



DEPARTMENT OF CITY PLANNING

APPEAL RECOMMENDATION REPORT

City Planning Commission

Date: December 17, 2020
*Continued from November 5, 2020,
October 22, 2020*

Time: after 8:30 a.m.

Place: In conformity with the Governor's Executive Order N-29-20 (March 17, 2020) and due to concerns over COVID-19, the CPC meeting will be conducted entirely telephonically by Zoom [<https://zoom.us/>].

The meeting's telephone number and access code access number will be provided no later than 72 hours before the meeting on the meeting agenda published at <https://planning.lacity.org/about/commissions-boards-hearings> and/or by contacting cpc@lacity.org

Public Hearing: Required

Appeal Status: Not further appealable

Expiration Date: Subject to Mayor's Public Order, Tolling of Deadlines

Multiple Approval: No

PROJECT LOCATION: 1600-1614 East Venice Boulevard

PROPOSED PROJECT: The demolition of three existing residential structures (nine dwelling units) and the construction of a new four-story, maximum 41-feet in height, 49,948 square foot, multi-family apartment building consisting of 77 units over one level of subterranean parking containing 43 automobile stalls. The project reserves seven of the units for Extremely Low Income Households.

REQUEST:

1. Pursuant to Sections 12.22 A.31 and 12.22 A.25(g) of the Los Angeles Municipal Code ("LAMC"), an appeal of the entire determination by the Director of Planning in approving a Transit Oriented Communities Affordable Housing Incentive Program for a project totaling 77 dwelling units, reserving seven units for Very Low Income Household occupancy for a period of 55 years, with the following requested incentives:
 - a. **Yards/Setback.** The project shall be permitted a reduced front yard setback of 5 feet.
 - b. **Height.** The project shall be permitted an increase of 11 feet in building height, equal to a maximum building height of 41 feet.
2. An appeal of a determination that based on the whole of the administrative record, that the Project is exempt from the California Environmental Quality Act (CEQA) pursuant to State CEQA Guidelines, Article 19, Section 15332 (Class 32), and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.

Case No.: DIR-2019-4920-TOC-1A

CEQA No.: ENV-2019-4921-CE

Incidental Cases: None

Related Cases: None

Council No.: 11 – Bonin

Plan Area: Venice

Plan Overlay: None

Certified NC: Venice

GPLU: High Medium Residential

Zone: [Q]R4-1XL

Applicant: Venice Wave LP,
1600 Venice LLC

Applicant Representative: Michael Cohanzad,
Matthew Hayden, Venice Wave LP

Appellant 1: Mickey Ramos and 35 Additional Persons

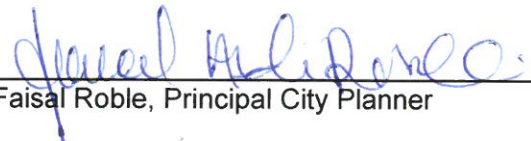
Appellant 1 Representative: Kate Scanlon-Double

Appellant 2: Allen Sarlo

RECOMMENDED ACTIONS:

1. **Deny** the appeals;
2. **Determine** that, based on the whole of the administrative record, the Project is exempt from the California Environmental Quality Act (CEQA) pursuant to State CEQA Statute and Guidelines, Article 19, Section 15332 (Class 32), and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to State CEQA Statute and Guidelines, Section 15300.2 applies;
3. **Sustain** the action of the Director of Planning to conditionally approve the TOC Affordable Housing Incentive Program request for a project totaling 77 dwelling units, reserving seven units for Extremely Low Income Household occupancy for a period of 55 years, with the following incentives:
 - a. **Yards/Setback.** The project shall be permitted a reduced front yard setback of 5 feet.
 - b. **Height.** The project shall be permitted an increase of 11 feet in building height, equal to a maximum building height of 41 feet.

VINCENT P. BERTONI, AICP
Director of Planning


Faisal Roble, Principal City Planner


Juliet Oh, Senior City Planner


Jordann Turner, City Planner


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ADVICE TO PUBLIC: *The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Written communications may be mailed to the *Commission Secretariat, Room 272, City Hall, 200 North Spring Street, Los Angeles, CA 90012* (Phone No. 213-978-1300). While all written communications are given to the Commission for consideration, the initial packets are sent to the week prior to the Commission's meeting date. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters, assistive listening devices, or other auxiliary aids and/or other services may be provided upon request. To ensure availability of services, please make your request not later than three working days (72 hours) prior to the meeting by calling the Commission Secretariat at (213) 978-1300.

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PROJECT ANALYSIS

Project Summary

On April 20, 2020, the Director of Planning approved a Transit Oriented Communities (“TOC”) Affordable Housing Incentive Program for a project totaling 77 dwelling units, reserving 7 units for Extremely Low Income household occupancy for a period of 55 years, with three Base Incentives (Density, Floor Area Ratio (“FAR”), and Parking) and two Additional Incentives under Tier 2 for a reduction in the front yard setback and an increase in height.

The project is for the construction of a new four-story, 49,948 square foot apartment building consisting of 77 residential dwelling units. The project will have a maximum height of 41 feet and a Floor Area Ratio (FAR) of approximately 3.64:1. One level of subterranean parking will be provided, consisting of 43 parking spaces. The project provides 72 long-term, 10 short-term bicycle parking spaces. The project proposes the demolition of the existing residences.

Background

Subject Property

The project site consists of three (3) contiguous lots located at 1600-1614 East Venice Boulevard, within the Venice Community Plan. The subject lots are zoned [Q]R4-1XL with a General Plan land use designation of High Medium Residential. The site is located within the Los Angeles Coastal Transportation Corridor Specific Plan area and the Livable Boulevards Streetscape Plan area. Pursuant to Ordinance No. 169,327, the project is also subject to [Q] Qualified Permanent Conditions of Approval.

The site is rectangular-shaped and flat, with a lot a total lot area of 17,891 square feet, to be reduced to 17,443 square feet after street dedications. The site occupies the southeasterly corner of Venice Boulevard and Glencoe Avenue, with a 138-foot frontage along Venice Boulevard and a 130-foot frontage along Glencoe Avenue. Pursuant to LAMC Section 12.37, the Bureau of Engineering requires the applicant to provide various street improvements along with the following street dedications: a 20-foot corner radius dedication at the corner of Venice Boulevard and Glencoe Avenue and a 2.5-foot dedication along the alley.

The subject property is located in an area with a mix of single- and multi-family dwellings. A multi-family residence abuts the property to the northeast. To the south of the property is a single-family neighborhood, zoned R1V2. Immediately across Venice Boulevard is a multi-family structure, and immediately across Glencoe Avenue is a duplex. The site is bound to the northwest by Venice Boulevard, to the southwest by Glencoe Avenue, and to the southeast by an alleyway.

The property was previously developed with a two-unit apartment, a three-unit apartment, and a four-unit apartment, which have been demolished under the following permits: 1600 East Venice Boulevard (Permit No. 19019-30000-01327, No. 19019-30000-01329, No. 19019-30000-00999), 1604 East Venice Boulevard (Permit No. 19019-30000-01001, No. 19019-30000-01157), 1610 Venice Boulevard (Permit No. 19019-30000-01002, No. 19019-30000-01155, No. 19019-30000-01159). According to a determination issued by the Los Angeles Housing and Community Investment Department (HCIDLA) on June 11, 2019, nine (9) units are subject to replacement under AB 2556 (formerly AB 2222).

The project proposes the demolition of three existing residential structures, and the construction of a 41-foot tall, four-story, 49,948 square-foot, multi-family structure over one level of subterranean parking, containing a total of 77 dwelling units, reserving 7 dwelling units (9 percent

of the total units) for Extreme Low Income households for 55 years. The project proposes a total of 43 vehicular parking spaces and 82 bicycle parking spaces (72 long-term, 10 short-term). The project will grade and export 8,800 cubic yards of earth. Access to the proposed subterranean parking level will be located along the rear alleyway. Two pedestrian entrances are proposed: one at the rear alley and one on the corner of Venice Boulevard and Glencoe Avenue.

Zoning and Land Use Designation

The project site is located in the Venice Community Plan, which designates the site for High Medium Residential land uses with R4 as the corresponding zone. The subject site is zoned [Q]R4-1XL, with a Height District No. 1XL limitation that restricts building height to 30 feet and FAR to 3:1. The Q Conditions were established by Ordinance No. 169,327 (effective February 28, 1994), with requirements including noticing requirements for tenants, guest parking, and a maximum height of 45 feet. In addition, the project is within a TOC Affordable Housing Incentive Area of the Transit Oriented Communities Program. Pursuant to LAMC Section 12.22 A.31 and the TOC Guidelines, the applicant requests a Transit Oriented Communities Compliance Review.

Transit Oriented Communities Affordable Housing Incentive Program

The project qualifies for the Transit Oriented Communities ("TOC") Affordable Housing Incentive Program, which allows a variety of incentives for increased density, height, and floor area, among others, for Eligible Housing Projects. Measure JJJ was adopted by the Los Angeles City Council and established the TOC Affordable Housing Incentive Program. The measure required that the Department adopt a set of TOC Guidelines, which establish incentives for residential or mixed use projects located within ½ mile of a major transit stop, as defined under existing State law.

The TOC Guidelines, adopted September 22, 2017 and amended on February 26, 2018, establish a tier-based system with varying development bonuses and incentives based on a project's distance from different types of transit. The largest bonuses are reserved for those areas in the closest proximity to significant rail stops or the intersection of major bus rapid transit lines. Required affordability levels are increased incrementally in each higher tier. The incentives provided in the TOC Guidelines describe the range of bonuses from particular zoning standards that applicants may select.

The subject property is located within 2,640 feet from the intersection of a Regular plus Rapid Bus Major Transit Stop. The Santa Monica Rapid Bus line 3 has a service interval of approximately 13.12 minutes and regular Metro Bus line 33 has a service interval of approximately 12.7 minutes, as identified in the Affordable Housing Referral Form dated August 8, 2019. Therefore, the subject property is located within a TOC Affordable Housing Incentive Area.

As a Housing Development located within a TOC Affordable Housing Incentive Area, the project is eligible for TOC Incentives as provided in the TOC Guidelines. The subject site is eligible for TOC Tier 2 Incentives due to its proximity to the intersection of two Rapid Bus lines. Specifically, the site is within 2,640 feet from the intersection of Metro Rapid Line 733 and Santa Monica Rapid Line 3.

Tier 2 Base Incentives require On-Site Restricted Affordable Units at the rate of 9 percent for Extremely Low Income, 12 percent for Very Low Income, or 21 percent for Lower Income, of the total number of units. Two Additional Incentives may be granted for projects that include at least 7 percent of the base units for Extremely Low Income Households, at least 10 percent of the base units for Very Low Income Households, at least 20 percent of the base units for Lower Income Households, or at least 20 percent of the base units for persons and families of Moderate Income in a common interest development. The applicant is proposing seven (7) Extremely Low Income

units consistent with the Base Incentive requirements, and which make the project eligible for two Additional Incentives.

Tier 2 Base Incentives:

- a. **Density.** Increase the maximum allowable number of dwelling units permitted by up to 60 percent. The [Q]R4-1XL Zone allows for a maximum residential density of one dwelling unit per 400 square feet of lot area. For the purpose of calculating density, the property has a total lot area of 18,922 square feet. The Los Angeles Municipal Code allows 47 units by right ($18,922 / 400 = 47.3$), however, the TOC Guidelines round base density up to the next whole number, 48. The maximum allowed density for the subject site under the Tier 2 Density Incentive would be 77 dwelling units. The project is proposing 77 dwelling units.
- b. **Floor Area Ratio.** Percentage increase of up to 45 percent. In the R4 Zone in Height District 1XL, the Los Angeles Municipal Code provides for a maximum FAR of 3:1. Los Angeles Municipal Code Section 12.03 states, "Buildable Area" is, "all that portion of a lot located within the proper zone for the proposed main building, excluding those portions of the lot which must be reserved for yard spaces, building line setback space, or which may only be used for accessory buildings or uses." The buildable area for the subject site is 13,725 square feet for a by-right floor area 41,175 square feet. The Tier 2 FAR Incentive allows for a maximum FAR percentage increase up to 45%, for a maximum floor area of approximately 59,699 square feet. The project is proposing 49,948 square feet, which is consistent with the TOC requirements.
- c. **Residential Parking.** Parking for all residential units in an Eligible Housing Development for a Tier 2 project shall not be required to exceed one-half space per bedroom. The project is required to provide 43 parking spaces under the TOC incentive and is providing 43 spaces, consistent with the requirements of the Los Angeles Municipal Code 12.22-A.31.

Tier 2 Additional Incentives:

- a. **Yard/Setback.** In Tier 2 areas, the TOC incentive for projects located on a corner lot allows for the front yard setback to be in alignment with the façade of the adjoining building along the same front lot line. The adjacent property, located at 1616 East Venice Boulevard, provides a 5-foot front yard setback. The project is providing a 5-foot front yard, consistent with the TOC Yard Reduction Incentive.
- b. **Height Incentives.** The TOC Height Incentive allows an additional 11 feet in height, over the base height, for a Tier 2 project. The additional 11 feet would allow the subject site a maximum height of 41 feet. The project is proposing 41 feet, which is consistent with the TOC requirements.

The table below provides a summary of the relevant and underlying LAMC provisions for the subject property and requested TOC Base and Additional Incentives:

Incentive	Otherwise Allowed/Required	TOC Guidelines	Proposed
Density	47 units	77 units	77 units
FAR	3:1	4.35:1	3.64:1
Parking Spaces	117 spaces	43 spaces	43 spaces
Front Yard/Setback	15-feet	5-feet	5-feet
Height	30-feet	41-feet	41-feet

Urban Design Studio

The Proposed Project was reviewed by the Department of City Planning, Urban Design Studio (UDS) on August 12, 2020. The resulting comments and suggestions focus primarily on the pedestrian experience, 360-degree design, and climate adaptive design. The following includes a discussion of UDS comments and suggestions.

Pedestrian First:

- Appreciate that project is not parked over minimum required and that all spaces are fully subterranean
- Location of bike parking and work area is in farthest corner of garage from elevators, under ramp and without opportunity to access natural ventilation or light
- Pedestrian entry could use more emphasis and lobby is not particularly generous for a development of this scale

360° Design:

- Renderings/colored elevations are more convincing than line drawings but building seems bland over all; at least all sides are equally uninteresting
- Consider adding a variety of materials than just different colors of paint on stucco or other method

Climate-Adapted:

- Area on roof for future solar must be indicated, per 2019 CalGreen/Title 24 requirements
- Sizing courtyard at minimum 15' width may be a less than pleasant space, together with the interior units that, despite balconies, may not have optimal access to daylight and natural cross-ventilation
- Consider reducing size of a unit facing Glencoe on one or more levels, to provide additional connection with outdoors and increase cross-ventilation beyond open elevator lobbies; these smaller, more 'local' common open spaces might be more attractive to use than those on roof
- Be aware that 2019 CBC now provides an exception, allowing A-occupancies above residential in fully-sprinklered buildings, meaning that roof gardens no longer are limited to 750 sq. ft. max.
- Unclear as to which of those species listed are to be considered as the 20 required trees

APPEAL ANALYSIS

Two (2) appeals were filed in a timely manner within the appeal period ending on May 5, 2020 by abutting property owners:

- Appellant A. - Kate Scanlon-Double, on behalf of Mickey Ramos and 35 Additional Persons
- Appellant B. - Allen Sarlo

The following is a summary of the appeal points (provided in Exhibit A) and staff's response. Appeal points are organized by appeal point topic (1, 2, 3, etc) and then by appellant (A, B, C). Similar appeal points are grouped for staff response.

Appeal Point 1-A:

The TOC Guidelines contain requirements not authorized by Measure JJJ. The project violates Measure JJJ by utilizing the Tiers for determining the base incentives and by granting Additional Incentives not authorized by Measure JJJ, including a significant transitional height incentive. Nowhere does Measure JJJ authorize Additional Incentives for reduced setback.

Staff Response:

The applicant contends the *TOC Tiers and the Additional Incentives violate City Charter Section 464(a), because as substantive deviations from a ballot measure, Measure JJJ, they require voter approval as per Section 5.A. and City Charter Section 464(a).*

Voter approved Measure JJJ was officially adopted by the Los Angeles City Council as Ordinance No. 184,745 on December 13, 2016. Measure JJJ included the Transit Oriented Communities (TOC) Affordable Housing Incentive Program which is now codified at Los Angeles Municipal Code (LAMC) Section 12.22 A.31. LAMC 12.22.A.31(b) & (c) establishes the process for the preparation and adoption of the TOC Incentive Guidelines. Subsection (b) directs Director of Planning to prepare the TOC Affordable Housing Incentive Program Guidelines (TOC Guidelines). After the Director of Planning prepares the TOC Guidelines, subsection (c) requires that they be presented to the City Planning Commission for a recommendation to either adopt or reject them. After receiving the City Planning Commission's recommendation, the Director of Planning then either adopts or rejects the draft TOC Guidelines. Following this process, the Director of Planning prepared the TOC Guidelines and then presented them to the City Planning Commission at its regularly scheduled meeting on May 25, 2017. Upon consideration of the TOC Guidelines the City Planning Commission recommended their adoption on May 25, 2017. Thereafter, the Director of Planning adopted them and published them on September 22, 2017. The TOC Program became effective on September 22, 2017 and was subsequently revised February 26, 2018. The process followed for the adoption of the TOC Guidelines is documented in Department of City Planning Case File DIR-2017-1914-MSC. Measure JJJ did not include any requirement that the City Council adopt the TOC Guidelines. That function was expressly delegated to the Director of Planning, after receiving a recommendation from the City Planning Commission.

The Department of City Planning structured the Guidelines to provide levels of incentives linked to the quality and proximity of a transit stop. This strategy results in a system that provides different levels of development for a project located a half-mile from a regular bus lines than for one located adjacent to a Metro Rail Station. To reflect these important distinctions a Tier-based system classifies eligible areas into TOC Tiers depending on the project's distance from different types of transit service. All incentives and tiers are in proportion to the affordable housing requirements outlined in JJJ and the development incentives in the City's current Density Bonus program. The specific incentives offered through the program are determined by the TOC Guidelines and are consistent with the provisions of Measure JJJ, including up to either two or three Additional Incentives, depending on the percent of affordable housing provided.

Appeal Point 2-A:

The Decision Maker errs in neglecting to make a finding that an affordable housing assessment and the monitoring program have been accomplished.

Staff Response:

The applicant contends that *Measure JJJ Section 4 mandates that prior to making a material change in land use in a community plan area, the Planning Department must first conduct a comprehensive assessment of the impact of the change on affordable housing. In addition,*

Section 4.A. requires monitoring the inventory of affordable housing with recorded covenants, and an ordinance or law that restricts rents to affordable levels.

Section 4 of Measure JJJ established the following in LAMC Section 11.5.8:

SEC. 11.5.8. GENERAL PLAN REVIEW.

A. Planning Areas. The City is hereby divided into 37 planning areas. Each planning area constitutes an area for which either a community plan, a district plan, or other portion of the Land Use Element of the General Plan has been adopted by the City. The boundaries of each planning area shall be those of the applicable adopted community or district plan, or other portion of the Land Use Element of the General Plan as they existed on enactment of this section. These boundaries may be only changed by amendment to the General Plan pursuant to the procedures set forth in Section 11.5.6 of this Code. No amendment to a plan for any of the 37 planning areas, including reduction in the number of such areas, changes in their respective boundaries, land uses permitted within or at any particular location in any such area, or any other material change, may be made until the completion of a comprehensive assessment of such proposed changes by the Planning Department to ensure that such changes do not:

1. Reduce the capacity for creation and preservation of affordable housing and access to local jobs; or
2. Undermine California Government Code Section 65915 or any other affordable housing incentive program; and

The changes must include a program to create and monitor an inventory of units within the Community Plan Area that are: subject to a recorded covenant, ordinance or law that restricts rents to levels affordable to persons and families of Lower or Very Low-Income; subject to the City Rent Stabilization Ordinance; and/or occupied by Lower-Income or Very Low-Income households.

The requirement for a comprehensive assessment and monitoring is limited to the amendment to a plan for any of the 37 planning areas, including reduction in the number of such areas, changes in their respective boundaries, land uses permitted within or at any particular location in any such area, or any other material change. The subject project does not propose a land use designation change nor an update to the Community Plan Area, and is therefore not subject to this provision. Therefore, the Director did not err or abuse in its discretion in approving the project.

Appeal Point 3-A:

The project fails to meet the criteria for Tier 2 TOC incentives.

Staff Response:

The applicant contends the decision maker erred in finding that the project qualifies for Tier 2 TOC incentives.

The TOC Guidelines provide the following:

III. TOC AFFORDABLE HOUSING INCENTIVE AREA

1. Each one-half mile radius (2,640 feet) around a Major Transit Stop, as defined in subdivision (b) of Section 21155 of the California Public Resources Code, and provided in

Section II of these Guidelines, shall constitute a unique TOC Affordable Housing Incentive Area.

2. Each lot in a TOC Affordable Housing Incentive Area shall be determined to be in a specific Tier (1-4) based on the shortest distance between any point on the lot and a qualified Major Transit Stop, as shown in Chart 1 and Map 1 below. The applicant shall be responsible for providing documentation showing that the location qualifies as a Major Transit Stop and for providing a radius map showing the distance to the Major Transit Stop. Establishment of the appropriate Tier shall take place at the time an application is accepted and the Tier is verified by the City.

Chart 1. TOC Affordable Housing Incentive Area Tiers

Type of Major Transit Stop	Tier 1 (Low)	Tier 2 (Medium)	Tier 3 (High)	Tier 4 (Regional)
	Distance to Major Transit Stop			
Two Regular Buses (intersection of 2 non Rapid Bus* lines, each w/ at least 15 min. average peak headways)	750 - 2640 ft.	< 750 ft.	-	-
Regular plus Rapid Bus* (intersection of a Regular Bus and Rapid Bus line)	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Two Rapid Buses* (intersection of two Rapid Bus lines)	-	1500-2640 ft.	< 1500 ft.	-
Metrolink Rail Stations	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Metro Rail Stations	-	-	≤ 2640 ft.	< 750 ft. from intersection with another rail line or a Rapid Bus*

In accordance with the TOC Guidelines, the subject property was first determined to be located within a TOC Affordable Housing Incentive Area. The subject property is located within 2,640 feet from the intersection of a Regular plus Rapid Bus. The Santa Monica Rapid Bus line 3 has a service interval of approximately 13.12 minutes and regular Metro Bus line 33 has a service interval of approximately 12.7 minutes, as identified in the Affordable Housing Referral Form dated August 8, 2019. Since the Santa Monica Rapid Bus line 3 and the regular Metro Bus line 33 have service intervals of less than 15 minutes, their intersection qualifies as a Major Transit Stop, and subsequently establish a TOC Affordable Housing Incentive Area. The TOC Guidelines do not require 15-minute intervals of two rapid buses in order for a Major Transit Stop be established. Therefore, the subject property is located within a TOC Affordable Housing Incentive Area and may qualify for TOC Incentives.

As specified in the TOC Guidelines, the subject property was determined to be in a specific Tier (1-4) based on the shortest distance between any point on the lot and a qualified Major Transit Stop, as shown in Chart 1. The subject site is within 2,640 feet from the intersection of two Rapid

buses: Metro Rapid Line 733 and Santa Monica Rapid Line 3. Therefore, the subject site is eligible for TOC Tier 2 Incentives. Eligible housing projects may utilize Tier 2 incentives, because of intersection of two rapid bus lines.

The following revision to page 8 the Letter of Determination has been provided:

The project site is located less than ~~1,500~~ 2,640 feet from Venice Boulevard and Glencoe Avenue, the intersection of two Regular Buses, a Santa Monica Rapid Bus line 3, with a service interval of approximately 13.12 minutes, and regular Metro Bus line 33 ~~and Rapid line 733~~, with a service interval of approximately 12.7 minutes. As such, the site ~~qualifies as a Tier 2~~ is located within a TOC Affordable Housing Incentive Area.

As a Housing Development located within a TOC Affordable Housing Incentive Area, the project is eligible for TOC Incentives as provided in the TOC Guidelines. While the Housing Development qualifies for TOC Tier 1 Incentives due to the intersection of two Regular Buses, the Housing Development is further eligible for Tier 2 Incentives. The subject site is eligible for TOC Tier 2 Incentives due to its proximity to the intersection of two Rapid Bus lines. Specifically, the site is within 2,640 feet from the intersection of Metro Rapid Line 733 and Santa Monica Rapid Line 3. Therefore, the Project qualifies for Tier 2 TOC Incentives as provided in the TOC Guidelines.

Therefore, the Director did not err or abuse in its discretion in approving the project.

Appeal Point 4-A:

According to the Decision Maker [a 15-foot setback] would eliminate 16-units, and without the yard incentive, the residential floor area “reserved for Extremely Low Households” might also be reduced. We challenge the Decision Maker’s conclusion that the number of units would decrease by 16. The fact is that 5,000 sf divided by 16 yields an average unit size of less than 313 sf, considerably smaller than any of the apartments proposed in the plans. The alleged loss of 16 units is clearly an exaggeration. Moreover, the loss of 16 units would not affect the residential floor area of the Affordable Units. Only reducing the floor area of the market-rate units would affect the size of the ELI units.

The Decision Maker errs when he ties the height incentive and the resulting increase in square footage to the number of Affordable Units.

Staff Response:

The appellant contends that the reduced front yard is inconsistent with existing front yard setbacks and is not required to avoid the loss of affordable units. The appellant further contends that the number of Extremely Low Income units is not dependent on the additional level made possible by the Height Incentive.

As provided in the Director’s Determination (Exhibit C) and Project Background above, the project site is located in the Venice Community Plan, which designates the site for High Medium Residential land uses with a zoning designation of [Q]R4-1XL with Height District No. 1XL. The site’s zoning would allow a base density of 47 dwelling units, building height of 30 feet, with a maximum 3:1 FAR by-right. The site’s zoning would also require a front yard setback of 15 feet for the R4 Zone by LAMC Section 12.11 C.1.

Pursuant to the TOC Guidelines, the project is eligible for, and has been granted two Tier 2 Additional Incentives to construct the proposed project. In Tier 2 areas, the TOC incentive for projects located on a corner lot allows for the front yard setback to be in alignment with the façade

of the adjoining building along the same front lot line. The adjacent property, located at 1616 East Venice Boulevard, provides a 5-foot front yard setback. The project is providing a 5-foot front yard, consistent with the TOC Yard Reduction Incentive.

Additionally, the Tier 2 TOC Height Incentive allows an additional 11 feet in height over the base height. The site's zoning would allow building height of 30 feet. The 11-foot increase would allow the subject site a maximum height of 41 feet. The project is proposing 41 feet, which is consistent with the TOC requirements.

Under the Transit Oriented Communities (TOC) Affordable Housing Incentive Program, LAMC Section 12.22 A.31, and the findings required therein, the Director must approve a transit oriented communities review with additional requested incentives unless the Director makes a finding based on substantial evidence that the incentives do not result in identifiable and actual cost reductions to provide for affordable housing costs. The record does not contain substantial evidence that would allow the Director to deny the incentive by making a finding that the requested incentives do not provide for affordable housing costs per State Law. The California Health & Safety Code Sections 50052.5 and 50053 define formulas for calculating affordable housing costs for Very Low, Low, and Moderate Income Households. Section 50052.5 addresses owner-occupied housing and Section 50053 addresses rental households. Affordable housing costs are a calculation of residential rent or ownership pricing not to exceed 25 percent gross income based on area median income thresholds dependent on affordability levels.

The list of incentives in TOC Guidelines were pre-evaluated at the time the TOC Affordable Housing Incentive Program Ordinance was adopted to include types of relief that minimize restrictions on the size of the project. As such, the Director will always arrive at the conclusion that the on-menu incentives are required to provide for affordable housing costs because the incentives by their nature increase the scale of the project. The requested incentives, an increase of residential density, an increase in FAR, a decrease in required residential parking, a decrease in the front yard setback, and an increase in building height, are expressed in the TOC On-Menu Additional Incentives and, as such, permit exceptions to zoning requirements that result in building design or construction efficiencies that provide for affordable housing costs. The requested incentives allow the developer to expand the building envelope so the additional units can be constructed and the overall space dedicated to residential uses is increased. These incentives support the applicant's decision to set aside 7 Extremely Low Income dwelling units for 55 years.

Therefore, the Director did not err or abuse in its discretion in approving the project.

Appeal Point 5-A:

The decision maker erred and abused its discretion in determining a categorical exemption for this project.

Staff Response:

Project Description

The appellant contends the Decision Maker in describing the proposed project as three-story; it is a four-story residential building.

Finding No. 3 (Environmental Review Findings) contains a typographic error in describing the project as a three-story multi-family structure. The project is correctly described as a four-story multi-family structure in the rest of the Director's Determination. The determination of Categorical Exemption still applies and the Director did not err or abuse in its discretion in approving the project.

Exceptions

The appellant contends there are cumulative impacts of related projects (1015 Venice Blvd., 1808-1816 Lincoln Blvd., 2467 S. Lincoln Blvd., 1503 Venice Blvd., 2422 S. Lincoln Blvd.)

There is not a succession of known projects of the same type and in the same place as the subject project. A 1,000 square foot radius search was conducted on the subject site and there are no proposed Transit Oriented Communities projects within the radius search. As mentioned, the project proposes a 77 unit, maximum 41 feet-tall, four-story multi-family structure with 49,948 square feet of floor area and one level of subterranean parking in an area zoned and designated for such development. Properties in the vicinity are developed with multi-family residential buildings and single-family homes and the subject site is of a similar size and slope to nearby properties. The project is subject to citywide Regulatory Compliance Measures. Therefore, in compliance with other applicable regulations, no foreseeable cumulative impacts are expected.

Traffic Analysis

The appellant contends the vehicle trips generated for the proposed project is an error. *Using the DOT algorithm, we extrapolated the car trips generated from the currently approved 202 New Dwelling Units are estimated at about 900 daily.*

As stated in the Director's Determination, a traffic study dated September 3, 2019 was prepared by Overland Traffic Consultants. The project trip rates used in the assessment were based on the those provided by the Institute of Transportation Engineers (ITE) and LADOT. LADOT issued a Traffic Assessment, dated February 4, 2020, indicating that the analysis conducted adequately describes the project-related impact of the proposed development and that the project would generate a net increase of 341 trips. The Traffic Assessment concluded that the project would not create a significant transportation impact.

Additionally, as stated in the Director's Determination and in Staff Response No 2, thus is eligible for Tier 2 incentives precisely because it is within close proximity to alternative modes of travel, therefore it is more likely that residents of the project will use alternative modes of travel, other than cars. No further evidence, data or analysis, was provided by the appellant to further support their claim or explain how the decision-maker erred.

Appeal Point 6-A:

The Intersection of Venice and Lincoln Boulevards is no longer an eligible Major Transit Stop.

Staff Response:

The applicant states that *as of August 2019, Santa Monica Big Blue Bus Line 3 has reduced service and no longer meets the requirements set by the SCAG methodology (TOC Guidelines Appendix A) and thereby makes the intersection ineligible.*

The Affordable Housing Referral Form dated August 8, 2019, was issued prior to the change of service for the Santa Monica Big Blue Bus Line 3. The application was subsequently filed on August 19, 2019. As discussed in Staff Response to Appeal Point 3-A, the subject property was determined to be within 2,640 feet from the intersection of two Rapid buses: Metro Rapid Line 733 and Santa Monica Rapid Line 3. Therefore, the subject site is eligible for TOC Tier 2 Incentives.

Appeal Point 7-A:

The decision maker ignores the issue of parking impacts in his Determination Letter.

Appeal Point 7-B:

The Project provides insufficient parking to meet parking demand.

Staff Response:

Pursuant to LAMC Section 12.21-A,4 and the established [Q] Qualified Permanent Conditions of Approval (Ordinance No. 169,327), the proposed 77 unit residential building would be required to provide a total of 156 residential parking spaces (117 residential parking spaces and 39 guest parking spaces). As an Eligible Housing Development in Tier 2, the project is entitled to provide ½ parking space per bedroom, or 43 parking spaces. Therefore, the project shall not be required to provide more than ½ parking space per bedroom. As proposed, the project is providing 43 parking spaces. In addition, the project is providing 72 long-term bicycle spaces and 10 short-term bicycle spaces and is situated near the Metro Bus Line 33 and Rapid Line 733 and Santa Monica Rapid Line 3, which encourage alternative modes of transportation and potentially reduces the demand for automobile parking. Therefore, the project is not required to provide additional on-site parking beyond code requirements. Furthermore, with regard to the environmental analysis performed pursuant to CEQA, the adequacy of parking for a project, in and of itself, is not an environmental impact. (See Public Resource Code, section 21099(b)(3).)

Appeal Point 8-A:

There are underlying procedural problems with the PAR.

Staff Response:

The appellant contends the related PAR (pre-application review) process does not provide notification, nor does it afford the public the opportunity to appeal its conclusions. The pre-application review serves as a referral to the Department of City Planning Development Services Center for case filing purposes and as a referral to other City agency for project status and entitlement needs purposes.

Pursuant to LAMC Section 12.22-A,25(g), upon making a decision, the Director shall transmit the determination to the applicant and to all owners of properties abutting, across the street or alley from, or having common corner with the subject property, and to the local Certified Neighborhood Council. Abutting and adjacent were notified and provided the opportunity to appeal the Transit Oriented Communities Determination. Further, the appellant does not cite substantial evidence to make the claim that there are procedural problems with the PAR. Therefore, the right to appeal this project was not comprised.

Appeal Point 9-A:

The appeal instructions are misleading and due process rights are violated.

Staff Response:

The appellant contends *the instructions refer to Density Bonus Compliance Review Determination and not Transit Oriented Affordable Housing Incentive Program Compliance Review.*

Pursuant to the TOC Guidelines, applications for TOC Incentives shall follow the density bonus procedures outlined in LAMC 12.22 A.25(g). As such, upon making a decision, the Director shall transmit the determination to the applicant and to all owners of properties abutting, across the street or alley from, or having common corner with the subject property, and to the local Certified Neighborhood Council. The right to appeal this project was not compromised, and therefore the Director did not err or abuse in its discretion.

CONCLUSION

In consideration of the foregoing, it is submitted that the Director of Planning acted reasonably in approving Case No. DIR-2019-4920-TOC. Upon in-depth review and analysis of the issues raised by the appellants for the proposed project at 1600 East Venice Boulevard, no errors or abuse of discretion by the Director of Planning or his/her designees were found in regards to the appeal points raised. For the reasons stated herein, and as provided in the Findings in the Director's Determination, the proposed project does comply with the applicable provisions of the Transit Oriented Communities Affordable Housing Incentive Program, the Los Angeles Municipal Code, and the California Environmental Quality Act. Based upon the above, the Department of City Planning recommends to the City Planning Commission that the appeals be denied and the decision of the Director of Planning be sustained.

EXHIBIT A
APPEAL APPLICATIONS
DIR-2019-4920-TOC-1A



APPLICATIONS:

APPEAL APPLICATION

Instructions and Checklist

Related Code Section: Refer to the City Planning case determination to identify the Zone Code section for the entitlement and the appeal procedure.

Purpose: This application is for the appeal of Department of City Planning determinations authorized by the Los Angeles Municipal Code (LAMC).

A. APPELLATE BODY/CASE INFORMATION

1. APPELLATE BODY

- ☐ Area Planning Commission ☒ City Planning Commission ☐ City Council ☐ Director of Planning
☐ Zoning Administrator

Regarding Case Number: DIR-2019-4920-TOC; ENV-2019-4921-CE; PAR-2019-3781-TOC

Project Address: 1600-1614 E. Venice Blvd., Venice, CA 90291

Final Date to Appeal: 05/05/2020

2. APPELLANT

Appellant Identity:
(check all that apply)

- ☐ Representative ☐ Property Owner
☐ Applicant ☐ Operator of the Use/Site

☒ Person, other than the Applicant, Owner or Operator claiming to be aggrieved
Mickey Ramos

☐ Person affected by the determination made by the Department of Building and Safety

- ☐ Representative ☐ Owner ☐ Aggrieved Party
☐ Applicant ☐ Operator

3. APPELLANT INFORMATION

Appellant's Name: Mickey Ramos

Company/Organization: _____

Mailing Address: 2407 Glencoe Ave.

City: Venice State: Ca Zip: 90291

Telephone: 3108500077 E-mail: EVNA.Venice@gmail.com

a. Is the appeal being filed on your behalf or on behalf of another party, organization or company?

☒ Self ☐ Other: _____

b. Is the appeal being filed to support the original applicant's position? ☐ Yes ☒ No

4. REPRESENTATIVE/AGENT INFORMATION

Representative/Agent name (if applicable): Kate Scanlon-Double

Company: _____

Mailing Address: 1832 Penmar Ave.

City: Venice State: Ca Zip: 90291

Telephone: 3103846277 E-mail: EVNA.Venice@gmail.com

5. JUSTIFICATION/REASON FOR APPEAL

a. Is the entire decision, or only parts of it being appealed? ☒ Entire ☒ Part

b. Are specific conditions of approval being appealed? ☐ Yes ☒ No

If Yes, list the condition number(s) here: _____

Attach a separate sheet providing your reasons for the appeal. Your reason must state:

- ☐ The reason for the appeal ☐ How you are aggrieved by the decision
☐ Specifically the points at issue ☐ Why you believe the decision-maker erred or abused their discretion

6. APPLICANT'S AFFIDAVIT

I certify that the statements contained in this application are complete and true:

Appellant Signature:  Date: May 4, 2020

Mickey (May 4, 2020)

GENERAL APPEAL FILING REQUIREMENTS

B. ALL CASES REQUIRE THE FOLLOWING ITEMS - SEE THE ADDITIONAL INSTRUCTIONS FOR SPECIFIC CASE TYPES

1. Appeal Documents

a. **Three (3) sets** - The following documents are required for each appeal filed (1 original and 2 duplicates)
Each case being appealed is required to provide three (3) sets of the listed documents.

