. The Media, Village and Western Edge Sub-Districts have extensive public Los Angeles River frontage. The Los Angeles River's proximity to the Boulevard can be used to add a unique dimension to the commercial viability of especially The Village. This plan suggests that biking and jogging paths, as well as a potential riverside pedestrian promenade with special paving, discreet lighting, benches, modest signage, and

plantings be created at the top of a re-engineered and specially textured river wall along the south side of the Los Angeles River. These amenities and furniture should be selected to complement those used on the Boulevard.

Plant materials along the promenade should be urban, well behaved, and related to the adjacent parking lots—Camphor (*Cinnamomum camphora*) or London Plane Trees (*Platanus acerifolia* 'Bloodgood') planted at approximately thirty feet (30') on center. Vines and trees could also be planted to spill over the promenade walls to soften the view from the residential side.

Planting on the sloped, terraced, residential, natural side of the river shall be riparian in nature and native to this area's wet-dry seasons. They should be planted in naturalistic plant communities to emulate how they might grow in the wild. Appropriate trees are:

Common Name/Botanical Name	Features
California Pepper/ <i>Schinus molle</i>	Bright green narrow leaflets
Western Redbud/ <i>Cercis occidentalis</i>	Pink winter flowers
Southern California Walnut/ <i>Juglans californica</i>	Decorative catkins and fruit
California Sycamore/ <i>Platanus racemosa</i>	Strikingly patterned trunk
Western/Fremont Cottonwood/ <i>Populus fremontii</i>	Leaves flutter in wind
The Modesto Ash/ <i>Faxinus velutina</i>	Glossy leaflets yellow in autumn
The Incense Cedar/ <i>Calocedrus decurrens</i>	Aromatic evergreen foliage

Section 5.

STREETSCAPE ELEMENTS AND IMPROVEMENTS

5.1 Landscaping

Landscaping in the public right-of-way primarily consists of trees because of their low maintenance and visual impact.

5.1.1 Soil Preparation

Soil amendments are normally unnecessary, although, if used, they should be composed of no more than 25 percent of the backfill by volume and should not change the soil texture of the tree pit. Any foreign material in the tree well, such as construction debris, is to be removed: fill soil is to match the site soil in texture. This will avoid a soil matrix interface problem.

5.1.2 Trees

The general standard for planting street trees is to space them approximately twenty-five feet apart; however, actual spacing is to be determined by the Street Tree Division. The minimum size is a 24 inch box. The trees are to be planted double staked, with stakes parallel to the roadway, and are to be self-supporting. Size standards shall be as referenced in the U.S.A. Standard for Nursery Stocks (ANSI 300. 1-current edition). The trees are to be untopped.

The tree planting standard for the palms is 25 feet from the deciduous trees. The deciduous trees are planted approximately 35 feet apart. The minimum size is a 24 inch box. The trees are to be planted staked, and are to be self-supporting for deciduous trees. The trees are to be untopped. The Mexican Fan Palm is to have 12 feet of brown trunk. The trees are to be planted according to the Los Angeles Public Works Standard for Street Tree Planting. Actual tree spacing shall be determined by the Street Tree Division.

The Mexican Fan Palms, *Washingtonia robusta*, shall be extended along the length of Ventura/Cahuenga Boulevard. In all Studio City districts the Mexican Fan Palms will alternate with the Chinese Pistache Trees, and with Pink Trumpet Trees in the Cahuenga Pass community.

In all Studio City districts and the Cahuenga Pass community, flowering/canopy tree, will be planted closest to the intersections to provide a sense of shelter for pedestrians and to add color and visual interest to the intersections.

5.2 Lighting

The lighting element of any streetscape process has a two-fold purpose. The primary purpose is to provide adequate illumination for the safety of vehicular and pedestrian traffic. This is generally accomplished by the taller roadway fixtures. The lighting provided by these fixtures are designed to meet the current minimum national standards. Street lights are also used in a streetscape plan to provide aesthetic appeal and offer identity to a specific community. This is often accomplished by using lower scale pedestrian poles. These poles provide additional light to the roadway lighting that will exceed the minimum national standard. This additional light also encourages commercial activity since it gives the pedestrian a greater sense of security.

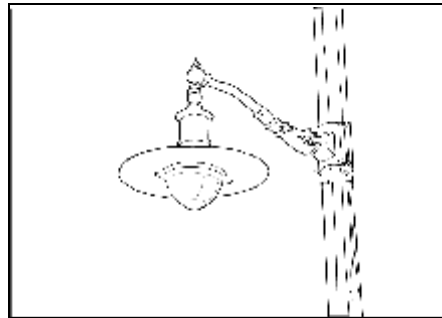
5.2.1 Pedestrian Lighting

Pedestrian lighting is to be lower scale and interspersed among the street trees.

After researching old photos of the Boulevard, images of the light standards that graced it in the 1930's have been located. One of those original luminaires still exists, from which molds for new fixtures could be made. Other pedestrian scale light standards are also available which emulate the old classic style. As the classic style works well with the desired village scale and general architecture of Studio City, either a replica or a more modern design emulating the style of the old street lights of Los Angeles should be selected when an appropriate business or property owners group sets up an assessment district through the Bureau of Street Lighting.

The Cahuenga Pass community has indicated a preference for the "King Viscount" luminaires, should a funding source become available. This particular pedestrian light is on the City's approved equipment list and has been installed as part of a field demonstration (2 poles S/S Hollywood Boulevard W/O Highland Avenue). The pedestrian light should be installed with a 175W Metal Halide lamp, Type II Distribution, polycarbonate teardrop lens and decorative arm and simple clamp mount.

All equipment, construction, and assessment provisions shall conform to the standards and procedures of the Bureau of Street Lighting, including use of the new standard color, "Spring Street Green."



5.2.2 Roadway Lighting

Roadway lighting is to be Bureau of Street Lighting standard davit electroliers.

The Bureau of Street Lighting (BSL) has proposed upgrading the lighting on Ventura/-Cahuenga Boulevard with a new system for the entire seventeen mile length. BSL's proposed standards call for 40 foot poles painted Spring Street Green, spaced

approximately 180 feet on center, along both sides of the Ventura/Cahuenga Boulevard. Final spacing shall be determined in relationship to and coordinated with the planned street tree plantings so that street trees and light poles do not conflict with each other.



5.2.3 Maintenance

Any proposed street design that adds to or changes existing maintenance assessment rates must adhere to the requirement of Proposition 218 prior to construction. This involves a ballot process that must be approved by the affected property owners.

5.3 Street Fixtures, Furniture and Equipment

The Studio City & Cahuenga Pass Communities' Streetscape Plan has chosen materials, finishes, and street furniture distinctive to each neighborhood. Any improvements in the public right of way are to use these or similar materials, finishes, and street furniture. Site developments are to extend the materials, finishes and street furniture into the development as appropriate, in order to unify the entire "look" of the Boulevard. Unless called out, the default color for all fixtures, furniture, and equipment is "Spring Street Green."

Street furniture should be arranged to facilitate pedestrian activity and be placed on the sidewalk so as to provide the least obstruction to pedestrian right of way. Vehicular visibility between driveways and streets must remain unobstructed.