- ☒ Appeal Application (form CP-7769)
☒ Justification/Reason for Appeal
☒ Copies of Original Determination Letter

b. Electronic Copy

- ☒ Provide an electronic copy of your appeal documents on a flash drive (planning staff will upload materials during filing and return the flash drive to you) or a CD (which will remain in the file). The following items must be saved as individual PDFs and labeled accordingly (e.g. "Appeal Form.pdf", "Justification/Reason Statement.pdf", or "Original Determination Letter.pdf" etc.). No file should exceed 9.8 MB in size.

c. Appeal Fee

- ☐ Original Applicant - A fee equal to 85% of the original application fee, provide a copy of the original application receipt(s) to calculate the fee per LAMC Section 19.01B 1.
☒ Aggrieved Party - The fee charged shall be in accordance with the LAMC Section 19.01B 1.

d. Notice Requirement

- ☐ Mailing List - All appeals require noticing per the applicable LAMC section(s). Original Applicants must provide noticing per the LAMC
☐ Mailing Fee - The appeal notice mailing fee is paid by the project applicant, payment is made to the City Planning's mailing contractor (BTC), a copy of the receipt must be submitted as proof of payment.

SPECIFIC CASE TYPES - APPEAL FILING INFORMATION

C. DENSITY BONUS / TRANSIT ORIENTED COMMUNITIES (TOC)

1. Density Bonus/TOC

Appeal procedures for Density Bonus/TOC per LAMC Section 12.22.A 25 (g) f.

NOTE:

- Density Bonus/TOC cases, only the *on menu or additional incentives* items can be appealed.
- Appeals of Density Bonus/TOC cases can only be filed by adjacent owners or tenants (must have documentation), and always only appealable to the Citywide Planning Commission.
- ☒ Provide documentation to confirm adjacent owner or tenant status, i.e., a lease agreement, rent receipt, utility bill, property tax bill, ZIMAS, drivers license, bill statement etc.

D. WAIVER OF DEDICATION AND OR IMPROVEMENT

Appeal procedure for Waiver of Dedication or Improvement per LAMC Section 12.37 I.

NOTE:

- Waivers for By-Right Projects, can only be appealed by the owner.
- When a Waiver is on appeal and is part of a master land use application request or subdivider's statement for a project, the applicant may appeal pursuant to the procedures that governs the entitlement.

E. TENTATIVE TRACT/VESTING

1. Tentative Tract/Vesting - Appeal procedure for Tentative Tract / Vesting application per LAMC Section 17.54 A.

NOTE: Appeals to the City Council from a determination on a Tentative Tract (TT or VTT) by the Area or City Planning Commission must be filed within 10 days of the date of the written determination of said Commission.

- ☐ Provide a copy of the written determination letter from Commission.

F. BUILDING AND SAFETY DETERMINATION

- ☐ **1. Appeal of the Department of Building and Safety determination, per LAMC 12.26 K 1, an appellant is considered the **Original Applicant** and must provide noticing and pay mailing fees.**

a. Appeal Fee

- ☐ Original Applicant - The fee charged shall be in accordance with LAMC Section 19.01B 2, as stated in the Building and Safety determination letter, plus all surcharges. (the fee specified in Table 4-A, Section 98.0403.2 of the City of Los Angeles Building Code)

b. Notice Requirement

- ☐ Mailing Fee - The applicant must pay mailing fees to City Planning's mailing contractor (BTC) and submit a copy of receipt as proof of payment.

- ☐ **2. Appeal of the Director of City Planning determination per LAMC Section 12.26 K 6, an applicant or any other aggrieved person may file an appeal, and is appealable to the Area Planning Commission or Citywide Planning Commission as noted in the determination.**

a. Appeal Fee

- ☐ Original Applicant - The fee charged shall be in accordance with the LAMC Section 19.01 B 1 a.

b. Notice Requirement

- ☐ Mailing List - The appeal notification requirements per LAMC Section 12.26 K 7 apply.
- ☐ Mailing Fees - The appeal notice mailing fee is made to City Planning's mailing contractor (BTC), a copy of receipt must be submitted as proof of payment.

G. NUISANCE ABATEMENT

1. Nuisance Abatement - Appeal procedure for Nuisance Abatement per LAMC Section 12.27.1 C 4

NOTE:

- Nuisance Abatement is only appealable to the City Council.

a. Appeal Fee

- ☐ Aggrieved Party the fee charged shall be in accordance with the LAMC Section 19.01 B 1.

2. Plan Approval/Compliance Review

Appeal procedure for Nuisance Abatement Plan Approval/Compliance Review per LAMC Section 12.27.1 C 4.

a. Appeal Fee

- ☐ Compliance Review - The fee charged shall be in accordance with the LAMC Section 19.01 B.
- ☐ Modification - The fee shall be in accordance with the LAMC Section 19.01 B.

NOTES

A Certified Neighborhood Council (CNC) or a person identified as a member of a CNC or as representing the CNC may not file an appeal on behalf of the Neighborhood Council; persons affiliated with a CNC may only file as an individual on behalf of self.

Please note that the appellate body must act on your appeal within a time period specified in the Section(s) of the Los Angeles Municipal Code (LAMC) pertaining to the type of appeal being filed. The Department of City Planning will make its best efforts to have appeals scheduled prior to the appellate body's last day to act in order to provide due process to the appellant. If the appellate body is unable to come to a consensus or is unable to hear and consider the appeal prior to the last day to act, the appeal is automatically deemed denied, and the original decision will stand. The last day to act as defined in the LAMC may only be extended if formally agreed upon by the applicant.

This Section for City Planning Staff Use Only		
Base Fee:	Reviewed & Accepted by (DSC Planner):	Date:
Receipt No:	Deemed Complete by (Project Planner):	Date:
<input type="checkbox"/> Determination authority notified		<input type="checkbox"/> Original receipt and BTC receipt (if original applicant)

Appeal Application

Final Audit Report

2020-05-04

Created:	2020-05-04
By:	Planning Intranet (planning.intranet@lacity.org)
Status:	Signed
Transaction ID:	CBJCHBCAABAAwqgLeNmFX99xZaxyRJ5As6hKBJa-G13i

"Appeal Application" History



Web Form created by Planning Intranet (planning.intranet@lacity.org)

2020-04-17 - 11:54:27 PM GMT



Web Form filled in by Mickey (EVNA.Venice@gmail.com)

2020-05-04 - 5:48:19 PM GMT- IP address: 104.238.46.187



Document emailed to Mickey (EVNA.Venice@gmail.com) for signature

2020-05-04 - 5:48:22 PM GMT



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E-signature verified by Mickey (EVNA.Venice@gmail.com)

2020-05-04 - 5:49:09 PM GMT- IP address: 47.151.157.13



Signed document emailed to Mickey (EVNA.Venice@gmail.com) and Planning Intranet (planning.intranet@lacity.org)

2020-05-04 - 5:49:09 PM GMT

April 30, 2020

APPEAL OF DIR-2019-4920-TOC, ENV-2019-4921-CE and PAR-2019-3781-TOC
1600-1614 EAST VENICE BLVD, VENICE CA 90291**LIST OF ADDITIONAL APPELLANTS:****1.**Appellant' Name: **RONALD DOUGLAS**

Company/Organization

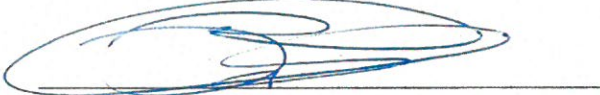
Mailing Address: 1616 Venice Blvd #101

City: Venice

State: CA

Zip: 90291

Telephone: (818) 517-8237 + (818) 606-1565

E-mail: nelsondouglas@gmail.com + dana@danamdouglaslaw.com**2.**Appellant' Name: **ERICA LYSE MOORE**

Company/Organization: Bonne Bouffe Catering & Events

Mailing Address: 1521 Venice Blvd

City: Venice

State: CA

Zip: 90291

Telephone: (310) 629-7423

E-mail: bbcatering@earthlink.net**3.**Appellant' Name: **RUSSEL EPHRAIM**

Company/Organization

Mailing Address: Beethoven Avenue

City: Mar Vista

State: CA

Zip: 90066

Telephone: (845) 628-4709

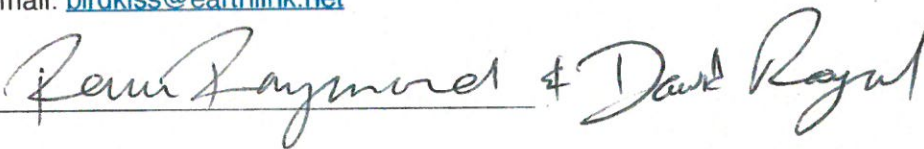
E-mail: rephraim50@yahoo.com**NO!**

4.

Appellant' Name: **PATRICK HOUSTOUN**
Company/Organization
Mailing Address: Appleton Avenue
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State: CA
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Telephone: (310) 402-9952
E-mail: phoustoun@yahoo.com

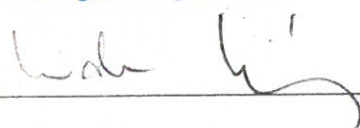
5.

Appellant' Name: **RENEE & DAVID RAYMOND**
Company/Organization
Mailing Address: 1035 Marco Place
City: Venice
State: CA
Zip: 90291
Telephone: (310) 497-8266
E-mail: birdkiss@earthlink.net



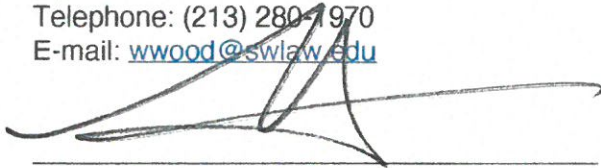
6.

Appellant' Name: **VIRAG VIDA**
Company/Organization
Mailing Address: 1620 Venice Blvd #304
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State: CA
Zip: 90291
Telephone: (310) 437-3021
E-mail: viragvida@gmail.com



7.

Appellant' Name: **WILLIAM WOOD**
Company/Organization
Mailing Address: 2001 Penmar Avenue #3
City: Venice
State: CA
Zip: 90291
Telephone: (213) 280-1970
E-mail: wwood@swlaw.edu



8.

Appellant' Name: **DAVID & LISETTE PIDGEON**
Company/Organization
Mailing Address: 2470 Penmar Avenue
City: Venice
State: CA
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Telephone: (206) 962-9111
E-mail: dpinch1@yahoo.com



9.

Appellant' Name: **LAWRENCE SZABO**
Company/Organization
Mailing Address: 1245 Palms Blvd
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State: CA
Zip: 90291
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E-mail: szabo.lawrence@gmail.com



10.Appellant' Name: **MICHAEL 'MIKE' GANTZ**

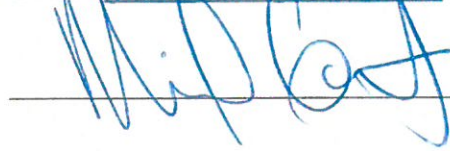
Company/Organization

Mailing Address: 2411 Glencoe Avenue

City: Venice

State: CA

Zip: 90291

Telephone: (323) ²⁵³ 628-2079E-mail: [gantzmike@yahoo.com](mailto:gantz_mike@yahoo.com)

11.Appellant' Name: **GREG NAHABEDIAN**

Company/Organization

Mailing Address: 1506 Venice Blvd #305

City: Venice

State: CA

Zip: 90291

Telephone: (818) 613-0593

E-mail: shaggyone600@gmail.com

12.Appellant' Name: **KATIE LENEGHAN**

Company/Organization

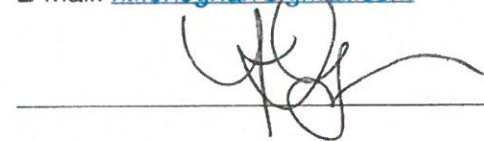
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City: Venice

State: CA

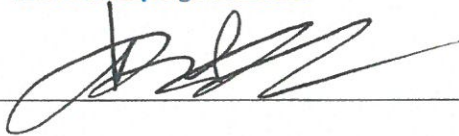
Zip: 90291

Telephone: (904) 400-2751

E-mail: kt.leneghan@gmail.com

13.

Appellant' Name: **LAITISHA JONES**
Company/Organization
Mailing Address: 1520 Venice Blvd #1
City: Venice
State: CA
Zip: 90291
Telephone: (406) 599-8589
E-mail: latisha.raej@gmail.com



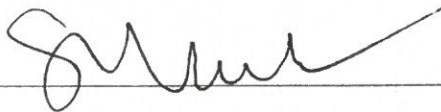
14.

Appellant' Name: **KRISTOPHER VALENTINE**
Company/Organization
Mailing Address: 1520 Venice Blvd #2
City: Venice
State: CA
Zip: 90291
Telephone: (805) 341-4577
E-mail: kristophervallentine@gmail.com



15.

Appellant' Name: **SARAH JEAN MARK**
Company/Organization
Mailing Address: 1520 Venice Blvd #3
City: Venice
State: CA
Zip: 90291
Telephone: (339) 832-0329
E-mail: sarahjeanmark@gmail.com



16.Appellant' Name: **MEGHAN HAYDEN**

Company/Organization

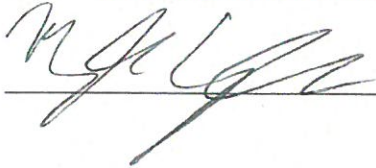
Mailing Address: 1520 Venice Blvd #4

City: Venice

State: CA

Zip: 90291

Telephone: (772) 486-8361

E-mail: meghanlhayden@gmail.com

17.Appellant' Name: **AMBER JONES**

Company/Organization

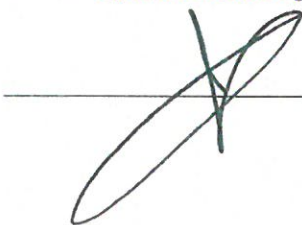
Mailing Address: 1520 Venice Blvd #1

City: Venice

State: CA

Zip: 90291

Telephone: (406) 599-8587

E-mail: ajones.nicole@gmail.com

18.Appellant' Name: **SIERRA BARTER**

Company/Organization

Mailing Address: 1520 Venice Blvd #6

City: Venice

State: CA

Zip: 90291

Telephone: (530) 448-3397

E-mail: sierrakbarter@gmail.com

19.

Appellant' Name: **PHILIPPE GAUDARD**

Company/Organization

Mailing Address: 1520 Venice Blvd #6

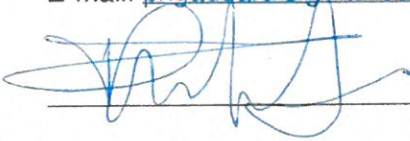
City: Venice

State: CA

Zip: 90291

Telephone: (949) 444-1116

E-mail: phgaudard@gmail.com



20.

Appellant' Name: **LYNDSEY OWSLEY**

Company/Organization

Mailing Address: 1520 Venice Blvd #1

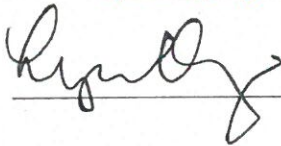
City: Venice

State: CA

Zip: 90291

Telephone: (406) 599-4284

E-mail: lyowsley@gmail.com



21.

Appellant' Name: **DAVID S. EWING**

Company/Organization

Mailing Address: 1234 Preston Way

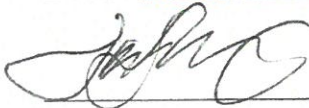
City: Venice

State: CA

Zip: 90291

Telephone: (310) 339-0848

E-mail: moreseriousbus@gmail.com



22.

Appellant' Name: **KATE SCANLON-DOUBLE**

Company/Organization

Mailing Address: 1832 Penmar Avenue

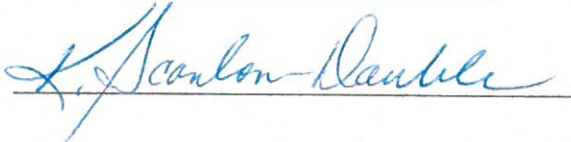
City: Venice

State: CA

Zip: 90291

Telephone: (310) 384-6277

E-mail: katescanlondouble@gmail.com



23.

Appellant' Name: **MARY ELLEN DEGNAN**

Company/Organization

Mailing Address: 1520 Venice Blvd #4

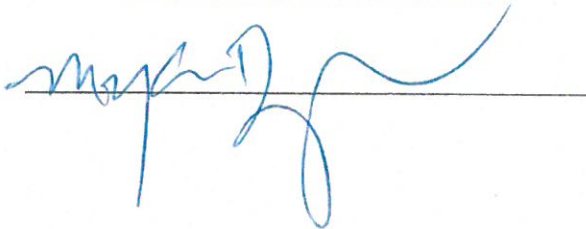
City: Venice

State: CA

Zip: 90291

Telephone: (561) 251-1552

E-mail: Maryellendegnan@gmail.com



24.

Appellant' Name: **ANAIS WADE**

Company/Organization

Mailing Address: 1520 Venice Blvd #5

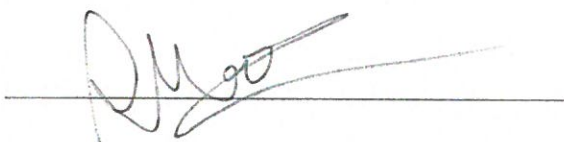
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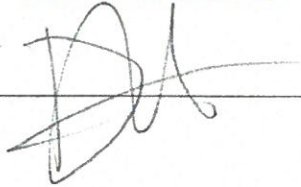
Telephone: (904) 400-2751

E-mail: anais.wade@gmail.com



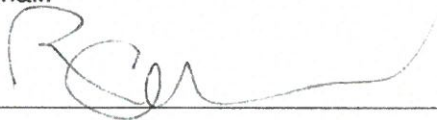
25.

Appellant' Name: **DAX HENRY**
Company/Organization
Mailing Address: 1520 Venice Blvd #5
City: Venice
State: CA
Zip: 90291
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E-mail: dax@anaisdax.com



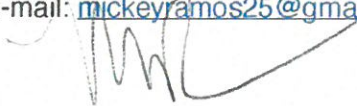
26.

Appellant' Name: **RYIAN ANDRAOS**
Company/Organization
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State: CA
Zip: 90291
Telephone: (213) 362-8779
E-mail:



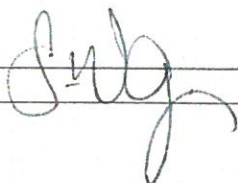
27.

Appellant' Name: **MICKEY RAMOS**
Company/Organization
Mailing Address: 2704 Glencoe Avenue
City: Venice
State: CA
Zip: 90291
Telephone: (310) 850-0077
E-mail: mickeyramos25@gmail.com



28.

Appellant' Name: **SEAN GOSSER**
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29.

Appellant' Name: **RUTH PONTIUS**
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E-mail: EVNA.Venice@gmail.com

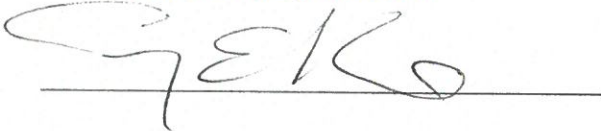
30.

Appellant' Name: **JAMES CUNNINGHAM**
Company/Organization
Mailing Address: 2417 Walgrove Avenue
City: Venice
State: CA
Zip: 90291
Telephone: (310) 980-7461
E-mail: james@objectanimal.com



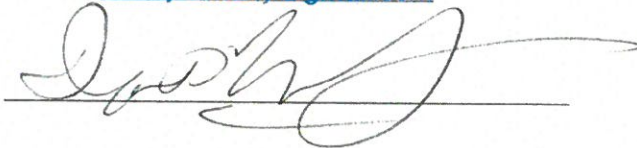
31.

Appellant' Name: **AMY KANE**
Company/Organization
Mailing Address: 2417 1/2 Walgrove Avenue
City: Venice
State: CA
Zip: 90291
Telephone: (310) 779-3674
E-mail: kane_amy@yahoo.com



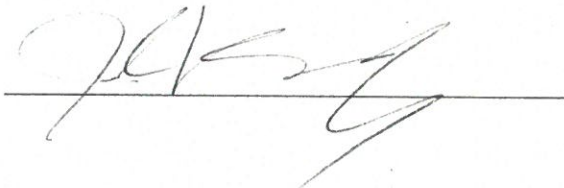
32.

Appellant' Name: **DAVID MORIARTY**
Company/Organization
Mailing Address: 1601 Venice Blvd #401
City: Venice
State: CA
Zip: 90291
Telephone: (310) 990-4010
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33.

Appellant' Name: **JOHN KOWALSKY**
Company/Organization
Mailing Address: 1601 Venice Blvd #402
City: Venice
State: CA
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34.

Appellant' Name: **RYAN LOUIS COOPER**
Company/Organization
Mailing Address: 1616 Venice Blvd # 106
City: Venice
State: CA
Zip: 90291
Telephone: (206) 330-8632
E-mail: rlcooper72@gmail.com



35.

Appellant' Name: **ALEX POLLINI**
Company/Organization
Mailing Address: 1601 Venice Blvd # 302
City: Venice
State: CA
Zip: 90291
Telephone: (760) 920-3215
E-mail: pollini.alex@gmail.com





APPLICATIONS:

APPEAL APPLICATION

Instructions and Checklist

Related Code Section: Refer to the City Planning case determination to identify the Zone Code section for the entitlement and the appeal procedure.

Purpose: This application is for the appeal of Department of City Planning determinations authorized by the Los Angeles Municipal Code (LAMC).

A. APPELLATE BODY/CASE INFORMATION

1. APPELLATE BODY

- ☐ Area Planning Commission ☒ City Planning Commission ☐ City Council ☐ Director of Planning
☐ Zoning Administrator

Regarding Case Number: DIR-2019-4920-TOC ; ENV-2019-4921-CE ; PAR-2019-3781-TOC

Project Address: 1600 -1614 East Venice Blvd, Venice CA 90291

Final Date to Appeal: 05/05/2020

2. APPELLANT

Appellant Identity:
(check all that apply)

- ☐ Representative ☐ Property Owner
☐ Applicant ☐ Operator of the Use/Site

☒ Person, other than the Applicant, Owner or Operator claiming to be aggrieved

☐ Person affected by the determination made by the **Department of Building and Safety**

- ☐ Representative ☐ Owner ☐ Aggrieved Party
☐ Applicant ☐ Operator

3. APPELLANT INFORMATION

Appellant's Name: RUTH PONTIUS

Company/Organization: _____

Mailing Address: 2418 GLENCOE AVENUE

City: VENICE State: CA Zip: 90291

Telephone: (310) 306-8056 E-mail: EVNA.Venice@gmail.com

a. Is the appeal being filed on your behalf or on behalf of another party, organization or company?

☒ Self ☐ Other: _____

b. Is the appeal being filed to support the original applicant's position? ☐ Yes ☒ No

4. REPRESENTATIVE/AGENT INFORMATION

Representative/Agent name (if applicable): KATE SCAULON - DOUBLE

Company: _____

Mailing Address: 1832 PENMAR AVENUE

City: VENICE State: CA Zip: 90291

Telephone: (310) 384-6277 E-mail: EVNA.Venice@gmail.com

5. JUSTIFICATION/REASON FOR APPEAL

a. Is the entire decision, or only parts of it being appealed? ☒ Entire ☒ Part

b. Are specific conditions of approval being appealed? ☐ Yes ☒ No

If Yes, list the condition number(s) here: _____

Attach a separate sheet providing your reasons for the appeal. Your reason must state:

- ☐ The reason for the appeal ☐ How you are aggrieved by the decision
☐ Specifically the points at issue ☐ Why you believe the decision-maker erred or abused their discretion

6. APPLICANT'S AFFIDAVIT

I certify that the statements contained in this application are complete and true:

Appellant Signature: Ruth Portius Date: 4/30/20

GENERAL APPEAL FILING REQUIREMENTS

B. ALL CASES REQUIRE THE FOLLOWING ITEMS - SEE THE ADDITIONAL INSTRUCTIONS FOR SPECIFIC CASE TYPES

1. Appeal Documents

a. **Three (3) sets** - The following documents are required for each appeal filed (1 original and 2 duplicates)
Each case being appealed is required to provide three (3) sets of the listed documents.

- ☒ Appeal Application (form CP-7769)
☒ Justification/Reason for Appeal
☒ Copies of Original Determination Letter

b. Electronic Copy

- ☒ Provide an electronic copy of your appeal documents on a flash drive (planning staff will upload materials during filing and return the flash drive to you) or a CD (which will remain in the file). The following items must be saved as individual PDFs and labeled accordingly (e.g. "Appeal Form.pdf", "Justification/Reason Statement.pdf", or "Original Determination Letter.pdf" etc.). No file should exceed 9.8 MB in size.

c. Appeal Fee

- ☐ Original Applicant - A fee equal to 85% of the original application fee, provide a copy of the original application receipt(s) to calculate the fee per LAMC Section 19.01B 1.
☒ Aggrieved Party - The fee charged shall be in accordance with the LAMC Section 19.01B 1.

d. Notice Requirement

- ☐ Mailing List - All appeals require noticing per the applicable LAMC section(s). Original Applicants must provide noticing per the LAMC
☐ Mailing Fee - The appeal notice mailing fee is paid by the project applicant, payment is made to the City Planning's mailing contractor (BTC), a copy of the receipt must be submitted as proof of payment.

SPECIFIC CASE TYPES - APPEAL FILING INFORMATION

C. DENSITY BONUS / TRANSIT ORIENTED COMMUNITIES (TOC)

1. Density Bonus/TOC

Appeal procedures for Density Bonus/TOC per LAMC Section 12.22.A 25 (g) f.

NOTE:

- Density Bonus/TOC cases, only the *on menu or additional incentives* items can be appealed.
- Appeals of Density Bonus/TOC cases can only be filed by adjacent owners or tenants (must have documentation), and always only appealable to the Citywide Planning Commission.
- ☒ Provide documentation to confirm adjacent owner or tenant status, i.e., a lease agreement, rent receipt, utility bill, property tax bill, ZIMAS, drivers license, bill statement etc.

D. WAIVER OF DEDICATION AND OR IMPROVEMENT

Appeal procedure for Waiver of Dedication or Improvement per LAMC Section 12.37 I.

NOTE:

- Waivers for By-Right Projects, can only be appealed by the owner.
- When a Waiver is on appeal and is part of a master land use application request or subdivider's statement for a project, the applicant may appeal pursuant to the procedures that governs the entitlement.

E. TENTATIVE TRACT/VESTING

- 1. Tentative Tract/Vesting** - Appeal procedure for Tentative Tract / Vesting application per LAMC Section 17.54 A.

NOTE: Appeals to the City Council from a determination on a Tentative Tract (TT or VTT) by the Area or City Planning Commission must be filed within 10 days of the date of the written determination of said Commission.

- ☐ Provide a copy of the written determination letter from Commission.

F. BUILDING AND SAFETY DETERMINATION

- ☐ 1. Appeal of the Department of Building and Safety determination, per LAMC 12.26 K 1, an appellant is considered the **Original Applicant** and must provide noticing and pay mailing fees.
- a. Appeal Fee**
- ☐ Original Applicant - The fee charged shall be in accordance with LAMC Section 19.01B 2, as stated in the Building and Safety determination letter, plus all surcharges. (the fee specified in Table 4-A, Section 98.0403.2 of the City of Los Angeles Building Code)
- b. Notice Requirement**
- ☐ Mailing Fee - The applicant must pay mailing fees to City Planning's mailing contractor (BTC) and submit a copy of receipt as proof of payment.
- ☐ 2. Appeal of the Director of City Planning determination per LAMC Section 12.26 K 6, an applicant or any other aggrieved person may file an appeal, and is appealable to the Area Planning Commission or Citywide Planning Commission as noted in the determination.
- a. Appeal Fee**
- ☐ Original Applicant - The fee charged shall be in accordance with the LAMC Section 19.01 B 1 a.
- b. Notice Requirement**
- ☐ Mailing List - The appeal notification requirements per LAMC Section 12.26 K 7 apply.
- ☐ Mailing Fees - The appeal notice mailing fee is made to City Planning's mailing contractor (BTC), a copy of receipt must be submitted as proof of payment.

G. NUISANCE ABATEMENT

1. Nuisance Abatement - Appeal procedure for Nuisance Abatement per LAMC Section 12.27.1 C 4

NOTE:

- Nuisance Abatement is only appealable to the City Council.

a. Appeal Fee

- ☐ Aggrieved Party the fee charged shall be in accordance with the LAMC Section 19.01 B 1.

2. Plan Approval/Compliance Review

Appeal procedure for Nuisance Abatement Plan Approval/Compliance Review per LAMC Section 12.27.1 C 4.

a. Appeal Fee

- ☐ Compliance Review - The fee charged shall be in accordance with the LAMC Section 19.01 B.
- ☐ Modification - The fee shall be in accordance with the LAMC Section 19.01 B.

NOTES

A Certified Neighborhood Council (CNC) or a person identified as a member of a CNC or as representing the CNC may not file an appeal on behalf of the Neighborhood Council; persons affiliated with a CNC may only file as an individual on behalf of self.

Please note that the appellate body must act on your appeal within a time period specified in the Section(s) of the Los Angeles Municipal Code (LAMC) pertaining to the type of appeal being filed. The Department of City Planning will make its best efforts to have appeals scheduled prior to the appellate body's last day to act in order to provide due process to the appellant. If the appellate body is unable to come to a consensus or is unable to hear and consider the appeal prior to the last day to act, the appeal is automatically deemed denied, and the original decision will stand. The last day to act as defined in the LAMC may only be extended if formally agreed upon by the applicant.

This Section for City Planning Staff Use Only		
Base Fee:	Reviewed & Accepted by (DSC Planner):	Date:
Receipt No:	Deemed Complete by (Project Planner):	Date:
<input type="checkbox"/> Determination authority notified		<input type="checkbox"/> Original receipt and BTC receipt (if original applicant)

May 4, 2020

APPEAL OF DIR-2019-4920-TOC, ENV-2019-4921-CE, and PAR-2019-3781-TOC
1600-1614 EAST VENICE BLVD, VENICE CA 90291

APPEAL JUSTIFICATION

We are aggrieved by the approval of this project for many reasons. Some reasons are basic, substantive reasons related to the legitimacy of the Transit Oriented Community Affordable Housing Incentive Program Guidelines (TOC Guidelines), and other reasons speak to the Department of City Planning decisions in the Director's Determination letter of April 20, 2020.

Additionally, this proposed development is going to impact the street parking that's vital to renters and business owners alike. It's going to increase congestion on Venice Blvd. and nearby residential streets, which will tend to discourage customers from coming to businesses. Glencoe Avenue is already used by commuters as an alternate link between Venice Blvd and Washington Blvd. It will adversely impact the pedestrian experience on this block for members of the community and the city, as the boulevard sidewalk is used daily for strolling and exercise. The entire stretch of Venice Blvd between Lincoln Blvd and Walgrove Ave. is therefore negatively affected.

The Decision Maker errs when he approves the reduced front yard setback in lieu of the current, minimum, conforming 15-foot setback found along the entire length of Venice Blvd between Lincoln Blvd. and Walgrove Ave. This reduced setback will negatively impact the physical environment for the Venice Community, while benefitting the interests of a single Applicant.

There must be a way to balance the need for more housing with the Venice Community desire to preserve the physical cohesion of the built environment as recommended by the Venice Community Plan.

1. The TOC Guidelines contain requirements not authorized by Measure JJJ.

The language of Measure JJJ limits the incentives to those specified in the Initiative and does not grant carte blanche to a City's Planning Department or Planning Commission to add discretionary Additional Incentives or create TOC Tiers to grant different bonuses within the half mile radius of a major transit stop without going back to the voters.

In addition, the discretionary Additional Incentives approved by the Director of Planning and the CPC were never adopted by the legislative body as required by California Government Code 65915(d)(3). Measure JJJ Section 6 bases its ministerial incentives on the base zone and not on Tiers. Measure JJJ limits the number of incentives to a maximum of three ministerial incentives, which are specified as FAR, density and reduced parking. Each of the Additional Incentives bestowed by the Determination alters otherwise applicable limitations in the LAMC

without complying with the procedural requirements for zone changes, height district amendments and General Plan amendments or variances, all of which procedures provide due process and full transparency.

TOC Tiers and the Additional Incentives violate City Charter Section 464(a), because as substantive deviations from a ballot measure, Measure JJJ, they require voter approval as per Section 5.A. and City Charter Section 464(a). There is no authority to grant discretionary Additional Incentives unless the voters approve this change to the law. City Charter Section 464(a) provides that "any ordinance adopted by a vote of the electors of the City pursuant to an initiative petition cannot be amended or repealed, except by an ordinance proposed either by petition or by the Council at its own instance and adopted by a vote of the electors, or by an amendment of the Charter superseding the ordinance."

Measure JJJ Section 4 mandates that prior to making a material change in land use in a community plan area, the Planning Department must first conduct a comprehensive assessment of the impact of the change on affordable housing. In addition, Section 4.A. requires monitoring the inventory of affordable housing with recorded covenants, and an ordinance or law that restricts rents to affordable levels. The Decision Maker errs in neglecting to make a finding that such assessment and the monitoring program have been accomplished. Therefore, no TOC incentives can be lawfully granted in any Community Plan area until those two prerequisites are met in any TOC case.

Neither the TOC Tiers nor the discretionary Additional Incentives are authorized by Measure JJJ. The project violates Measure JJJ by utilizing the Tiers for determining the base incentives and by granting Additional Incentives not authorized by Measure JJJ, including a significant transitional height incentive.

2. The project fails to meet the criteria for Tier 2 TOC incentives.

As stated above, Measure JJJ does not create TOC Tiers granting different bonuses within the half mile radius of a major transit stop. This feature of the City Planning TOC incentive program cannot override State laws.

However, even if the Tier system were allowed under Measure JJJ, the Decision Maker would still be incorrect in finding that the project qualifies for Tier 2. It is incorrect on two points: distance from intersection and qualifying bus types.

a. Distance:

The Determination Letter errs in stating on page 8, under "Transit Oriented Communities Affordable Housing Incentive Program Background:"

"The subject property is located within a Tier 2 TOC Affordable Housing Incentive Area, qualified by its proximity to the intersection of two Major Transit Stops [sic], Venice Boulevard and Lincoln Boulevard. Specifically, the site is within 1,500 feet from Metro Bus Line 33 and

Rapid Line 733 and Santa Monica Rapid Line 3. As such, the project is located on a site that qualifies for the TOC Tier 2 Incentives.”

This statement is incorrect as the project site is located approximately **1,965 feet** from the Major Transit Stop intersection of Venice Blvd. and Lincoln Blvd. Zimas identifies the lot as TOC Tier 1 because of its distance from the Major Transit Stop.

b. Qualifying Bus Line Types:

Furthermore, the distance from a Major Transit Stop is not the sole criterion a property must meet to qualify for its Tier designation. (see attached Appeal Supporting Material Exhibit A: TOC Guidelines Chart 1)

The Determination statement above is incorrect, as the service intervals of the intersecting bus lines do not meet the 15-minute average interval as required by the TOC Guidelines. (see attached Appeal Supporting Material Exhibit B: TOC Guidelines Appendix A: Methodology for Determining a Major Transit Stop)

The only two qualifying bus lines for the Venice Blvd and Lincoln Blvd Major Transit Stop Intersection are Metro Local Line 33 (Regular bus) and Santa Monica Blue Bus Rapid 3 (Rapid bus).

According to TOC Guidelines Chart 1: a property located between 1500 – 2640 feet from a Major Transit Stop with “Regular plus Rapid Bus” (intersection of a Regular Bus and A Rapid Bus line) qualifies for Tier 1.

The project property’s highest Tier qualification is Tier 1, as its distance from the Major Transit Stop is approximately 1,965 feet and there are two qualifying bus lines, a Regular and a Rapid, at the Major Transit Stop of Venice Blvd and Lincoln Blvd.

Intervals

The eligibility of an intersection to qualify as a Major Transit Stop depends on the frequency of bus service (intervals), and the TOC demands adherence to a specific formula in its guidelines.

The Determination Letter errs (see page 9) in its claims regarding trip intervals:

“...The project site is located less than 1,500 feet [sic] from the intersection of a Santa Monica Rapid Bus line 3 with a service interval of approximately 13.12 minutes and regular Metro Bus line 33 and Rapid line 733 with a service interval of approximately 12.7 minutes. Therefore, the project meets the eligibility requirement for proximity to a Major Transit Stop.”

TOC Guidelines Appendix A: SCAG Methodology: “To determine the eligibility of the bus line, the average number of minutes per trip for each direction is calculated separately. **If one or both directions fail to meet the 15 minute frequency limit, the entire bus line is ineligible for a Major Transit Stop.**”

Therefore, besides citing the wrong distance again, this Determination Letter is erroneous on three additional issues:

1. it lists the service interval for only one direction of each qualifying bus;
2. it does not list separate service intervals for the regular Metro Bus Line 33 and the Rapid Line 733;

it improperly approximates the intervals of regular Bus line 33 and Rapid 733 at 12.7 minutes. According to the Incentive Area Tier Verification Summary Table prepared by the appellants, following SCAG methodology (see attached Appeal Supporting Material Exhibit C), the service intervals for all the buses at this intersection are:

- Santa Monica Bus Line Rapid 3: Northbound -13.13 min.; Southbound - 12.73 min.
- Regular Santa Monica Bus Line 3 Northbound – 20.00 min.; Southbound – 21.00 min.
- Metro Rapid Line 733: Eastbound and Westbound - both 16.8 min.
- Regular Metro Bus Line 33: Eastbound - 12.35 min.; Westbound- 12.73 min.

The Determination Letter errs by omitting the fact that the service intervals of Metro Rapid Line 733 are 16.8 minutes both Eastbound and Westbound, and therefore this Rapid line is not a qualifying bus line. The Transit Oriented Community - Referral Form (Form CP-4050) Section II, Project Eligibility, erroneously identifies Metro Line 733 as a “transportation qualifier”, which resulted in the assignment of the wrong tier to this project. The correct tier is Tier 1.

The Determination Letter is erroneous in assigning Tier 2. The property does not meet the criteria specified in the TOC Guidelines for both the distance from a Major Transit Stop and qualifying bus types to achieve Tier 2 status. As previously demonstrated, the property is in a Tier 1 Affordable Housing Incentive Area.

(See Section 5 below for a further discussion of the Pre-Application Review (PAR))

The “Augmentation” Issue

The TOC Guidelines and SCAG Methodology make no provision for combining separate but overlapping bus routes that do not individually meet the 15-minutes threshold (see attached Appeal Supporting Material, Exhibit D: Connect SoCal, Draft 2020 RTP/SCS Transit Technical Appendix).

There are no criteria in the TOC Guidelines that allow the Planning Department to use a qualifying Regular Bus to “augment” a non-qualifying Rapid Bus in order to achieve qualifying Rapid Bus status and thereby elevate the Type of Major Transit Stop.

However, the Appellant requested any existing written record or documents that permit City Planning to disregard the Tier Verification requirement set out in Chart 1 of the TOC Guidelines. No evidence of such permission was made available, nor were any documents memorializing a different interpretation of the TOC Guidelines requirements.

There is no support for the legitimacy of an alternate interpretation of the requirements for TOC Affordable Housing Incentive Area Tiers as a basis for this project's determination.

The East Venice Neighborhood Association filed a California Public Records Act request with the Planning Department for all communications, documents and other records related to the "augmentation" policy direction regarding Los Angeles City Transit Oriented Community (TOC) guidelines. "Augmentation" is the term used by City Planner Eric Claros in orally describing the use of a qualifying Regular Bus to "augment" a non-qualifying Rapid Bus in order to achieve qualifying Rapid Bus status and thereby elevate the Type of Major Transit Stop.

The Planning Department was unable to provide any such document and stated that it had provided every record it had on the subject other than those exempted from production under California Government Code section 6255 by "deliberative process privilege." Until such documentation, dated on or before the date of the TOC Tier Verification Form (CP-4051) and TOC Referral Form (CP-4050), can be provided, the Appellants and the Planning Commission have no reason to accede to the unsupported assertion that such "augmentation" is legal. If it is not supported in the law, there is no legal basis for granting Tier 2 incentives to this project.

3. Finding 1

a. Yard/Setback

Nowhere does Measure JJJ authorize Additional Incentives for reduced setback.

Per TOC Guidelines the reduction of Yard/Setback in Tier 1 and Tier 2 is limited as follows:

"Residential Zones: Eligible Housing Developments in Residential zones may utilize a reduction in the front, rear or side yards as follows:

1. *Front Yards: Front yard reductions are limited to no more than the average of the front yards of adjoining buildings along the same street frontage. Or, if located on a corner lot or adjacent to a vacant lot, the front yard setback may align with the façade of the adjoining building along the same front lot line. If there are no adjoining buildings, no reduction is permitted. In Tier 3 and Tier 4, the front yard reduction may be paired with one other individual yard reduction, per subsection 2 below, which will require the use of only one incentive."*

Finding 1, page 12, paragraph 4, the Decision Maker grants the 5-foot setback in lieu of the minimum 15-foot setback required in R4 zone based on the fact that the building at 1616 Venice Blvd., a 2-story apartment building with 16 RSO units, has a front yard setback of 5-feet.

The 1616 Venice Blvd. apartment building was built in 1928 only two years after the City of Los Angeles annexed Venice. It is the only residential building between Lincoln Blvd and Walgrove Ave. that has a front yard setback that is less than the required minimum 15-foot front yard setback for the R4 zone. There are 36 lots on the south side of Venice Blvd, 22 properties total, and only this 92-year-old building at 1616 Venice Blvd. has a front yard setback that is less than

15-feet. On the north side of the boulevard there are also 36 lots, 26 properties, and only the two commercial-use properties have setbacks that are less than 15-feet (1321 Venice Blvd. with a setback of 7-feet and 1523 Venice with a setback of 9-feet). (See attached Appeal Supporting Material Exhibit E: Materials Supporting Contestation of Reduced Setbacks)

The granting of a 5-foot setback contradicts an 80-years-or-more Planning Department policy and erodes its effect. Permitting this one building to depart from the norm will interrupt the frontage-built plane of the boulevard and oppress the pedestrian experience. Going back to at least the 1940s, building records shows a consistent minimum front yard setback for this section of Venice Blvd. between Lincoln Blvd. and Walgrove Ave. of at least 15-feet, including all the 4 and 5-story multi-family buildings of the 1980s. It appears the Planning Department has required a minimum of 15-feet front yard setback for all residential developments along this portion of Venice Blvd.

Unfortunately, instead of honoring an almost 100-year old building as a point of pride for the community, this misuse of 1616 Venice Blvd to allow adjacent developments to drastically reduce setbacks would now cast it as a liability for enabling the degradation of the block's streetscape. In this particular case, the impact is multiplied, as the proposed development spans three lots.

In addition, the balconies at the upper floors of the proposed development extend 30-inches (2.5-feet) into the 5-foot setback, and the balconies located at the 2nd floor are only 8'-6" above the sidewalk furthering the sense of intrusion on the pedestrian experience.

"... Without the yard incentive, the project's floor area would be reduced by approximately 5,000 square feet, resulting in a decrease of 16 units. With the yard incentive, the project is able to include more residential floor area for dwelling units reserved for Extremely Low Households." (Determination Letter, page 12.)

A 15-foot setback would decrease the project floor area by close to 5,000 sf. According to the Decision Maker it would eliminate 16-units, and without the yard incentive, the residential floor area "reserved for Extremely Low Households" might also be reduced.

We challenge the Decision Maker's conclusion that the number of units would decrease by 16. The fact is that 5,000 sf divided by 16 yields an average unit size of less than 313 sf, considerably smaller than any of the apartments proposed in the plans. The alleged loss of 16 units is clearly an exaggeration.

Moreover, the loss of 16 units would not affect the residential floor area of the Affordable Units. Only reducing the floor area of the market-rate units would affect the size of the ELI units.

b. Height

The Decision Maker errs by stating that the number of ELI units is dependent on the additional level made possible by the Height Incentive.

“... Utilization of the height incentive enables the construction of an additional level, and thus facilitates the addition of approximately 21 units. The additional units support the applicant's decision to reserve seven (7) units for Extremely Low Income Households.”
(Determination Letter, page 12.)

However, in this case, the total number of Affordable Units cannot be any fewer than seven (7) units. This number is not solely dictated by TOC Guidelines; the project must also replace the nine (9) former, on-site Rent Stabilization Ordinance (RSO) Units as mandated by the Los Angeles Housing and Community Investment Department (HCIDLA).

“... A determination dated June 11, 2019 from the Los Angeles Housing and Community Investment Department (HCIDLA) states that nine (9) dwelling units are subject to replacement under Assembly Bill (AB) 2556. Pursuant to HCIDLA's AB 2556 Determination, seven (7) units shall be replaced with equivalent bedroom type, with three (3) units restricted to Extremely Low Income Households, two (2) units restricted to Very Low Income Households, and two (2) units to Low Income Households. For the two (2) remaining units presume [sic] to have been occupied by an above-low income person or that household, as permitted by California Government Code Section 65915(c)(3)(C)(ii), the City has opted to require that those units be replaced in compliance with the City's Rent Stabilization Ordinance (RSO).”
(Determination Letter, page 9, Item 3.)

To be eligible for TOC Incentives a development must provide a minimum number of On-Site Restricted Affordable Units. (The applicant might have chosen to achieve TOC eligibility by providing a different level of affordability. However, that would have increased the total, required number of Affordable Units.)

Moreover, TOC Guidelines do not provide an option for mixing affordability levels to meet the minimum percentage of On-Site Restricted Affordable Units. Therefore, in order to meet both the TOC and HCIDLA requirements with the fewest number of Affordable units, the applicant had no choice but ELI.

The Decision Maker errs when he ties the height incentive and the resulting increase in square footage to the number of Affordable Units.

(Incidentally, no design plans are referenced in, or attached to, the Determination Letter.)

Maximizing the property's number of units may be one of the developer's goals in order to maximize financial gain; but it is not in the interest of the City and the Venice Community to reduce the front yard setback for the sole financial benefit of the Applicant, while being detrimental to the pedestrian experience and the neighborhood as a whole. This is also in conflict with the vision of the Great Streets Initiative and with the Westside Mobility Plan's objective of creating pedestrian friendly, “livable boulevards,” among which Venice Blvd is specified.

4. **Finding 3 - Environmental Review Finding**

CEQA Determination – Class 32 Categorical Exemption Applies

Item 3a. “[...] The project proposes the construction of a three-story residential building containing 77 dwelling units.” (Determination Letter, page 14)

The Decision Maker is negligent in this finding, as the proposed construction is not three-story; it is a **four-story** residential building.

Item 3e. “The project site will be adequately served by all public utilities and services given that the property is located in an urban tract with water supply, sewage and waste disposal infrastructure, and power lines installed.” (Determination Letter, page 15)

The Determination Letter states that “Venice Boulevard and Glencoe Avenue are improved with existing utilities that service the lots in the area” (page 15). What it neglects to state are the age and condition of the utilities cited. In recent years, Venice has increasingly experienced ruptures in its century-old water and sewage systems.

But without an Environmental Impact Report to prove whether they can “adequately” cope with the increase this project will create, this is unknown.

Additionally, the true cumulative impact on water, waste management, and other infrastructure is unknown until an overdue, community-wide study is undertaken.

The Decision Maker’s findings regarding the existing infrastructure lack factual support and do not provide a legitimate as the basis for the Class 32 Categorical Exemption. Therefore, the exemption itself is unsupported and conclusory. By law Categorical exemptions are permitted when they fulfill all the required criteria. There is no factual support is provided that this is the case with regard to State CEQA Guidelines, Article 19, Section 15332 item e.

Exceptions Findings

In order to make a finding for a Class 32 Categorical Exemption from the requirement for a CEQA determination, a finding must be made that the project fulfills all the required criteria, which are listed by sequential letters from “a” to “e” in the Director’s Determination Letter.

The Determination Letter goes on to list the State CEQA Guidelines’ Exceptions that, if any of them should apply to the project, would disqualify the project for the exemption. These exceptions are also identified by sequential letters from “a” to “e,” but applied to the individual exceptions in a slightly different sequence than they are in the actual state CEQA guidelines.

The Decision maker errs in this sloppy misidentification. (When we refer to any of these exceptions, we use the identifying letter as assigned in the Letter of Determination, not as used in State CEQA Guidelines Section 15300.2.

The Decision Maker errs for the following reasons in finding that "...the Exceptions outlined in the State CEQA Guidelines Section 15300.2 do not apply to the Project."

Item a. "Cumulative Impact"

Housing

The Decision Maker errs in claiming that *"...No comparable pending projects (TOC or Density Bonus) have been identified within a 500-foot radius of the subject site..."* (Determination Letter page 16)

In making this claim the Decision Maker errs in stating that there is no comparable pending project.

The cumulative impact does not come from TOC or Density Bonus projects only. The impact comes from Multi-Family Dwelling Buildings in general, as well as any significant increase in density that would affect traffic or parking. We are certainly not arguing against multifamily dwellings. Venice needs housing. But neither do we condone denying the reality that the proliferation of multifamily dwellings and consequent increased density may result in cumulative CEQA impacts, and that those potential impacts need to be assessed.

Clearly the mix of by-right zoning, Density Bonus and TOC is creating a "succession of known projects" in the vicinity.

As listed in the Determination Letter, there are some TOC projects around the Major Transit Stop at the Intersection of Venice and Lincoln Blvds., some closer to and some farther from the intersection but all affecting the quality of life of the Venice Community.

Since the inception of TOC in 2018, Los Angeles City Planning has approved 3 TOC projects in Venice totaling a Net Increase of Dwelling Units in the amount of 171 Units (183 new minus 12 demolished).

1. 56-Units at 1015 Venice (replacing 3 units) (residential – TOC)
2. 50-Units at 1808-1816 Lincoln Blvd. (mixed-use – TOC)
3. 77-units at 1600 Venice Blvd. (replacing 9 units) (residential – TOC)

This list of comparable projects by the Decision Maker is arbitrary and incomplete and presents a false impression by failing to include three other buildings.

4. 31-Units at 1503 Venice Blvd. (residential)
5. 20-Units at 2422 S. Lincoln Blvd. (mixed use – TOC)
6. 40-Units at 2467 S. Lincoln Blvd. (Supportive Housing – DB)

Comparable project #4, at 1503 Venice Blvd., is well within the 500 ft radius referenced in the Determination Letter. Less than a block west of the proposed development, this Apartment Building Project replacing an old Church is currently under construction just 280 ft. from the applicant's project site.

The other two projects (#s 5 and 6), both with active planning applications, are at almost the exact same distance from the proposed development site as project #1 at 1015 Venice Blvd., that is referenced in the Determination Letter.

With 202 Approved New Dwelling Units (the net 171 units plus the 31-units replacing the church) and more in the pipeline, when will the City take into consideration the consequences of an individual project on the cumulative impact?

No additional known TOC Projects are anticipated at the moment, as **the Intersection of Venice and Lincoln Blvds. is no longer eligible a Major Transit Stop**. As of August 2019, Santa Monica Big Blue Bus Line 3 has reduced service and no longer meets the requirements set by the SCAG methodology (TOC Guidelines Appendix A) and thereby makes the intersection ineligible.

However, in the 2020 SCAG Regional Transportation Plan, the methodology will change as indicated in the Connect SoCal, Draft 2020 RTP/SCS Transit Technical Appendix (see attached Appeal Supporting Material Exhibit D)

“Separate but overlapping bus routes that do not individually meet the 15-minute threshold may not be combined in order to qualify as an HQTC. However, based on RTTAC feedback, there are certain corridors where overlapping “line families” or local/bus rapid transit (BRT) lines are intended to function as one bus route. On these corridors, transit riders typically board the first bus available, whether it be a local, express, or BRT line. For these line families or local/BRT corridors, SCAG uses the combined routes to calculate the frequency.”

TOC Guidelines Chart 1 will need to be amended to reflect this change in the Type of Major Transit Stop definitions as it currently does not list any option for these “line families” combining overlapping types of bus.

Once the **new** SCAG Methodology is in effect and the TOC amended to include this major change; we can expect that more TOC Projects will to be proposed for the Venice Community. Until then, the current methodology must be followed.

The Decision Maker and the City Planning Department seem to be abdicating their responsibility to “plan” for our community, by approving individual housing projects without considering past approvals and anticipated future approvals.

The cumulative impact of anticipated projects must take into account the State mandated increase of housing that will be built in Venice. The Decision Maker errs by not considering this mandated housing increase

Traffic and Parking

The Department of Transportation (DOT) indicated a net increase of 341 trips for a 77-unit Project.

Using the DOT algorithm, we extrapolated the car trips generated from the currently approved 202 New Dwelling Units are estimated at about 900 daily. Apparently these additional trips are insufficient to compel remediation of the substandard and overburdened intersection of Walgrove and Venice, at the corner of the proposed project's block. We believe this is an error, either on the part of the Decision Maker or of Overland Traffic Consultants, who did the project's traffic study. The appellants have not yet had an opportunity to read this traffic study, which we requested approximately a week ago. We hope to amend this Justification document once they've had that opportunity.

The Decision Maker ignores the issue of parking impacts in his Determination Letter.

While the Decision Letter asserts that "...the appurtenant parking reductions are not a discretionary action and therefore cannot be appealed," the appellants challenge the basis of the project's TOC designation and therefore demand that the requisite analysis of parking and traffic impacts be completed.

This project is in an especially parking-challenged location. Street parking is always overtaxed at night. The 1928 apartment building next door at 1616 Venice was built when the Pacific Red Car line ran down the median of Venice Blvd. and has no onsite parking (a Transit Oriented Community of its day). This already creates a strain on street parking. The proposed project, with ½ parking space per unit, will negatively impact the residents of that adjacent building and other neighbor's dependent on street parking, TOC exemptions notwithstanding.

We need a community-wide analysis of cumulative traffic and parking impacts to responsibly plan for community transportation and parking needs.

Additionally, Venice needs a community-wide, **cumulative** impact analysis of our other infrastructure resources (i.e. police, fire, water, sewage, parks, etc.). taking into consideration the age and condition of Venice's infrastructure.

The project's proposed parking of 43 automobile spaces for 77 units, though permitted by the TOC Guidelines for a laudable purpose, is in reality foolish. Although the intention is to reduce the carbon footprint and motivate tenants to use public transportation, this is unlikely to happen any time soon, especially when bus service has already been cut back due to diminishing ridership.

Data show that public transit is down to unprecedented levels (even pre-Covid-19). Metro's direction (NextGen Bus Plan) is to remove most of the Rapid Bus lines and convert regular bus lines to be more like Rapid lines by reducing the number of bus stops. This would remove the one feature - short walking distances - that makes taking the bus convenient for most riders.

Categorical Exemptions are granted to “environmentally benign in-fill projects that are consistent with local General Plan and zoning requirements” (Public Resources Code Section 15332). In other words, Categorical Exemptions are meant for by-right projects, not projects seeking discretionary approvals that have significant impacts.

The Decision Maker errs in making this Finding. A CEQA analysis is required.

5. Underlying Procedural Problems with PAR

The appellants recognize that the Decision Maker used the information in the Referral Form properly but because the information in that form was incorrect, the result is in error. The appellants also realize that the appeal of the referral qualification may not be considered appealable in this case as it has a planning case of its own, PAR-2019-3781-TOC. Although it has its own case number it is “related” to this case and the public does not have the opportunity to challenge its conclusions. Not only is there no notification to neighbors of the existence of the PAR case, there is another more serious problem.

The PAR has an expiration date of 180 days where an applicant during that time may or may not apply for an entitlement which then becomes a case, furthermore once a TOC application has been submitted, neighbors are still not notified. Until demolitions starts on a property no one may know about the project, which may be well beyond any appeal opportunity. may be issued to someone who does not yet own the property therefore by the time the TOC application is filed any opportunity for the public to challenge the facts of the verification may no longer exist

6. The appeal instructions are misleading and due process rights are violated.

On page 23, the instructions refer to Density Bonus Compliance Review Determination and not a Transit Oriented Affordable Housing Incentive Program Compliance Review. It is not clear if the appeal instructions are accurate as they are referencing the wrong type of project. It is not clear what portion of the project can be appealed or by whom. It is not clear whether only abutting property owners and tenants can appeal this determination or whether that language only applies to Density Bonus projects. The determination should have been reissued with the correct appeal information.

Thank you for your diligent attention to these important issues.

1. EXHIBIT A

Reference Materials of TOC Guidelines Chart 1 supporting Contestation of TOC Tier Assignment:

case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

Chart 1. TOC Affordable Housing Incentive Area Tiers

Type of Major Transit Stop	Tier 1 (Low)	Tier 2 (Medium)	Tier 3 (High)	Tier 4 (Regional)
	Distance to Major Transit Stop			
Two Regular Buses (intersection of 2 non Rapid Bus* lines, each w/ at least 15 min. average peak headways)	750 - 2640 ft.	< 750 ft.	-	-
Regular plus Rapid Bus* (intersection of a Regular Bus and Rapid Bus line)	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Two Rapid Buses* (intersection of two Rapid Bus lines)	-	1500-2640 ft.	< 1500 ft.	-
Metrolink Rail Stations	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Metro Rail Stations	-	-	≤ 2640 ft.	< 750 ft. from intersection with another rail line or a Rapid Bus*

Notes:

To be an eligible TOC Housing Development, the project must meet the Eligibility criteria in Section IV, including being located within one-half mile of a Major Transit Stop. In the case of bus stops, this always requires an intersection of two bus routes. An intersection of two bus lines is defined as the midpoint of the street intersection where two or more eligible bus routes meet or cross, and passengers have the direct ability to transfer on foot. This does not include bus routes that travel along the same street. For Tier 4, an intersection between a rail station and an eligible Rapid Bus line is defined as either the rail station entrance(s) or the Rapid Bus stop when the bus stop is within 660 feet of a rail station entrance and can be accessed by foot.

Distance is measured from the closest point on any lot to the entrance(s) of a rail transit station (including elevators and stairways), or the middle of the street intersection of two or more bus routes with a service interval of 15 minutes or less during the morning and afternoon peak commute periods. Please see Appendix A for additional information on how to calculate the 15 minute service interval. In the case of a Tier 4 Major Transit Stop, the distance will be measured from the closest point on any lot to the closer of either the entrance of the rail transit station or the bus stop. If no entrance information is known for a station that is under construction, then the distance will be measured from the center of the platform of the station.

*Rapid Bus is a higher quality bus service that may include several key attributes, including dedicated bus lanes, branded vehicles and stations, high frequency, limited stops at major intersections, intelligent transportation systems, and possible off-board fare collection and/or all door boarding. It includes, but is not limited to, Metro Bus Rapid Transit lines, Metro Rapid 700 lines, Metro Orange and Silver Lines, Big Blue Rapid lines and the Rapid 6 Culver City bus.

2. EXHIBIT B

Reference Materials of TOC Guidelines Appendix A supporting Contestation of TOC Tier Assignment:

case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

Appendix A: Methodology for Determining Major Transit Stops

Definition of Major Transit Stop:

A site containing a rail station or the intersection of two or more bus routes with a service interval of 15 minutes or less during the morning and afternoon peak commute periods. The stations or bus routes may be existing, under construction or included in the most recent SCAG Southern California Association of Governments (SCAG) Regional Transportation Plan (RTP).

SCAG and OPR Methodology:

Peak Periods are considered to be between 6:00 to 9:00 AM and 3:00 to 7:00 PM. Bus routes must have a service frequency of 15 minutes or less for the entire duration of the peak hour periods.

To determine the eligibility of the bus line, the average number of minutes per trip for each direction is calculated separately. If one or both directions fail to meet the 15 minute frequency limit, the entire bus line is ineligible for a Major Transit Stop.

- The total number of trips from the point of origin during peak hours (Monday to Friday) is used. A trip is included if its median time falls within the peak hour.
- To calculate the median time, the time at trip origin is subtracted from the time at arrival at final station, divided by two, and then added to origin time.
For example: Origin time 5:42 AM, Arrival time 6:22 AM
Total trip time = 40 Minutes (6:22 AM – 5:42 AM)
Median trip time = 40 Minutes/2 + 5:42 AM, or 6:02 AM
- The total peak hour time is then divide by the number of trips for the average number of minutes per trip.

Below is a sample calculation based on the 750 Metro Rapid Bus Line (see schedule on Page 16):

Eastbound Trips for 750

During the morning peak hours between 6:00 AM to 9:00 AM, there is a total of 12 Eastbound trips.

The trip originating from Warner Center at 5:42 AM is the first eligible trip with an arrival time at 6:22 AM. This is calculated by dividing the total trip time of 40 minutes by two and adding the 20 minutes to the trip origination time at 5:42 AM, resulting in a median trip time that falls within peak hours at 6:02 AM (not shown in bus schedule).

The trip originating from Warner Center at 8:29 AM is the last eligible trip, with the median time at 8:57 AM.

During the afternoon peak hours between 3:00 PM and 7: PM, there is a total of 16 Eastbound trips.

With 28 total Eastbound trips during the 420 peak hour minutes, the average frequency of the 750 bus line is 15 minutes.

Westbound Trips for 750

Looking at the Westbound trips, there are 11 trips and 15 trips in the AM and PM peak hours respectively. This results in an average frequency of 16.15 minutes.

Result

Despite the Eastbound portion of the 750 Metro Rapid Bus Line meeting the 15 minute frequency as required by a Major Transit Stop, the Westbound portion, with an average frequency of 16.15 minutes, fails to meet that criteria. Therefore, the 750 Metro Rapid Bus Line is ineligible for inclusion in a Major Transit Stop.

Sample Metro Bus Line Schedule with Qualified Peak Hour Trips Boxed in Red

Monday through Friday				750			
Effective Jun 25 2017							
Eastbound <i>Al Este</i> (Approximate Times / Tiempos Aproximados)				Westbound <i>Al Oeste</i> (Approximate Times / Tiempos Aproximados)			
WARNER CENTER	TARZANA	SHERMAN OAKS	STUDIO CITY	STUDIO CITY	SHERMAN OAKS	TARZANA	WARNER CENTER
1	2	3	4	4	3	2	1
Warner Center Transit Hub (Owensmouth & Erwin)	Ventura & Reseda	Ventura & Van Nuys	Universal/ Studio City Station	Universal/ Studio City Station	Ventura & Van Nuys	Ventura & Reseda	Warner Center Transit Hub (Owensmouth & Erwin)
5:13A	5:25A	5:36A	5:50A	5:23A	5:38A	5:52A	6:10A
5:28	5:40	5:51	6:06	5:45	6:00	6:14	6:32
5:42	5:54	6:07	6:22	6:00	6:15	6:29	6:48
5:55	6:09	6:23	6:38	6:15	6:30	6:47	7:04
6:09	6:23	6:39	6:54	6:30	6:46	7:04	7:25
6:23	6:37	6:55	7:10	6:45	7:01	7:22	7:43
6:37	6:51	7:11	7:27	7:00	7:17	7:39	8:00
6:50	7:05	7:27	7:44	7:15	7:32	7:54	8:15
7:05	7:20	7:43	8:00	7:30	7:49	8:11	8:32
7:21	7:36	7:59	8:18	7:45	8:04	8:26	8:47
7:35	7:52	8:15	8:34	8:00	8:19	8:41	9:02
7:54	8:10	8:32	8:51	8:15	8:34	8:56	9:17
8:12	8:28	8:49	9:08	8:30	8:49	9:11	9:32
8:29	8:45	9:06	9:25	8:45	9:04	9:26	9:47
8:46	9:02	9:23	9:42	9:00	9:19	9:41	10:02
9:11	9:27	9:48	10:07	9:18	9:37	9:59	10:20
9:41	9:57	10:18	10:37	9:40	9:59	10:18	10:39
10:10	10:27	10:48	11:07	10:05	10:24	10:43	11:05
10:40	10:57	11:18	11:37	10:35	10:54	11:13	11:35
11:10	11:27	11:48	12:07P	11:04	11:24	11:43	12:06P
11:38	11:57	12:18P	12:37	11:34	11:54	12:13P	12:36
12:08P	12:27P	12:48	1:08	12:03P	12:24P	12:44	1:07
12:35	12:54	1:15	1:35	12:33	12:54	1:14	1:37
1:00	1:19	1:40	2:01	1:02	1:24	1:44	2:07
1:21	1:41	2:02	2:23	1:32	1:54	2:14	2:37
1:46	2:06	2:27	2:48	2:01	2:24	2:44	3:08
1:58	2:18	2:39	3:00	2:25	2:49	3:09	3:34
2:13	2:33	2:54	3:15	2:44	3:10	3:31	3:56
2:27	2:47	3:09	3:30	3:00	3:25	3:46	4:11
2:42	3:02	3:24	3:45	3:15	3:40	4:01	4:25
2:55	3:17	3:39	4:00	3:30	3:55	4:16	4:40
3:10	3:31	3:53	4:14	3:45	4:10	4:31	4:55
3:25	3:47	4:09	4:30	4:00	4:26	4:47	5:11
3:44	4:06	4:28	4:49	4:15	4:41	5:02	5:26
3:55	4:17	4:39	5:00	4:30	4:56	5:17	5:41
4:10	4:32	4:54	5:15	4:45	5:11	5:32	5:56
4:25	4:47	5:09	5:30	5:00	5:26	5:48	6:12
4:40	5:02	5:24	5:45	5:15	5:41	6:03	6:27
4:55	5:17	5:39	6:00	5:30	5:56	6:18	6:41
5:10	5:32	5:54	6:15	5:45	6:11	6:33	6:56
5:26	5:48	6:09	6:30	6:00	6:26	6:47	7:09
5:41	6:03	6:24	6:45	6:15	6:40	7:00	7:21
5:57	6:19	6:39	7:00	6:35	6:58	7:17	7:38
6:20	6:40	6:59	7:19	6:54	7:16	7:34	7:54
6:42	7:01	7:19	7:39	7:15	7:36	7:54	8:13
7:04	7:21	7:39	7:58	7:40	8:01	8:19	8:38
7:35	7:51	8:08	8:26	8:09	8:27	8:44	9:03
8:10	8:25	8:42	8:58	8:39	8:57	9:12	9:28
				9:10	9:27	9:42	9:58

3. EXHIBIT C

Materials supporting Contestation of TOC Tier Assignment:

in case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

LA Planning Form CP-4051: Transit Oriented Communities - Tier Verification

*“[...] The Department of City Planning reserves the right to require an updated form for the project if more than **180** days have transpired since the approval date, or as necessary, to reflect project modifications, policy changes and/or amendments to the LAMC, local laws, and State laws. ”*

As clarified by LA Planning Housing Department the expiration date is to incentivize applicant to promptly submit an application for entitlement; if a project moves forward the Approved Tier Verification stands even if the Bus Line might lose its eligibility status. A new Tier Verification might also be submitted in the case a Bus Line achieves new eligibility. That might explain why there are three (3) different Tier Verification Applications. (see Appendix D -Appeal Supporting Material)

It is the Appellants understanding that Case #: ADM-2018-1208-TOC submitted on 02/28/2019 and approved on 03/20/2019 indicate the Bus Lines assessed for the Tier Verification as these Qualifier Service Intervals are indicated in the Project Eligibility Transportation Section on Case#: PAR-2019-3781-TOC Form CP-4050, Transit Oriented Communities – Referral Form.

PAR-2019-3781-TOC, section II.2 indicate:

Qualifier #1: Local Line 33 + R733

Service Interval #1: 12.7

Service Interval #2:12.4

Qualifier #2: Santa Monica Rapid Line 3

Service Interval #1: 13.12

Service Interval #2: 12.72

The Appellants did an independent Tier Verification for all Tier Verification ADM Cases related to this project submitted to City Planning, and although there are minor differences in the Service Intervals, the Qualifiers Bus Lines used for the Intersection did not change their qualification status.

Incentive Area Tier Verification Summary Table

by EVNA

CASE#: **ADM-2018-1208-TOC (tier verification)**

SUBMITTAL DATE: **2/28/19**

Project Address: 1600 E. Venice Blvd., Venice

	Bus #	Bus Type	Intervals per SCAG (min)	Effective Date	TOC Eligible
LA Metro	33	Regular	East 12.35, West 12.73	12/16/18	YES
LA Metro	733	Rapid	East 16.80, West 16.80	12/16/18	No
Big Blue Bus	3	Regular	North 20.00, South 21.00	9/16/18	No
Big Blue Bus	Rapid 3	Rapid	North 13.13, South 12.73	9/16/18	YES

ELIGIBLE TIER PER CHART 1: **TIER 1**

Chart 1. TOC Affordable Housing Incentive Area Tiers

Type of Major Transit Stop	Tier 1 (Low)	Tier 2 (Medium)	Tier 3 (High)	Tier 4 (Regional)
	Distance to Major Transit Stop			
Two Regular Buses (intersection of 2 non Rapid Bus* lines, each w/ at least 15 min. average peak headways)	750 - 2640 ft.	< 750 ft.	-	-
Regular plus Rapid Bus* (intersection of a Regular Bus and Rapid Bus line)	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Two Rapid Buses* (intersection of two Rapid Bus lines)	-	1500-2640 ft.	< 1500 ft.	-
Metrolink Rail Stations	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Metro Rail Stations	-	-	≤ 2640 ft.	< 750 ft. from intersection with another rail line or a Rapid Bus*

LA Metro Bus Line 33

(December 16 2018)

LAM - 33

12/16/18
6/23/19

Eastbound

AM (6-9am)	Main & Sunset	Main & 7th	Travel Time	Median	Start + Median	
	5:07	6:06	0:59	0:29	5:36	Not eligible
1	5:30	6:32	1:02	0:31	6:01	
2	5:49	6:53	1:04	0:32	6:21	
3	6:07	7:15	1:08	0:34	6:41	
4	6:25	7:37	1:12	0:36	7:01	
5	6:43	7:59	1:16	0:38	7:21	
6	7:02	8:19	1:17	0:38	7:40	
7	7:18	8:39	1:21	0:40	7:58	
8	7:37	8:59	1:22	0:41	8:18	
9	7:56	9:18	1:22	0:41	8:37	
10	8:16	9:34	1:18	0:39	8:55	
	8:36	9:57	1:21	0:40	9:16	Not eligible

Total AM trips 10

PM (3-7pm)	Main & Sunset	Main & 7th	Travel Time	Median	Start + Median	
	2:05	3:39	1:34	0:47	2:52	Not eligible
1	2:15	3:47	1:32	0:46	3:01	
2	2:26	4:01	1:35	0:47	3:13	
3	2:35	4:11	1:36	0:48	3:23	
4	2:42	4:16	1:34	0:47	3:29	
5	2:48	4:27	1:39	0:49	3:37	
6	2:56	4:33	1:37	0:48	3:44	
7	3:04	4:46	1:42	0:51	3:55	
8	3:13	4:52	1:39	0:49	4:02	
9	3:23	5:05	1:42	0:51	4:14	
10	3:33	5:13	1:40	0:50	4:23	
11	3:42	5:26	1:44	0:52	4:34	
12	3:52	5:33	1:41	0:50	4:42	
13	4:00	5:43	1:43	0:51	4:51	
14	4:11	5:54	1:43	0:51	5:02	
15	4:22	6:05	1:43	0:51	5:13	
16	4:33	6:15	1:42	0:51	5:24	
17	4:44	6:29	1:45	0:52	5:36	
18	4:55	6:36	1:41	0:50	5:45	
19	5:07	6:46	1:39	0:49	5:56	
20	5:18	6:55	1:37	0:48	6:06	
21	5:29	7:05	1:36	0:48	6:17	
22	5:40	7:15	1:35	0:47	6:27	
23	5:51	7:24	1:33	0:46	6:37	
24	6:05	7:35	1:30	0:45	6:50	
	6:28	7:49	1:21	0:40	7:08	Not eligible

Total PM trips 24

Eastbound Total Trips 34 Intervals Time 420/34 12.35

Westbound

AM (6-9am)	Main & 7th	Main & Sunset	Travel Time	Median	Start + Median	
	5:12	6:15	1:03	0:31	5:43	Not eligible
1	5:31	6:38	1:07	0:33	6:04	
2	5:44	6:55	1:11	0:35	6:19	
3	5:57	7:13	1:16	0:38	6:35	
4	6:08	7:24	1:16	0:38	6:46	
5	6:11	7:34	1:23	0:41	6:52	
6	6:15	7:41	1:26	0:43	6:58	
7	6:20	7:47	1:27	0:43	7:03	
8	6:24	7:52	1:28	0:44	7:08	
9	6:28	7:57	1:29	0:44	7:12	
10	6:32	8:03	1:31	0:45	7:17	
11	6:37	8:12	1:35	0:47	7:24	
12	6:46	8:22	1:36	0:48	7:34	
13	6:55	8:32	1:37	0:48	7:43	
14	7:04	8:42	1:38	0:49	7:53	
15	7:14	8:52	1:38	0:49	8:03	
16	7:25	9:04	1:39	0:49	8:14	
17	7:37	9:16	1:39	0:49	8:26	
18	7:51	9:28	1:37	0:48	8:39	
19	8:07	9:41	1:34	0:47	8:54	
	8:22	9:54	1:32	0:46	9:08	Not eligible

Total AM trips 19

PM (3-7pm)	Main & 7th	Main & Sunset	Travel Time	Median	Start + Median	
	2:03	3:34	1:31	0:45	2:48	Not eligible
1	2:19	3:50	1:31	0:45	3:04	
2	2:35	4:06	1:31	0:45	3:20	
3	2:51	4:22	1:31	0:45	3:36	
4	3:06	4:38	1:32	0:46	3:52	
5	3:21	4:54	1:33	0:46	4:07	
6	3:37	5:10	1:33	0:46	4:23	
7	3:53	5:26	1:33	0:46	4:39	
8	4:09	5:41	1:32	0:46	4:55	
9	4:26	5:58	1:32	0:46	5:12	
10	4:43	6:14	1:31	0:45	5:28	
11	4:56	6:30	1:34	0:47	5:43	
12	5:11	6:46	1:35	0:47	5:58	
13	5:29	7:04	1:35	0:47	6:16	
14	5:49	7:23	1:34	0:47	6:36	
	6:25	7:50	1:25	0:42	7:07	Not eligible

Total PM trips 14

Westbound Total Trips 33 Intervals Time 420/33 12.73

Monday through Friday

Effective Dec 16 2018

Eastbound Al Este (Approximate Times / Tiempos Aproximados)

SANTA MONICA	VENICE	CULVER CITY	LOS ANGELES		DOWNTOWN LOS ANGELES
1	2	3	4	5	6
2nd & Santa Monica	Main & Sunset	Culver City Station	Venice & La Brea	Venice & Vermont	Main & 7th
					Patsaouras Bus Plaza / LA Union Station
—	5:07A	5:29A	5:40A	5:51A	6:06A
5:40A	5:30	5:52	6:03	6:16	6:32
—	5:49	6:12	6:23	6:36	6:53
—	6:07	6:31	6:43	6:57	7:15
—	6:25	6:51	7:03	7:19	7:37
—	6:43	7:10	7:23	7:40	7:59
—	7:02	7:30	7:43	8:00	8:19
—	7:18	7:50	8:03	8:20	8:39
—	7:37	8:10	8:23	8:40	8:59
—	7:56	8:30	8:43	9:00	9:18
—	8:16	8:50	9:03	9:19	9:34
—	8:36	9:10	9:23	9:39	9:57
—	8:56	9:30	9:43	9:59	10:14
—	9:18	9:52	10:05	10:21	10:39
—	9:43	10:16	10:29	10:45	11:00
—	10:06	10:39	10:53	11:09	11:27
—	10:30	11:03	11:17	11:33	11:48
—	10:53	11:26	11:41	11:57	12:15P
—	11:14	11:48	12:03P	12:19P	12:37
—	11:37	12:11P	12:26	12:42	1:00
—	11:57	12:31	12:46	1:02	1:20
—	12:14P	12:49	1:04	1:20	1:38
—	12:32	1:07	1:22	1:38	1:53
—	12:48	1:23	1:38	1:54	2:09
—	1:01	1:37	1:52	2:09	2:27
—	1:15	1:51	2:06	2:24	2:42
—	1:29	2:05	2:20	2:38	2:56
—	1:42	2:19	2:34	2:52	3:10
—	1:54	2:32	2:48	3:07	3:25
—	2:05	2:45	3:01	3:21	3:39
—	2:15	2:55	3:12	3:32	3:47
—	2:26	3:06	3:23	3:43	4:01
—	2:35	3:16	3:33	3:53	4:11
—	2:42	3:24	3:41	4:01	4:16
—	2:48	3:31	3:49	4:09	4:27
—	2:56	3:40	3:58	4:18	4:33
—	3:04	3:49	4:07	4:27	4:46
—	3:13	3:58	4:16	4:36	4:52
—	3:23	4:08	4:26	4:46	5:05
—	3:33	4:18	4:36	4:56	5:13
—	3:42	4:27	4:46	5:06	5:26
—	3:52	4:37	4:56	5:16	5:33
—	4:00	4:47	5:06	5:26	5:43
—	4:11	4:58	5:17	5:37	5:54
—	4:22	5:09	5:28	5:48	6:05
—	4:33	5:20	5:39	5:59	6:15
—	4:44	5:31	5:50	6:10	6:29
—	4:55	5:42	6:01	6:21	6:36
—	5:07	5:54	6:12	6:32	6:46
—	5:18	6:05	6:23	6:41	6:55
—	5:29	6:16	6:34	6:51	7:05
—	5:40	6:27	6:45	7:02	7:15
—	5:51	6:38	6:56	7:11	7:24
—	6:05	6:51	7:07	7:22	7:35
—	6:28	7:07	7:21	7:36	7:49
—	6:45	7:22	7:36	7:51	8:06
—	7:04	7:40	7:52	8:07	8:20
—	7:23	7:58	8:10	8:24	8:39
—	7:46	8:16	8:28	8:42	8:57
—	8:05	8:34	8:46	9:00	9:14
—	8:24	8:52	9:04	9:17	9:31
—	8:45	9:12	9:24	9:37	9:49
—	9:08	9:35	9:47	10:00	10:14
—	9:28	9:55	10:07	10:20	10:32
—	9:50	10:17	10:29	10:40	10:52
—	10:14	10:41	10:52	11:03	11:15
—	10:48	11:12	11:22	11:33	11:45
11:11P	11:20	11:42	11:52	12:03A	12:15A
11:43	11:52	12:14A	12:24A	12:35	12:47
12:19A	12:28A	12:50	1:00	1:11	1:23
12:40	12:49	1:11	1:21	1:32	1:44
1:00	1:09	1:31	1:41	1:52	2:04
1:23	1:32	1:54	2:04	2:15	2:27
1:50	1:59	2:21	2:31	2:42	2:54
2:20	2:29	2:51	3:01	3:12	3:24
2:55	3:04	3:26	3:36	3:47	3:59
3:35	3:44	4:06	4:16	4:27	4:39
4:35	4:44	5:06	5:16	5:27	5:39