5.3.1 Ash Urns

Ash urns are to be provided at bus stops. They are to be Landscape Forms ash urn Model 62LX83-XP in Panguard finish, Ivy color or equivalent. They are to be mounted on bus shelter poles where appropriate to the design of the shelters.

5.3.2 Benches

Benches are to be provided at bus stops and elsewhere as desired.

In Cahuenga Pass they are to be Landscape Forms Hyde Park bench with metal rod seat, Model HP3005-BS-59 or equivalent.



In Studio City they are to be Landscape Forms Scarborough bench with horizontal straps, Model SC 3005-BS-72 or equivalent.



5.3.3 Bicycle Racks

Bicycle racks may be provided as needed. They shall be TimberForm CycLoop 2170 original series or equivalent. Existing bicycle racks should be replaced with this model to assure visual consistency in the Studio City Area.

5.3.4 Bollards

Bollards may be provided as needed. The bollards shall be TimberForm Metal Bollard Model 2190 or equivalent.

If barrier chains are required in specific situations, Models 2193 and 2196 or equivalent may also be used.

5.3.5 Bus Shelters and Bus Benches

Bus shelters will be a simple unified design throughout this portion of the Boulevard. They should be a combination of separate but related elements: trellis, shade structure, bench, planted terra-cotta colored concrete planter pot, trash receptacle, pole-mounted ash receptacle, night lighting, possible pay phone and public kiosk, specific design to be determined. When the bus shelter model is selected, existing bus shelters of a different design should be replaced for visual consistency.

Bus shelters are private facilities, not linked to the Metropolitan Transit Authority (MTA) transit shelters. Nevertheless, provision of a bus shelter may accrue credit under a Transportation Demand Management (TDM) plan.

5.3.6 Fountains

Fountains offer one of the best focal points in any public streetscape or private landscape and, if they are provided, their maintenance must be guaranteed for a minimum period of ten years.

5.3.7 Kiosks and Monuments

Kiosks and Monuments shall be determined in the future by the Studio City and Cahuenga Pass Design Advisory Committees with input from the respective communities.

5.3.8 Neighborhood Logos

If the communities so desire, neighborhood logos are to be added after selection by each of the two communities, and in conjunction with the appropriate committee of the Plan Review Board.

5.3.9 Sculptures, Artwork, and Interpretive Monuments

Sculptures, Artwork, and Interpretive Monuments normally are stand alone works and will be dependent upon neighborhood and Design Advisory Committee review to assure integration with the streetscape.

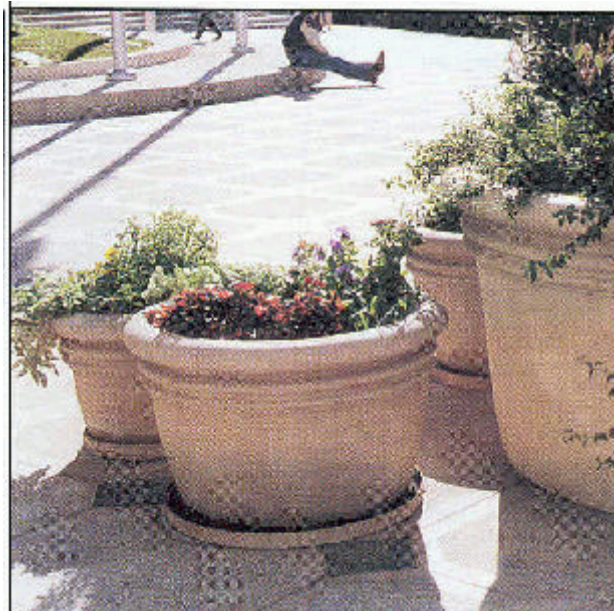
Murals may be provided at the sites suggested in Section 3.6 or may be proposed at additional sites subject to neighborhood and Design Advisory Committee review and approval to assure integration with the streetscape.

5.3.10 Newspaper Racks

In all cases the default color for newspaper racks shall be, "Spring Street Green." Placement of newspaper racks (newsracks) shall be pursuant to Sec. 42 of the Municipal Code which, in addition to other requirements, prohibits the installation or maintenance of a newsrack within 5 feet of any area improved with lawn, flowers, shrubs or trees, within 6 feet of any bus bench, and, within 10 feet of any transit shelter.

5.3.11 Pots/Planters

Pots and planters are to be provided throughout this area. They shall be Quick Crete Baja Series Model QR-BJ3630P or equivalent.



5.3.12 Telephones

Telephones are to be determined by Studio City Design Advisory Committee and the Plan Review Board, prior to Commission approval.

5.3.13 Trash Receptacles

In the Cahuenga Pass the receptacles shall be the Landscape Forms Presidio trash receptacle Model PD5001-26-30 Top Opening or equivalent.



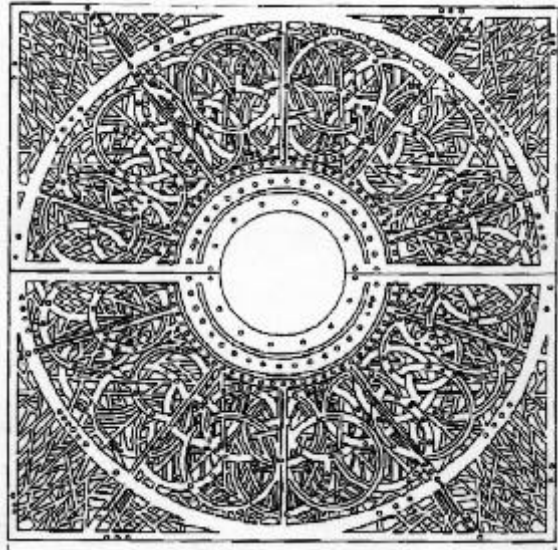
In Studio City the receptacles shall be the Landscape Forms Scarborough trash receptacle Model SC5002-24-33 or the approved City default trash receptacle in locations where funds are not available.



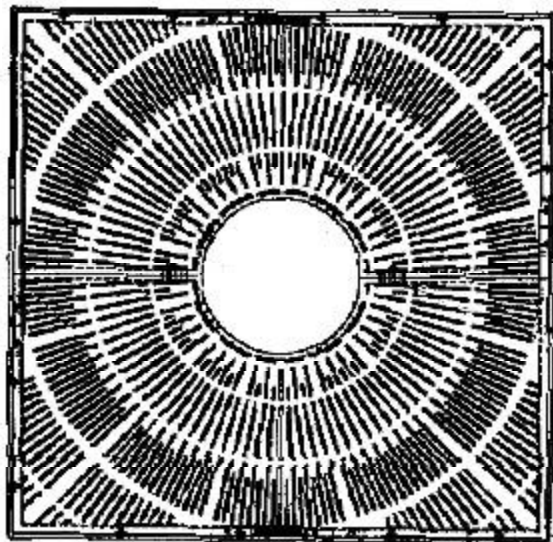
5.3.14 Tree Well Covers

Metal tree well covers shall be maintained by the funding party.

In the Cahuenga Pass District tree well covers shall be Urban Accessories OT Title 24 square 5' x 5' tree well covers or equivalent. These tree well covers have a complex, old-style pattern as shown below:



In the Studio City District tree well covers shall be the Neenah R-8707, 180 degree square, 5' x 5'. These offer a simpler pattern that works well with other streetscape elements.



5.3.15 Tree Wells

The tree wells are to be a minimum of 5 feet long parallel to the roadway and 5 feet wide. Each well is to be lined with a continuous 12 inch deep "Deep Root" or equalinear root barrier. Irrigation is to be by means of bubblers in perforated pipes supplied by the adjacent site development. A gate valve is to be provided on the non-pressure line from the site development to isolate the bubblers in case of equipment breakage. A ground cover of decomposed granite, a minimum of 1 inch deep, is to be provided in the tree well. Soil amendments are normally unnecessary. Although if used, they should comprise no more than 25 percent of the backfill by volume and should not change soil texture of the tree pit. Any foreign material in the tree well, such as construction debris, is to be removed by the installer. Fill soil is to match the site soil texture.

Alternate groundcover for the tree wells can be proposed. They should be pre-approved by the City's Street Tree Division and should have only permeable surfaces with a coefficient of run-off of less than or equal to 0.6.

5.3.16 Vaults (Aboveground) and Utility Boxes

Aboveground vaults and utility boxes, such as those for phone service and traffic signals, are to be painted in a color selected to match the adjacent paving or Spring Street green.

5.4 Flatwork Improvements

5.4.1 Interlocking Pavers

Interlocking pavers in the Studio City District shall be red set in a herringbone pattern at a 45 degree angle to the street edge. An example of the red brick color and pattern can be found at 12345 Ventura Boulevard.

5.4.2 Curbs and Gutters

Curbs and gutters shall be poured in place concrete, with a natural concrete color and a smooth finish.

5.4.3 Flatwork, Concrete

Concrete flatwork required by this Streetscape Plan is to be designed in conjunction with the story pavers, sidewalks and streetlights installed by the Bureau of Street Lighting. A concrete border along the building side of the sidewalks shall be poured as a stable edge for the sidewalk field concrete pavers. Expansion joints shall align with edges of story pavers and tree well covers. As sidewalk width may vary along the Boulevard, the width of this

concrete band may also vary; however, this band shall be a minimum of fifteen inches (15") wide. It shall provide a consistent field width for the concrete pavers in any given block.

For other areas, any surface material which drains effectively (with approximately a 1 percent minimum slope), is slip-resistant (equivalent to a medium salt finish, or a broom finish), and conforms to handicap accessibility criteria, shall be adequate. Colored concrete should contain integral color, rather than being stained, since integral color tends to achieve a more even color, and wears longer.

5.4.4 Flatwork, Other

Other flatwork in the streetscape may be proposed by the applicant. For instance, enhanced paving at building entrances can effectively relate the streetscape to site development. Historical examples can be found in the forecourts of movie theaters from the 1920's and 1930's. An example is the decorative terrazzo designs in the forecourt of "Bookstar" located at 12136 Ventura Boulevard in Studio City.

The applicant shall demonstrate the handicap accessible and slip-resistant properties of proposed materials. Certain materials normally will not satisfy these criteria, e.g., ceramic tile, terrazzo, polished stone, or steel trowel finished concrete.

5.4.5 Sidewalks

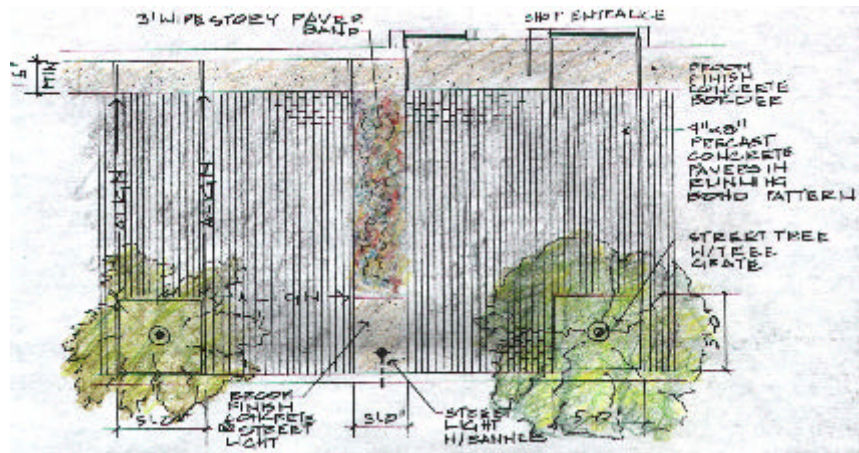
Sidewalks in the Cahuenga Pass shall be compatible to Ackerstone 4" x 8" Holland stone precast concrete pavers in charcoal color. They shall be sand set and laid in a running bond pattern.

Three foot wide (3') concrete strips perpendicular to the curb are to be poured with the new street lights to be installed by the Bureau of Street Lighting centered in them. A score line/expansion joint shall be included to align with the 5' x 5' tree well cover edge five feet in from the curb.

The base for these pavers shall be properly prepared for future stability and long-term use.

5.4.6 Story Pavers

The rich history of Studio City and the Cahuenga Pass should be displayed in a series of "story pavers" describing bits of this area's history using images and/or words. Details, materials, and accents could change based upon the artists' ideas and, as needed, to mark a special event or location.



These pavers could initially appear as plain concrete strips running perpendicular to the buildings that line the Boulevard. They will be located in conjunction with the new street lights that are to be installed by the Bureau of Street Lighting (approximately 120' on center along the Boulevard). These strips will function as dividers between sections of the field of precast concrete pavers. They could be sponsored and underwritten by interested patrons and the City of Los Angeles' One Per Cent for Art Program. They would be an opportunity to involve local artists and schools in their design. Under no circumstances should they be used for commercial advertising. However, a small bronze plaque (two inches by four inches [2" x 4"] or less in size placed in one of the corners closest to the street light) may identify the sponsor or donor of the individual paver in plain Roman type. As designers and designs are approved by the Design Advisory Committee, community and Cultural Affairs Commission, these three foot wide strips, starting at the expansion joint five feet in from the curb (aligning with the tree well covers) and running into the concrete edge against the buildings, will be removed and replaced permanently with the artists' work. Each artist will have the opportunity to select the specific location for his or her work as sites are available. They should develop the design of a story paver taking into consideration the specific location and area.

These pavers must comply with slip resistance, drainage and handicap accessibility requirements of the Department of Public Works.

5.5 Infrastructure Improvements

5.5.1 Medians, Planted

Irrigation is to be provided by shrub heads (flat spray) installed 2 inches from the curb or on double swing joints. The medians will be planted primarily with ornamental, dry climate grasses, flowering shrubs and perennials. The companion plants may change in type and color, but they will do so in waves, drifts, or

eddies, not necessarily aligning with the "borders" of the districts.

Construction details of raised medians will require review and selection by the Design Advisory Committee, Bureau of Street Services, and the Department of Transportation. Medians should have as few breaks in them as possible yet allow reasonable access to businesses along the Boulevard. The details and accents of the medians may vary to help highlight a special feature or Opportunity Zone along the Boulevard.