Westbound Al Oeste (Approximate Times / Tiempos Aproximados)

DOWNTOWN LOS ANGELES	LOS ANGELES		CULVER CITY	VENICE	SANTA MONICA
8	7	5	4	3	2
Patsaouras Bus Plaza / LA Union Station	Spring & 6th	Venice & Vermont	Venice & La Brea	Culver City Station	Main & Sunset
					2nd & Santa Monica
4:00A	4:10A	4:25A	4:37A	4:46A	5:08A
—	4:38	4:53	5:07	5:17	5:41
—	4:52	5:07	5:21	5:31	5:55
—	5:12	5:27	5:41	5:51	6:15
—	5:31	5:47	6:01	6:13	6:38
—	5:44	6:00	6:17	6:29	6:55
—	5:57	6:15	6:32	6:46	7:13
5:58	6:08	6:23	6:42	6:56	7:24
—	6:11	6:29	6:49	7:04	7:34
—	6:15	6:34	6:54	7:10	7:41
—	6:20	6:39	6:59	7:16	7:47
—	6:24	6:43	7:04	7:21	7:52
—	6:28	6:47	7:09	7:26	7:57
—	6:32	6:51	7:14	7:32	8:03
—	6:37	6:56	7:20	7:41	8:12
—	6:46	7:06	7:30	7:51	8:22
—	6:55	7:16	7:40	8:01	8:32
—	7:04	7:26	7:50	8:11	8:42
—	7:14	7:36	8:00	8:21	8:52
—	7:25	7:48	8:12	8:33	9:04
—	7:37	8:00	8:24	8:45	9:16
—	7:51	8:13	8:36	8:57	9:28
—	8:07	8:29	8:50	9:09	9:41
—	8:22	8:44	9:04	9:22	9:54
—	8:38	9:00	9:18	9:35	10:07
—	8:52	9:14	9:32	9:49	10:21
—	9:07	9:29	9:47	10:03	10:35
—	9:22	9:44	10:02	10:16	10:48
—	9:38	10:00	10:18	10:32	11:04
—	9:55	10:17	10:35	10:49	11:21
—	10:12	10:34	10:52	11:06	11:38
—	10:28	10:51	11:09	11:23	11:55
—	10:46	11:09	11:27	11:41	12:13P
—	11:04	11:27	11:45	11:59	12:31
—	11:21	11:44	12:02P	12:16P	12:48
—	11:38	12:01P	12:19	12:33	1:05
—	11:55	12:18	12:36	12:50	1:22
—	12:12P	12:35	12:53	1:07	1:39
—	12:28	12:51	1:09	1:23	1:55
—	12:44	1:07	1:25	1:39	2:12
—	1:00	1:23	1:41	1:55	2:28
—	1:16	1:39	1:57	2:11	2:44
—	1:32	1:55	2:13	2:27	3:00
—	1:48	2:11	2:29	2:45	3:18
—	2:03	2:26	2:45	3:01	3:34
—	2:19	2:42	3:01	3:17	3:50
—	2:35	2:58	3:17	3:33	4:06
—	2:51	3:14	3:33	3:49	4:22
—	3:06	3:30	3:49	4:05	4:38
—	3:21	3:46	4:05	4:21	4:54
—	3:37	4:02	4:21	4:37	5:10
—	3:53	4:18	4:37	4:53	5:26
—	4:09	4:34	4:53	5:09	5:41
—	4:26	4:51	5:10	5:26	5:58
—	4:43	5:08	5:27	5:42	6:14
—	4:56	5:24	5:43	5:58	6:30
—	5:11	5:40	5:59	6:14	6:46
—	5:29	5:58	6:17	6:32	7:04
—	5:49	6:18	6:37	6:52	7:23
—	6:25	6:49	7:07	7:20	7:50
—	7:03	7:22	7:39	7:50	8:19
—	7:35	7:53	8:09	8:20	8:47
—	8:05	8:23	8:39	8:50	9:17
—	8:32	8:49	9:04	9:15	9:41
—	8:57	9:20	9:33	9:44	10:09
—	9:31	9:46	9:59	10:09	10:32
—	10:01	10:23	10:36	10:46	11:09
—	10:51	11:06	11:18	11:28	11:50
11:17P	11:25	11:46	11:58	12:08A	12:30A
11:42	11:51	12:06A	12:18A	12:28	12:50
11:56	12:04A	12:25	12:37	12:47	1:09
12:56A	1:04	1:25	1:37	1:46	2:08
—	1:25	1:46	1:58	2:07	2:25
—	1:50	2:11	2:23	2:32	2:50
—	2:20	2:41	2:53	3:02	3:20
—	2:50	3:11	3:23	3:32	3:50

LA Metro Bus Line 733

(December 16 2018)

LAM - 733

12/16/18
current

Eastbound

AM (6-9am)	Santa Monica	LA Union Stnt	Travel Time	Median	Start + Median	
	5:09	6:24	1:15	0:37	5:46	Not eligible
1	5:29	6:45	1:16	0:38	6:07	
2	5:49	7:07	1:18	0:39	6:28	
3	6:07	7:28	1:21	0:40	6:47	
4	6:26	7:50	1:24	0:42	7:08	
5	6:43	8:13	1:30	0:45	7:28	
6	7:01	8:33	1:32	0:46	7:47	
7	7:20	8:52	1:32	0:46	8:06	
8	7:40	9:12	1:32	0:46	8:26	
9	8:00	9:32	1:32	0:46	8:46	
	8:21	9:53	1:32	0:46	9:07	Not eligible

Total AM trips 9

PM(3-7pm)	Santa Monica	LA Union Stnt	Travel Time	Median	Start + Median	
	2:00	3:44	1:44	0:52	2:52	Not eligible
1	2:14	4:03	1:49	0:54	3:08	
2	2:28	4:20	1:52	0:56	3:24	
3	2:41	4:35	1:54	0:57	3:38	
4	2:54	4:51	1:57	0:58	3:52	
5	3:07	5:07	2:00	1:00	4:07	
6	3:20	5:24	2:04	1:02	4:22	
7	3:32	5:40	2:08	1:04	4:36	
8	3:46	5:55	2:09	1:04	4:50	
9	4:01	6:09	2:08	1:04	5:05	
10	4:16	6:22	2:06	1:03	5:19	
11	4:31	6:35	2:04	1:02	5:33	
12	4:47	6:48	2:01	1:00	5:47	
13	5:03	7:02	1:59	0:59	6:02	
14	5:20	7:14	1:54	0:57	6:17	
15	5:37	7:27	1:50	0:55	6:32	
16	5:54	7:40	1:46	0:53	6:47	
	6:14	7:55	1:41	0:50	7:04	Not eligible

Total PM trips 16

Eastbound Total Trips 25 Intervals Time 420/25 16.80

Westbound

AM (6-9am)	LA Union Stnt	Santa Monica	Travel Time	Median	Start + Median	
	5:11	6:29	1:18	0:39	5:50	Not eligible
1	5:27	6:48	1:21	0:40	6:07	
2	5:40	7:04	1:24	0:42	6:22	
3	5:51	7:17	1:26	0:43	6:34	
4	6:01	7:31	1:30	0:45	6:46	
5	6:10	7:45	1:35	0:47	6:57	
6	6:18	7:58	1:40	0:50	7:08	
7	6:25	8:11	1:46	0:53	7:18	
8	6:34	8:24	1:50	0:55	7:29	
9	6:43	8:34	1:51	0:55	7:38	
10	6:55	8:47	1:52	0:56	7:51	
11	7:07	8:59	1:52	0:56	8:03	
12	7:21	9:13	1:52	0:56	8:17	
13	7:35	9:27	1:52	0:56	8:31	
14	7:56	9:44	1:48	0:54	8:50	
	8:11	9:58	1:47	0:53	9:04	Not eligible

Total AM trips 14

PM(3-7pm)	Main & 7th	Main & Sunset	Travel Time	Median	Start + Median	
	2:01	3:45	1:44	0:52	2:53	Not eligible
1	2:21	4:04	1:43	0:51	3:12	
2	2:41	4:24	1:43	0:51	3:32	
3	3:01	4:44	1:43	0:51	3:52	
4	3:20	5:04	1:44	0:52	4:12	
5	3:42	5:24	1:42	0:51	4:33	
6	4:02	5:44	1:42	0:51	4:53	
7	4:21	6:05	1:44	0:52	5:13	
8	4:42	6:27	1:45	0:52	5:34	
9	5:03	6:48	1:45	0:52	5:55	
10	5:25	7:09	1:44	0:52	6:17	
11	5:48	7:31	1:43	0:51	6:39	
	6:14	7:54	1:40	0:50	7:04	Not eligible

Total PM trips 11

Total Trips 25 Intervals Time 420/25 16.80

Monday through Friday

Effective Dec 16 2018

733

Eastbound Al Este (Approximate Times/Tiempos Aproximados)

SANTA MONICA	VENICE	CULVER CITY	MID-CITY	DOWNTOWN LOS ANGELES
1	2	3	4	5
2nd & Santa Monica	Venice & Lincoln	Culver City Station	Venice & La Brea	Venice & Vermont
4:45A	4:59A	5:14A	5:23A	5:33A
5:09	5:24	5:40	5:51	6:02
5:29	5:44	6:00	6:11	6:23
5:49	6:04	6:20	6:31	6:43
6:07	6:23	6:40	6:51	7:03
6:26	6:42	7:00	7:11	7:24
6:43	6:59	7:20	7:31	7:46
7:01	7:18	7:40	7:51	8:06
7:20	7:37	8:00	8:11	8:25
7:40	7:57	8:21	8:32	8:45
8:00	8:18	8:42	8:53	9:06
8:21	8:39	9:03	9:14	9:27
8:43	9:01	9:24	9:35	9:48
9:04	9:22	9:45	9:56	10:08
9:25	9:43	10:06	10:17	10:29
9:46	10:04	10:26	10:37	10:49
10:05	10:24	10:46	10:57	11:09
10:25	10:44	11:06	11:17	11:29
10:45	11:04	11:26	11:37	11:49
11:05	11:24	11:46	11:57	12:09P
11:25	11:44	12:06P	12:17P	12:29
11:44	12:03P	12:26	12:37	12:49
12:03P	12:22	12:46	12:57	1:10
12:23	12:42	1:06	1:17	1:30
12:41	1:00	1:24	1:35	1:48
12:59	1:18	1:42	1:53	2:06
1:16	1:35	2:00	2:11	2:25
1:33	1:52	2:18	2:29	2:43
1:48	2:08	2:35	2:46	3:00
2:00	2:22	2:51	3:03	3:18
2:14	2:36	3:07	3:20	3:36
2:28	2:50	3:22	3:35	3:52
2:41	3:03	3:36	3:50	4:07
2:54	3:17	3:51	4:05	4:22
3:07	3:31	4:06	4:20	4:37
3:20	3:45	4:21	4:35	4:53
3:32	3:57	4:33	4:50	5:08
3:46	4:12	4:48	5:05	5:23
4:01	4:27	5:03	5:20	5:38
4:16	4:42	5:18	5:35	5:53
4:31	4:57	5:33	5:50	6:07
4:47	5:13	5:48	6:05	6:21
5:03	5:29	6:04	6:20	6:36
5:20	5:46	6:20	6:35	6:50
5:37	6:03	6:35	6:50	7:04
5:54	6:20	6:51	7:05	7:18
6:14	6:39	7:08	7:20	7:33
6:32	6:55	7:23	7:35	7:47
6:50	7:13	7:39	7:51	8:03
7:11	7:34	7:58	8:09	8:21
7:34	7:56	8:18	8:29	8:41
7:56	8:16	8:38	8:49	9:01
8:19	8:38	8:59	9:10	9:22
8:42	9:01	9:20	9:31	9:42
9:04	9:22	9:41	9:52	10:03
9:29	9:47	10:06	10:16	10:27
9:55	10:13	10:31	10:40	10:50
10:26	10:42	10:59	11:08	11:18
10:57	11:13	11:29	11:37	11:47
11:28	11:43	11:59	12:07A	12:17A
12:02A	12:17A	12:33A	12:41	12:51

Westbound Al Oeste (Approximate Times/Tiempos Aproximados)

DOWNTOWN LOS ANGELES	MID-CITY	CULVER CITY	VENICE	SANTA MONICA
6	5	4	3	2
Patsaouras Bus Plaza / LA Union Station	Venice & Vermont	Venice & La Brea	Culver City Station	Venice & Lincoln
4:55A	5:11A	5:19A	5:30A	5:40A
5:11	5:27	5:35	5:47	5:57
5:27	5:43	5:51	6:04	6:16
5:40	5:56	6:04	6:19	6:31
5:51	6:07	6:16	6:31	6:43
6:01	6:17	6:26	6:43	6:55
6:10	6:26	6:35	6:53	7:07
6:18	6:35	6:44	7:03	7:19
6:25	6:44	6:53	7:14	7:30
6:34	6:53	7:02	7:24	7:43
6:43	7:02	7:12	7:34	7:54
6:55	7:14	7:24	7:46	8:07
7:07	7:26	7:36	7:58	8:19
7:21	7:41	7:51	8:12	8:33
7:35	7:56	8:06	8:27	8:47
7:56	8:15	8:25	8:44	9:04
8:11	8:32	8:42	9:00	9:19
8:27	8:48	8:58	9:16	9:34
8:44	9:05	9:15	9:33	9:49
9:03	9:24	9:34	9:51	10:05
9:22	9:43	9:53	10:09	10:23
9:43	10:04	10:13	10:28	10:41
10:04	10:25	10:34	10:48	11:01
10:24	10:45	10:54	11:08	11:21
10:44	11:05	11:14	11:28	11:41
11:05	11:26	11:35	11:49	12:02P
11:25	11:46	11:55	12:09P	12:22
11:44	12:06P	12:15P	12:29	12:42
12:04	12:26	12:35	12:49	1:02
12:24	12:46	12:55	1:09	1:22
12:44	1:06	1:15	1:29	1:42
1:04	1:26	1:35	1:49	2:02
1:23	1:45	1:54	2:09	2:23
1:42	2:04	2:14	2:29	2:43
2:01	2:24	2:34	2:49	3:03
2:21	2:44	2:54	3:09	3:23
2:41	3:04	3:15	3:29	3:43
3:01	3:24	3:35	3:49	4:03
3:20	3:43	3:55	4:09	4:23
3:42	4:04	4:15	4:29	4:43
4:02	4:24	4:35	4:49	5:03
4:21	4:44	4:55	5:10	5:24
4:42	5:05	5:17	5:32	5:46
5:03	5:27	5:39	5:54	6:08
5:25	5:49	6:01	6:16	6:29
5:48	6:12	6:24	6:39	6:52
6:14	6:36	6:48	7:03	7:16
6:41	7:03	7:14	7:29	7:42
7:12	7:33	7:43	7:57	8:09
7:44	8:03	8:12	8:25	8:36
8:12	8:30	8:39	8:52	9:03
8:42	8:59	9:07	9:20	9:31
9:09	9:26	9:34	9:46	9:56
9:39	9:56	10:04	10:16	10:26
10:21	10:38	10:46	10:57	11:06
11:04	11:21	11:28	11:39	11:48
				12:02A
				12:17A

Big Blue Bus Route 3

(Septmber 16 2018 to March 16 2019)

BBB - 3

9/16/18
3/16/19

Northbound

AM (6-9am)

	Aviation	Arizona	Travel Time	Median	Start + Median	
	5:23	6:02	0:39	0:19	5:42	Not eligible
1	5:45	6:27	0:42	0:21	6:06	
2	6:05	6:50	0:45	0:22	6:27	
3	6:26	7:17	0:51	0:25	6:51	
4	6:40	7:37	0:57	0:28	7:08	
5	7:00	8:02	1:02	0:31	7:31	
6	7:15	8:19	1:04	0:32	7:47	
7	7:35	8:39	1:04	0:32	8:07	
8	7:55	8:59	1:04	0:32	8:27	
9	8:15	9:17	1:02	0:31	8:46	
	8:35	9:37	1:02	0:31	9:06	Not eligible

Schedule Date
16-Sep-18

In Blue are trip extrapolated
from the schedule following
the intervals indicated on
the schedule (e.g. 10 min.,
12 min., 14 min., 15 min.,
20 min.)

Total AM trips

9

PM(3-7pm)

	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:15	3:14	0:59	0:29	2:44	Not eligible
1	2:35	3:34	0:59	0:29	3:04	
2	2:55	3:54	0:59	0:29	3:24	
3	3:15	4:14	0:59	0:29	3:44	
4	3:35	4:34	0:59	0:29	4:04	
5	3:55	4:54	0:59	0:29	4:24	
6	4:15	5:14	0:59	0:29	4:44	
7	4:35	5:34	0:59	0:29	5:04	
8	4:55	5:54	0:59	0:29	5:24	
9	5:15	6:14	0:59	0:29	5:44	
10	5:35	6:34	0:59	0:29	6:04	
11	5:55	6:54	0:59	0:29	6:24	
12	6:15	7:11	0:56	0:28	6:43	
	6:35	7:31	0:56	0:28	7:03	Not eligible

Total PM trips

12

Northbound Total Trips 21 Intervals Time 420/21 20.00

Southbound

AM (6-9am)

	Arizona	Aviation	Travel Time	Median	Start + Median	
	5:20	6:00	0:40	0:20	5:40	Not eligible
1	5:50	6:30	0:40	0:20	6:10	
2	6:20	7:06	0:46	0:23	6:43	
3	6:48	7:34	0:46	0:23	7:11	
4	7:08	7:59	0:51	0:25	7:33	
5	7:28	8:19	0:51	0:25	7:53	
6	7:48	8:46	0:58	0:29	8:17	
7	8:08	9:06	0:58	0:29	8:37	
8	8:28	9:26	0:58	0:29	8:57	
	8:48	9:46	0:58	0:29	9:17	Not eligible

Total AM trips

8

PM(3-7pm)

	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:05	3:19	1:14	0:37	2:42	Not eligible
1	2:25	3:39	1:14	0:37	3:02	
2	2:45	4:04	1:19	0:39	3:24	
3	3:05	4:28	1:23	0:41	3:46	
4	3:25	4:48	1:23	0:41	4:06	
5	3:45	5:06	1:21	0:40	4:25	
6	4:05	5:26	1:21	0:40	4:45	
7	4:25	5:46	1:21	0:40	5:05	
8	4:45	6:06	1:21	0:40	5:25	
9	5:05	6:26	1:21	0:40	5:45	
10	5:25	6:46	1:21	0:40	6:05	
11	5:45	7:06	1:21	0:40	6:25	
12	6:05	7:22	1:17	0:38	6:43	
	6:25	7:42	1:17	0:38	7:03	Not eligible

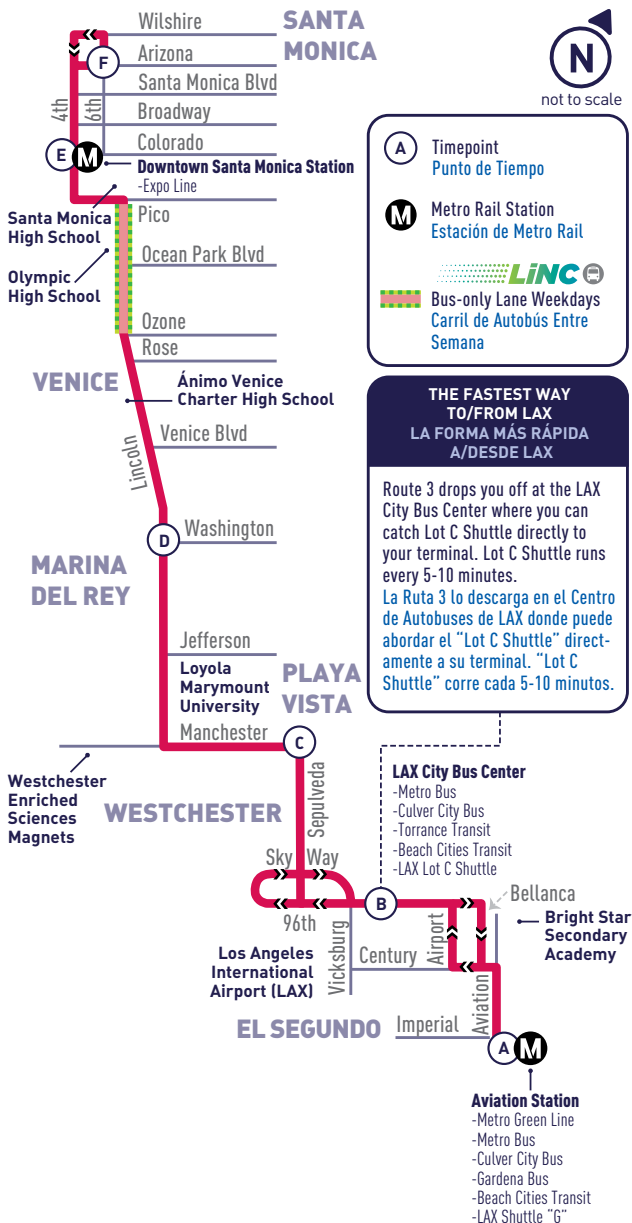
Total PM trips

12

Southbound Total Trips 20 Intervals Time 420/20 21.00

LINCOLN BLVD

3



AVIATION STATION GREEN LINE TO DOWNTOWN SANTA MONICA

WEEKDAY DURANTE LA SEMANA

Aviation Station (Bay 1) (A)	LAX City Bus Center (Bay 12) (B)	Manchester & Sepulveda (C)	Lincoln & Washington (D)	Arizona & 5th (F)
4:53	5:01	5:05	5:15	5:30
5:03	5:11	5:15	5:25	5:40
5:23	5:31	5:35	5:46	6:02
5:45	5:54	5:58	6:10	6:27
6:05	6:14	6:18	6:31	6:50
6:26	6:35	6:40	6:55	7:17
6:40	6:49	6:55	7:12	7:37
7:00	7:10	7:16	7:35	8:02
7:15	7:25	7:32	7:51	8:19
7:35	7:45	7:52	8:11	8:39
7:55	8:05	8:12	8:31	8:59
8:15	8:25	8:32	8:50	9:17
8:35	8:45	8:52	9:10	9:37
8:55	9:05	9:12	9:30	9:57
Then service every 20 minutes at:				
:15	:25	:32	:49	:14
:35	:45	:52	:09	:34
:55	:05	:12	:29	:54
Until:				
6:15	6:25	6:31	6:46	7:11
6:35	6:45	6:51	7:06	7:31
6:55	7:04	7:10	7:24	7:47
7:15	7:24	7:30	7:43	8:03
7:35	7:44	7:49	8:02	8:20
7:55	8:04	8:09	8:22	8:40
8:15	8:24	8:29	8:42	9:00
8:40	8:49	8:54	9:07	9:25
9:10	9:18	9:23	9:35	9:51
9:40	9:48	9:53	10:05	10:21
10:10	10:18	10:23	10:35	10:51
10:40	10:48	10:53	11:04	11:19
11:10	11:18	11:23	11:34	11:49
11:40	11:48	11:53	12:04	12:19
12:10	12:17	12:21	12:31	12:45

Good Bus Karma

TMI! TMI! Save the cell phone calls
until you get off the bus.

ALL PM
TIMES IN
BOLD

ROUTE

3

DOWNTOWN SANTA MONICA TO AVIATION STATION GREEN LINE

WEEKDAY
DURANTE LA SEMANA

Arizona & 5th (F)	4th & Santa Monica Place (Downtown SM Station) (E)	Lincoln & Washington (D)	Manchester & Sepulveda (C)	LAX City Bus Center (Bay 11) (B)	Aviation Station (A)
4:45	4:48	4:57	5:07	5:12	5:20
5:20	5:24	5:34	5:45	5:51	6:00
5:50	5:54	6:04	6:15	6:21	6:30
6:20	6:25	6:37	6:50	6:56	7:06
6:48	6:53	7:05	7:18	7:24	7:34
7:08	7:13	7:27	7:41	7:48	7:59
7:28	7:33	7:47	8:01	8:08	8:19
7:48	7:54	8:11	8:26	8:34	8:46
8:08	8:14	8:31	8:46	8:54	9:06
8:28	8:34	8:51	9:06	9:14	9:26
8:48	8:54	9:11	9:26	9:34	9:46
9:08	9:14	9:31	9:46	9:54	10:06
9:28	9:34	9:51	10:06	10:14	10:26
9:48	9:54	10:11	10:26	10:34	10:46
10:08	10:14	10:33	10:48	10:56	11:09
10:26	10:32	10:51	11:06	11:14	11:27
10:46	10:52	11:11	11:26	11:34	11:47
11:06	11:12	11:31	11:46	11:54	12:07
11:25	11:31	11:50	12:05	12:13	12:26
11:45	11:51	12:10	12:25	12:33	12:46
12:05	12:11	12:30	12:45	12:53	1:06
12:25	12:32	12:54	1:09	1:17	1:30
12:45	12:52	1:14	1:29	1:37	1:50
1:05	1:13	1:37	1:53	2:01	2:15
1:25	1:33	1:57	2:13	2:21	2:35
1:45	1:53	2:19	2:36	2:44	2:59
2:05	2:13	2:39	2:56	3:04	3:19
2:25	2:33	2:59	3:16	3:24	3:39
2:45	2:53	3:21	3:39	3:48	4:04
3:05	3:13	3:43	4:02	4:11	4:28
3:25	3:33	>4:03	>4:22	>4:31	4:48
3:45	3:53	>4:21	>4:40	>4:49	5:06
4:05	4:13	>4:41	>5:00	>5:09	5:26
4:25	4:33	>5:01	>5:20	>5:29	5:46
4:45	4:53	>5:21	>5:40	>5:49	6:06
5:05	5:13	>5:41	>6:00	>6:09	6:26
5:25	5:33	>6:01	>6:20	>6:29	6:46
5:45	5:53	>6:21	>6:40	>6:49	7:06
6:05	6:12	>6:39	>6:57	>7:06	7:22
6:25	6:32	>6:59	>7:17	>7:26	7:42
6:45	6:52	>7:18	>7:35	>7:44	7:58
7:05	7:12	7:38	7:55	8:04	8:18
7:25	7:32	7:56	8:11	8:20	8:33
7:45	7:52	8:12	8:26	8:34	8:46
8:05	8:12	8:31	8:44	8:51	9:01
8:25	8:32	8:51	9:04	9:11	9:21
8:45	8:51	9:08	9:20	9:26	9:35
9:10	9:16	9:33	9:45	9:51	10:00
9:40	9:46	10:03	10:15	10:21	10:30
10:10	10:16	10:33	10:45	10:51	11:00
10:40	10:46	11:01	11:11	11:17	11:26
11:10	11:16	11:31	11:41	11:47	11:56
11:45	11:51	12:06	12:16	12:22	12:31
12:25	12:31	12:46	12:56	1:02	1:11

► Trips use the bus-only lane and may depart up to 8 minutes earlier than time indicated./Viajes usan el carril del autobús y pueden salir hasta 8 minutos antes del tiempo indicado.

ALL PM
TIMES IN
BOLD

ROUTE
3

EFFECTIVE DATE: SEPTEMBER 16, 2018

Big Blue Bus Route RAPID 3

(September 16 2018 to March 16 2019)

BBB - R3

9/16/18
3/16/19

Northbound

AM (6-9am)	Aviation	Arizona	Travel Time	Median	Start + Median	
	5:35	6:11	0:36	0:18	5:53	Not eligible
1	5:48	6:24	0:36	0:18	6:06	
2	6:00	6:40	0:40	0:20	6:20	
3	6:12	6:55	0:43	0:21	6:33	
4	6:24	7:07	0:43	0:21	6:45	
5	6:35	7:21	0:46	0:23	6:58	
6	6:45	7:31	0:46	0:23	7:08	
7	6:55	7:45	0:50	0:25	7:20	
8	7:02	7:58	0:56	0:28	7:30	
9	7:10	8:11	1:01	0:30	7:40	
10	7:20	8:25	1:05	0:32	7:52	
11	7:30	8:35	1:05	0:32	8:02	
12	7:40	8:45	1:05	0:32	8:12	
13	7:50	8:55	1:05	0:32	8:22	
14	8:00	9:05	1:05	0:32	8:32	
15	8:10	9:15	1:05	0:32	8:42	
16	8:21	9:23	1:02	0:31	8:52	
	8:33	9:31	0:58	0:29	9:02	Not eligible

Schedule Effective Date:
16-Sep-18

In Blue are trip extrapolated from the schedule following the intervals indicated on the schedule (e.g. 10 min., 12 min., 14 min., 15 min., 20 min.)

Total AM trips

16

PM(3-7pm)	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:34	3:25	0:51	0:25	2:59	Not eligible
1	2:49	3:40	0:51	0:25	3:14	
2	3:04	3:55	0:51	0:25	3:29	
3	3:19	4:10	0:51	0:25	3:44	
4	3:34	4:25	0:51	0:25	3:59	
5	3:49	4:40	0:51	0:25	4:14	
6	4:03	4:54	0:51	0:25	4:28	
7	4:17	5:08	0:51	0:25	4:42	
8	4:31	5:22	0:51	0:25	4:56	
9	4:45	5:36	0:51	0:25	5:10	
10	4:59	5:50	0:51	0:25	5:24	
11	5:13	6:04	0:51	0:25	5:38	
12	5:27	6:18	0:51	0:25	5:52	
13	5:42	6:33	0:51	0:25	6:07	
14	5:57	6:48	0:51	0:25	6:22	
15	6:12	7:01	0:49	0:24	6:36	
16	6:30	7:15	0:45	0:22	6:52	
	7:00	7:45	0:45	0:22	7:22	Not eligible

Total PM trips

16

Northbound Total Trips	32	Intervals Time	420/32	13.13
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Southbound

AM (6-9am)	Arizona	Aviation	Travel Time	Median	Start + Median	
1	6:32	7:15	0:43	0:21	6:53	
2	6:47	7:32	0:45	0:22	7:09	
3	7:02	7:47	0:45	0:22	7:24	
4	7:16	8:03	0:47	0:23	7:39	
5	7:29	8:16	0:47	0:23	7:52	
6	7:42	8:33	0:51	0:25	8:07	
7	7:54	8:45	0:51	0:25	8:19	
8	8:06	8:57	0:51	0:25	8:31	
9	8:19	9:10	0:51	0:25	8:44	
10	8:34	9:25	0:51	0:25	8:59	
	8:49	9:40	0:51	0:25	9:14	Not eligible

Total AM trips

10

PM(3-7pm)	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:18	3:23	1:05	0:32	2:50	Not eligible
1	2:28	3:33	1:05	0:32	3:00	
2	2:39	3:49	1:10	0:35	3:14	
3	2:50	4:00	1:10	0:35	3:25	
4	3:00	4:10	1:10	0:35	3:35	
5	3:10	4:20	1:10	0:35	3:45	
6	3:20	4:30	1:10	0:35	3:55	
7	3:30	4:40	1:10	0:35	4:05	
8	3:40	4:50	1:10	0:35	4:15	
9	3:50	5:00	1:10	0:35	4:25	
10	4:00	5:10	1:10	0:35	4:35	
11	4:10	5:20	1:10	0:35	4:45	
12	4:20	5:30	1:10	0:35	4:55	
13	4:30	5:40	1:10	0:35	5:05	
14	4:40	5:50	1:10	0:35	5:15	
15	4:50	6:00	1:10	0:35	5:25	
16	5:00	6:10	1:10	0:35	5:35	
17	5:10	6:20	1:10	0:35	5:45	
18	5:20	6:30	1:10	0:35	5:55	
19	5:30	6:40	1:10	0:35	6:05	
20	5:40	6:50	1:10	0:35	6:15	
21	5:50	7:00	1:10	0:35	6:25	
22	6:00	7:10	1:10	0:35	6:35	
23	6:15	7:30	1:15	0:37	6:52	
	6:30	7:45	1:15	0:37	7:07	Not eligible

Total PM trips

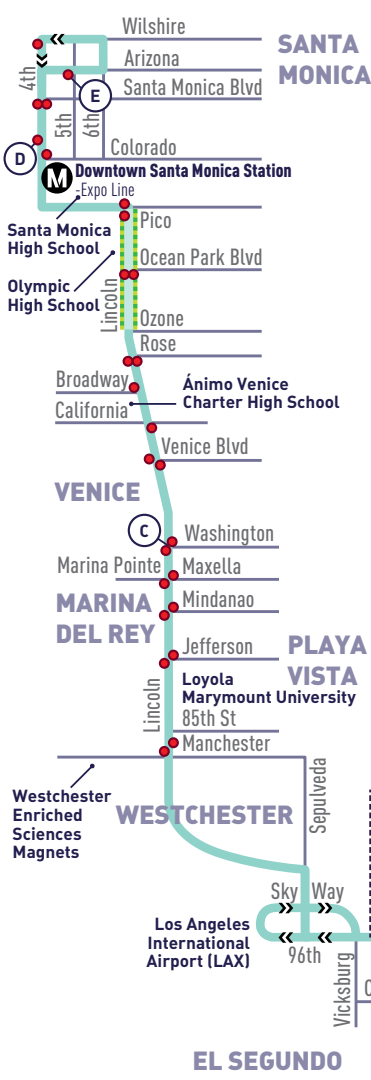
23

Southbound Total Trips	33	Intervals Time	420/33	12.73
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LINCOLN BLVD

rapid

3



SANTA MONICA



● Big Blue Bus Stop
Parada de Big Blue Bus

Ⓐ Timepoint
Punto de Tiempo

Ⓜ Metro Rail Station
Estación de Metro Rail

LINC
Bus-Only Lane Weekdays
Carril de Autobús Entre
Semana

**THE FASTEST WAY
TO/FROM LAX
LA FORMA MÁS RÁPIDA
A/DESDE LAX**

Rapid 3 drops you off at the LAX City Bus Center where you can catch Lot C Shuttle directly to your terminal. Lot C Shuttle runs every 5-10 minutes.
El Rapid 3 lo descarga en el Centro de Autobuses de LAX donde puede abordar el "Lot C Shuttle" directamente a su terminal. "Lot C Shuttle" corre cada 5-10 minutos.