5.6 Intersection Improvements

5.6.1 Major Intersections

The roadway at major intersections may eventually be widened to handle anticipated traffic volume. This can be expected to eliminate many opportunities for planting street trees near these intersections. The Visibility Triangle requirements of the Municipal Code (Section 62.200) further require that obstructions to sightlines not be introduced near intersections. Narrowed sidewalks and requirements for handicap accessibility may further restrict the potential for planting street trees in those areas. Medians in the Boulevard may need to be eliminated at these Major Intersections in order to accommodate left turn lanes. All of these factors will work against a continued "visual line" of street trees. The Ventura/Cahuenga Boulevard Streetscape will thus tend to weaken at these intersections.

The break in the streetscape rhythm at the Major Intersections is to be mitigated by encouraging significant *on-site landscaping*. Each intersection is to be landscaped with a large flowering or canopy tree species as specified in Section 4.5, Off Boulevard/Extended Streetscape Areas. To achieve enough bulk to distinguish the intersection, several trees are to be planted on each site, near the right of way, but not in the Triangle, and at sufficient distance away from the curb to allow future enhancements of street capacity allowances for growth.

The trees are to be planted at each of the four corners of the intersection on the site development. Each tree requires a minimum root space needed for optimal growth. These root spaces are indicated for the various trees specified for each corner site development since the character and size of the tree will determine the appropriate number of trees to be planted. Corners designed with primarily vertical trees may require more trees for a "full" effect than those with a more rounded profile.

The minimum box size for a tree is 24 inches, unless otherwise stated for the individual corners. Trees unavailable in 24 inch box sizes may be planted in smaller sizes, and no greater quantity is to be required. The trees are to be planted double staked and are

to be vigorously self-supporting. Tree standards such as height and box size, shall be comparable with those listed in the Valley Crest Nursery catalog. The trees are to be untopped. Sidewalks are to be protected from root intrusion by a continuous 12 inch Deep Root or comparable root barrier.

5.6.2 Secondary Intersections

This Streetscape Plan requires that the selected flowering or canopy tree selected for each district be planted as close to intersections as possible, while still respecting Visibility Triangle requirements. The use of additional on-site landscaping is recommended to make all intersections as colorful as possible and serve to extend the streetscape plan from the street onto private property.

5.7 Street Banners and Flags

Community events may be announced with temporary banners mounted on light poles, as permitted by the Bureau of Street Services, at a rate of not more than one banner per mile. The banner shall be in place no more than one week before and two days after the event. No advertising for outside events and commercial products is permitted.

Flags mounted on light poles shall be permitted at a rate of one per blockface. No advertising for outside events or commercial products is permitted on these light pole flags.

All projects shall submit a sign program for the project site or conform to an existing sign program if one is already in effect.

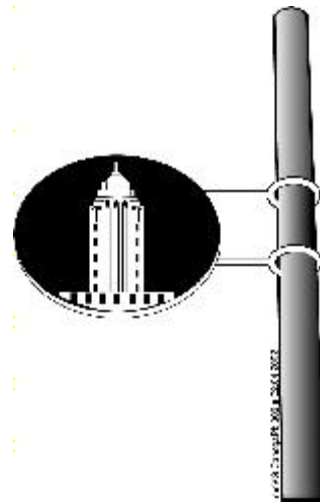
All signs shall be compatible with existing signage in the same block frontage or, if at an intersection, with the signage on the other corners.

5.8 Medallions

Permanent public art displays in the form of medallions shall be permitted to establish district identity in accordance with the following guidelines:

- Medallions shall be permitted in Pedestrian Oriented Areas (POAs) as defined in the Ventura/Cahuenga Boulevard Corridor Specific Plan, and in “approved” Business Improvement Districts (BIDs).
- Art display for medallions shall be representative of generic district businesses or community themes, e.g. animal motif in Tarzana.

- Placement of medallions weight and material specifications shall be determined by the Bureau of Street Lighting.



- Design of medallions shall be approved by the Design Advisory Committee.
- A permit shall be obtained from Bureau of Street Lighting prior to approval.
- Maintenance including graffiti removal, restoration, replacement and preservation shall be the responsibility of the funding entity or organization.
- Medallions shall be approved by the Bureau of Street Lighting, Department of Public Works, Department of Transportation and Department of Cultural Affairs.

Section 6.

DOCUMENT SUBMITTAL GUIDELINES FOR PROJECTS ON THE PUBLIC-RIGHT-OF-WAY

In addition to any documents required for review of streetscape projects by the Bureau of Street Services of the Department of Public Works, specific submittals are required by the Department of City Planning for approval under this Streetscape Plan. These include the following:

6.1 Site Plans

- 2 sets full size
- Minimum scale 1"=20' or 1"=1'-0", fully dimensioned with a north arrow.
- All public rights-of-way and easements on or adjacent to the property shall be shown, including existing and required street dedications. Improvements, including sidewalks, street trees, street lights and transit stops shall be indicated.
- Access from off-site areas such as pedestrian, automobile, delivery, building service including curb cuts, pathways, and fire lanes should be clear on the drawings.
- On-site circulation and parking, including driveways, parking spaces, loading areas/docks, pedestrian paths, and disabled access shall be shown.
- Arrangement/form (height, setback, massing) and use of all buildings shall be shown.
- Walls, fences, retaining walls, ramps, and stairs shall be shown.
- Landscape plans showing the location of existing trees to be removed or saved and new trees, as well as proposed landscaping. New plant material needs to be identified by location, name, size, and number of plants.
- Existing topography, proposed grading and drainage design plan.
- Outdoor lighting (building mounted, parking and landscape areas) shall be shown.

6.2 Photographs

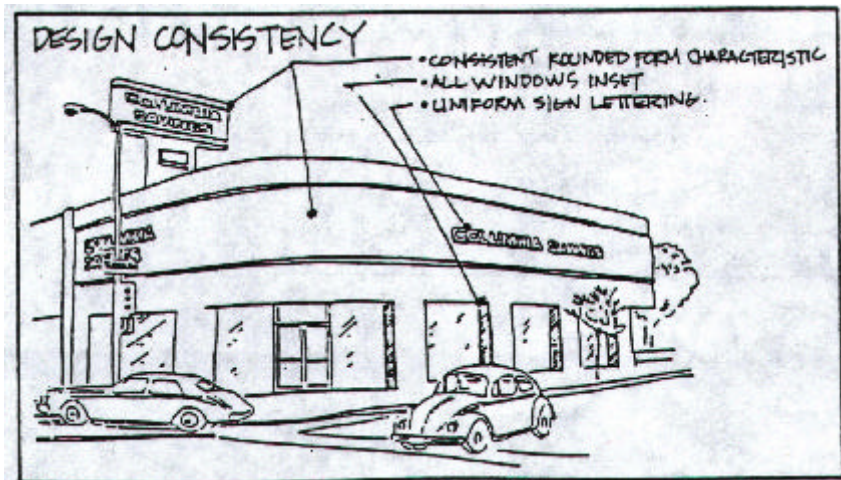
- Elevation photos of the entire site and surrounding properties shall be mounted on 8 ½" x 14" paper (not card stock) or equivalent.

7.1 Paint Colors/Surface Treatments for Commercial Buildings and Facades

No more than five (5) colors shall be used per business/shop including the building itself, canopies/awnings, door and window frames, hardware, adjacent pole and/or monument signs, other than murals on buildings approved by the Cultural Affairs Commission. The use of bright red, bright orange, and bright yellow, especially in combination with each other, is discouraged. Colors should relate to the overall building design and not be the dominant characteristic that is primarily used as an attention getter device.

7.2 Detail and Ornamentation of Buildings

Buildings should incorporate detail in the building design such as trim around windows and doors, protrusion and depressions in the facade of the building, delineated columns and other elements which provide visual relief to flat surfaces and are consistently used on all visible building elevations. The building trim and ornamental features should relate to the style of the building in a consistent manner.



7.3 Application of Glass

Glass should be used in window openings of the building only and not as the sole exterior building material, such as reflective glass curtain wall application. Glass used at the pedestrian level should be clear, untinted, or very lightly tinted to allow visibility of people and activities in and out of the building.

7.4 Screening Roof Top Utilities

Equipment, such as air conditioning units and ventilation structures, shall not be mounted on roofs unless it is fully screened from the view of the street and adjacent properties.

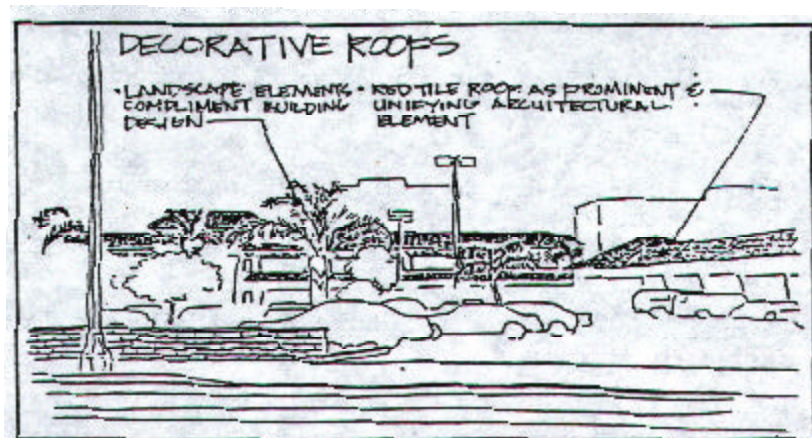
7.5 Street Level Design

Buildings, where a substantial length of windowless wall may be unavoidable, should incorporate eye-level displays, contrast in wall treatment, decorative features, outdoor seating and or landscaping in order to enhance visual interest and pedestrian vitality. The use of awnings, overhangs, porticoes, trellises and other design elements are encouraged to provide protection to pedestrians. These elements should be located at a height to provide sufficient room for pedestrians.

The use of recessed windows, balconies, offset planes or other architectural details which provide dimensional relief along buildings facing the sidewalk should be incorporated into the building design.

7.6 Multi-tenant Buildings

Multi-tenant buildings or shopping centers should incorporate the use of copper, slate, clay tile or concrete tile roofs to provide color and texture to individual buildings and enhance the aesthetic quality of the Boulevard.



A sign program reflecting the proportional distribution of signage, as dictated by *Section 8 Sign Regulations* of the Ventura/Cahuenga Boulevard Corridor Specific Plan, for single and multiple-story, multi-tenant buildings should accompany requests for Project Permit Compliance.

7.7 Arrangement and Form of Buildings on the Site

The arrangement and form of buildings on site should be in accordance with the following guidelines:

- Accessory structures such as transformer vaults, HVAC equipment, satellite dishes, and free-standing canopies, etc., should be designed such that they integrate with the architectural and /or landscape design of the project. They should not occupy the site frontage.
- The primary entrance of all commercial sites should face the street (not alleys or side or rear parking lots).

7.8 On-Site Signage (Corresponds to Section 8 of the Ventura/Cahuenga Boulevard Corridor Specific Plan)

- Signs are to be located in the area above the individual store front or perpendicular to it in an area at least 8' above sidewalk level. The perpendicular signs shall not protrude into the public right-of-way more than 3'.
- Signage for individual businesses is limited to one sign per business on each façade that the business fronts in accordance with Section 8 of the Ventura/Cahuenga Boulevard Corridor Specific Plan. The signs are allowed only for business identification.
- Signs are to be coordinated with the design of the building including materials, color, size, and existing signage. In each streetscape district, signs should be in keeping with the intended character of the area. Sign submittals must include a building or center sign inventory, photographs, and evidence of conformity with a building sign program.
- Signs should be well maintained and repaired when deteriorated or damaged.
- Window displays are permitted to augment individual business signage. These displays can help catch the eye of the shopper, identify the store and feature merchandise. These displays are not intended to promote signage for sales or discounts. No window signs are permitted, except as permitted by Section 8 of the Ventura/ Cahuenga Boulevard Corridor Specific Plan.
- Total signage, including wall signs, and projecting/perpendicular signs, should not exceed two square feet per linear foot of street frontage of the lot.

- All projects shall submit a sign program for the project site or conform to an existing sign program if one is already in effect. This sign program needs to identify the sign area allocated to each tenant, sign type, as well as permissible fonts, color, and letter heights. Consideration of building design and surface texture should be considered in determining a sign's style and color.
- All signs should not create a visual conflict and should be compatible with existing signage in the same development and block frontage, or if at an intersection, with the signage on other corners.
- All new signs, both individually, and as part of a sign program, shall take into consideration the location of various streetscape elements such as street lights, bus shelters, and trees. In the case of trees, their initial size and growth shall be considered when determining the size and location of signage.
- To allow views of the signage, the transfer of a percentage of fascia signage to any wall between the parking lot and the street is allowed by the LAMC. The sign area on the wall is restricted to a 1-foot band near the top of the wall.

7.9 Flatwork Materials

Flatwork on-site may be proposed by the applicant. For instance, enhanced on-site paving at building entrances can effectively relate the streetscape on the public-right-of-way to the site development.

7.10 Landscaping - On-Site Trees

Mexican Fan Palms and Flowering/Canopy Trees may be continued into the site landscaping, so as to interrelate streetscape and site landscape. However, site landscape need not feature only Mexican Fan Palms or district Flowering/Canopy Trees.

7.11 Lighting

Lighting should be directed from the perimeter onto the site, and be adequately aimed and shielded to not spill over onto adjacent properties, especially into areas planned and zoned for residential uses. On-site lighting of walkways and patios is encouraged. Lighting fixtures used as ornamental features of the site and buildings should be consistent.

Lighting in parking structures should be sited and designed to reduce glare and be shielded from the direct view of any vehicles and pedestrians.

7.12 Parking Lot/Structure Design

Parking lots should be planted with broad canopy evergreen or briefly deciduous shade or flowering trees. If several parking lots are contiguous, the plantings should be consistent throughout the entire length of the lot.