LAX City Bus Center
-Metro Bus
-Culver City Bus
-Torrance Transit
-Beach Cities Transit
-LAX Lot C Shuttle

Bright Star Secondary Academy

Aviation Station
-Metro Green Line
-Metro Bus
-Culver City Bus
-Gardena Bus
-Beach Cities Transit
-LAX Shuttle "G"

AVIATION STATION GREEN LINE TO DOWNTOWN SANTA MONICA

WEEKDAY DURANTE LA SEMANA

Aviation Station (Bay 1) (A)	LAX City Bus Center (Bay 12) (B)	Lincoln & Washington (C)	Arizona & 5th (E)
5:35	5:43	5:56	6:11
5:48	5:56	6:09	6:24
6:00	6:09	6:22	6:40
6:12	6:21	6:35	6:55
6:24	6:33	6:47	7:07
6:35	6:45	7:01	7:21
6:45	6:55	7:11	7:31
6:55	7:06	7:23	7:45
7:02	7:13	7:35	7:58
7:10	7:22	7:46	8:11
7:20	7:32	8:00	8:25
7:30	7:42	8:10	8:35
7:40	7:52	8:20	8:45
7:50	8:02	8:30	8:55
8:00	8:12	8:40	9:05
8:10	8:22	8:50	9:15
8:21	8:32	8:58	9:23
8:33	8:44	9:07	9:31
8:45	8:56	9:19	9:43
8:57	9:08	9:31	9:55
9:10	9:21	9:44	10:08
9:22	9:32	9:52	10:16
9:34	9:44	10:04	10:28
Then service every 15 minutes at:			
:04	:14	:32	:55
:19	:29	:47	:10
:34	:44	:02	:25
:49	:59	:17	:40
Until:			
3:04	3:14	3:32	3:55
3:19	3:29	3:47	4:10
3:34	3:44	4:02	4:25
3:49	3:59	4:17	4:40
Then service every 14 minutes until:			
5:27	5:37	5:55	6:18
5:42	5:52	6:10	6:33
5:57	6:07	6:25	6:48
6:12	6:22	6:39	7:01
6:30	6:39	6:55	7:15
7:00	7:09	7:25	7:45
7:30	7:39	7:54	8:13

ALL PM
TIMES IN
BOLD

RAPID

3

DOWNTOWN SANTA MONICA TO AVIATION STATION GREEN LINE

WEEKDAY DURANTE LA SEMANA

Arizona & 5th (E)	4th & Santa Monica Place (Downtown SM Station)	Lincoln & Washington (C)	LAX City Bus Center (Bay 11) (B)	Aviation Station (A)
6:32	6:37	6:49	7:04	7:15
6:47	6:52	7:05	7:21	7:32
7:02	7:07	7:20	7:36	7:47
7:16	7:21	7:35	7:52	8:03
7:29	7:34	7:48	8:05	8:16
7:42	7:48	8:04	8:22	8:33
7:54	8:00	8:16	8:34	8:45
8:06	8:12	8:28	8:46	8:57
8:19	8:25	8:41	8:59	9:10
8:34	8:40	8:56	9:14	9:25
8:49	8:55	9:11	9:29	9:40
Then service every 15 minutes at:				
:04	:10	:26	:44	:55
:19	:25	:41	:59	:10
:34	:40	:56	:14	:25
:49	:55	:11	:29	:40
Until:				
11:03	11:09	11:26	11:44	11:56
11:18	11:24	11:41	11:59	12:11
11:33	11:39	11:56	12:14	12:26
11:48	11:54	12:11	12:29	12:41
12:03	12:10	12:28	12:47	12:58
12:18	12:25	12:43	1:02	1:13
12:33	12:40	12:58	1:17	1:28
12:48	12:55	1:13	1:32	1:43
1:03	1:10	1:28	1:47	1:58
1:18	1:25	1:43	2:02	2:13
1:33	1:40	2:00	2:20	2:32
1:48	1:55	2:15	2:35	2:47
2:03	2:11	2:34	2:55	3:08
2:18	2:26	2:49	3:10	3:23
2:28	2:36	2:59	3:20	3:33
2:39	2:47	3:13	3:35	3:49
2:50	2:58	3:24	3:46	4:00
Then service every 10 minutes until:				
6:15	6:23	>6:51	>7:15	7:30
6:30	6:38	>7:06	>7:30	7:45
6:45	6:52	>7:18	>7:40	7:54
7:00	7:07	7:31	7:52	8:05
7:15	7:22	7:44	8:03	8:15
7:35	7:42	8:04	8:23	8:35
7:55	8:01	8:20	8:38	8:49
8:15	8:21	8:37	8:54	9:05
8:35	8:41	8:57	9:14	9:25

- Trips use the bus-only lane and may depart up to 8 minutes earlier than time indicated./Viajes usan el carril de autobús y pueden salir hasta 8 minutos antes del tiempo indicado.

ALL PM
TIMES IN
BOLD

RAPID
3

4. APPENDIX D

Reference Materials of Connect SoCal, Draft 2020 RTP/SCS Transit Technical Appendix supporting Contestation of TOC Tier Assignment:

case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

The following will be included in the forthcoming Connect SoCal, Draft 2020 RTP/SCS Transit Technical Appendix.

HIGH QUALITY TRANSIT CORRIDORS AND MAJOR TRANSIT STOPS

BACKGROUND

The Sustainable Communities and Climate Protection Act of 2008, Senate Bill (SB) 375, requires that Metropolitan Planning Organizations (MPOs) develop a Sustainable Communities Strategy (SCS) to reduce per capita greenhouse gas emissions through integrated transportation, land use, housing and environmental planning. SB 375 creates incentives for residential or mixed-use residential projects that may be exempt from, or subject to a limited review of, the California Environmental Quality Act (CEQA), provided they are consistent with the MPO's adopted SCS. These "transit priority projects" must, among other criteria, be located within one-half mile of a major transit stop or high-quality transit corridor (HQTC).

SB 743, signed into law in 2013, provides further opportunities for CEQA exemption and streamlining to facilitate transit oriented development (TOD). Specifically, certain types of projects within "transit priority areas" (TPAs) can benefit from a CEQA exemption if they are consistent with an adopted specific plan and the SCS. A TPA is an area within one-half mile of a major transit stop that is existing or planned, if the planned stop is scheduled to be completed within the planning horizon included in a Federal Transportation Improvement Program (FTIP).

STATUTORY DEFINITIONS

California statute defines [major transit stop](#) and [high quality transit corridor](#) as follows.

CA Pub. Res. Code § 21155(b)

For purposes of this chapter, a transit priority project shall (1) contain at least 50 percent residential use, based on total building square footage and, if the project contains between 26 percent and 50 percent nonresidential uses, a floor area ratio of not less than 0.75; (2) provide a minimum net density of at least 20 dwelling units per acre; and (3) be within one-half mile of a major transit stop or high-quality transit corridor included in a regional transportation plan. A [major transit stop](#) is as defined in Section 21064.3, except that, for purposes of this section, it also includes major transit stops that are included in the applicable regional transportation plan. For purposes of this section, a [high-quality transit corridor](#) means a corridor with fixed route bus service with service intervals no longer than 15 minutes during peak commute hours. A project shall be considered to be within one-half mile of a major transit stop or high-quality transit corridor if all parcels within the project have no more than 25 percent of their area farther than one-half mile from the stop or corridor and if not more than 10 percent of the residential units or 100 units, whichever is less, in the project are farther than one-half mile from the stop or corridor.

CA Pub. Res. Code § 21064.3

"Major transit stop" means a site containing an existing rail transit station, a ferry terminal served by either a bus or rail transit service, or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods.

METHODOLOGY

For planning and SCS purposes, SCAG identifies a "high quality transit area" as generally a walkable transit village or corridor that is within one-half mile of a major transit stop or HQTC as defined in statute. SCAG's technical methodology for identifying HQTCs and major transit stops is based on input from the Regional Transit Technical Advisory Committee (RTTAC), as well as consultation with local agencies, other large MPOs in California, and the Governor's Office of Planning and Research. The methodology and assumptions are discussed below. This methodology may be periodically updated to incorporate revisions or clarifications. Questions should be directed to Steve Fox, at fox@scag.ca.gov, or Philip Law, at law@scag.ca.gov.

SCAG maps and data depicting HQTCs and major transit stops are intended for planning purposes only. SCAG shall incur no responsibility or liability as to the completeness, currentness, or accuracy of this information. SCAG assumes no responsibility arising from use of this information by individuals, businesses, or other public entities. The information is provided with no warranty of any kind, expressed or implied, including but not limited to the implied warranties of merchantability and fitness for a particular purpose.

Existing HQTCs and Major Transit Stops

SCAG updates its inventory of existing major transit stops and HQTCs with the adoption of a new Regional Transportation Plan (RTP) and SCS, once every four years. Data for the existing ("base year") condition for the RTP/SCS are typically obtained several years before plan adoption. The base year transit network for *Connect SoCal*, the 2020 RTP/SCS, is based primarily on data for 2016. This inventory of existing major transit stops and HQTCs is therefore only a snapshot in time as of 2016, and does not reflect the existing levels of transit service for any other timeframe.

<insert map of 2016 Base Year "existing" major transit stops and high quality transit corridors>

Transit agencies make adjustments to bus service on a regular basis. Therefore, given the limitations of the RTP/SCS base year transit network, local jurisdictions should consult with the appropriate transit provider(s) to obtain the latest information on existing transit routes, stop locations, and service intervals before making determinations regarding CEQA exemption or streamlining. It is the responsibility of the lead agency under CEQA to determine if a project meets statutory requirements.

Stop-Based Analysis

SCAG calculates peak commute bus service intervals at the stop level using schedule data published by transit agencies in the General Transit Feed Specification (GTFS) format (see for example,

www.transitfeeds.com). An HQTC therefore comprises or is determined by the qualifying stops on an individual bus route.

Peak Period Bus Service Interval (Frequency)

To determine whether the peak commute bus service interval (also called frequency) meets the statutory threshold of 15 minutes or less, SCAG uses the peak period defined in its regional travel demand model. The morning peak is defined as 6am to 9am and the afternoon peak is defined as 3pm to 7pm. A transit operator may have a different, board-adopted or de facto peak period; in such cases SCAG will accept requests to use operator-specific peak-hour periods on a case-by-case basis.

SCAG uses the total population of bus trips during the combined seven-hour morning and afternoon peak periods to determine the peak frequency at a bus stop. This is done for each bus route, by direction. The peak frequency is calculated by dividing 420 minutes (the seven-hour peak converted to minutes) by the total peak bus trips. This average frequency should be 15 minutes or less in order to qualify. The threshold is strict, at 15.0 minutes.

Directional Frequency

A bus route must only meet the 15-minute service interval threshold in one direction to qualify as an HQTC. This is based on RTTAC feedback that transit agencies often operate very peak-directional service or operate predominantly one-way service on a corridor.

Corridors with Multiple Overlapping Bus Routes

Separate but overlapping bus routes that do not individually meet the 15-minute threshold may not be combined in order to qualify as an HQTC. However, based on RTTAC feedback, there are certain corridors where overlapping “line families” or local/bus rapid transit (BRT) lines are intended to function as one bus route. On these corridors, transit riders typically board the first bus available, whether it be a local, express, or BRT line. For these line families or local/BRT corridors, SCAG uses the combined routes to calculate the frequency.

Route Alignment

The entire alignment of a bus route, based on the stops that meet the 15-minute peak frequency threshold, is considered an HQTC. This would include, for example, express bus services that operate along freeways where there are no stops along the freeway right-of-way.

Major Transit Stops and Intersecting Service Transfer Zones

As defined in statute, major transit stops include the intersection of two or more HQTCs. For purposes of transferring between intersecting service, SCAG uses a 500-foot buffer to determine a major transit stop. In other words, two intersecting HQTCs must have stops that are within 500 feet of each other to qualify as a major transit stop. A 500-foot buffer is assumed to be a reasonable limit to the distance that a transit

patron would walk to transfer between bus routes. It is also consistent with the Metro Transfers Design Guide definition of a transfer zone.

Amtrak Stations and Ferry Stations

Amtrak intercity passenger rail stations with only limited long-distance service are not automatically included as a major transit stop unless requested by a local agency. Similarly, ferry stations with seasonal and/or non-commuter based service (and that are served by bus or rail transit) are not automatically included as a major transit stop unless requested by a local agency.

Planned HQTCs and Major Transit Stops

Planned HQTCs and major transit stops are future improvements that are expected to be implemented by transit agencies by the RTP/SCS horizon year of 2045. These are assumed by definition to meet the statutory requirements of an HQTC or major transit stop. SCAG updates its inventory of planned major transit stops and HQTCs with the adoption of a new RTP/SCS, once every four years. However, transit planning studies may be completed by transit agencies on a more frequent basis than the RTP/SCS is updated by SCAG. Local jurisdictions should consult with the appropriate transit provider(s) to obtain the latest information on planned transit routes, stop locations, and service intervals/frequencies before making determinations regarding CEQA exemption or streamlining.

<insert map of planned (2045) major transit stops and high quality transit corridors>

5. EXHIBIT E

Materials supporting Contestation of Reduce Setback:

in case # DIR-2019-4920-TOC,

and based on anomalous granting of Reduced Front Yard setback.

Properties information for all R4-1XL Lots on Venice Blvd between Lincoln Blvd and Walgrove Avenue. The data want to highlight the fact that only **1 residential property with a building dated 1928** does not meet the required minimum setback of 15-feet.

The property at 1616 Venice Blvd. is adjacent to the new development and it is the only reason a reduced setback is granted to the Project.

The building on this one property is dated from 1928, it is a Multi-Dwelling 2-story building with 16 RSO Unit that cultural significance to the Venice Community, housing a large mural of Allen Sarlo, a professional Venice DogTown Surfer greatly admired by the Venice Surfer Community.

Lincoln Blvd to Walgrove Avenue Property Info - R4-1XL (High Medium Residential)

South 36 lots = 22 properties

Address	ASP	Year Built	# Units	# Bedroom	#Bath	RSO - Yes/No	Lot Size	SQFT	ZONE	Front Setback (approximate)	Front Setback from Sidewalk	Notes
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Lincoln Blvd.

1140	4236001021	1986	40	33	40	No	18,435	30,355	R4-1XL	14'-9"	16'-9"	
1146	4236001014	1952	4	4	4	Yes	4,489	3,116	R4-1XL	15'-0"	17'-0"	
1150	4236001015	1952	4	4	4	Yes	4,442	3,116	R4-1XL	15'-0"	17'-0"	

Penmar Avenue

1202	4236003001	1952	4	6	4	Yes	4,248	5,986	R4-1XL	14'-9"	16'-9"	
1208	4236003019	1988	32	52	52	No	29,080	12,214	R4-1XL	15'-9"	17'-9"	
1216	4236003018	1990	48	48	48	No	36,618	18,242	R4-1XL	15'-0"	17'-0"	

Walnut Avenue

2400 Walnut	4236003015	1988	9	13	13	No	7,260	5,867	R4-1XL	15'-3"	17'-3"	
1306	4236003009	1949	1	2	1	No	930	5,956	R4-1XL	33'-3"	35'-3"	Assessor Info do not match what is built
1310	4236003010	1968	17	18	18	Yes	9,328	5,956	R4-1XL	33'-3"	35'-3"	
1316	4236003011	1966	7	8	8	Yes	5,184	5,954	R4-1XL	14'-9"	16'-9"	
1320	4236003017	1987	31	52	51	No	25,500	11,916	R4-1XL	15'-0"	17'-0"	

Glyndon Avenue

1400	4236006026	1988	31	37	48	No	25,560	11,822	R4-1XL	14'-6"	16'-6"	
1410	4236006003	1969	7	9	9	Yes	6,103	5,956	R4-1XL	17'-3"	19'-3"	
1420	4236006022	1989	31	41	41	No	27,539	11,913	R4-1XL	15'-0"	17'-0"	
1426	4236006006	1950	1	2	1	Yes	5,856	1,169	R4-1XL	15'-0"	17'-0"	Assessor Info do not match what is built

Louella Avenue

1500	4236006021	1985	9	11	14	No	7,512	5,850	R4-1XL	15'-0"	17'-0"	
1506	4236006027	1988	47	50	58	No	37,630	17,885	R4-1XL	15'-0"	17'-0"	
1520	4236006011	1963	6	15	6	Yes	6,208	5,962	R4-1XL	15'-3"	17'-3"	
2407 Glencoe	4236006012	1921	1	1	1	Yes	512	5,963		40'-0"	42'-0"	Assessor Info do not match what is built
	4236006012	1929	1	?	?	Yes	1,527				Back House	2 houses on the lot

Glencoe Avenue

1600	4236006013	1947	3	6	3	yes	2,184	5,957		survey (no info)	demo	Proposed Development
1604	4236006014	1946	4	4	4	Yes	2,016	5,956		14'-8" (survey)	demo	Proposed Development
1610	4236006015	1914	2	3	2	Yes	1,240	5,956		19'-0" (survey)	demo	Proposed Development
1616	4236006016	1928	16	18	16	Yes	8,685	5,959		5'-0" (survey)	7'-0"	
1630	4236006025	1988	31	46	58	No	24,106	11,831		15'-0"	17'-0"	

Walgrove Avenue

Lincoln Blvd to Walgrove Avenue Property Info - R4-1XL (High Medium Residential)

North

36 lots = 25 properties

Address	ASP	Year Built	# Units	# Bedroom	#Bath	RSO - Yes/No	Lot Size	SQFT	ZONE	Front Setback (approximate)	Front Setback from Sidewalk	Notes
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Lincoln Blvd.

1015	424200128		56	56	56	No	13,063	36,566		14'-7"		New TOC Project
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Superior

1107	4242002038	2009	38	40	40	No	33,714	23,568		15'-7"	15'-7"	
1117	4242002020	1947	2	2	2	Yes	1,402	7,542		15'-3"	15'-3"	
1127	4242002036	1955	8	10	8	Yes	5,957	8,302		16'-0"	16'-0"	

Penmar Avenue

2338 Penmar	4242030001	1956	8	10	8	Yes	5,857	6,145		16'-0"	16'-0"	
1201	4242030002	1956	8	10	8	Yes	5,857	6,109		15'-0"	15'-0"	
1211	4242030023	1970	18	18	18	Yes	9,276	12,217		34'-3"	34'-3"	
1221	4242030025	1986	12	21	20	No	10,558	7,709		16'-3"	16'-3"	

Walnut Avenue

1301	4242031001	1963	9	14	9	Yes	5,736	5,958		16'-3"	16'-3"	
1305	4242031002	1652	4	4	4	Yes	2,368	5,956		17'-6"	17'-6"	
1311	4242031023	1963	16	22	18	Yes	9,004	11,911		16'-3"	16'-3"	
1321	4242031005	1925					2,800	11,912		0'-0"	7'-0"	Commercial Space

Glyndon Avenue

1401	4242032001	1968	8	9	8	Yes	5,708	5,927		15'-3"	15'-3"	
1407	4242032002	2013	9	9	14	No	6,610	5,957		15'-0"	15'-0"	
1409	4242032023	1989	29	43	43	No	27,700	11,916		14'-9"	14'-9"	
1421	4242032005	1957	20	24	19	Yes	11,303	11,917		18'-0"	18'-0"	

Louella Avenue

1501	4242033001		31			No		11,896		15'-9"		New TOC Project- Former Curch
1511	4242033002	1941	7	7	7	Yes	4,032	5,959		14'-3"	14'-3"	
1517	4242033003	1954	3	2	5	Yes	1,782	4,007		16'-0"	16'-0"	
1519	4242033004	1932	1	2	1	No	600	1,950		N/A	N/A	Single Family?
1523	4242033005	1925	1			No	5,400	7,608		2'-0"	9'-0"	Commercial

Glencoe Avenue

1607	4242034031	1990	30	44	44	No	26,208	11,877		17'-9"	17'-9"	
1611	4242034003	1963	7	12	7	Yes	5,384	5,958		16'-0"	16'-0"	
1619	4242034004	1907	1	3	2	No	2,297	5,958		34'-0"	34'-0"	Single Family
1621	4242034032	1990	28	39	39	No	23,328	11,112		16'-3"	16'-3"	

Walgrove Avenue

1. EXHIBIT A

Reference Materials of TOC Guidelines Chart 1 supporting Contestation of TOC Tier Assignment:

case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

Chart 1. TOC Affordable Housing Incentive Area Tiers

Type of Major Transit Stop	Tier 1 (Low)	Tier 2 (Medium)	Tier 3 (High)	Tier 4 (Regional)
	Distance to Major Transit Stop			
Two Regular Buses (intersection of 2 non Rapid Bus* lines, each w/ at least 15 min. average peak headways)	750 - 2640 ft.	< 750 ft.	-	-
Regular plus Rapid Bus* (intersection of a Regular Bus and Rapid Bus line)	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Two Rapid Buses* (intersection of two Rapid Bus lines)	-	1500-2640 ft.	< 1500 ft.	-
Metrolink Rail Stations	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Metro Rail Stations	-	-	≤ 2640 ft.	< 750 ft. from intersection with another rail line or a Rapid Bus*

Notes:

To be an eligible TOC Housing Development, the project must meet the Eligibility criteria in Section IV, including being located within one-half mile of a Major Transit Stop. In the case of bus stops, this always requires an intersection of two bus routes. An intersection of two bus lines is defined as the midpoint of the street intersection where two or more eligible bus routes meet or cross, and passengers have the direct ability to transfer on foot. This does not include bus routes that travel along the same street. For Tier 4, an intersection between a rail station and an eligible Rapid Bus line is defined as either the rail station entrance(s) or the Rapid Bus stop when the bus stop is within 660 feet of a rail station entrance and can be accessed by foot.

Distance is measured from the closest point on any lot to the entrance(s) of a rail transit station (including elevators and stairways), or the middle of the street intersection of two or more bus routes with a service interval of 15 minutes or less during the morning and afternoon peak commute periods. Please see Appendix A for additional information on how to calculate the 15 minute service interval. In the case of a Tier 4 Major Transit Stop, the distance will be measured from the closest point on any lot to the closer of either the entrance of the rail transit station or the bus stop. If no entrance information is known for a station that is under construction, then the distance will be measured from the center of the platform of the station.

*Rapid Bus is a higher quality bus service that may include several key attributes, including dedicated bus lanes, branded vehicles and stations, high frequency, limited stops at major intersections, intelligent transportation systems, and possible off-board fare collection and/or all door boarding. It includes, but is not limited to, Metro Bus Rapid Transit lines, Metro Rapid 700 lines, Metro Orange and Silver Lines, Big Blue Rapid lines and the Rapid 6 Culver City bus.

2. EXHIBIT B

Reference Materials of TOC Guidelines Appendix A supporting Contestation of TOC Tier Assignment:

case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

Appendix A: Methodology for Determining Major Transit Stops

Definition of Major Transit Stop:

A site containing a rail station or the intersection of two or more bus routes with a service interval of 15 minutes or less during the morning and afternoon peak commute periods. The stations or bus routes may be existing, under construction or included in the most recent SCAG Southern California Association of Governments (SCAG) Regional Transportation Plan (RTP).

SCAG and OPR Methodology:

Peak Periods are considered to be between 6:00 to 9:00 AM and 3:00 to 7:00 PM. Bus routes must have a service frequency of 15 minutes or less for the entire duration of the peak hour periods.

To determine the eligibility of the bus line, the average number of minutes per trip for each direction is calculated separately. If one or both directions fail to meet the 15 minute frequency limit, the entire bus line is ineligible for a Major Transit Stop.

- The total number of trips from the point of origin during peak hours (Monday to Friday) is used. A trip is included if its median time falls within the peak hour.
- To calculate the median time, the time at trip origin is subtracted from the time at arrival at final station, divided by two, and then added to origin time.
For example: Origin time 5:42 AM, Arrival time 6:22 AM
Total trip time = 40 Minutes (6:22 AM – 5:42 AM)
Median trip time = 40 Minutes/2 + 5:42 AM, or 6:02 AM
- The total peak hour time is then divide by the number of trips for the average number of minutes per trip.

Below is a sample calculation based on the 750 Metro Rapid Bus Line (see schedule on Page 16):

Eastbound Trips for 750

During the morning peak hours between 6:00 AM to 9:00 AM, there is a total of 12 Eastbound trips.

The trip originating from Warner Center at 5:42 AM is the first eligible trip with an arrival time at 6:22 AM. This is calculated by dividing the total trip time of 40 minutes by two and adding the 20 minutes to the trip origination time at 5:42 AM, resulting in a median trip time that falls within peak hours at 6:02 AM (not shown in bus schedule).

The trip originating from Warner Center at 8:29 AM is the last eligible trip, with the median time at 8:57 AM.

During the afternoon peak hours between 3:00 PM and 7: PM, there is a total of 16 Eastbound trips.

With 28 total Eastbound trips during the 420 peak hour minutes, the average frequency of the 750 bus line is 15 minutes.

Westbound Trips for 750

Looking at the Westbound trips, there are 11 trips and 15 trips in the AM and PM peak hours respectively. This results in an average frequency of 16.15 minutes.

Result

Despite the Eastbound portion of the 750 Metro Rapid Bus Line meeting the 15 minute frequency as required by a Major Transit Stop, the Westbound portion, with an average frequency of 16.15 minutes, fails to meet that criteria. Therefore, the 750 Metro Rapid Bus Line is ineligible for inclusion in a Major Transit Stop.

Sample Metro Bus Line Schedule with Qualified Peak Hour Trips Boxed in Red

Monday through Friday				750			
Effective Jun 25 2017							
Eastbound <i>Al Este</i> (Approximate Times / Tiempos Aproximados)				Westbound <i>Al Oeste</i> (Approximate Times / Tiempos Aproximados)			
WARNER CENTER	TARZANA	SHERMAN OAKS	STUDIO CITY	STUDIO CITY	SHERMAN OAKS	TARZANA	WARNER CENTER
1	2	3	4	4	3	2	1
Warner Center Transit Hub (Owensmouth & Erwin)	Ventura & Reseda	Ventura & Van Nuys	Universal/ Studio City Station	Universal/ Studio City Station	Ventura & Van Nuys	Ventura & Reseda	Warner Center Transit Hub (Owensmouth & Erwin)
5:13A	5:25A	5:36A	5:50A	5:23A	5:38A	5:52A	6:10A
5:28	5:40	5:51	6:06	5:45	6:00	6:14	6:32
5:42	5:54	6:07	6:22	6:00	6:15	6:29	6:48
5:55	6:09	6:23	6:38	6:15	6:30	6:47	7:04
6:09	6:23	6:39	6:54	6:30	6:46	7:04	7:25
6:23	6:37	6:55	7:10	6:45	7:01	7:22	7:43
6:37	6:51	7:11	7:27	7:00	7:17	7:39	8:00
6:50	7:05	7:27	7:44	7:15	7:32	7:54	8:15
7:05	7:20	7:43	8:00	7:30	7:49	8:11	8:32
7:21	7:36	7:59	8:18	7:45	8:04	8:26	8:47
7:35	7:52	8:15	8:34	8:00	8:19	8:41	9:02
7:54	8:10	8:32	8:51	8:15	8:34	8:56	9:17
8:12	8:28	8:49	9:08	8:30	8:49	9:11	9:32
8:29	8:45	9:06	9:25	8:45	9:04	9:26	9:47
8:46	9:02	9:23	9:42	9:00	9:19	9:41	10:02
9:11	9:27	9:48	10:07	9:18	9:37	9:59	10:20
9:41	9:57	10:18	10:37	9:40	9:59	10:18	10:39
10:10	10:27	10:48	11:07	10:05	10:24	10:43	11:05
10:40	10:57	11:18	11:37	10:35	10:54	11:13	11:35
11:10	11:27	11:48	12:07P	11:04	11:24	11:43	12:06P
11:38	11:57	12:18P	12:37	11:34	11:54	12:13P	12:36
12:08P	12:27P	12:48	1:08	12:03P	12:24P	12:44	1:07
12:35	12:54	1:15	1:35	12:33	12:54	1:14	1:37
1:00	1:19	1:40	2:01	1:02	1:24	1:44	2:07
1:21	1:41	2:02	2:23	1:32	1:54	2:14	2:37
1:46	2:06	2:27	2:48	2:01	2:24	2:44	3:08
1:58	2:18	2:39	3:00	2:25	2:49	3:09	3:34
2:13	2:33	2:54	3:15	2:46	3:10	3:31	3:56
2:27	2:47	3:09	3:30	3:00	3:25	3:46	4:11
2:42	3:02	3:24	3:45	3:15	3:40	4:01	4:25
2:55	3:17	3:39	4:00	3:30	3:55	4:16	4:40
3:10	3:31	3:53	4:14	3:45	4:10	4:31	4:55
3:25	3:47	4:09	4:30	4:00	4:26	4:47	5:11
3:44	4:06	4:28	4:49	4:15	4:41	5:02	5:26
3:55	4:17	4:39	5:00	4:30	4:56	5:17	5:41
4:10	4:32	4:54	5:15	4:45	5:11	5:32	5:56
4:25	4:47	5:09	5:30	5:00	5:26	5:48	6:12
4:40	5:02	5:24	5:45	5:15	5:41	6:03	6:27
4:55	5:17	5:39	6:00	5:30	5:56	6:18	6:41
5:10	5:32	5:54	6:15	5:45	6:11	6:33	6:56
5:26	5:48	6:09	6:30	6:00	6:26	6:47	7:09
5:41	6:03	6:24	6:45	6:15	6:40	7:00	7:21
5:57	6:19	6:39	7:00	6:35	6:58	7:17	7:38
6:20	6:40	6:59	7:19	6:54	7:16	7:34	7:54
6:42	7:01	7:19	7:39	7:15	7:36	7:54	8:13
7:04	7:21	7:39	7:58	7:40	8:01	8:19	8:38
7:35	7:51	8:08	8:26	8:09	8:27	8:44	9:03
8:10	8:25	8:42	8:58	8:39	8:57	9:12	9:28
				9:10	9:27	9:42	9:58

3. EXHIBIT C

Materials supporting Contestation of TOC Tier Assignment:

in case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

LA Planning Form CP-4051: Transit Oriented Communities - Tier Verification

*“[...] The Department of City Planning reserves the right to require an updated form for the project if more than **180** days have transpired since the approval date, or as necessary, to reflect project modifications, policy changes and/or amendments to the LAMC, local laws, and State laws. ”*

As clarified by LA Planning Housing Department the expiration date is to incentivize applicant to promptly submit an application for entitlement; if a project moves forward the Approved Tier Verification stands even if the Bus Line might lose its eligibility status. A new Tier Verification might also be submitted in the case a Bus Line achieves new eligibility. That might explain why there are three (3) different Tier Verification Applications. (see Appendix D -Appeal Supporting Material)

It is the Appellants understanding that Case #: ADM-2018-1208-TOC submitted on 02/28/2019 and approved on 03/20/2019 indicate the Bus Lines assessed for the Tier Verification as these Qualifier Service Intervals are indicated in the Project Eligibility Transportation Section on Case#: PAR-2019-3781-TOC Form CP-4050, Transit Oriented Communities – Referral Form.

PAR-2019-3781-TOC, section II.2 indicate:

Qualifier #1: Local Line 33 + R733

Service Interval #1: 12.7

Service Interval #2:12.4

Qualifier #2: Santa Monica Rapid Line 3

Service Interval #1: 13.12

Service Interval #2: 12.72

The Appellants did an independent Tier Verification for all Tier Verification ADM Cases related to this project submitted to City Planning, and although there are minor differences in the Service Intervals, the Qualifiers Bus Lines used for the Intersection did not change their qualification status.

Incentive Area Tier Verification Summary Table

by EVNA

CASE#: **ADM-2018-1208-TOC (tier verification)**

SUBMITTAL DATE: **2/28/19**

Project Address: 1600 E. Venice Blvd., Venice

	Bus #	Bus Type	Intervals per SCAG (min)	Effective Date	TOC Eligible
LA Metro	33	Regular	East 12.35, West 12.73	12/16/18	YES
LA Metro	733	Rapid	East 16.80, West 16.80	12/16/18	No
Big Blue Bus	3	Regular	North 20.00, South 21.00	9/16/18	No
Big Blue Bus	Rapid 3	Rapid	North 13.13, South 12.73	9/16/18	YES

ELIGIBLE TIER PER CHART 1: **TIER 1**

Chart 1. TOC Affordable Housing Incentive Area Tiers

Type of Major Transit Stop	Tier 1 (Low)	Tier 2 (Medium)	Tier 3 (High)	Tier 4 (Regional)
	Distance to Major Transit Stop			
Two Regular Buses (intersection of 2 non Rapid Bus* lines, each w/ at least 15 min. average peak headways)	750 - 2640 ft.	< 750 ft.	-	-
Regular plus Rapid Bus* (intersection of a Regular Bus and Rapid Bus line)	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Two Rapid Buses* (intersection of two Rapid Bus lines)	-	1500-2640 ft.	< 1500 ft.	-
Metrolink Rail Stations	1500 – 2640 ft.	750 – <1500 ft.	< 750 ft.	-
Metro Rail Stations	-	-	≤ 2640 ft.	< 750 ft. from intersection with another rail line or a Rapid Bus*

LA Metro Bus Line 33

(December 16 2018)

LAM - 33

12/16/18
6/23/19

Eastbound

AM (6-9am)	Main & Sunset	Main & 7th	Travel Time	Median	Start + Median	
	5:07	6:06	0:59	0:29	5:36	Not eligible
1	5:30	6:32	1:02	0:31	6:01	
2	5:49	6:53	1:04	0:32	6:21	
3	6:07	7:15	1:08	0:34	6:41	
4	6:25	7:37	1:12	0:36	7:01	
5	6:43	7:59	1:16	0:38	7:21	
6	7:02	8:19	1:17	0:38	7:40	
7	7:18	8:39	1:21	0:40	7:58	
8	7:37	8:59	1:22	0:41	8:18	
9	7:56	9:18	1:22	0:41	8:37	
10	8:16	9:34	1:18	0:39	8:55	
	8:36	9:57	1:21	0:40	9:16	Not eligible

Total AM trips 10

PM (3-7pm)	Main & Sunset	Main & 7th	Travel Time	Median	Start + Median	
	2:05	3:39	1:34	0:47	2:52	Not eligible
1	2:15	3:47	1:32	0:46	3:01	
2	2:26	4:01	1:35	0:47	3:13	
3	2:35	4:11	1:36	0:48	3:23	
4	2:42	4:16	1:34	0:47	3:29	
5	2:48	4:27	1:39	0:49	3:37	
6	2:56	4:33	1:37	0:48	3:44	
7	3:04	4:46	1:42	0:51	3:55	
8	3:13	4:52	1:39	0:49	4:02	
9	3:23	5:05	1:42	0:51	4:14	
10	3:33	5:13	1:40	0:50	4:23	
11	3:42	5:26	1:44	0:52	4:34	
12	3:52	5:33	1:41	0:50	4:42	
13	4:00	5:43	1:43	0:51	4:51	
14	4:11	5:54	1:43	0:51	5:02	
15	4:22	6:05	1:43	0:51	5:13	
16	4:33	6:15	1:42	0:51	5:24	
17	4:44	6:29	1:45	0:52	5:36	
18	4:55	6:36	1:41	0:50	5:45	
19	5:07	6:46	1:39	0:49	5:56	
20	5:18	6:55	1:37	0:48	6:06	
21	5:29	7:05	1:36	0:48	6:17	
22	5:40	7:15	1:35	0:47	6:27	
23	5:51	7:24	1:33	0:46	6:37	
24	6:05	7:35	1:30	0:45	6:50	
	6:28	7:49	1:21	0:40	7:08	Not eligible

Total PM trips 24

Eastbound Total Trips 34 Intervals Time 420/34 12.35

Westbound

AM (6-9am)	Main & 7th	Main & Sunset	Travel Time	Median	Start + Median	
	5:12	6:15	1:03	0:31	5:43	Not eligible
1	5:31	6:38	1:07	0:33	6:04	
2	5:44	6:55	1:11	0:35	6:19	
3	5:57	7:13	1:16	0:38	6:35	
4	6:08	7:24	1:16	0:38	6:46	
5	6:11	7:34	1:23	0:41	6:52	
6	6:15	7:41	1:26	0:43	6:58	
7	6:20	7:47	1:27	0:43	7:03	
8	6:24	7:52	1:28	0:44	7:08	
9	6:28	7:57	1:29	0:44	7:12	
10	6:32	8:03	1:31	0:45	7:17	
11	6:37	8:12	1:35	0:47	7:24	
12	6:46	8:22	1:36	0:48	7:34	
13	6:55	8:32	1:37	0:48	7:43	
14	7:04	8:42	1:38	0:49	7:53	
15	7:14	8:52	1:38	0:49	8:03	
16	7:25	9:04	1:39	0:49	8:14	
17	7:37	9:16	1:39	0:49	8:26	
18	7:51	9:28	1:37	0:48	8:39	
19	8:07	9:41	1:34	0:47	8:54	
	8:22	9:54	1:32	0:46	9:08	Not eligible

Total AM trips 19

PM (3-7pm)	Main & 7th	Main & Sunset	Travel Time	Median	Start + Median	
	2:03	3:34	1:31	0:45	2:48	Not eligible
1	2:19	3:50	1:31	0:45	3:04	
2	2:35	4:06	1:31	0:45	3:20	
3	2:51	4:22	1:31	0:45	3:36	
4	3:06	4:38	1:32	0:46	3:52	
5	3:21	4:54	1:33	0:46	4:07	
6	3:37	5:10	1:33	0:46	4:23	
7	3:53	5:26	1:33	0:46	4:39	
8	4:09	5:41	1:32	0:46	4:55	
9	4:26	5:58	1:32	0:46	5:12	
10	4:43	6:14	1:31	0:45	5:28	
11	4:56	6:30	1:34	0:47	5:43	
12	5:11	6:46	1:35	0:47	5:58	
13	5:29	7:04	1:35	0:47	6:16	
14	5:49	7:23	1:34	0:47	6:36	
	6:25	7:50	1:25	0:42	7:07	Not eligible

Total PM trips 14

Westbound Total Trips 33 Intervals Time 420/33 12.73

Monday through Friday

Effective Dec 16 2018

Eastbound Al Este (Approximate Times / Tiempos Aproximados)