When planting in parking lots, the applicant should ensure that trees, lighting, utility monuments and/or vaults, and building signage are coordinated. *One* sheet of drawings should be submitted on which *all* features are located and called out. Conflicts such as lighting poles directly next to trees or trees that block all views of building signage, are specifically and strictly prohibited. Trees should not have to be pruned to keep them away from light poles or to avoid blocking building signage. Trees should be selected so that they will grow *above* the building signage if they are properly maintained, and *never* should be topped to allow views of the signage.

To allow views of the signage, the transfer of a percentage of fascia signage to any wall between the parking lot and the street is allowed by the LAMC. The sign area on the wall is restricted to a 1-foot band near the top of the wall.

When the parking is in front of the building, the parking arrangement can have the aisles parallel or perpendicular to the street. If the aisles are parallel, pedestrian walkways may be required to provide access to the building through the parking lot and parking lot landscaping should enhance this feature. (In perpendicular arrangements, pedestrians will use the parking lot aisles to access the buildings.)

Planting areas with trees shall have no dimensions less than 5 feet. Where parking stalls abut landscaped areas, no bumper stop shall be within 4 feet of the normal center of a tree. Where feasible, all trees should have a minimum of 50 square feet of unpaved ground surrounding them, thus providing a minimum ground area for water filtration and gas exchange.

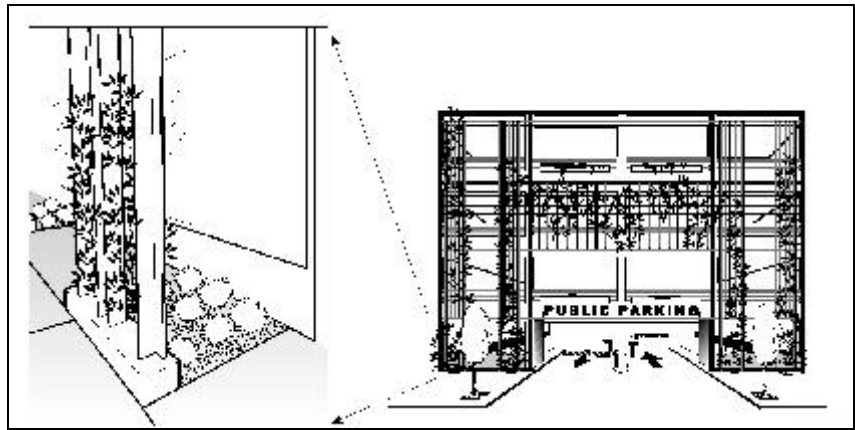
Lawns should be kept at least 3 feet away from trees. This prevents one plant from interfering with the growth of another plant.

For triangular gores at the end of parking rows, or islands between parking spaces, only that planting within 4 feet of the head of the stall, measured along the length of the stall can be counted toward the 15 percent landscaping requirements for parking lots in the Ventura /Cahuenga Boulevard Specific Plan.

Parking lot/structure design is to be arranged such that the following guidelines are observed:

Landscaping should follow these guidelines applicable to private as well as public parking structures:

- Parking areas should be adequately buffered/screened from adjacent rights-of-way and less intense uses.
- Landscape in parking lots must follow these standards to be counted toward the 15% requirement of the Ventura/Cahuenga Boulevard Corridor Specific Plan.
- Parking areas should be adequately shaded by the placement of trees on the surface parking area.
- Handicapped parking should be clearly shown by signage and easy access should be provided to building entrances.
- In parking lots with more than 50 spaces, a phone for emergencies should be available.
- Foliage should not be higher than three feet tall to maintain eye-level visibility for a person seated in a car and utility clearance in parking lots behind stores.
- A ten foot landscaped buffer should be provided between a parking lot or structure and property lines. When parking lots or structures are adjacent to other parking lots or structures, said landscape buffer shall incorporate walkways, stepping stones, or other pedestrian access and linkage items.
- Parking structures shall integrate with the design of the building they serve.



- Parking structures and garages shall be screened with shrubs and various plant species within the roof, facade, or setbacks, and may incorporate planters, planter boxes, trellises, etc. as part of the landscape design.
- The views of parking areas shall be softened from adjacent uses, buffered and concealed from view with sufficient planting material.
- In parking lots behind stores, trees are to be used that are guaranteed to stay small and which do not have to be pruned to remain clear of existing utility poles and wires.

Parking lot trees may be selected from the following table. Other trees may be considered subject to the recommendations of the Community Design Advisory Committee and approval by the Department of City Planning.

Common Name/ Botanical Name	Features	Height	Plantings
Peppermint Tree/ <i>Agonis flexusa</i>	evergreen; weeping habit	to 35 feet	5'x5' minimum
Chinese Hackberry/ <i>Celtis sinensis</i>	deciduous, upright branching	30 feet to 50 feet	5'x5' minimum
Chinese Fringe Tree/ <i>Chionanthus retusus</i>	graceful, heavy flowering in spring, resembles large white lilacs; male trees only	to 20 feet	5'x5' minimum
Australian Willow/ <i>Geijera parviflora</i>	evergreen; graceful, weeping	to 40 feet	5'x5' minimum
Honey Locust/ <i>Gleditsia triacanthos</i> <i>varieties</i>	stays small because not adapted to mild winters	small	5'x5' minimum
Flowering Crabapple/ <i>Malus floribunda</i>	vary in size	choose variety that grows to no more than 20 feet	5'x5' minimum
African Sumac/ <i>Rhus lancea</i>	informal, airy, graceful, weeping	to 30 feet	5'x5' minimum
Chinese Elm/ <i>Ulmus parvifolia</i>	evergreen or semi-deciduous; spreading, arching, weeping habit	to 60 feet	5'x5' minimum root area
Saw-leaf Zelkova/ <i>Zelkova serrata</i>	vase-shaped	to 40 feet	5'x5' minimum

7.13 Utility Undergrounding

At present, alleys usually have overhead utilities. In the future, and as funds allow, all utilities should be placed underground. Utility undergrounding is crucial to the visual improvement of the Boulevard. This policy extends to include major cross street intersections, parking lots, and streets crossing or abutting Ventura/Cahuenga Boulevard at least to the limits of this Specific Plan.

Section 8.

BUFFERS

8.1 Buffers Between Vehicular Use Areas and Residential Areas

Buffers between vehicular use areas and residential areas, site developments and the Freeway, are to be arranged such that the following guidelines are observed:

An opaque barrier, such as a densely planted hedge, concrete block wall, or similar structure, 6 feet high and finished to complement the building architecture, should be provided.

A landscaped area with appropriate planting is to be provided along the wall, facing the site development. The landscaping is to provide further effective screening.

8.2 Buffers Between Outdoor Uses

Uses with potentially negative effects, such as some eating areas, outdoor storage areas, satellite dishes, solar panels, pool equipment, or mechanical equipment, should be located at a suitable distance from or be buffered from lots with residential zoning or sensitive uses (e.g., child care, etc.).

New on-site uses that are incompatible with on- and off-site uses, such as service stations, outdoor recreation areas, or commercial or industrial uses close to a residential use or zone, should be adequately buffered and screened with trees and landscaping and/or architectural devices such as walls, fences, and screens. Greater yard setbacks can be used, but only when entitlements are not affected.