SANTA MONICA	VENICE	CULVER CITY	LOS ANGELES		DOWNTOWN LOS ANGELES
1	2	3	4	5	6
2nd & Santa Monica	Main & Sunset	Culver City Station	Venice & La Brea	Venice & Vermont	Main & 7th
					Patsaouras Bus Plaza / LA Union Station
5:40A	5:07A	5:29A	5:40A	5:51A	6:06A
5:50	5:30	5:52	6:03	6:16	6:32
5:49	6:12	6:23	6:23	6:36	6:53
6:07	6:31	6:43	6:43	6:57	7:15
6:25	6:51	7:03	7:03	7:19	7:37
6:43	7:10	7:23	7:23	7:40	7:59
7:02	7:30	7:43	7:43	8:00	8:19
7:18	7:50	8:03	8:03	8:20	8:39
7:37	8:10	8:23	8:23	8:40	8:59
7:56	8:30	8:43	8:43	9:00	9:18
8:16	8:50	9:03	9:03	9:19	9:34
8:36	9:10	9:23	9:23	9:39	9:57
8:56	9:30	9:43	9:43	9:59	10:14
9:18	9:52	10:05	10:05	10:21	10:39
9:43	10:16	10:29	10:29	10:45	11:00
10:06	10:39	10:53	10:53	11:09	11:27
10:30	11:03	11:17	11:17	11:33	11:48
10:53	11:26	11:41	11:41	11:57	12:15P
11:14	11:48	12:03P	12:03P	12:19P	12:37
11:37	12:11P	12:26	12:26	12:42	1:00
11:57	12:31	12:46	12:46	1:02	1:20
12:14P	12:49	1:04	1:04	1:20	1:38
12:32	1:07	1:22	1:22	1:38	1:53
12:48	1:23	1:38	1:38	1:54	2:09
1:01	1:37	1:52	1:52	2:09	2:27
1:15	1:51	2:06	2:06	2:24	2:42
1:29	2:05	2:20	2:20	2:38	2:56
1:42	2:19	2:34	2:34	2:52	3:10
1:54	2:32	2:48	2:48	3:07	3:25
2:05	2:45	3:01	3:01	3:21	3:39
2:15	2:55	3:12	3:12	3:32	3:47
2:26	3:06	3:23	3:23	3:43	4:01
2:35	3:16	3:33	3:33	3:53	4:11
2:42	3:24	3:41	3:41	4:01	4:16
2:48	3:31	3:49	3:49	4:09	4:27
2:56	3:40	3:58	3:58	4:18	4:33
3:04	3:49	4:07	4:07	4:27	4:46
3:13	3:58	4:16	4:16	4:36	4:52
3:23	4:08	4:26	4:26	4:46	5:05
3:33	4:18	4:36	4:36	4:56	5:13
3:42	4:27	4:46	4:46	5:06	5:26
3:52	4:37	4:56	4:56	5:16	5:33
4:00	4:47	5:06	5:06	5:26	5:43
4:11	4:58	5:17	5:17	5:37	5:54
4:22	5:09	5:28	5:28	5:48	6:05
4:33	5:20	5:39	5:39	5:59	6:15
4:44	5:31	5:50	5:50	6:10	6:29
4:55	5:42	6:01	6:01	6:21	6:36
5:07	5:54	6:12	6:12	6:32	6:46
5:18	6:05	6:23	6:23	6:41	6:55
5:29	6:16	6:34	6:34	6:51	7:05
5:40	6:27	6:45	6:45	7:02	7:15
5:51	6:38	6:56	6:56	7:11	7:24
6:05	6:51	7:07	7:07	7:22	7:35
6:28	7:07	7:21	7:21	7:36	7:49
6:45	7:22	7:36	7:36	7:51	8:06
7:04	7:40	7:52	7:52	8:07	8:20
7:23	7:58	8:10	8:10	8:24	8:39
7:46	8:16	8:28	8:28	8:42	8:57
8:05	8:34	8:46	8:46	9:00	9:14
8:24	8:52	9:04	9:04	9:17	9:31
8:45	9:12	9:24	9:24	9:37	9:49
9:08	9:35	9:47	9:47	10:00	10:14
9:28	9:55	10:07	10:07	10:20	10:32
9:50	10:17	10:29	10:29	10:40	10:52
10:14	10:41	10:52	10:52	11:03	11:15
10:48	11:12	11:22	11:22	11:33	11:45
11:11P	11:20	11:42	11:42	12:03A	12:15A
11:43	11:52	12:14A	12:14A	12:35	12:47
12:19A	12:28A	12:50	1:00	1:11	1:23
12:40	12:49	1:11	1:21	1:32	1:44
1:00	1:09	1:31	1:41	1:52	2:04
1:23	1:32	1:54	2:04	2:15	2:27
1:50	1:59	2:21	2:31	2:42	2:54
2:20	2:29	2:51	3:01	3:12	3:24
2:55	3:04	3:26	3:36	3:47	3:59
3:35	3:44	4:06	4:16	4:27	4:39
4:35	4:44	5:06	5:16	5:27	5:39

Westbound Al Oeste (Approximate Times / Tiempos Aproximados)

DOWNTOWN LOS ANGELES	LOS ANGELES		CULVER CITY	VENICE	SANTA MONICA
8	7	5	4	3	2
Patsaouras Bus Plaza / LA Union Station	Spring & 6th	Venice & Vermont	Venice & La Brea	Culver City Station	Main & Sunset
					2nd & Santa Monica
4:00A	4:10A	4:25A	4:37A	4:46A	5:08A
4:38	4:48	4:53	5:07	5:17	5:41
4:52	5:02	5:07	5:21	5:31	5:55
5:12	5:22	5:27	5:41	5:51	6:15
5:31	5:41	5:47	6:01	6:13	6:38
5:44	5:54	6:00	6:17	6:29	6:55
5:57	6:07	6:15	6:32	6:46	7:13
6:08	6:18	6:23	6:42	6:56	7:24
6:11	6:29	6:29	6:49	7:04	7:34
6:15	6:34	6:34	6:54	7:10	7:41
6:20	6:39	6:39	6:59	7:16	7:47
6:24	6:43	6:43	7:04	7:21	7:52
6:28	6:47	6:47	7:09	7:26	7:57
6:32	6:51	6:51	7:14	7:32	8:03
6:37	6:56	6:56	7:20	7:41	8:12
6:46	7:06	7:06	7:30	7:51	8:22
6:55	7:16	7:16	7:40	8:01	8:32
7:04	7:26	7:26	7:50	8:11	8:42
7:14	7:36	7:36	8:00	8:21	8:52
7:25	7:48	7:48	8:12	8:33	9:04
7:37	8:00	8:00	8:24	8:45	9:16
7:51	8:13	8:13	8:36	8:57	9:28
8:07	8:29	8:29	8:50	9:09	9:41
8:22	8:44	8:44	9:04	9:22	9:54
8:38	9:00	9:00	9:18	9:35	10:07
8:52	9:14	9:14	9:32	9:49	10:21
9:07	9:29	9:29	9:47	10:03	10:35
9:22	9:44	9:44	10:02	10:16	10:48
9:38	10:00	10:00	10:18	10:32	11:04
9:55	10:17	10:17	10:35	10:49	11:21
10:12	10:34	10:34	10:52	11:06	11:38
10:28	10:51	10:51	11:09	11:23	11:55
10:46	11:09	11:09	11:27	11:41	12:13P
11:04	11:27	11:27	11:45	11:59	12:31
11:21	11:44	12:02P	12:16P	12:48	12:48
11:38	12:01P	12:19	12:33	1:05	1:05
11:55	12:18	12:36	12:50	1:22	1:22
12:12P	12:35	12:53	1:07	1:39	1:39
12:28	12:51	1:09	1:23	1:55	1:55
12:44	1:07	1:25	1:39	2:12	2:12
1:00	1:23	1:41	1:55	2:28	2:28
1:16	1:39	1:57	2:11	2:44	2:44
1:32	1:55	2:13	2:27	3:00	3:00
1:48	2:11	2:29	2:45	3:18	3:18
2:03	2:26	2:45	3:01	3:34	3:34
2:19	2:42	3:01	3:17	3:50	3:50
2:35	2:58	3:17	3:33	4:06	4:06
2:51	3:14	3:33	3:49	4:22	4:22
3:06	3:30	3:49	4:05	4:38	4:38
3:21	3:46	4:05	4:21	4:54	4:54
3:37	4:02	4:21	4:37	5:10	5:10
3:53	4:18	4:37	4:53	5:26	5:26
4:09	4:34	4:53	5:09	5:41	5:41
4:26	4:51	5:10	5:26	5:58	5:58
4:43	5:08	5:27	5:42	6:14	6:14
4:56	5:24	5:43	5:58	6:30	6:30
5:11	5:40	5:59	6:14	6:46	6:46
5:29	5:58	6:17	6:32	7:04	7:04
5:49	6:18	6:37	6:52	7:23	7:23
6:25	6:49	7:07	7:20	7:50	7:50
7:03	7:22	7:39	7:50	8:19	8:19
7:35	7:53	8:09	8:20	8:47	8:47
8:05	8:23	8:39	8:50	9:17	9:17
8:32	8:49	9:04	9:15	9:41	9:41
8:57	9:20	9:33	9:44	10:09	10:09
9:31	9:46	9:59	10:09	10:32	10:32
10:01	10:23	10:36	10:46	11:09	11:19
10:51	11:06	11:18	11:28	11:50	11:59
11:25	11:46	11:58	12:08A	12:30A	12:40A
11:42	12:06A	12:18A	12:28	12:50	1:00
11:56	12:25A	12:37	12:47	1:09	1:18
12:56A	1:04	1:25	1:46	2:08	2:17
1:03:04	2:25	2:37	2:46	3:08	3:17
	3:25	3:37	3:46	4:08	4:17

LA Metro Bus Line 733

(December 16 2018)

LAM - 733

12/16/18
current

Eastbound

AM (6-9am)	Santa Monica	LA Union Stnt	Travel Time	Median	Start + Median	
	5:09	6:24	1:15	0:37	5:46	Not eligible
1	5:29	6:45	1:16	0:38	6:07	
2	5:49	7:07	1:18	0:39	6:28	
3	6:07	7:28	1:21	0:40	6:47	
4	6:26	7:50	1:24	0:42	7:08	
5	6:43	8:13	1:30	0:45	7:28	
6	7:01	8:33	1:32	0:46	7:47	
7	7:20	8:52	1:32	0:46	8:06	
8	7:40	9:12	1:32	0:46	8:26	
9	8:00	9:32	1:32	0:46	8:46	
	8:21	9:53	1:32	0:46	9:07	Not eligible

Total AM trips 9

PM(3-7pm)	Santa Monica	LA Union Stnt	Travel Time	Median	Start + Median	
	2:00	3:44	1:44	0:52	2:52	Not eligible
1	2:14	4:03	1:49	0:54	3:08	
2	2:28	4:20	1:52	0:56	3:24	
3	2:41	4:35	1:54	0:57	3:38	
4	2:54	4:51	1:57	0:58	3:52	
5	3:07	5:07	2:00	1:00	4:07	
6	3:20	5:24	2:04	1:02	4:22	
7	3:32	5:40	2:08	1:04	4:36	
8	3:46	5:55	2:09	1:04	4:50	
9	4:01	6:09	2:08	1:04	5:05	
10	4:16	6:22	2:06	1:03	5:19	
11	4:31	6:35	2:04	1:02	5:33	
12	4:47	6:48	2:01	1:00	5:47	
13	5:03	7:02	1:59	0:59	6:02	
14	5:20	7:14	1:54	0:57	6:17	
15	5:37	7:27	1:50	0:55	6:32	
16	5:54	7:40	1:46	0:53	6:47	
	6:14	7:55	1:41	0:50	7:04	Not eligible

Total PM trips 16

Eastbound Total Trips 25 Intervals Time 420/25 16.80

Westbound

AM (6-9am)	LA Union Stnt	Santa Monica	Travel Time	Median	Start + Median	
	5:11	6:29	1:18	0:39	5:50	Not eligible
1	5:27	6:48	1:21	0:40	6:07	
2	5:40	7:04	1:24	0:42	6:22	
3	5:51	7:17	1:26	0:43	6:34	
4	6:01	7:31	1:30	0:45	6:46	
5	6:10	7:45	1:35	0:47	6:57	
6	6:18	7:58	1:40	0:50	7:08	
7	6:25	8:11	1:46	0:53	7:18	
8	6:34	8:24	1:50	0:55	7:29	
9	6:43	8:34	1:51	0:55	7:38	
10	6:55	8:47	1:52	0:56	7:51	
11	7:07	8:59	1:52	0:56	8:03	
12	7:21	9:13	1:52	0:56	8:17	
13	7:35	9:27	1:52	0:56	8:31	
14	7:56	9:44	1:48	0:54	8:50	
	8:11	9:58	1:47	0:53	9:04	Not eligible

Total AM trips 14

PM(3-7pm)	Main & 7th	Main & Sunset	Travel Time	Median	Start + Median	
	2:01	3:45	1:44	0:52	2:53	Not eligible
1	2:21	4:04	1:43	0:51	3:12	
2	2:41	4:24	1:43	0:51	3:32	
3	3:01	4:44	1:43	0:51	3:52	
4	3:20	5:04	1:44	0:52	4:12	
5	3:42	5:24	1:42	0:51	4:33	
6	4:02	5:44	1:42	0:51	4:53	
7	4:21	6:05	1:44	0:52	5:13	
8	4:42	6:27	1:45	0:52	5:34	
9	5:03	6:48	1:45	0:52	5:55	
10	5:25	7:09	1:44	0:52	6:17	
11	5:48	7:31	1:43	0:51	6:39	
	6:14	7:54	1:40	0:50	7:04	Not eligible

Total PM trips 11

Total Trips 25 Intervals Time 420/25 16.80

Monday through Friday

Effective Dec 16 2018

733

Eastbound Al Este (Approximate Times/Tiempos Aproximados)

SANTA MONICA	VENICE	CULVER CITY	MID-CITY	DOWNTOWN LOS ANGELES
1	2	3	4	5
2nd & Santa Monica	Venice & Lincoln	Culver City Station	Venice & La Brea	Venice & Vermont
4:45A	4:59A	5:14A	5:23A	5:33A
5:09	5:24	5:40	5:51	6:02
5:29	5:44	6:00	6:11	6:23
5:49	6:04	6:20	6:31	6:43
6:07	6:23	6:40	6:51	7:03
6:26	6:42	7:00	7:11	7:24
6:43	6:59	7:20	7:31	7:46
7:01	7:18	7:40	7:51	8:06
7:20	7:37	8:00	8:11	8:25
7:40	7:57	8:21	8:32	8:45
8:00	8:18	8:42	8:53	9:06
8:21	8:39	9:03	9:14	9:27
8:43	9:01	9:24	9:35	9:48
9:04	9:22	9:45	9:56	10:08
9:25	9:43	10:06	10:17	10:29
9:46	10:04	10:26	10:37	10:49
10:05	10:24	10:46	10:57	11:09
10:25	10:44	11:06	11:17	11:29
10:45	11:04	11:26	11:37	11:49
11:05	11:24	11:46	11:57	12:09P
11:25	11:44	12:06P	12:17P	12:29
11:44	12:03P	12:26	12:37	12:49
12:03P	12:22	12:46	12:57	1:10
12:23	12:42	1:06	1:17	1:30
12:41	1:00	1:24	1:35	1:48
12:59	1:18	1:42	1:53	2:06
1:16	1:35	2:00	2:11	2:25
1:33	1:52	2:18	2:29	2:43
1:48	2:08	2:35	2:46	3:00
2:00	2:22	2:51	3:03	3:18
2:14	2:36	3:07	3:20	3:36
2:28	2:50	3:22	3:35	3:52
2:41	3:03	3:36	3:50	4:07
2:54	3:17	3:51	4:05	4:22
3:07	3:31	4:06	4:20	4:37
3:20	3:45	4:21	4:35	4:53
3:32	3:57	4:33	4:50	5:08
3:46	4:12	4:48	5:05	5:23
4:01	4:27	5:03	5:20	5:38
4:16	4:42	5:18	5:35	5:53
4:31	4:57	5:33	5:50	6:07
4:47	5:13	5:48	6:05	6:21
5:03	5:29	6:04	6:20	6:36
5:20	5:46	6:20	6:35	6:50
5:37	6:03	6:35	6:50	7:04
5:54	6:20	6:51	7:05	7:18
6:14	6:39	7:08	7:20	7:33
6:32	6:55	7:23	7:35	7:47
6:50	7:13	7:39	7:51	8:03
7:11	7:34	7:58	8:09	8:21
7:34	7:56	8:18	8:29	8:41
7:56	8:16	8:38	8:49	9:01
8:19	8:38	8:59	9:10	9:22
8:42	9:01	9:20	9:31	9:42
9:04	9:22	9:41	9:52	10:03
9:29	9:47	10:06	10:16	10:27
9:55	10:13	10:31	10:40	10:50
10:26	10:42	10:59	11:08	11:18
10:57	11:13	11:29	11:37	11:47
11:28	11:43	11:59	12:07A	12:17A
12:02A	12:17A	12:33A	12:41	12:51

Westbound Al Oeste (Approximate Times/Tiempos Aproximados)

DOWNTOWN LOS ANGELES	MID-CITY	CULVER CITY	VENICE	SANTA MONICA
6	5	4	3	2
Patsaouras Bus Plaza / LA Union Station	Venice & Vermont	Venice & La Brea	Culver City Station	Venice & Lincoln
4:55A	5:11A	5:19A	5:30A	5:40A
5:11	5:27	5:35	5:47	5:57
5:27	5:43	5:51	6:04	6:16
5:40	5:56	6:04	6:19	6:31
5:51	6:07	6:16	6:31	6:43
6:01	6:17	6:26	6:43	6:55
6:10	6:26	6:35	6:53	7:07
6:18	6:35	6:44	7:03	7:19
6:25	6:44	6:53	7:14	7:30
6:34	6:53	7:02	7:24	7:43
6:43	7:02	7:12	7:34	7:54
6:55	7:14	7:24	7:46	8:07
7:07	7:26	7:36	7:58	8:19
7:21	7:41	7:51	8:12	8:33
7:35	7:56	8:06	8:27	8:47
7:56	8:15	8:25	8:44	9:04
8:11	8:32	8:42	9:00	9:19
8:27	8:48	8:58	9:16	9:34
8:44	9:05	9:15	9:33	9:49
9:03	9:24	9:34	9:51	10:05
9:22	9:43	9:53	10:09	10:23
9:43	10:04	10:13	10:28	10:41
10:04	10:25	10:34	10:48	11:01
10:24	10:45	10:54	11:08	11:21
10:44	11:05	11:14	11:28	11:41
11:05	11:26	11:35	11:49	12:02P
11:25	11:46	11:55	12:09P	12:22
11:44	12:06P	12:15P	12:29	12:42
12:04P	12:26	12:35	12:49	1:02
12:24	12:46	12:55	1:09	1:22
12:44	1:06	1:15	1:29	1:42
1:04	1:26	1:35	1:49	2:02
1:23	1:45	1:54	2:09	2:23
1:42	2:04	2:14	2:29	2:43
2:01	2:24	2:34	2:49	3:03
2:21	2:44	2:54	3:09	3:23
2:41	3:04	3:15	3:29	3:43
3:01	3:24	3:35	3:49	4:03
3:20	3:43	3:55	4:09	4:23
3:42	4:04	4:15	4:29	4:43
4:02	4:24	4:35	4:49	5:03
4:21	4:44	4:55	5:10	5:24
4:42	5:05	5:17	5:32	5:46
5:03	5:27	5:39	5:54	6:08
5:25	5:49	6:01	6:16	6:29
5:48	6:12	6:24	6:39	6:52
6:14	6:36	6:48	7:03	7:16
6:41	7:03	7:14	7:29	7:42
7:12	7:33	7:43	7:57	8:09
7:44	8:03	8:12	8:25	8:36
8:12	8:30	8:39	8:52	9:03
8:42	8:59	9:07	9:20	9:31
9:09	9:26	9:34	9:46	9:56
9:39	9:56	10:04	10:16	10:26
10:21	10:38	10:46	10:57	11:06
11:04	11:21	11:28	11:39	11:48
				12:02A
				12:17A

Big Blue Bus Route 3

(Septmber 16 2018 to March 16 2019)

BBB - 3

9/16/18
3/16/19

Northbound

AM (6-9am)

	Aviation	Arizona	Travel Time	Median	Start + Median	
	5:23	6:02	0:39	0:19	5:42	Not eligible
1	5:45	6:27	0:42	0:21	6:06	
2	6:05	6:50	0:45	0:22	6:27	
3	6:26	7:17	0:51	0:25	6:51	
4	6:40	7:37	0:57	0:28	7:08	
5	7:00	8:02	1:02	0:31	7:31	
6	7:15	8:19	1:04	0:32	7:47	
7	7:35	8:39	1:04	0:32	8:07	
8	7:55	8:59	1:04	0:32	8:27	
9	8:15	9:17	1:02	0:31	8:46	
	8:35	9:37	1:02	0:31	9:06	Not eligible

Schedule Date
16-Sep-18

In Blue are trip extrapolated
from the schedule following
the intervals indicated on
the schedule (e.g. 10 min.,
12 min., 14 min., 15 min.,
20 min.)

Total AM trips

9

PM(3-7pm)

	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:15	3:14	0:59	0:29	2:44	Not eligible
1	2:35	3:34	0:59	0:29	3:04	
2	2:55	3:54	0:59	0:29	3:24	
3	3:15	4:14	0:59	0:29	3:44	
4	3:35	4:34	0:59	0:29	4:04	
5	3:55	4:54	0:59	0:29	4:24	
6	4:15	5:14	0:59	0:29	4:44	
7	4:35	5:34	0:59	0:29	5:04	
8	4:55	5:54	0:59	0:29	5:24	
9	5:15	6:14	0:59	0:29	5:44	
10	5:35	6:34	0:59	0:29	6:04	
11	5:55	6:54	0:59	0:29	6:24	
12	6:15	7:11	0:56	0:28	6:43	
	6:35	7:31	0:56	0:28	7:03	Not eligible

Total PM trips

12

Northbound Total Trips 21 Intervals Time 420/21 20.00

Southbound

AM (6-9am)

	Arizona	Aviation	Travel Time	Median	Start + Median	
	5:20	6:00	0:40	0:20	5:40	Not eligible
1	5:50	6:30	0:40	0:20	6:10	
2	6:20	7:06	0:46	0:23	6:43	
3	6:48	7:34	0:46	0:23	7:11	
4	7:08	7:59	0:51	0:25	7:33	
5	7:28	8:19	0:51	0:25	7:53	
6	7:48	8:46	0:58	0:29	8:17	
7	8:08	9:06	0:58	0:29	8:37	
8	8:28	9:26	0:58	0:29	8:57	
	8:48	9:46	0:58	0:29	9:17	Not eligible

Total AM trips

8

PM(3-7pm)

	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:05	3:19	1:14	0:37	2:42	Not eligible
1	2:25	3:39	1:14	0:37	3:02	
2	2:45	4:04	1:19	0:39	3:24	
3	3:05	4:28	1:23	0:41	3:46	
4	3:25	4:48	1:23	0:41	4:06	
5	3:45	5:06	1:21	0:40	4:25	
6	4:05	5:26	1:21	0:40	4:45	
7	4:25	5:46	1:21	0:40	5:05	
8	4:45	6:06	1:21	0:40	5:25	
9	5:05	6:26	1:21	0:40	5:45	
10	5:25	6:46	1:21	0:40	6:05	
11	5:45	7:06	1:21	0:40	6:25	
12	6:05	7:22	1:17	0:38	6:43	
	6:25	7:42	1:17	0:38	7:03	Not eligible

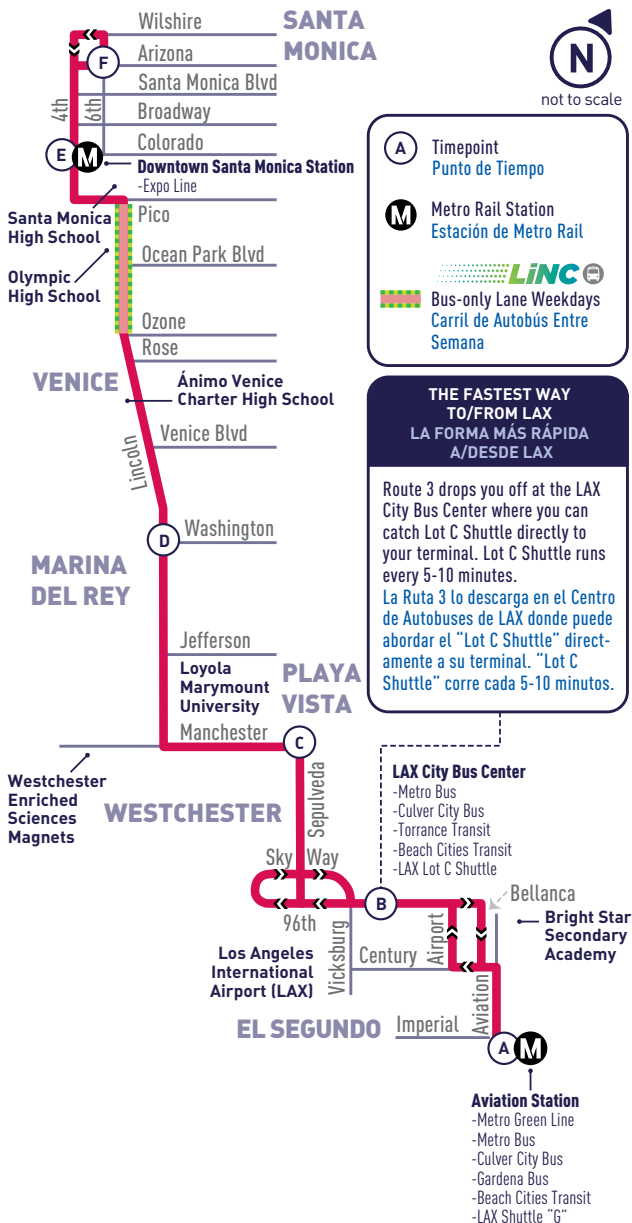
Total PM trips

12

Southbound Total Trips 20 Intervals Time 420/20 21.00

LINCOLN BLVD

3



AVIATION STATION GREEN LINE TO DOWNTOWN SANTA MONICA

WEEKDAY DURANTE LA SEMANA

Aviation Station (Bay 1) (A)	LAX City Bus Center (Bay 12) (B)	Manchester & Sepulveda (C)	Lincoln & Washington (D)	Arizona & 5th (F)
4:53	5:01	5:05	5:15	5:30
5:03	5:11	5:15	5:25	5:40
5:23	5:31	5:35	5:46	6:02
5:45	5:54	5:58	6:10	6:27
6:05	6:14	6:18	6:31	6:50
6:26	6:35	6:40	6:55	7:17
6:40	6:49	6:55	7:12	7:37
7:00	7:10	7:16	7:35	8:02
7:15	7:25	7:32	7:51	8:19
7:35	7:45	7:52	8:11	8:39
7:55	8:05	8:12	8:31	8:59
8:15	8:25	8:32	8:50	9:17
8:35	8:45	8:52	9:10	9:37
8:55	9:05	9:12	9:30	9:57
Then service every 20 minutes at:				
:15	:25	:32	:49	:14
:35	:45	:52	:09	:34
:55	:05	:12	:29	:54
Until:				
6:15	6:25	6:31	6:46	7:11
6:35	6:45	6:51	7:06	7:31
6:55	7:04	7:10	7:24	7:47
7:15	7:24	7:30	7:43	8:03
7:35	7:44	7:49	8:02	8:20
7:55	8:04	8:09	8:22	8:40
8:15	8:24	8:29	8:42	9:00
8:40	8:49	8:54	9:07	9:25
9:10	9:18	9:23	9:35	9:51
9:40	9:48	9:53	10:05	10:21
10:10	10:18	10:23	10:35	10:51
10:40	10:48	10:53	11:04	11:19
11:10	11:18	11:23	11:34	11:49
11:40	11:48	11:53	12:04	12:19
12:10	12:17	12:21	12:31	12:45

Good Bus Karma

TMI! TMI! Save the cell phone calls
until you get off the bus.

ALL PM
TIMES IN
BOLD

ROUTE
3

DOWNTOWN SANTA MONICA TO AVIATION STATION GREEN LINE

WEEKDAY
DURANTE LA SEMANA

Arizona & 5th (F)	4th & Santa Monica Place (Downtown SM Station) (E)	Lincoln & Washington (D)	Manchester & Sepulveda (C)	LAX City Bus Center (Bay 11) (B)	Aviation Station (A)
4:45	4:48	4:57	5:07	5:12	5:20
5:20	5:24	5:34	5:45	5:51	6:00
5:50	5:54	6:04	6:15	6:21	6:30
6:20	6:25	6:37	6:50	6:56	7:06
6:48	6:53	7:05	7:18	7:24	7:34
7:08	7:13	7:27	7:41	7:48	7:59
7:28	7:33	7:47	8:01	8:08	8:19
7:48	7:54	8:11	8:26	8:34	8:46
8:08	8:14	8:31	8:46	8:54	9:06
8:28	8:34	8:51	9:06	9:14	9:26
8:48	8:54	9:11	9:26	9:34	9:46
9:08	9:14	9:31	9:46	9:54	10:06
9:28	9:34	9:51	10:06	10:14	10:26
9:48	9:54	10:11	10:26	10:34	10:46
10:08	10:14	10:33	10:48	10:56	11:09
10:26	10:32	10:51	11:06	11:14	11:27
10:46	10:52	11:11	11:26	11:34	11:47
11:06	11:12	11:31	11:46	11:54	12:07
11:25	11:31	11:50	12:05	12:13	12:26
11:45	11:51	12:10	12:25	12:33	12:46
12:05	12:11	12:30	12:45	12:53	1:06
12:25	12:32	12:54	1:09	1:17	1:30
12:45	12:52	1:14	1:29	1:37	1:50
1:05	1:13	1:37	1:53	2:01	2:15
1:25	1:33	1:57	2:13	2:21	2:35
1:45	1:53	2:19	2:36	2:44	2:59
2:05	2:13	2:39	2:56	3:04	3:19
2:25	2:33	2:59	3:16	3:24	3:39
2:45	2:53	3:21	3:39	3:48	4:04
3:05	3:13	3:43	4:02	4:11	4:28
3:25	3:33	>4:03	>4:22	>4:31	4:48
3:45	3:53	>4:21	>4:40	>4:49	5:06
4:05	4:13	>4:41	>5:00	>5:09	5:26
4:25	4:33	>5:01	>5:20	>5:29	5:46
4:45	4:53	>5:21	>5:40	>5:49	6:06
5:05	5:13	>5:41	>6:00	>6:09	6:26
5:25	5:33	>6:01	>6:20	>6:29	6:46
5:45	5:53	>6:21	>6:40	>6:49	7:06
6:05	6:12	>6:39	>6:57	>7:06	7:22
6:25	6:32	>6:59	>7:17	>7:26	7:42
6:45	6:52	>7:18	>7:35	>7:44	7:58
7:05	7:12	7:38	7:55	8:04	8:18
7:25	7:32	7:56	8:11	8:20	8:33
7:45	7:52	8:12	8:26	8:34	8:46
8:05	8:12	8:31	8:44	8:51	9:01
8:25	8:32	8:51	9:04	9:11	9:21
8:45	8:51	9:08	9:20	9:26	9:35
9:10	9:16	9:33	9:45	9:51	10:00
9:40	9:46	10:03	10:15	10:21	10:30
10:10	10:16	10:33	10:45	10:51	11:00
10:40	10:46	11:01	11:11	11:17	11:26
11:10	11:16	11:31	11:41	11:47	11:56
11:45	11:51	12:06	12:16	12:22	12:31
12:25	12:31	12:46	12:56	1:02	1:11

ALL PM
TIMES IN
BOLD

ROUTE
3

► Trips use the bus-only lane and may depart up to 8 minutes earlier than time indicated./Viajes usan el carril del autobús y pueden salir hasta 8 minutos antes del tiempo indicado.

EFFECTIVE DATE: SEPTEMBER 16, 2018

Big Blue Bus Route RAPID 3

(September 16 2018 to March 16 2019)

BBB - R3

9/16/18
3/16/19

Northbound

AM (6-9am)	Aviation	Arizona	Travel Time	Median	Start + Median	
	5:35	6:11	0:36	0:18	5:53	Not eligible
1	5:48	6:24	0:36	0:18	6:06	
2	6:00	6:40	0:40	0:20	6:20	
3	6:12	6:55	0:43	0:21	6:33	
4	6:24	7:07	0:43	0:21	6:45	
5	6:35	7:21	0:46	0:23	6:58	
6	6:45	7:31	0:46	0:23	7:08	
7	6:55	7:45	0:50	0:25	7:20	
8	7:02	7:58	0:56	0:28	7:30	
9	7:10	8:11	1:01	0:30	7:40	
10	7:20	8:25	1:05	0:32	7:52	
11	7:30	8:35	1:05	0:32	8:02	
12	7:40	8:45	1:05	0:32	8:12	
13	7:50	8:55	1:05	0:32	8:22	
14	8:00	9:05	1:05	0:32	8:32	
15	8:10	9:15	1:05	0:32	8:42	
16	8:21	9:23	1:02	0:31	8:52	
	8:33	9:31	0:58	0:29	9:02	Not eligible

Schedule Effective Date:
16-Sep-18

In Blue are trip extrapolated from the schedule following the intervals indicated on the schedule (e.g. 10 min., 12 min., 14 min., 15 min., 20 min.)

Total AM trips

16

PM(3-7pm)	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:34	3:25	0:51	0:25	2:59	Not eligible
1	2:49	3:40	0:51	0:25	3:14	
2	3:04	3:55	0:51	0:25	3:29	
3	3:19	4:10	0:51	0:25	3:44	
4	3:34	4:25	0:51	0:25	3:59	
5	3:49	4:40	0:51	0:25	4:14	
6	4:03	4:54	0:51	0:25	4:28	
7	4:17	5:08	0:51	0:25	4:42	
8	4:31	5:22	0:51	0:25	4:56	
9	4:45	5:36	0:51	0:25	5:10	
10	4:59	5:50	0:51	0:25	5:24	
11	5:13	6:04	0:51	0:25	5:38	
12	5:27	6:18	0:51	0:25	5:52	
13	5:42	6:33	0:51	0:25	6:07	
14	5:57	6:48	0:51	0:25	6:22	
15	6:12	7:01	0:49	0:24	6:36	
16	6:30	7:15	0:45	0:22	6:52	
	7:00	7:45	0:45	0:22	7:22	Not eligible

Total PM trips

16

Northbound Total Trips	32	Intervals Time	420/32	13.13
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Southbound

AM (6-9am)	Arizona	Aviation	Travel Time	Median	Start + Median	
1	6:32	7:15	0:43	0:21	6:53	
2	6:47	7:32	0:45	0:22	7:09	
3	7:02	7:47	0:45	0:22	7:24	
4	7:16	8:03	0:47	0:23	7:39	
5	7:29	8:16	0:47	0:23	7:52	
6	7:42	8:33	0:51	0:25	8:07	
7	7:54	8:45	0:51	0:25	8:19	
8	8:06	8:57	0:51	0:25	8:31	
9	8:19	9:10	0:51	0:25	8:44	
10	8:34	9:25	0:51	0:25	8:59	
	8:49	9:40	0:51	0:25	9:14	Not eligible

Total AM trips

10

PM(3-7pm)	Aviation	Arizona	Travel Time	Median	Start + Median	
	2:18	3:23	1:05	0:32	2:50	Not eligible
1	2:28	3:33	1:05	0:32	3:00	
2	2:39	3:49	1:10	0:35	3:14	
3	2:50	4:00	1:10	0:35	3:25	
4	3:00	4:10	1:10	0:35	3:35	
5	3:10	4:20	1:10	0:35	3:45	
6	3:20	4:30	1:10	0:35	3:55	
7	3:30	4:40	1:10	0:35	4:05	
8	3:40	4:50	1:10	0:35	4:15	
9	3:50	5:00	1:10	0:35	4:25	
10	4:00	5:10	1:10	0:35	4:35	
11	4:10	5:20	1:10	0:35	4:45	
12	4:20	5:30	1:10	0:35	4:55	
13	4:30	5:40	1:10	0:35	5:05	
14	4:40	5:50	1:10	0:35	5:15	
15	4:50	6:00	1:10	0:35	5:25	
16	5:00	6:10	1:10	0:35	5:35	
17	5:10	6:20	1:10	0:35	5:45	
18	5:20	6:30	1:10	0:35	5:55	
19	5:30	6:40	1:10	0:35	6:05	
20	5:40	6:50	1:10	0:35	6:15	
21	5:50	7:00	1:10	0:35	6:25	
22	6:00	7:10	1:10	0:35	6:35	
23	6:15	7:30	1:15	0:37	6:52	
	6:30	7:45	1:15	0:37	7:07	Not eligible

Total PM trips

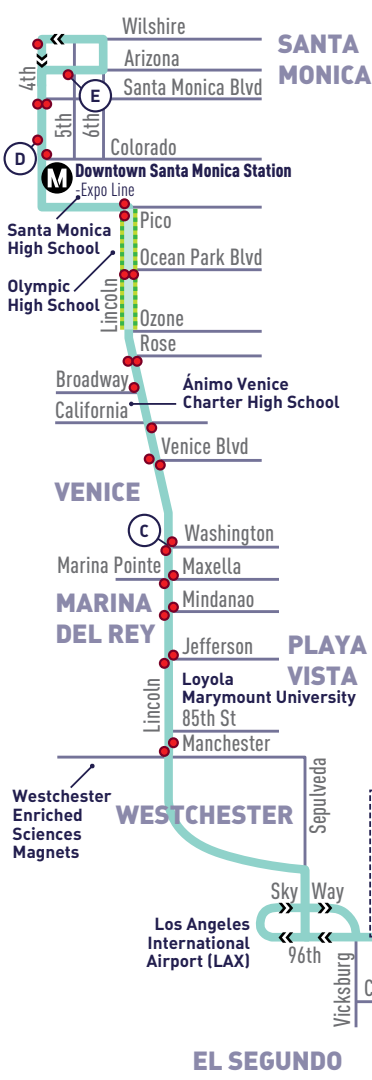
23

Southbound Total Trips	33	Intervals Time	420/33	12.73
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LINCOLN BLVD

rapid

3



SANTA MONICA



● Big Blue Bus Stop
Parada de Big Blue Bus

Ⓐ Timepoint
Punto de Tiempo

Ⓜ Metro Rail Station
Estación de Metro Rail

LINC
Bus-Only Lane Weekdays
Carril de Autobús Entre
Semana

**THE FASTEST WAY
TO/FROM LAX
LA FORMA MÁS RÁPIDA
A/DESDE LAX**

Rapid 3 drops you off at the LAX City Bus Center where you can catch Lot C Shuttle directly to your terminal. Lot C Shuttle runs every 5-10 minutes.
El Rapid 3 lo descarga en el Centro de Autobuses de LAX donde puede abordar el "Lot C Shuttle" directamente a su terminal. "Lot C Shuttle" corre cada 5-10 minutos.