Section 9.

MAINTENANCE

9.1 Maintenance Guidelines

All work shall conform to the City's Standard Plans (#050018) available from Building News, (714) 517-0970. The Standard Plans are also available from the City's Web Site, <http://www.lacity.org> through the Department of Public Works, Bureau of Engineering. All other street tree and street furnishing guidelines are available at the Bureau of Street Services' Street Tree Division and Street Use Division respectively. The Los Angeles Department of Transportation has standards available upon request.

9.2 Cleaning

Hardscaped areas shall be maintained for a clear and litter-free condition on a daily basis by the on-site business proprietor.

Planting areas shall be kept free from trimmings, litter, or other objectionable items.

Removal of any extraordinary accumulation shall be accomplished by separate agreement with the Business Improvement District (BID), committees responsible for the implementation and maintenance of the streetscape, or by the appropriate on-site business proprietor in the absence of a functioning BID.

9.3 Irrigation

Irrigation systems shall be installed to assure that the plantings maintain healthy conditions. If plants are not watered and fed properly, they not only lose their appearance, but also become susceptible to pest and disease problems. The landscapes selected are to be water conserving in nature, and therefore will need less water as they mature, and less at different times of the year. All irrigation system plans shall be reviewed and approved by the Bureau of Street Services and Street Use Division. Maintenance of the irrigation system shall be the responsibility of the party that installed and/or funded that system.

Watering must be effectively controlled to minimize costly water waste resulting from over watering and water damage resulting from sprinkler over spray onto walks, fences, walls, and buildings. For maximum water conservation, the irrigation system shall be operated only at night and in the early morning hours.

Backflow devices shall be checked and certified yearly or at the interval recommended by the water purveyor having jurisdiction.

9.4 Street Tree Management

Street tree management includes tree species selection, spacing, oversight of proper installation, maintenance, and pruning. In Studio City and Cahuenga Pass the Department of Public Works shall maintain oversight of the management of the City's tree program and any other streetscape program, including coordination with the Department of Water and Power. If a business improvement district, or some other form of citizen support, is established, then planning for budgetary needs for future years and development of policies for removal of trees could be shared by that organization with the Department of Public Works, through contact with that Department. However, the ultimate removal of trees is at the discretion of the Board of Public Works.

9.5 Pruning and Trimming

The objective of tree pruning is to improve the health and beauty of the plant, to modify the growth of the tree, and to adjust to the conditions and use patterns of the site. Trees shall never be sheared or topped.

Prior to pruning any street tree, a permit must first be secured from the Street Tree Division. Pruning shall be done primarily for the removal of deadwood, cross-branching, and to thin out weak or crowded branches per American National Standards Institute (ANSI) A 300 standards. *The natural character of the tree is to be preserved.* Tree guy wires and stake ties should be inspected and adjusted periodically and removed when necessary to insure that they are adequately surrounding the tree without girdling trunks or branches. Tree stakes and ties shall be removed within (2) years or whenever the tree is self-supporting, whichever comes first.

Low branches that interfere with passage underneath should be removed to a height of approximately 8 feet. If a branch is cut off, it should be cut at the trunk rather than left as a stub. Do not prune the tree into a lollipop form as this results in excessive side branching and destroys the natural form of the tree.

9.6 Repairs and Replacements

Wherever possible, replace broken irrigation equipment with parts made by the same manufacturer. Otherwise, replace with compatible equipment. Replacements should be made within one watering period.

Dead trees and shrubs should immediately be brought to the attention of the Business Improvement District, the Design Advisory Committee, and the City Planner administering the Specific Plan. Before any plant material is replaced, the cause of

death should be determined and corrected. Replacement plants shall be of the same size and variety as the dead plant.

Section 10

DOCUMENT SUBMITTAL GUIDELINES FOR ON-SITE PROJECTS

In addition to any documents required for plan check submittals, specific submittals are to be made for approval under the Design Guidelines. These include the following:

10.1 Site Plans

- 2 sets full size
- Minimum scale 1"=20' or 1"=1'-0", fully dimensioned with a north arrow.
- All public rights-of-way and easements on or adjacent to the property shall be shown, including existing and required street dedications, improvements, including sidewalks, street trees, street lights and transit stops.
- Property line(s) and any relevant lease lines around the project within the ownership or control of the applicant should be shown.
- Access from off-site areas (pedestrian, automobile, delivery, building service including curb cuts, pathways, fire lanes, etc.) should be clear on the drawings.
- On-site circulation and parking including driveways, parking spaces, loading areas/docks, pedestrian paths, and disabled access shall be shown.
- Arrangement/form (height, setback, massing) and use of all buildings shall be shown.
- Walls, fences, retaining walls, ramps, and stairs should be shown.
- Landscape plans, including the location of existing trees to be removed or saved and new trees, the number and type of new plants, and a maintenance program should be provided.
- Existing topography, proposed grading, and drainage design plan.

- Outdoor lighting (building mounted, parking and landscape areas) should be shown. The locations of lighting, trees, and signage should be coordinated, such that one will not interfere with the other, upon installation, and taking into consideration the trees' maximum projected size.

10.2 Floor Plans, Elevations and Sections

- Use designation of all rooms/areas, e.g., medical office, retail store, fast food restaurant, and loading dock/areas.
- Exterior wall openings.
- Roof top projections and mechanical equipment.

10.3 Photographs

- Elevation photos of the entire site and of surrounding properties.
- Mounted on 8 ½" x 14" paper (not card stock) or equivalent.

10.4 Material Boards

- Material Boards should be submitted for each project that required approval under the Specific Plan.
- No larger than 24 inches x 36 inches in size.
- Should not include Streetscape materials (the approving body already knows what they look like), unless they are useful to an understanding of the project's materials.

10.5 Project Thresholds

Documents for projects involving major facade changes, additions of more than 100 sq. ft., or new construction will be presented to the Studio City & Cahuenga Pass Design Advisory Committees for review and comments.

Section 11

REVIEW PROCESS

All streetscape projects should be submitted to the Department of City Planning and Board of Public Works staff. Upon review, staff will forward streetscape projects to the either the Studio City Design Advisory Committees or the Cahuenga Pass Design

Advisory Committee, for their evaluation and input prior to the review and determination by City Planning Department staff. This committee, with representatives from the business community, the residents association, and the Plan Review Board, provides a broad-based evaluation of the project from the point of view of the whole community.

11.1 Shop Inspection

All projects in the public right-of-way are subject to “Shop Inspection” by the Department of Public Works, Bureau of Contract Administration. This requirement applies to major and minor projects including, but not limited to, the construction of bus shelters, benches, bike racks, gateway monuments, and permanent signs in the public right-of-way. Shop fabrication shall be made only from approved shop drawings and under inspection by the Bureau of Contract Administration. To arrange for inspection, the interested party shall call (213) 580-1392 at least two weeks in advance for items more than fifty (50) miles outside of the City of Los Angeles, and 24 hours in advance for others.