LAX City Bus Center
-Metro Bus
-Culver City Bus
-Torrance Transit
-Beach Cities Transit
-LAX Lot C Shuttle

Ⓐ Ⓜ **Aviation Station**
-Metro Green Line
-Metro Bus
-Culver City Bus
-Gardena Bus
-Beach Cities Transit
-LAX Shuttle "G"

AVIATION STATION GREEN LINE TO DOWNTOWN SANTA MONICA

WEEKDAY DURANTE LA SEMANA

Aviation Station (Bay 1) (A)	LAX City Bus Center (Bay 12) (B)	Lincoln & Washington (C)	Arizona & 5th (E)
5:35	5:43	5:56	6:11
5:48	5:56	6:09	6:24
6:00	6:09	6:22	6:40
6:12	6:21	6:35	6:55
6:24	6:33	6:47	7:07
6:35	6:45	7:01	7:21
6:45	6:55	7:11	7:31
6:55	7:06	7:23	7:45
7:02	7:13	7:35	7:58
7:10	7:22	7:46	8:11
7:20	7:32	8:00	8:25
7:30	7:42	8:10	8:35
7:40	7:52	8:20	8:45
7:50	8:02	8:30	8:55
8:00	8:12	8:40	9:05
8:10	8:22	8:50	9:15
8:21	8:32	8:58	9:23
8:33	8:44	9:07	9:31
8:45	8:56	9:19	9:43
8:57	9:08	9:31	9:55
9:10	9:21	9:44	10:08
9:22	9:32	9:52	10:16
9:34	9:44	10:04	10:28
Then service every 15 minutes at:			
:04	:14	:32	:55
:19	:29	:47	:10
:34	:44	:02	:25
:49	:59	:17	:40
Until:			
3:04	3:14	3:32	3:55
3:19	3:29	3:47	4:10
3:34	3:44	4:02	4:25
3:49	3:59	4:17	4:40
Then service every 14 minutes until:			
5:27	5:37	5:55	6:18
5:42	5:52	6:10	6:33
5:57	6:07	6:25	6:48
6:12	6:22	6:39	7:01
6:30	6:39	6:55	7:15
7:00	7:09	7:25	7:45
7:30	7:39	7:54	8:13

ALL PM
TIMES IN
BOLD

RAPID

3

DOWNTOWN SANTA MONICA TO AVIATION STATION GREEN LINE

WEEKDAY DURANTE LA SEMANA

Arizona & 5th (E)	4th & Santa Monica Place (Downtown SM Station)	Lincoln & Washington (C)	LAX City Bus Center (Bay 11) (B)	Aviation Station (A)
6:32	6:37	6:49	7:04	7:15
6:47	6:52	7:05	7:21	7:32
7:02	7:07	7:20	7:36	7:47
7:16	7:21	7:35	7:52	8:03
7:29	7:34	7:48	8:05	8:16
7:42	7:48	8:04	8:22	8:33
7:54	8:00	8:16	8:34	8:45
8:06	8:12	8:28	8:46	8:57
8:19	8:25	8:41	8:59	9:10
8:34	8:40	8:56	9:14	9:25
8:49	8:55	9:11	9:29	9:40
Then service every 15 minutes at:				
:04	:10	:26	:44	:55
:19	:25	:41	:59	:10
:34	:40	:56	:14	:25
:49	:55	:11	:29	:40
Until:				
11:03	11:09	11:26	11:44	11:56
11:18	11:24	11:41	11:59	12:11
11:33	11:39	11:56	12:14	12:26
11:48	11:54	12:11	12:29	12:41
12:03	12:10	12:28	12:47	12:58
12:18	12:25	12:43	1:02	1:13
12:33	12:40	12:58	1:17	1:28
12:48	12:55	1:13	1:32	1:43
1:03	1:10	1:28	1:47	1:58
1:18	1:25	1:43	2:02	2:13
1:33	1:40	2:00	2:20	2:32
1:48	1:55	2:15	2:35	2:47
2:03	2:11	2:34	2:55	3:08
2:18	2:26	2:49	3:10	3:23
2:28	2:36	2:59	3:20	3:33
2:39	2:47	3:13	3:35	3:49
2:50	2:58	3:24	3:46	4:00
Then service every 10 minutes until:				
6:15	6:23	>6:51	>7:15	7:30
6:30	6:38	>7:06	>7:30	7:45
6:45	6:52	>7:18	>7:40	7:54
7:00	7:07	7:31	7:52	8:05
7:15	7:22	7:44	8:03	8:15
7:35	7:42	8:04	8:23	8:35
7:55	8:01	8:20	8:38	8:49
8:15	8:21	8:37	8:54	9:05
8:35	8:41	8:57	9:14	9:25

- Trips use the bus-only lane and may depart up to 8 minutes earlier than time indicated./Viajes usan el carril de autobús y pueden salir hasta 8 minutos antes del tiempo indicado.

ALL PM
TIMES IN
BOLD

RAPID
3

4. APPENDIX D

Reference Materials of Connect SoCal, Draft 2020 RTP/SCS Transit Technical Appendix supporting Contestation of TOC Tier Assignment:

case # DIR-2019-4920-TOC,

as conferred by PAR-2019-3781-TOC

and based on erroneous assignment of Tier 2 status to the Major Transit Stop at Venice Blvd. and Lincoln Blvd.

The following will be included in the forthcoming Connect SoCal, Draft 2020 RTP/SCS Transit Technical Appendix.

HIGH QUALITY TRANSIT CORRIDORS AND MAJOR TRANSIT STOPS

BACKGROUND

The Sustainable Communities and Climate Protection Act of 2008, Senate Bill (SB) 375, requires that Metropolitan Planning Organizations (MPOs) develop a Sustainable Communities Strategy (SCS) to reduce per capita greenhouse gas emissions through integrated transportation, land use, housing and environmental planning. SB 375 creates incentives for residential or mixed-use residential projects that may be exempt from, or subject to a limited review of, the California Environmental Quality Act (CEQA), provided they are consistent with the MPO's adopted SCS. These "transit priority projects" must, among other criteria, be located within one-half mile of a major transit stop or high-quality transit corridor (HQTC).

SB 743, signed into law in 2013, provides further opportunities for CEQA exemption and streamlining to facilitate transit oriented development (TOD). Specifically, certain types of projects within "transit priority areas" (TPAs) can benefit from a CEQA exemption if they are consistent with an adopted specific plan and the SCS. A TPA is an area within one-half mile of a major transit stop that is existing or planned, if the planned stop is scheduled to be completed within the planning horizon included in a Federal Transportation Improvement Program (FTIP).

STATUTORY DEFINITIONS

California statute defines [major transit stop](#) and [high quality transit corridor](#) as follows.

CA Pub. Res. Code § 21155(b)

For purposes of this chapter, a transit priority project shall (1) contain at least 50 percent residential use, based on total building square footage and, if the project contains between 26 percent and 50 percent nonresidential uses, a floor area ratio of not less than 0.75; (2) provide a minimum net density of at least 20 dwelling units per acre; and (3) be within one-half mile of a major transit stop or high-quality transit corridor included in a regional transportation plan. A [major transit stop](#) is as defined in Section 21064.3, except that, for purposes of this section, it also includes major transit stops that are included in the applicable regional transportation plan. For purposes of this section, a [high-quality transit corridor](#) means a corridor with fixed route bus service with service intervals no longer than 15 minutes during peak commute hours. A project shall be considered to be within one-half mile of a major transit stop or high-quality transit corridor if all parcels within the project have no more than 25 percent of their area farther than one-half mile from the stop or corridor and if not more than 10 percent of the residential units or 100 units, whichever is less, in the project are farther than one-half mile from the stop or corridor.

CA Pub. Res. Code § 21064.3

"Major transit stop" means a site containing an existing rail transit station, a ferry terminal served by either a bus or rail transit service, or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods.

METHODOLOGY

For planning and SCS purposes, SCAG identifies a "high quality transit area" as generally a walkable transit village or corridor that is within one-half mile of a major transit stop or HQTC as defined in statute. SCAG's technical methodology for identifying HQTCs and major transit stops is based on input from the Regional Transit Technical Advisory Committee (RTTAC), as well as consultation with local agencies, other large MPOs in California, and the Governor's Office of Planning and Research. The methodology and assumptions are discussed below. This methodology may be periodically updated to incorporate revisions or clarifications. Questions should be directed to Steve Fox, at fox@scag.ca.gov, or Philip Law, at law@scag.ca.gov.

SCAG maps and data depicting HQTCs and major transit stops are intended for planning purposes only. SCAG shall incur no responsibility or liability as to the completeness, currentness, or accuracy of this information. SCAG assumes no responsibility arising from use of this information by individuals, businesses, or other public entities. The information is provided with no warranty of any kind, expressed or implied, including but not limited to the implied warranties of merchantability and fitness for a particular purpose.

Existing HQTCs and Major Transit Stops

SCAG updates its inventory of existing major transit stops and HQTCs with the adoption of a new Regional Transportation Plan (RTP) and SCS, once every four years. Data for the existing ("base year") condition for the RTP/SCS are typically obtained several years before plan adoption. The base year transit network for *Connect SoCal*, the 2020 RTP/SCS, is based primarily on data for 2016. This inventory of existing major transit stops and HQTCs is therefore only a snapshot in time as of 2016, and does not reflect the existing levels of transit service for any other timeframe.

<insert map of 2016 Base Year "existing" major transit stops and high quality transit corridors>

Transit agencies make adjustments to bus service on a regular basis. Therefore, given the limitations of the RTP/SCS base year transit network, local jurisdictions should consult with the appropriate transit provider(s) to obtain the latest information on existing transit routes, stop locations, and service intervals before making determinations regarding CEQA exemption or streamlining. It is the responsibility of the lead agency under CEQA to determine if a project meets statutory requirements.

Stop-Based Analysis

SCAG calculates peak commute bus service intervals at the stop level using schedule data published by transit agencies in the General Transit Feed Specification (GTFS) format (see for example,

www.transitfeeds.com). An HQTc therefore comprises or is determined by the qualifying stops on an individual bus route.

Peak Period Bus Service Interval (Frequency)

To determine whether the peak commute bus service interval (also called frequency) meets the statutory threshold of 15 minutes or less, SCAG uses the peak period defined in its regional travel demand model. The morning peak is defined as 6am to 9am and the afternoon peak is defined as 3pm to 7pm. A transit operator may have a different, board-adopted or de facto peak period; in such cases SCAG will accept requests to use operator-specific peak-hour periods on a case-by-case basis.

SCAG uses the total population of bus trips during the combined seven-hour morning and afternoon peak periods to determine the peak frequency at a bus stop. This is done for each bus route, by direction. The peak frequency is calculated by dividing 420 minutes (the seven-hour peak converted to minutes) by the total peak bus trips. This average frequency should be 15 minutes or less in order to qualify. The threshold is strict, at 15.0 minutes.

Directional Frequency

A bus route must only meet the 15-minute service interval threshold in one direction to qualify as an HQTc. This is based on RTTAC feedback that transit agencies often operate very peak-directional service or operate predominantly one-way service on a corridor.

Corridors with Multiple Overlapping Bus Routes

Separate but overlapping bus routes that do not individually meet the 15-minute threshold may not be combined in order to qualify as an HQTc. However, based on RTTAC feedback, there are certain corridors where overlapping “line families” or local/bus rapid transit (BRT) lines are intended to function as one bus route. On these corridors, transit riders typically board the first bus available, whether it be a local, express, or BRT line. For these line families or local/BRT corridors, SCAG uses the combined routes to calculate the frequency.

Route Alignment

The entire alignment of a bus route, based on the stops that meet the 15-minute peak frequency threshold, is considered an HQTc. This would include, for example, express bus services that operate along freeways where there are no stops along the freeway right-of-way.

Major Transit Stops and Intersecting Service Transfer Zones

As defined in statute, major transit stops include the intersection of two or more HQTcs. For purposes of transferring between intersecting service, SCAG uses a 500-foot buffer to determine a major transit stop. In other words, two intersecting HQTcs must have stops that are within 500 feet of each other to qualify as a major transit stop. A 500-foot buffer is assumed to be a reasonable limit to the distance that a transit

patron would walk to transfer between bus routes. It is also consistent with the Metro Transfers Design Guide definition of a transfer zone.

Amtrak Stations and Ferry Stations

Amtrak intercity passenger rail stations with only limited long-distance service are not automatically included as a major transit stop unless requested by a local agency. Similarly, ferry stations with seasonal and/or non-commuter based service (and that are served by bus or rail transit) are not automatically included as a major transit stop unless requested by a local agency.

Planned HQTCs and Major Transit Stops

Planned HQTCs and major transit stops are future improvements that are expected to be implemented by transit agencies by the RTP/SCS horizon year of 2045. These are assumed by definition to meet the statutory requirements of an HQTC or major transit stop. SCAG updates its inventory of planned major transit stops and HQTCs with the adoption of a new RTP/SCS, once every four years. However, transit planning studies may be completed by transit agencies on a more frequent basis than the RTP/SCS is updated by SCAG. Local jurisdictions should consult with the appropriate transit provider(s) to obtain the latest information on planned transit routes, stop locations, and service intervals/frequencies before making determinations regarding CEQA exemption or streamlining.

<insert map of planned (2045) major transit stops and high quality transit corridors>

5. EXHIBIT E

Materials supporting Contestation of Reduce Setback:

in case # DIR-2019-4920-TOC,

and based on anomalous granting of Reduced Front Yard setback.

Properties information for all R4-1XL Lots on Venice Blvd between Lincoln Blvd and Walgrove Avenue. The data want to highlight the fact that only **1 residential property with a building dated 1928** does not meet the required minimum setback of 15-feet.

The property at 1616 Venice Blvd. is adjacent to the new development and it is the only reason a reduced setback is granted to the Project.

The building on this one property is dated from 1928, it is a Multi-Dwelling 2-story building with 16 RSO Unit that cultural significance to the Venice Community, housing a large mural of Allen Sarlo, a professional Venice DogTown Surfer greatly admired by the Venice Surfer Community.

Lincoln Blvd to Walgrove Avenue Property Info - R4-1XL (High Medium Residential)

South

36 lots = 22 properties

Address	ASP	Year Built	# Units	# Bedroom	#Bath	RSO - Yes/No	Lot Size	SQFT	ZONE	Front Setback (approximate)	Front Setback from Sidewalk	Notes
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Lincoln Blvd.

1140	4236001021	1986	40	33	40	No	18,435	30,355	R4-1XL	14'-9"	16'-9"	
1146	4236001014	1952	4	4	4	Yes	4,489	3,116	R4-1XL	15'-0"	17'-0"	
1150	4236001015	1952	4	4	4	Yes	4,442	3,116	R4-1XL	15'-0"	17'-0"	

Penmar Avenue

1202	4236003001	1952	4	6	4	Yes	4,248	5,986	R4-1XL	14'-9"	16'-9"	
1208	4236003019	1988	32	52	52	No	29,080	12,214	R4-1XL	15'-9"	17'-9"	
1216	4236003018	1990	48	48	48	No	36,618	18,242	R4-1XL	15'-0"	17'-0"	

Walnut Avenue

2400 Walnut	4236003015	1988	9	13	13	No	7,260	5,867	R4-1XL	15'-3"	17'-3"	
1306	4236003009	1949	1	2	1	No	930	5,956	R4-1XL	33'-3"	35'-3"	Assessor Info do not match what is built
1310	4236003010	1968	17	18	18	Yes	9,328	5,956	R4-1XL	33'-3"	35'-3"	
1316	4236003011	1966	7	8	8	Yes	5,184	5,954	R4-1XL	14'-9"	16'-9"	
1320	4236003017	1987	31	52	51	No	25,500	11,916	R4-1XL	15'-0"	17'-0"	

Glyndon Avenue

1400	4236006026	1988	31	37	48	No	25,560	11,822	R4-1XL	14'-6"	16'-6"	
1410	4236006003	1969	7	9	9	Yes	6,103	5,956	R4-1XL	17'-3"	19'-3"	
1420	4236006022	1989	31	41	41	No	27,539	11,913	R4-1XL	15'-0"	17'-0"	
1426	4236006006	1950	1	2	1	Yes	5,856	1,169	R4-1XL	15'-0"	17'-0"	Assessor Info do not match what is built

Louella Avenue

1500	4236006021	1985	9	11	14	No	7,512	5,850	R4-1XL	15'-0"	17'-0"	
1506	4236006027	1988	47	50	58	No	37,630	17,885	R4-1XL	15'-0"	17'-0"	
1520	4236006011	1963	6	15	6	Yes	6,208	5,962	R4-1XL	15'-3"	17'-3"	
2407 Glencoe	4236006012	1921	1	1	1	Yes	512	5,963		40'-0"	42'-0"	Assessor Info do not match what is built
	4236006012	1929	1	?	?	Yes	1,527				Back House	2 houses on the lot

Glencoe Avenue

1600	4236006013	1947	3	6	3	yes	2,184	5,957		survey (no info)	demo	Proposed Development
1604	4236006014	1946	4	4	4	Yes	2,016	5,956		14'-8" (survey)	demo	Proposed Development
1610	4236006015	1914	2	3	2	Yes	1,240	5,956		19'-0" (survey)	demo	Proposed Development
1616	4236006016	1928	16	18	16	Yes	8,685	5,959		5'-0" (survey)	7'-0"	
1630	4236006025	1988	31	46	58	No	24,106	11,831		15'-0"	17'-0"	

Walgrove Avenue

Lincoln Blvd to Walgrove Avenue Property Info - R4-1XL (High Medium Residential)

North

36 lots = 25 properties

Address	ASP	Year Built	# Units	# Bedroom	#Bath	RSO - Yes/No	Lot Size	SQFT	ZONE	Front Setback (approximate)	Front Setback from Sidewalk	Notes
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Lincoln Blvd.

1015	424200128		56	56	56	No	13,063	36,566		14'-7"		New TOC Project
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Superior

1107	4242002038	2009	38	40	40	No	33,714	23,568		15'-7"	15'-7"	
1117	4242002020	1947	2	2	2	Yes	1,402	7,542		15'-3"	15'-3"	
1127	4242002036	1955	8	10	8	Yes	5,957	8,302		16'-0"	16'-0"	

Penmar Avenue

2338 Penmar	4242030001	1956	8	10	8	Yes	5,857	6,145		16'-0"	16'-0"	
1201	4242030002	1956	8	10	8	Yes	5,857	6,109		15'-0"	15'-0"	
1211	4242030023	1970	18	18	18	Yes	9,276	12,217		34'-3"	34'-3"	
1221	4242030025	1986	12	21	20	No	10,558	7,709		16'-3"	16'-3"	

Walnut Avenue

1301	4242031001	1963	9	14	9	Yes	5,736	5,958		16'-3"	16'-3"	
1305	4242031002	1652	4	4	4	Yes	2,368	5,956		17'-6"	17'-6"	
1311	4242031023	1963	16	22	18	Yes	9,004	11,911		16'-3"	16'-3"	
1321	4242031005	1925					2,800	11,912		0'-0"	7'-0"	Commercial Space

Glyndon Avenue

1401	4242032001	1968	8	9	8	Yes	5,708	5,927		15'-3"	15'-3"	
1407	4242032002	2013	9	9	14	No	6,610	5,957		15'-0"	15'-0"	
1409	4242032023	1989	29	43	43	No	27,700	11,916		14'-9"	14'-9"	
1421	4242032005	1957	20	24	19	Yes	11,303	11,917		18'-0"	18'-0"	

Louella Avenue

1501	4242033001		31			No		11,896		15'-9"		New TOC Project- Former Curch
1511	4242033002	1941	7	7	7	Yes	4,032	5,959		14'-3"	14'-3"	
1517	4242033003	1954	3	2	5	Yes	1,782	4,007		16'-0"	16'-0"	
1519	4242033004	1932	1	2	1	No	600	1,950		N/A	N/A	Single Family?
1523	4242033005	1925	1			No	5,400	7,608		2'-0"	9'-0"	Commercial

Glencoe Avenue

1607	4242034031	1990	30	44	44	No	26,208	11,877		17'-9"	17'-9"	
1611	4242034003	1963	7	12	7	Yes	5,384	5,958		16'-0"	16'-0"	
1619	4242034004	1907	1	3	2	No	2,297	5,958		34'-0"	34'-0"	Single Family
1621	4242034032	1990	28	39	39	No	23,328	11,112		16'-3"	16'-3"	

Walgrove Avenue



APPLICATIONS:

APPEAL APPLICATION

Instructions and Checklist

Related Code Section: Refer to the City Planning case determination to identify the Zone Code section for the entitlement and the appeal procedure.

Purpose: This application is for the appeal of Department of City Planning determinations authorized by the Los Angeles Municipal Code (LAMC).

A. APPELLATE BODY/CASE INFORMATION

1. APPELLATE BODY

- ☒ Area Planning Commission ☐ City Planning Commission ☐ City Council ☐ Director of Planning
☐ Zoning Administrator

Regarding Case Number: DIR-2019-4920-TOC

Project Address: 1600-1614 East Venice Boulevard

Final Date to Appeal: 05/05/2020

2. APPELLANT

Appellant Identity:
(check all that apply)

- ☐ Representative ☐ Property Owner
☐ Applicant ☐ Operator of the Use/Site

☐ Person, other than the Applicant, Owner or Operator claiming to be aggrieved

☐ Person affected by the determination made by the **Department of Building and Safety**

- ☐ Representative ☐ Owner ☒ Aggrieved Party
☐ Applicant ☐ Operator

3. APPELLANT INFORMATION

Appellant's Name: Allen Sarlo

Company/Organization: _____

Mailing Address: 124 Washington Street

City: Marina del Rey State: CA Zip: 90292

Telephone: 310 714 5499 E-mail: Allensarlo@gmail.com

a. Is the appeal being filed on your behalf or on behalf of another party, organization or company?

☒ Self ☐ Other: _____

b. Is the appeal being filed to support the original applicant's position? ☐ Yes ☒ No

4. REPRESENTATIVE/AGENT INFORMATION

Representative/Agent name (if applicable): Allen Sarlo

Company: _____

Mailing Address: 124 Washington Street

City: Marina del Rey State: CA Zip: 90929

Telephone: 310 714 5499 E-mail: Allensarlo@gmail.com

5. JUSTIFICATION/REASON FOR APPEAL

a. Is the entire decision, or only parts of it being appealed? ☐ Entire ☒ Part

b. Are specific conditions of approval being appealed? ☒ Yes ☐ No

If Yes, list the condition number(s) here: Item #7 Parking requirement reduction

Attach a separate sheet providing your reasons for the appeal. Your reason must state:

- ☐ The reason for the appeal ☒ How you are aggrieved by the decision
☐ Specifically the points at issue ☐ Why you believe the decision-maker erred or abused their discretion

6. APPLICANT'S AFFIDAVIT

I certify that the statements contained in this application are complete and true:

Appellant Signature: Allen Sarlo Date: May 1, 2020
Allen Sarlo (May 1, 2020)

GENERAL APPEAL FILING REQUIREMENTS

B. ALL CASES REQUIRE THE FOLLOWING ITEMS - SEE THE ADDITIONAL INSTRUCTIONS FOR SPECIFIC CASE TYPES

1. Appeal Documents

a. **Three (3) sets** - The following documents are required for each appeal filed (1 original and 2 duplicates)
Each case being appealed is required to provide three (3) sets of the listed documents.

- ☒ Appeal Application (form CP-7769)
☒ Justification/Reason for Appeal
☒ Copies of Original Determination Letter

b. Electronic Copy

- ☐ Provide an electronic copy of your appeal documents on a flash drive (planning staff will upload materials during filing and return the flash drive to you) or a CD (which will remain in the file). The following items must be saved as individual PDFs and labeled accordingly (e.g. "Appeal Form.pdf", "Justification/Reason Statement.pdf", or "Original Determination Letter.pdf" etc.). No file should exceed 9.8 MB in size.

c. Appeal Fee

- ☐ Original Applicant - A fee equal to 85% of the original application fee, provide a copy of the original application receipt(s) to calculate the fee per LAMC Section 19.01B 1.
☐ Aggrieved Party - The fee charged shall be in accordance with the LAMC Section 19.01B 1.

d. Notice Requirement

- ☐ Mailing List - All appeals require noticing per the applicable LAMC section(s). Original Applicants must provide noticing per the LAMC
☐ Mailing Fee - The appeal notice mailing fee is paid by the project applicant, payment is made to the City Planning's mailing contractor (BTC), a copy of the receipt must be submitted as proof of payment.

SPECIFIC CASE TYPES - APPEAL FILING INFORMATION

C. DENSITY BONUS / TRANSIT ORIENTED COMMUNITES (TOC)

1. Density Bonus/TOC

Appeal procedures for Density Bonus/TOC per LAMC Section 12.22.A 25 (g) f.

NOTE:

- Density Bonus/TOC cases, only the *on menu or additional incentives* items can be appealed.
- Appeals of Density Bonus/TOC cases can only be filed by adjacent owners or tenants (must have documentation), and always only appealable to the Citywide Planning Commission.
- ☒ Provide documentation to confirm adjacent owner or tenant status, i.e., a lease agreement, rent receipt, utility bill, property tax bill, ZIMAS, drivers license, bill statement etc.

D. WAIVER OF DEDICATION AND OR IMPROVEMENT

Appeal procedure for Waiver of Dedication or Improvement per LAMC Section 12.37 I.

NOTE:

- Waivers for By-Right Projects, can only be appealed by the owner.
- When a Waiver is on appeal and is part of a master land use application request or subdivider's statement for a project, the applicant may appeal pursuant to the procedures that governs the entitlement.

E. TENTATIVE TRACT/VESTING

1. Tentative Tract/Vesting - Appeal procedure for Tentative Tract / Vesting application per LAMC Section 17.54 A.

NOTE: Appeals to the City Council from a determination on a Tentative Tract (TT or VTT) by the Area or City Planning Commission must be filed within 10 days of the date of the written determination of said Commission.

- ☐ Provide a copy of the written determination letter from Commission.

F. BUILDING AND SAFETY DETERMINATION

- ☐ **1. Appeal of the Department of Building and Safety determination**, per LAMC 12.26 K 1, an appellant is considered the **Original Applicant** and must provide noticing and pay mailing fees.

a. Appeal Fee

- ☐ Original Applicant - The fee charged shall be in accordance with LAMC Section 19.01B 2, as stated in the Building and Safety determination letter, plus all surcharges. (the fee specified in Table 4-A, Section 98.0403.2 of the City of Los Angeles Building Code)

b. Notice Requirement

- ☐ Mailing Fee - The applicant must pay mailing fees to City Planning's mailing contractor (BTC) and submit a copy of receipt as proof of payment.

- ☒ **2. Appeal of the Director of City Planning determination** per LAMC Section 12.26 K 6, an applicant or any other aggrieved person may file an appeal, and is appealable to the Area Planning Commission or Citywide Planning Commission as noted in the determination.

a. Appeal Fee

- ☒ Original Applicant - The fee charged shall be in accordance with the LAMC Section 19.01 B 1 a.

b. Notice Requirement

- ☐ Mailing List - The appeal notification requirements per LAMC Section 12.26 K 7 apply.
- ☐ Mailing Fees - The appeal notice mailing fee is made to City Planning's mailing contractor (BTC), a copy of receipt must be submitted as proof of payment.

G. NUISANCE ABATEMENT

1. Nuisance Abatement - Appeal procedure for Nuisance Abatement per LAMC Section 12.27.1 C 4

NOTE:

- Nuisance Abatement is only appealable to the City Council.

a. Appeal Fee

- ☐ Aggrieved Party the fee charged shall be in accordance with the LAMC Section 19.01 B 1.

2. Plan Approval/Compliance Review

Appeal procedure for Nuisance Abatement Plan Approval/Compliance Review per LAMC Section 12.27.1 C 4.

a. Appeal Fee

- ☐ Compliance Review - The fee charged shall be in accordance with the LAMC Section 19.01 B.
- ☐ Modification - The fee shall be in accordance with the LAMC Section 19.01 B.

NOTES

A Certified Neighborhood Council (CNC) or a person identified as a member of a CNC or as representing the CNC may not file an appeal on behalf of the Neighborhood Council; persons affiliated with a CNC may only file as an individual on behalf of self.

Please note that the appellate body must act on your appeal within a time period specified in the Section(s) of the Los Angeles Municipal Code (LAMC) pertaining to the type of appeal being filed. The Department of City Planning will make its best efforts to have appeals scheduled prior to the appellate body's last day to act in order to provide due process to the appellant. If the appellate body is unable to come to a consensus or is unable to hear and consider the appeal prior to the last day to act, the appeal is automatically deemed denied, and the original decision will stand. The last day to act as defined in the LAMC may only be extended if formally agreed upon by the applicant.

This Section for City Planning Staff Use Only		
Base Fee:	Reviewed & Accepted by (DSC Planner):	Date:
Receipt No:	Deemed Complete by (Project Planner):	Date:
<input type="checkbox"/> Determination authority notified		<input type="checkbox"/> Original receipt and BTC receipt (if original applicant)

Case-NO: DIR-2019-4920-TOC

To Whom This Concerns:

My name is Allen Sarlo. and I own the 16 unit apartment building (all singles, built 1920) at 1616 E Venice Boulevard, directly adjacent to the proposed project (DIR-2019-5920-TOC) I am against the number of dwellings verses the amount of parking space that the builder is proposing. 77 Dwellings with only 48 parking spaces available to the tenants is unrealistic and dangerous to the existing neighborhood community. My property was built in the 1920's and has no parking, Venice High is located in the next block and burdens the neighborhood daily with their parking needs. There is a single family residential neighborhood that abuts the proposed project which is already heavy textured by the High School and other existing apartment buildings located on Venice Boulevard with inadequate parking.

Thank you for taking the time to read over my concerns and address them.

Your Truly

Allen Sarlo

310 714 5499

EXHIBIT B

**Director of Planning's Letter of Determination
(DIR-2019-4920-TOC)**

DIR-2019-4920-TOC-1A

**DEPARTMENT OF
CITY PLANNING**

COMMISSION OFFICE
(213) 978-1300

CITY PLANNING COMMISSION

SAMANTHA MILLMAN
PRESIDENT

VAHID KHORSAND
VICE-PRESIDENT

DAVID H. J. AMBROZ

CAROLINE CHOE

HELEN LEUNG

KAREN MACK

MARC MITCHELL

VERONICA PADILLA-CAMPOS

DANA M. PERLMAN

**CITY OF LOS ANGELES
CALIFORNIA**



ERIC GARCETTI
MAYOR

EXECUTIVE OFFICES

200 N. SPRING STREET, ROOM 525
LOS ANGELES, CA 90012-4801
(213) 978-1271

VINCENT P. BERTONI, AICP
DIRECTOR

KEVIN J. KELLER, AICP
EXECUTIVE OFFICER

SHANA M.M. BONSTIN
DEPUTY DIRECTOR

TRICIA KEANE
DEPUTY DIRECTOR

ARTHI L. VARMA, AICP
DEPUTY DIRECTOR

LISA M. WEBBER, AICP
DEPUTY DIRECTOR

DIRECTOR'S DETERMINATION

April 20, 2020

Applicant/Owner

Venice Wave LP
1600 Venice LLC
11601 Santa Monica Boulevard
Los Angeles, CA 90025

Representatives

Don Toletino
Venice Wave LP
11601 Santa Monica Boulevard
Los Angeles, CA 90025

Case No.: DIR-2019-4920-TOC

CEQA: ENV-2019-4921-CE

Related Cases: PAR-2019-3781-TOC

Location: 1600-1614 East Venice
Boulevard

Council District: 11 – Bonin

Neighborhood Council: Venice

Community Plan Area: Venice

Land Use Designation: High Medium Residential

Zone: [Q]R4-1XL

Legal Description: Lots 1, 2, & 3, Block 10,
Walgrove Tract

Last Day to File an Appeal: May 5, 2020

Pursuant to the Los Angeles Municipal Code (LAMC) Section 12.22 A.31, as the designee of the Director of Planning, I hereby:

Determine, based on the whole of the administrative record, that the project is exempt from CEQA pursuant to CEQA Guidelines, Section 15332 (Class 32 Urban In-Fill Development), and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies; and

Approve with Conditions a Transit Oriented Communities (TOC) Affordable Housing Incentive Program Compliance Review for a qualifying Tier 2 project, totaling 77 dwelling units, reserving 7 units for Extremely Low Income Household occupancy for a period of 55 years, with the following Base and Additional Incentives:

Base Incentives

- a. **Residential Density.** A 60 percent increase in the maximum density to permit a total of 77 dwelling units, in lieu of 48 units as otherwise permitted by the R4 base density; and
- b. **Floor Area Ratio (FAR).** A maximum FAR of 4.35:1, in lieu of the 3:1 as otherwise permitted by LAMC Section 12.21.1 A.1; and

- c. **Parking.** Reduced residential parking requirements to permit 0.5 parking spaces per bedroom; and

Additional Incentives

- d. **Yards/Setback.** A reduced front yard setback of 5 feet, in lieu of the 15 feet otherwise required by LAMC Section 12.11; and
- e. **Height.** An increase of 11 feet to allow a maximum height of 41 feet, in lieu of the 30 feet otherwise permitted by LAMC Section 12.21.1 A.1.

The project approval is based upon the attached Findings, and subject to the attached Conditions of Approval:

CONDITIONS OF APPROVAL

Transit Oriented Communities Conditions

1. **Site Development.** Except as modified herein, the project shall be in substantial conformance with the plans and materials submitted by the Applicant, stamped "Exhibit A," and attached to the subject case file. No change to the plans will be made without prior review by the Department of City Planning, and written approval by the Director of Planning. Each change shall be identified and justified in writing. Minor deviations may be allowed in order to comply with the provisions of the Municipal Code or the project conditions.
2. **Residential Density.** The project shall be limited to a maximum density of 77 residential dwelling units.
3. **Affordable Units.** A minimum of seven (7) dwelling units, or 9 percent of the 77 total units, shall be reserved for Extremely Low Income households as defined in Section 50106 of the California Health and Safety Code. The Transit Oriented Communities Affordable Housing Incentive Program Guidelines also requires a Housing Development to meet any applicable housing replacement requirements of California Government Code Section 65915(c)(3), as verified by the Department of Housing and Community Investment (HCIDLA) prior to the issuance of any building permit. Replacement housing units required per this section may also count towards other On-Site Restricted Affordable Units requirements.
4. **Changes in Restricted Units.** Deviations that increase the number of restricted affordable units, or that change the composition of units or parking numbers, shall be consistent with LAMC Section 12.22 A.31 and comply with the Transit Oriented Communities Affordable Housing Incentive Program Guidelines adopted by the City Planning Commission.
5. **Housing Requirements.** Prior to issuance of a building permit, the owner shall execute and record a covenant and agreement running with the land to the satisfaction of the Los Angeles Housing and Community Investment Department (HCIDLA). The covenant shall bind the owner to reserve seven (7) units available to Extremely Low Income Households for rental as determined to be affordable to such households by HCIDLA for a period of 55 years. Enforcement of the terms of said covenant shall be the responsibility of HCIDLA. The applicant will present a copy of the recorded covenant to the Department of City Planning for inclusion in this file. The project shall comply with the TOC Guidelines and any monitoring requirements established by the HCIDLA. Refer to the TOC Affordable Housing Incentive Program and Housing Replacement (AB 2556 Determination) Background sections of this determination.
6. **Floor Area Ratio (FAR).** The project shall be limited to a maximum FAR as shown in "Exhibit A".
7. **Automobile Parking.** The project qualifies for reduced parking requirements in accordance with LAMC 12.22 A.31 and the TOC Guidelines as a mixed-income project within one half mile of a major transit stop to which the project has unobstructed access. Under AB 744, parking requirements are based upon the number and type of dwelling units proposed. The project requires a minimum of 43 residential parking spaces for the 77 proposed dwelling units containing 85 bedrooms, based on a rate of 0.5 parking spaces per bedroom. Forty-three (43) parking spaces are proposed onsite for the proposed residential use, all located in the subterranean parking level.

8. **Adjustment of Parking.** In the event that the number of Restricted Affordable Units should increase, or the composition of such units should change (i.e. the number of bedrooms, or the number of units made available to Senior Citizens and/or Disabled Persons), or the applicant selects another Parking Option (including Bicycle Parking Ordinance) and no other Condition of Approval or incentive is affected, then no modification of this determination shall be necessary, and the number of parking spaces shall be recalculated by the Department of Building and Safety, based upon the ratios set forth ratios set forth in the Transit Oriented Communities Affordable Housing Incentive Program Guidelines (TOC Guidelines).
9. **Bicycle Parking.** Bicycle parking shall be provided consistent with LAMC Section 12.21 A.16.
10. **Additional Incentives.** In accordance with LAMC Section 12.22 A.31 and the TOC Guidelines, the following Additional Incentives (Tier 2) are requested:
 - a. **Yards/Setback.** The project shall be permitted a reduced front yard setback of 5 feet.
 - b. **Height.** The project shall be permitted an increase of 11 feet in building height, equal to a maximum building height of 41 feet.

[Q] Conditions (Ordinance No. 169,327)

11. **Tenants.** Prior to the issuance of a building permit or demolition permit, all tenants of an existing multi-unit residential building located on a lot where a project will be located shall be given a minimum 60-day written notice of termination of tenancy.

Administrative Conditions

12. **Final Plans.** Prior to the issuance of any building permits for the project by the Department of Building and Safety, the applicant shall submit all final construction plans that are awaiting issuance of a building permit by the Department of Building and Safety for final review and approval by the Department of City Planning. All plans that are awaiting issuance of a building permit by the Department of Building and Safety shall be stamped by Department of City Planning staff "Final Plans". A copy of the Final Plans, supplied by the applicant, shall be retained in the subject case file.
13. **Notations on Plans.** Plans submitted to the Department of Building and Safety, for the purpose of processing a building permit application shall include all of the Conditions of Approval herein attached as a cover sheet, and shall include any modifications or notations required herein.
14. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review of approval, plans, etc., as may be required by the subject conditions, shall be provided to the Department of City Planning prior to clearance of any building permits, for placement in the subject file.
15. **Code Compliance.** Use, area, height, and yard regulations of the zone classification of the subject property shall be complied with, except where granted conditions differ herein.
16. **Department of Building and Safety.** The granting of this determination by the Director of Planning does not in any way indicate full compliance with applicable provisions of the

Los Angeles Municipal Code Chapter IX (Building Code). Any corrections and/or modifications to plans made subsequent to this determination by a Department of Building and Safety Plan Check Engineer that affect any part of the exterior design or appearance of the project as approved by the Director, and which are deemed necessary by the Department of Building and Safety for Building Code compliance, shall require a referral of the revised plans back to the Department of City Planning for additional review and sign-off prior to the issuance of any permit in connection with those plans.

17. **Condition Compliance.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning.
18. Prior to the issuance of any permits relative to this matter, a covenant acknowledging and agreeing to comply with all the terms and conditions established herein shall be recorded in the County Recorder's Office. The agreement (Master Covenant and Agreement Form CP-6770) shall run with the land and shall be binding on any subsequent owners, heirs or assigns. The agreement with the conditions attached must be submitted to the Development Services Center for approval before being recorded. After recordation, a certified copy bearing the Recorder's number and date shall be provided to the Zoning Administrator for attachment to the subject case file
19. **Expiration.** In the event that this grant is not utilized within three years of its effective date (the day following the last day an appeal may be filed), the grant shall be considered null and void. Issuance of a building permit, and the initiation of, and diligent continuation of, construction activity shall constitute utilization for the purposes of this grant.
20. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assign. The agreement must be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the file.
21. **Indemnification and Reimbursement of Litigation Costs.**

Applicant shall do all of the following:

- (i) Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- (ii) Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
- (iii) Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial

deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).