Section 12

ACKNOWLEDGMENTS

The Department of City Planning would like to express its gratitude and appreciation to the following individuals and organizations for their valuable assistance in the preparation of graphics and illustrations for the Studio City and Cahuenga Pass Streetscape Plan:

Florence Blecher, Lic. Architect
Mary Sager McFadden, ASOA, and Lic. Architect
Art Curtis, Illustrator
Tree People
Learni Narvas, Computer Illustrator
Department of Water and Power

13.1 Other Relevant City Requirements

Other Departments' actions will be regulated by their sections of the Municipal Code—the Ventura/Cahuenga Boulevard Corridor Specific Plan says its relations to other portions of the Code overrule the Zoning Code only. These other Code sections include:

56.00	Definitions should be used to avoid confusion. "Street," for instance refers to a legal fiction; "roadway" refers to that on which cars drive.
56.08	Obstructions. Tree limb clearance over non-local streets: 13'-6". Tree limb clearance over local streets: 11'-0". Tree limb clearance over sidewalks and parkways: 9'-0".
62.00	More definitions.
62.103	Paving of parkways in commercial and industrial districts.
62.105.1	Locations of driveway approaches.
62.200	Visibility triangle at intersections
91.0301(b)2	Post-earthquake standards for block walls.
91.6209(g)	Transfer of sign area
91.6210(h)	Transfer of sign area
10-15-90	BOE and Bureau of Street Maintenance agreement on mitigation for removal of street trees.

13.2 Background-Ventura/Cahuenga Boulevard Corridor Specific Plan

The Ventura/Cahuenga Boulevard Corridor Specific Plan was adopted in February 1991 to control future development within the Specific Plan area. Its purpose is to provide funding to complete infrastructure and aesthetic improvements that address long standing problems such as parking, traffic congestion and blight which negatively impact the local business environment and the quality of life in adjacent residential neighborhoods.

New development, additions or remodels of properties within the Specific Plan area are subject to the provisions of the Ventura/Cahuenga Boulevard Corridor Specific Plan ordinance and the Streetscape Plan and Design Guidelines for the community in which the property is located. Property owners contemplating a new development, addition or remodel within the Specific Plan area are encouraged to consult with these

documents, available from the City of Los Angeles Planning Department.

The Ventura/Cahuenga Boulevard Corridor Specific Plan Ordinance document generally includes the land restrictions that control the scale of future development such as height requirements, density limitations, set-backs, allowable uses, signage restrictions, parking ratios and the application of various fees including the Project Impact Assessment Fees.

The Streetscape Plans coordinate the streetscape improvements for each community bordering the boulevard including landscaping and required hardscape materials to be used in public and private landscaped areas. The Plans also regulate materials, furniture and equipment not allowed to be used on sidewalks and public right-of-ways. Separate Streetscape Plans are to be adopted for each of the five Boulevard Communities.

The Streetscape Plan also includes design guidelines for on-site projects. The intent of the design guidelines is to carry over the streetscape elements onto private development in order to create a sense of unity between private property and the public-right-of-way. Incorporating design elements such as landscaping, hardscape, building components including color, facade treatment, and architectural features, will contribute the community's sense of place.

13.3 History

The Cahuenga Pass is a natural crossing in the Santa Monica mountains. The area was originally inhabited by Gabrieliño natives of Shoshonean descent. Specific information about these early residents is scanty, but they apparently had two primary settlements in the general area; that of Kawengna (which supposedly means "little hills") at the approximate site of present day Hollywood and that of Toluca (said to mean "fertile valley").

Don Gaspar de Portola and the missionary fathers came to the area and crossed the Pass in the mid 1700's. He named the valley the "Valley de Santa Catalina de Bonesia de los Encinos" or the Valley of the Oaks. After the death of Father Junipero Serra, the padres and laborers built Mission San Fernando Rey de España, and at the top of the Pass they also built Cahuenga Chapel, tributary to the Mission. In 1822, Spanish rule gave way to Mexican rule. At that time the entire valley was devoted to farming and ranching especially centered around the nearby mission. Unfortunately, the Cahuenga Chapel fell into disuse with the granting of the Cahuenga Rancho by the government to Señor Ramirez in 1836.

The pathway through the Pass grew into and became part of the old El Camino Real which linked the missions of Alta California

together. It grew further along the northern edge of the hills and became the roadway that is now Cahuenga/Ventura Boulevard.

The history of the Pass in the mid-nineteenth century is full of tales of battles fought, hidden gold and treasure, banditos and highwaymen. These stories include men like Pico, Fremont, Vasquez and Murietta. They also include reports of the U.S. Army's Camel Corps experiment during the Civil War where seventy-seven camels and dromedaries, imported from Egypt and Arabia, were tried as a means of transportation (the "Camel Freight Train"), through the rugged terrain of the area driven by, among others, a local man known as "Greek George" Caralambo.

On January 13, 1847, the Treaty of Cahuenga was signed near the site of the old Cahuenga Chapel by General Andrés Pico and General John C. Fremont. In this treaty, Mexico surrendered Alta California to the Americans. That site has recently been confirmed archeologically as being adjacent to the reconstructed Campo de Cahuenga.

California and Los Angeles ultimately became a series of large land grant holdings, transferred and/or purchased from the old California Mexican families. The property surrounding Mission San Fernando Rey passed to Andrés Pico and his family in the 1850's. They later sold it to the San Fernando Permanent Home Association. As of 1887 this general area was part of the old Lankershim Ranch Land and Water Company and was primarily used for citrus groves and nut orchards.

In 1915, the area later known as Studio City was annexed to the City of Los Angeles with adequate water assured by the recently opened aqueduct from the Owens Valley. Settlers moved to the San Fernando Valley where they continued to establish farms, orchards and cultivated fields.

The area grew with the times and with Hollywood. In the 1910's, with the advent of the moving picture, film studios began to take root where orchards once had been. In 1912 Carl Laemmle and his family started leasing (and in 1915 buying) a 230 acre former chicken ranch and chicken farm that had been part of the Oak Crest Ranch in the Lankershim Township. He set out to make movies under the banner of the Universal Film and Manufacturing Company. Producer Mack Sennett, not to be outdone, established his studio in 1913 at what is now approximately the intersection of Laurel Canyon and Ventura Boulevards. He made short silent comedic films until 1928 when "talkies" radically changed the movie industry with the advent of recorded sound. Because of these and other nearby studios, the new elite of movie stars, film people, and other wealthy individuals began buying property in the Cahuenga Pass hills and above Studio City, while the rest of the Valley remained mainly small farms and orchards. These people built estates and simple country

retreats somewhat more modestly than those on the other side of the Santa Monica mountains ridge. In 1933, Republic Studios bought the old Sennett property and held it until 1963 when it sold the lot to CBS, the current owner.

DEPARTMENT OF CITY PLANNING

Con Howe, Director of Planning
Franklin Eberhard, Deputy Director
Gordon B. Hamilton, Deputy Director
Robert Sutton, Deputy Director

COMMUNITY PLANNING BUREAU - VALLEY

Linn Wyatt, Senior City Planner
Rita Schack, City Planner
Tom Rath, City Planning Associate
Elva Nuno-O'Donnell, City Planning Associate

GRAPHICS

Elvia Hernandez, Graphic Designer II

PUBLICATION

Rita Schack, City Planner