- (iv) Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- (v) If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions includes actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

BACKGROUND

The project site consists of three (3) contiguous lots located at 1600-1614 East Venice Boulevard, within the Venice Community Plan. The subject lots are zoned [Q]R4-1XL with a General Plan land use designation of High Medium Residential. The site is located within the Los Angeles Coastal Transportation Corridor Specific Plan area and the Livable Boulevards Streetscape Plan area. Pursuant to Ordinance No. 169,327, the project is also subject to [Q] Qualified Permanent Conditions of Approval ([Q] conditions). Venice High School is located approximately 200 feet to the northeast.

The site is rectangular-shaped and flat, with a lot a total lot area of 17,891 square feet, to be reduced to 17,443 square feet after street dedications. The site occupies the southeasterly corner of Venice Boulevard and Glencoe Avenue, with a 138-foot frontage along Venice Boulevard and a 130-foot frontage along Glencoe Avenue. Pursuant to LAMC Section 12.37, the Bureau of Engineering requires the applicant to provide various street improvements along with the following street dedications: a 20-foot corner radius dedication at the corner of Venice Boulevard and Glencoe Avenue and a 2.5-foot dedication along the alley.

The subject property is located in an area with a mix of single- and multi-family dwellings. A multi-family residence abuts the property to the northeast. To the south of the property is a single-family neighborhood, zoned R1V2. Immediately across Venice Boulevard is a multi-family structure, and immediately across Glencoe Avenue is a duplex. The site is bound to the northwest by Venice Boulevard, to the southwest by Glencoe Avenue, and to the southeast by an alleyway.

At present, the property is developed with a 2-unit apartment, a 3-unit apartment, and a 4-unit apartment. According to a determination issued by the Los Angeles Housing and Community Investment Department (HCIDLA) on June 11, 2019, nine (9) units are subject to replacement under AB 2556 (formerly AB 2222).

The project proposes the demolition of three existing residential structures, and the construction of a 41-foot tall, four-story, 49,948 square-foot, multi-family structure over one level of subterranean parking, containing a total of 77 dwelling units, reserving 7 dwelling units (9 percent of the total units) for Extreme Low Income households for 55 years. The project proposes a total of 43 vehicular parking spaces and 82 bicycle parking spaces (72 long-term, 10 short-term). The project will grade and export 8,800 cubic yards of earth. Access to the proposed subterranean parking level will be located along the rear alleyway. Two pedestrian entrances are proposed: one at the rear alley and one on the corner of Venice Boulevard and Glencoe Avenue.

Pursuant to LAMC Section 12.22 A.31 and the TOC Guidelines, the applicant requests a Transit Oriented Communities Compliance Review. The subject property is located within a Tier 2 TOC Affordable Housing Incentive Area, qualified by its proximity to the intersection of two Major Transit Stops, Venice Boulevard and Lincoln Boulevard. Specifically, the site is within 1,500 feet from Metro Bus Line 33 and Rapid Line 733 and Santa Monica Rapid Line 3. As such, the project is located on a site that qualifies for the TOC Tier 2 Incentives.

The project is eligible for Base Incentives and three Additional Incentives. The Base Incentives include (1) a 60 percent increase in the maximum dwelling units allowed, (2) a 45 percent increase in FAR, and (3) reduced parking requirements to permit 0.5 parking spaces per bedroom. The applicant also requests two Additional Incentive: a reduced front yard setback of 5 feet along Venice Boulevard, and an 11-foot increase in the maximum allowable height, equal to a maximum building height of 41 feet.

For the purpose of calculating density, the property has a total lot area of 18,922 square feet. The R4 Zone allows residential projects to utilize a density of 400 square feet per dwelling unit. Although the subject property is permitted 47 dwelling units by right, the base density for this project is 48 units, rounded up pursuant to the TOC Guidelines. The project is located in a Tier 2 Incentive Area and therefore is eligible for a 60 percent increase in allowable density so long as 9 percent of the total units are set aside for Extremely Low Income households. With an increase of 60 percent, the maximum allowable density for the subject project is 77 dwelling units. The project proposes 77 units, of which 7 units will be set aside for Extremely Low Income households. The proposed building contains a total of 49,948 square feet of floor area, subject to a buildable area of 13,724 square feet; the resulting Floor Area Ratio (FAR) is 3.64 to 1.

HOUSING REPLACEMENT (AB 2556 DETERMINATION) BACKGROUND

On September 27, 2014, Governor Jerry Brown signed Assembly Bill (AB) 2222, as amended by AB 2556 on August 19, 2016, to amend sections of California's Density Bonus Law (Government Code Section 65915). AB 2556 requires applicants of Density Bonus projects filed as of January 1, 2015 to demonstrate compliance with the housing replacement provisions which require replacement of rental dwelling units that either exist at the time of application of a Density Bonus project, or have been vacated or demolished in the five-year period preceding the application of the project. This applies to all pre-existing units that have been subject to a recorded covenant, ordinance, or law that restricts rents to levels affordable to persons and families of lower or very low income; subject to any other form of rent or price control; or occupied by Low or Very Low Income Households.

Pursuant to the AB 2556 (TOC) Determination Letter dated June 11, 2019 and prepared by the Los Angeles Housing and Community Investment Department (HCIDLA), nine (9) units are subject to replacement under AB 2556 (formerly AB 2222). Pursuant to the HUD Comprehensive Housing Affordability Strategy (CHAS) database, seven (7) units need to be replaced with equivalent type affordable units. However, two (2) remaining units presumed to have been occupied by an above-lower income person or household, as permitted by California Government Code Section 65915(c)(3)(C)(ii), the City has opted to require that those units be replaced in compliance with the City's Rent Stabilization Ordinance (RSO).

TRANSIT ORIENTED COMMUNITIES AFFORDABLE HOUSING INCENTIVE PROGRAM BACKGROUND

Measure JJJ was adopted by the Los Angeles City Council on December 13, 2016. Section 6 of the Measure instructed the Department of City Planning to create the Transit Oriented Communities (TOC) Affordable Housing Incentive Program. The measure required that the Department adopt a set of TOC Guidelines, which establishes incentives for residential and mixed-use projects located within ½ mile of a major transit stop. Major transit stops are defined under existing State law.

The TOC Affordable Housing Incentive Program Guidelines (TOC Guidelines), released on September 22, 2017, establish a tier-based system with varying development bonuses and incentives based on a project's distance from different types of transit. The largest bonuses are reserved for those areas in the closest proximity to significant rail stops or the intersection of major bus rapid transit lines. Required affordability levels are increased incrementally in each higher tier. The incentives provided in the TOC Guidelines describe the range of bonuses from particular zoning standards that applicants may select.

The project site is located less than 1,500 feet from Venice Boulevard and Glencoe Avenue, the intersection of a Santa Monica Rapid Bus line 3, with a service interval of approximately 13.12

minutes, and regular Metro Bus line 33 and Rapid line 733, with a service interval of approximately 12.7 minutes. As such, the site qualifies as a Tier 2 TOC Affordable Housing Incentive Area.

TRANSIT ORIENTED COMMUNITIES AFFORDABLE HOUSING INCENTIVE PROGRAM ELIGIBILITY REQUIREMENTS

To be an eligible TOC Housing Development, a project must meet the Eligibility criteria set forth in Section IV of the TOC Guidelines. A Housing Development located within a TOC Affordable Housing Incentive Area shall be eligible for TOC Incentives if it meets all of the following requirements, which it does:

1. ***On-Site Restricted Affordable Units.*** *In each Tier, a Housing Development shall provide On-Site Restricted Affordable Units at a rate of at least the minimum percentages described below. The minimum number of On-Site Restricted Affordable Units shall be calculated based upon the total number of units in the final project.*
 - a. *Tier 1 - 8% of the total number of dwelling units shall be affordable to Extremely Low Income (ELI) Households, 11% of the total number of dwelling units shall be affordable to Very Low (VL) Income Households, or 20% of the total number of dwelling units shall be affordable to Lower Income Households.*
 - b. *Tier 2 - 9% ELI, 12% VL or 21% Lower.*
 - c. *Tier 3 - 10% ELI, 14% VL or 23% Lower.*
 - d. *Tier 4 - 11% ELI, 15% VL or 25% Lower.*

The project site is located within Tier 2. Tier 2 requires at least 9 percent of the 77 total units, or 7 units, to be set aside for Extremely Low Income Households. The project reserves 7 units for Extremely Low Income Households and, as such, the project meets the eligibility requirement for On-Site Restricted Affordable Units.

2. ***Major Transit Stop.*** *A Housing Development shall be located on a lot, any portion of which must be located within 2,640 feet of a Major Transit Stop, as defined in Section II and according to the procedures in Section III.2 of the TOC Guidelines.*

A Major Transit Stop is a site containing a rail station or the intersection of two or more bus routes with a service interval of 15 minutes or less during the morning and afternoon peak commute periods. The project site is located less than 1,500 feet from the intersection of a Santa Monica Rapid Bus line 3 with a service interval of approximately 13.12 minutes and regular Metro Bus line 33 and Rapid line 733 with a service interval of approximately 12.7 minutes. Therefore, the project meets the eligibility requirement for proximity to a Major Transit Stop.

3. ***Housing Replacement.*** *A Housing Development must meet any applicable housing replacement requirements of California Government Code Section 65915(c)(3), as verified by the Department of Housing and Community Investment (HCIDLA) prior to the issuance of any building permit. Replacement housing units required per this section may also count towards other On-Site Restricted Affordable Units requirements.*

A determination dated June 11, 2019 from the Los Angeles Housing and Community Investment Department (HCIDLA) states that nine (9) dwelling units are subject to replacement under Assembly Bill (AB) 2556. Pursuant to HCIDLA's AB 2556 Determination, seven (7) units shall be replaced with equivalent bedroom type, with three (3) units restricted to Extremely Low Income Households, two (2) units restricted to Very Low Income Households, and two (2) units to Low Income Households. For the two (2) remaining units presume to have been occupied by an above-low income person or

household, as permitted by California Government Code Section 65915(c)(3)(C)(ii), the City has opted to require that those units be replaced in compliance with the City's Rent Stabilization Ordinance (RSO).

4. ***Other Density or Development Bonus Provisions.*** *A Housing Development shall not seek and receive a density or development bonus under the provisions of California Government Code Section 65915 (State Density Bonus law) or any other State or local program that provides development bonuses. This includes any development bonus or other incentive granting additional residential units or floor area provided through a General Plan Amendment, Zone Change, Height District Change, or any affordable housing development bonus in a Transit Neighborhood Plan, Community Plan Implementation Overlay (CPIO), Specific Plan, or overlay district.*

The project is not seeking any additional density or development bonuses under the provisions of the State Density Bonus Law or any other State or local program that provides development bonuses, including, but not limited to a General Plan Amendment, Zone Change, Height District Change, or any affordable housing development bonus in a Transit Neighborhood Plan, Community Implementation Overlay (CPIO), Specific Plan, or overlay district. As such, the project meets this eligibility requirement.

5. ***Base Incentives and Additional Incentives.*** *All Eligible Housing Developments are eligible to receive the Base Incentives listed in Section VI of the TOC Guidelines. Up to three Additional Incentives listed in Section VII of the TOC Guidelines may be granted based upon the affordability requirements described below. For the purposes of this section below, "base units" refers to the maximum allowable density allowed by the zoning, prior to any density increase provided through these Guidelines. The affordable housing units required per this section may also count towards the On-Site Restricted Affordable Units requirement in the Eligibility Requirement No. 1 above (except Moderate Income units).*

- a. *One Additional Incentive may be granted for projects that include at least 4% of the base units for Extremely Low Income Households, at least 5% of the base units for Very Low Income Households, at least 10% of the base units for Lower Income Households, or at least 10% of the base units for persons and families of Moderate Income in a common interest development.*
- b. *Two Additional Incentives may be granted for projects that include at least 7% of the base units for Extremely Low Income Households, at least 10% of the base units for Very Low Income Households, at least 20% of the base units for Lower Income Households, or at least 20% of the base units for persons and families of Moderate Income in a common interest development.*
- c. *Three Additional Incentives may be granted for projects that include at least 11% of the base units for Extremely Low Income Households, at least 15% of the base units for Very Low Income Households, at least 30% of the base units for Lower Income Households, or at least 30% of the base units for persons and families of Moderate Income in a common interest development.*

The project is eligible for two Additional Incentives. The applicant elects to utilize two Additional Incentives: a reduced front yard setback of 5 feet along Venice Boulevard, and an 11-foot increase in the maximum allowable height, equal to a maximum building height of 41 feet. At least 7 percent shall be set aside for Extremely Low Income Households; the project sets aside 9 percent for Extremely Low Income Households. As such, the project meets the eligibility requirement for up to two Additional Incentives.

6. **Projects Adhering to Labor Standards.** *Projects that adhere to the labor standards required in LAMC 11.5.11 may be granted two Additional Incentives from the menu in Section VII of these Guidelines (for a total of up to five Additional Incentives).*

The project is not seeking Additional Incentives beyond those permitted in exchange for reserving 7 dwelling units for Extremely Low Income Households. As such, the project need not adhere to the labor standards required in LAMC Section 11.5.11, and this eligibility requirement does not apply.

7. **Multiple Lots.** *A building that crosses one or more lots may request the TOC Incentives that correspond to the lot with the highest Tier permitted by Section III above.*

The project site consists of three contiguous lots, all of which qualify for the TOC Tier 2 Incentives.

8. **Request for a Lower Tier.** *Even though an applicant may be eligible for a certain Tier, they may choose to select a Lower Tier by providing the percentage of On-Site Restricted Affordable Housing units required for any lower Tier and be limited to the Incentives available for the lower Tier.*

The applicant has not elected to utilize a Lower Tier. As such, this eligibility requirement does not apply.

9. **100% Affordable Housing Projects.** *Buildings that are Eligible Housing Developments that consist of 100% On-Site Restricted Affordable units, exclusive of a building manager's unit or units shall, for purposes of these Guidelines, be eligible for one increase in Tier than otherwise would be provided.*

The project is not considered a 100% Affordable Housing Project. As such, this eligibility requirement does not apply.

FINDINGS

TRANSIT ORIENTED COMMUNITIES AFFORDABLE HOUSING INCENTIVE PROGRAM / AFFORDABLE HOUSING INCENTIVES COMPLIANCE FINDINGS

Pursuant to Section 12.22 A.31(e) of the LAMC, the Director shall review a Transit Oriented Communities (TOC) Affordable Housing Incentive Program project application in accordance with the procedures outlined in LAMC Section 12.22 A.25(g).

1. **Pursuant to LAMC Section 12.22 A.25(g)(2), the Director shall approve a transit oriented communities review with additional requested incentives unless the Director finds that the incentives are not required to provide for affordable housing costs as defined in California Health and Safety Code Section 50052.5 or Section 50053 for rents for the affordable units.**

The record does not contain substantial evidence that would allow the Director to find that the requested incentives are not required to provide for affordable housing costs per State Law. The California Health & Safety Code Sections 50052.5 and 50053 define formulas for calculating affordable housing costs for Very Low, Low, and Moderate Income Households. Section 50052.5 addresses owner-occupied housing and Section 50053 addresses rental households.

The list of incentives in the TOC Guidelines were pre-evaluated at the time the TOC Affordable Housing Incentive Program Ordinance was adopted to include types of relief that minimize restrictions on the size of the project. As such, the Director will always arrive at the conclusion that the on-menu incentives are required to provide for affordable housing costs because the incentives, by their nature, increase the scale of the project. The following incentives allow the developer to increase the height of the mixed-use building so that affordable housing units can be constructed and the overall space dedicated to residential uses is increased. These incentives support the applicant's decision to reserve seven (7) units for Extremely Low Income Households.

Yards/Setback. The project is located within the R4 zone and is required to provide a 15-foot front yard setback on Venice Boulevard. The requested yard incentive allows the project to be designed with a reduced front yard setback of 5 feet along Venice Boulevard. Front yard reductions are limited to no more than the average of the front yards of adjoining buildings along the same street frontage; if the project is located on a corner lot or adjacent to a vacant lot, the front yard setback may align with the façade of the adjoining building along the same front lot line. Without the yard incentive, the project's floor area would be reduced by approximately 5,000 square feet, resulting in a decrease of 16 units. With the yard incentive, the project is able to include more residential floor area for dwelling units reserved for Extremely Low Households.

Height. The project is subject to the 1XL height district, which limits height to 30 feet. The requested height incentive allows an increase of 11 feet in building height, equal to a maximum building height of 41 feet. Although the underlying zone does not limit the number of stories, the project is limited by building height. Utilization of the height incentive enables the construction of an additional level, and thus facilitates the addition of approximately 21 units. The additional units support the applicant's decision to reserve seven (7) units for Extremely Low Income Households.

The use of the Incentives result in design and construction efficiencies, which in turn makes the inclusion of affordable units more financially feasible. The Additional Incentives requested for this project are part of a broader list of TOC On-Menu Additional

Incentives that have been pre-evaluated and deemed effective in providing applicants with greater flexibility to construct mixed-income developments. The project reserves at least 7 percent of the total units for Extremely Low Income Households and, as such, qualifies for up to two Additional Incentives.

2. **Pursuant to LAMC Section 12.22 A.25(g)(2), the Director shall approve a transit oriented communities review with additional requested incentives unless the Director finds that the incentives will have a specific adverse impact upon public health and safety or the physical environment, or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact without rendering the development unaffordable to Very Low, Low, and Moderate Income households. Inconsistency with the zoning ordinance or the general plan land use designation shall not constitute a specific, adverse impact upon the public health or safety.**

There is no evidence that the proposed incentive will have a specific adverse impact upon public health and safety or the physical environment. A "specific adverse impact" is defined as, "a significant, quantifiable, direct and unavoidable impact, based on objective, identified written public health or safety standards, policies, or conditions as they existed on the date the application was deemed complete" (LAMC Section 12.22.A.25(b)). The finding that there is no evidence in the record that the proposed incentives will have a specific adverse impact is further supported by the CEQA findings. The findings to deny an incentive under Density Bonus Law are not equivalent to the findings for determining the existence of a significant unavoidable impact under CEQA. However, under a number of CEQA impact thresholds, the City is required to analyze whether any environmental changes caused by the project have the possibility to result in health and safety impacts. For example, CEQA Guidelines Section 15065(a)(4), provides that the City is required to find a project will have a significant impact on the environment and require an EIR if the environmental effects of a project will cause a substantial adverse effect on human beings. The proposed project and potential impacts were analyzed in accordance with the State CEQA Statute and Guidelines and the City's L.A. CEQA Thresholds Guide. Analysis of the proposed project determined that the project is Categorically Exempt from environmental review pursuant to Article 19, Class 32 of the State CEQA Statute and Guidelines. Furthermore, the project was evaluated against the exceptions to use of Categorical Exemptions pursuant to Section 15300.2 of the State CEQA Statute and Guidelines and determined that none of the exceptions apply to the proposed project. Therefore, there is no substantial evidence that the proposed project will have a specific adverse impact upon public health and safety or the environment, or on any real property that is listed in the California Register of Historical Resources.

ENVIRONMENTAL REVIEW FINDINGS

3. As the designee of the Director of Planning, I have determined, based on the whole of the administrative record, that the Project is exempt from CEQA pursuant to State CEQA Guidelines, Article 19, Section 15332 (Class 32) and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.

The project consists of the demolition of three existing residential structures, and the construction of a 41-foot tall, four-story, 49,948 square-foot, multi-family structure over one level of subterranean parking, containing a total of 77 dwelling units, reserving 7 dwelling units (9 percent of the total units) for Extreme Low Income households for 55 years. The project provides a total of 43 vehicular parking spaces and 82 bicycle parking spaces (72

long-term, 10 short-term). The project will grade and export 8,800 cubic yards of earth. The project is an in-fill development and qualifies for the Class 32 Categorical Exemption.

CEQA Determination – Class 32 Categorical Exemption Applies

A project qualifies for a Class 32 Categorical Exemption if it is developed on an infill site and meets the following criteria:

- a) **The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with the applicable zoning designation and regulations.**

The site currently is developed with a 3-unit apartment building, a 4-unit apartment building, and a 2-unit apartment building. The site is zoned [Q]R4-1XL and has a General Plan Land Use designation of High Medium Residential. The project proposes the construction of a three-story residential building containing 77 dwelling units. As proposed, the project is conformance with the General Plan Zoning and Land Use designation.

- b) **The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.**

The subject site is wholly within the City of Los Angeles. Lots surrounding the subject site are developed with multi-family buildings and single-family residences.

- c) **The project site has no value as a habitat for endangered, rare, or threatened species.**

The site is not a wildland area, and is not inhabited by endangered, rare, or threatened species. The area around the site is highly urbanized and surrounded by commercial and residential uses. NavigateLA shows that the subject site is not located in a Significant Ecological Area.

- d) **Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.**

The project will be subject to Regulatory Compliance Measures (RCMs), which require compliance with the City of Los Angeles Noise Ordinance for pollutant discharge, dewatering, and stormwater mitigations; and Best Management Practices for stormwater runoff. More specifically, RCMs include but are not limited to:

- **Regulatory Compliance Measure RC-AQ-1 (Demolition, Grading and Construction Activities): Compliance with provisions of the SCAQMD District Rule 403.** The project shall comply with all applicable standards of the Southern California Air Quality Management District, including the following provisions of District Rule 403:
 - All unpaved demolition and construction areas shall be wetted at least twice daily during excavation and construction, and temporary dust covers shall be used to reduce dust emissions and meet SCAQMD District Rule 403. Wetting could reduce fugitive dust by as much as 50 percent.
 - The construction area shall be kept sufficiently dampened to control dust caused by grading and hauling, and at all times provide reasonable control of dust caused by wind.

- All clearing, earth moving, or excavation activities shall be discontinued during periods of high winds (i.e., greater than 15 mph), to prevent excessive amounts of dust.
 - All dirt/soil loads shall be secured by trimming, watering or other appropriate means to prevent spillage and dust.
 - All dirt/soil materials transported off-site shall be either sufficiently watered or securely covered to prevent excessive amount of dust.
 - General contractors shall maintain and operate construction equipment to minimize exhaust emissions.
 - Trucks having no current hauling activity shall not idle but be turned off.
- **Regulatory Compliance Measure RC-GEO-1 (Seismic):** The design and construction of the project shall conform to the California Building Code seismic standards as approved by the Department of Building and Safety.
 - **Regulatory Compliance Measure RC-NO-1 (Demolition, Grading, and Construction Activities):** The project shall comply with the City of Los Angeles Noise Ordinance and any subsequent ordinances, which prohibit the emission or creation of noise beyond certain levels at adjacent uses unless technically infeasible.

These RCMs will reduce any potential impacts on noise and water quality to less than significant. The creation of noise is limited to certain decibels, restricted to specific hours. A Noise Impact Analysis, prepared by Cadence Environmental, dated September 25, 2019, determined that the proposed development would not exceed the limits stated in the Noise Ordinance. Regarding traffic and air quality impacts, DOT staff determined that the project requires a traffic assessment study. A traffic study dated September 3, 2019 was prepared by Overland Traffic Consultants. The Department of Transportation (LADOT) issued a Traffic Assessment, dated February 4, 2020, indicating the project would generate a net increase of 341 trips. The Traffic Assessment included a VMT analysis that determined the project would not have a significant transportation impact under any of the above thresholds.

The project will not conflict with any adopted policies, plans, or programs regarding public transit, bicycle facilities, or pedestrian facilities. Therefore, the project will not have any significant impacts to traffic. Likewise, air quality will not worsen as a result of the proposed project. Interim thresholds were developed by DCP staff based on CalEEMod model runs relying on reasonable assumptions, consulting with AQMD staff, and surveying published air quality studies for which criteria air pollutants did not exceed the established SCAQMD construction and operational thresholds. The Air Quality Impact Analysis prepared by Cadence Environmental Consultants, dated September 24, 2019, concluded the project would not result in impacts to air quality. Regarding water quality, the proposed project will utilize existing municipal water sources through the Department of Water and Power (LADWP). The project is subject to the City's Low Impact Development Ordinance (Ordinance 181,889), which requires the capture and retention of storm water through onsite filtration and treatment. As such, the project has been designed to meet the City's Low Impact Development (LID) requirements.

e) The site can be adequately served by all required utilities and public services.

The project site will be adequately served by all public utilities and services given that the property is located in an urban tract with water supply, sewage and waste disposal infrastructure, and power lines installed. Venice Boulevard and Glencoe Avenue are improved with existing utilities that service the lots in the area. Both can be accessed

by emergency vehicles. The project includes a street dedication to widen the pedestrian right-of-way. The project will also repair and replace any broken or off-grade asphalt, sidewalk, curb, or gutter. The project shall comply with any street light requirements required by the Bureau of Street Lighting. The proposed project will not result in significant impacts on the capacity of existing utilities and services.

The project is a transit-oriented, infill development on a site within an urbanized area and meets the criteria outlined above. Therefore the project qualifies for a Class 32 Categorical Exemption.

Furthermore, the Exceptions outlined in the State CEQA Guidelines Section 15300.2 do not apply to the project:

- a) Cumulative Impact. There is not a succession of known projects of the same type located in the same place as the subject project. No comparable pending projects (TOC or Density Bonus) have been identified within a 500-foot radius of the subject site. One TOC project located approximately 0.33 miles away at 1015 East Venice Boulevard (DIR-2017-4421-TOC) was approved on September 7, 2018. Another TOC project, located at 1808-1816 South Lincoln Boulevard (DIR-2019-1133-TOC), was approved on February 20, 2020. Since these projects qualify for a Class 32 Categorical Exemption and are subject to Regulatory Compliance Measures, no cumulative impacts are anticipated. Further, the proposed 77-unit residence does not exceed thresholds identified for impacts to the area (i.e. traffic, noise, etc.) and will not result in significant cumulative impacts.
- b) Significant Effect Due to Unusual Circumstances. A categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances. The project proposes a multi-family building in an area zoned and designated for such use. Adjacent lots are developed with multi- and single-family residences. No unusual circumstances are present or foreseeable.
- c) Scenic Highways. The project site is not located on or near a designated state scenic highway. The only State Scenic Highway within the City of Los Angeles is the Topanga Canton State Scenic Highway, State Route 27, which travels through a portion of Topanga State Park.
- d) Hazardous Waste Sites. The project site is not identified as a hazardous waste site or is on any list compiled pursuant to Section 65962.5 of the Government Code.
- e) Historical Resources. The project site has not been identified as a historic resource by local or state agencies, and the project site has not been determined to be eligible for listing in the National Register of Historic Places, California Register of Historical Resources, the Los Angeles Historic-Cultural Monuments Register, and/or any local register. The site was not found to be a potential historic resource based on the City's HistoricPlacesLA website or SurveyLA, the citywide survey of Los Angeles. The City does not treat the site as a historic resource. Based on this, the project will not result in a substantial adverse change to the significance of a historic resource.

The project is determined to be categorically exempt and does not require mitigation or monitoring measures; no alternatives of the project were evaluated. An appropriate environmental clearance has been granted.

TIME LIMIT – OBSERVANCE OF CONDITIONS

All terms and conditions of the Director's Determination shall be fulfilled before the use may be established. Pursuant to LAMC 12.25 A.2, the instant authorization is further conditional upon the privileges being utilized within **three years** after the effective date of this determination and, if such privileges are not utilized, building permits are not issued, or substantial physical construction work is not begun within said time and carried on diligently so that building permits do not lapse, the authorization shall terminate and become void.

TRANSFERABILITY

This determination runs with the land. In the event the property is to be sold, leased, rented or occupied by any person or corporation other than yourself, it is incumbent that you advise them regarding the conditions of this grant. If any portion of this approval is utilized, then all other conditions and requirements set forth herein become immediately operative and must be strictly observed.

VIOLATIONS OF THESE CONDITIONS

The applicant's attention is called to the fact that this grant is not a permit or license and that any permits and licenses required by law must be obtained from the proper public agency. Furthermore, if any condition of this grant is violated or not complied with, then the applicant or his successor in interest may be prosecuted for violating these conditions the same as for any violation of the requirements contained in the Municipal Code, or the approval may be revoked.

Section 11.00 of the LAMC states in part (m): "It shall be unlawful for any person to violate any provision or fail to comply with any of the requirements of this Code. Any person violating any of the provisions or failing to comply with any of the mandatory requirements of this Code shall be guilty of a misdemeanor unless that violation or failure is declared in that section to be an infraction. An infraction shall be tried and be punishable as provided in Section 19.6 of the Penal Code and the provisions of this section. Any violation of this Code that is designated as a misdemeanor may be charged by the City Attorney as either a misdemeanor or an infraction.

Every violation of this determination is punishable as a misdemeanor unless provision is otherwise made, and shall be punishable by a fine of not more than \$2,500 or by imprisonment in the County Jail for a period of not more than six months, or by both a fine and imprisonment."

APPEAL PERIOD - EFFECTIVE DATE

The Determination in this matter will become effective and final fifteen (15) days after the date of mailing of the Notice of Director's Determination unless an appeal there from is filed with the City Planning Department. It is strongly advised that appeals be filed early during the appeal period and in person so that imperfections/incompleteness may be corrected before the appeal period expires. All appeals must be filed on the prescribed forms, accompanied by the required fee, a copy of this Determination, and received and receipted at any one of the Development Services Center of the Department of City Planning on or before the above date or the appeal will not be accepted. Forms are available on-line at <http://planning.lacity.org>. Development Services Center of the Department of City Planning are located at:

**Downtown Metro
Public Counter**
201 N. Figueroa St., 4th Floor
Los Angeles, CA 90012
(213) 482-7077

**San Fernando Valley
Public Counter**
6262 Van Nuys Blvd.,
2nd Floor
Los Angeles, CA 91401
(818) 374-5050

**West Los Angeles
Public Counter**
1828 Sawtelle Blvd.,
2nd Floor
Los Angeles, CA 90025
(310) 231-2901

Only an applicant or any owner or tenant of a property abutting, across the street or alley from, or having a common corner with the subject property can appeal this Density Bonus Compliance Review Determination. Per the Density Bonus Provision of State Law (Government Code Section §65915) the Density Bonus increase in units above the base density zone limits and the appurtenant parking reductions are not a discretionary action and therefore cannot be appealed. Only the requested incentives are appealable. Per Section 12.22 A.25 of the LAMC, appeals of Density Bonus Compliance Review cases are heard by the City Planning Commission.

Verification of condition compliance with building plans and/or building permit applications are done at any Development Services Center of the Department of City Planning. In order to assure that you receive service with a minimum amount of waiting, applicants are encouraged to schedule an appointment with the Development Services Center either through the Department of City Planning website at <http://planning.lacity.org> or by calling (213) 482-7077, (818) 374-5050, or (310) 213-2901. The applicant is further advised to notify any consultant of this requirement as well.

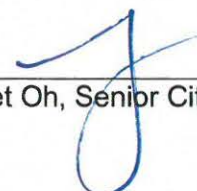
The time in which a party may seek judicial review of this determination is governed by California Code of Civil Procedures Section 1094.6. Under that provision, a petitioner may seek judicial review of any decision of the City pursuant to California Code of Civil Procedure Section 1094.5, only if the petition for writ of mandate pursuant to that section is filed no later than the 90th day following the date on which the City's decision becomes final.

VINCENT P. BERTONI, AICP
Director of Planning

Approved by:


Faisal Roble, Principal City Planner

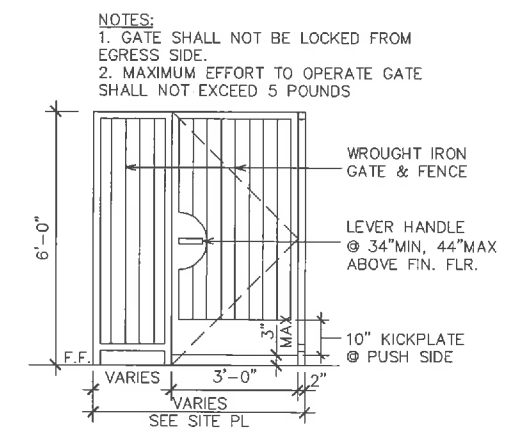
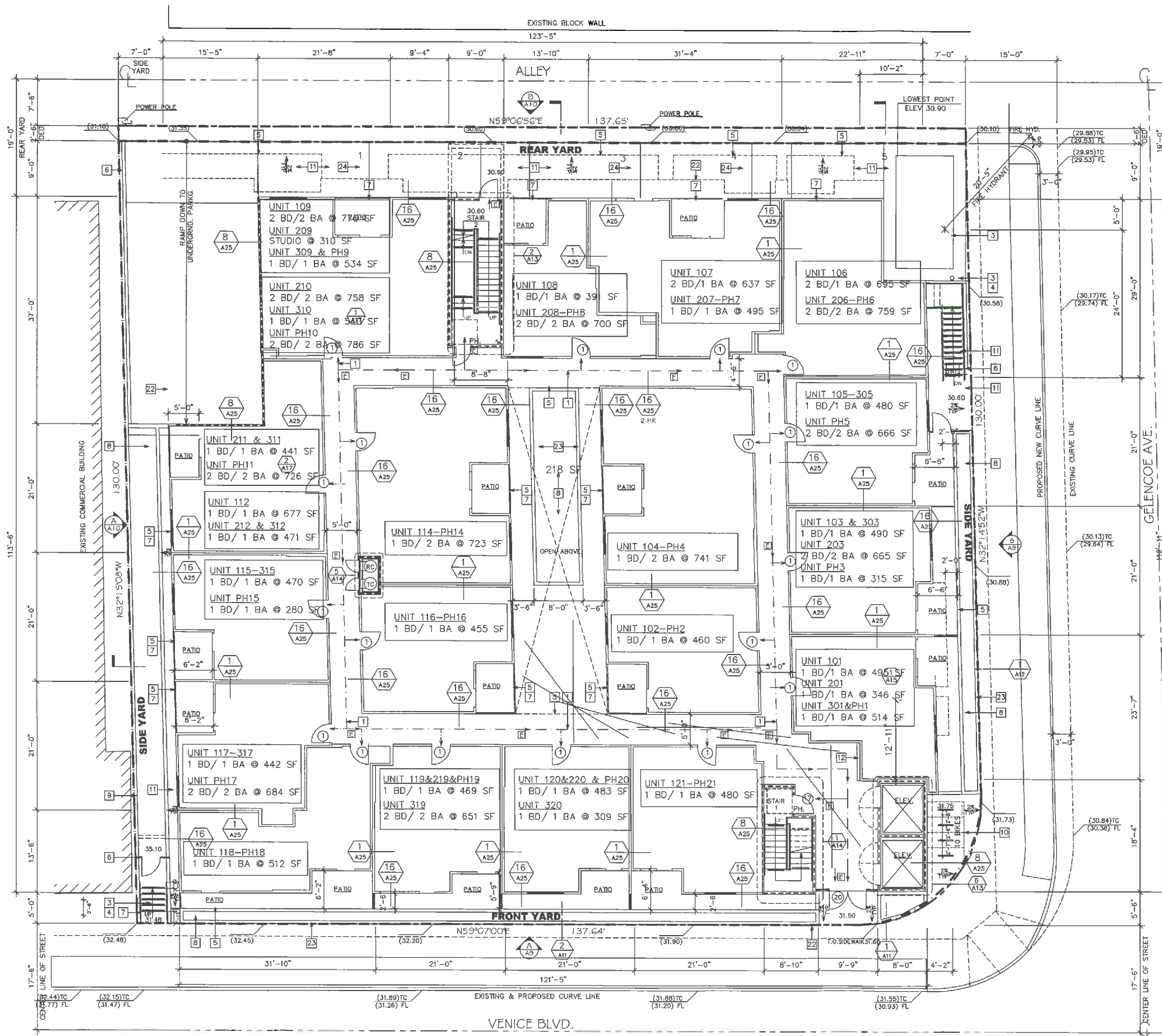
Reviewed by:


Juliet Oh, Senior City Planner

Prepared by:


Jeffrey Khau, AICP, City Planning Associate
Jeff.khau@lacity.org

EXHIBIT C
APPROVED PROJECT PLANS
DIR-2019-4920-TOC-1A



- NOTES:**
- GATE SHALL NOT BE LOCKED FROM EGRESS SIDE.
 - MAXIMUM EFFORT TO OPERATE GATE SHALL NOT EXCEED 5 POUNDS
- SITE REFERENCE NOTES:**
- ADA PATH OF TRAVEL
 - NON-FILTRATION PLANTER
 - CONCRETE STAIRS/STEPS - SEE D4r TREADS 11" MIN., RISERS 4"-7" TYP.
 - WARNING STRIPE & HANDRAIL EXTENSIONS AS PER 11A-6A/D4r
 - LINE OF BALCONY ABOVE
 - 6'-0" HIGH W.I. GATE & FENCE, SEE 2/A2
 - 42" HIGH WROUGHT IRON RAILING
 - STORMWATER/FILTRATION PLANTER
 - 6'-0" HIGH CMU WALL @ PROPERTY LINE
 - SHORT TERM BICYCLE PARKING - SEE 2/A3
 - 100% OF HARDSCAPE TO BE UNCOLORED CONCRETE w/ SMOOTH CEMENT FINISH and w/ SOLAR REFLECTANCE OF AT LEAST 0.30 AS DETERMINED PER ASTM E918 or ASTM C1549.
 - MAILBOXES. SEE DET 6/D5r
 - WEATHER BASED IRRIGATION CONTROLLER SEE SHEET A2X
 - FIRE DEPARTMENT CONNECTION
 - 2'-6"x4" MIN. WHEEL CHAIR REFUGE. THIS AREA TO BE IN ACCORDANCE w/ SECTION 1007.7.3 THRU 1007.7.6
 - PROVIDE INSTRUCTIONS AND TWO-WAY COMMUNICATION PER SECTIONS 1007.6.3 & 1007.6.4
 - INVERTER & METERING EQUIPMENT FOR SOLAR POWER SYSTEM
 - EXIST. CURB BREAK TO BE REMOVED - INSTALL NEW CURB, GUTTER AND SIDEWALK
 - CONC. APRON PER CITY STD.
 - 42" HI MAX. PLANTER
 - IRRIGATION CONTROLLER. SEE 1/A115
 - INSTALL ACCESSIBLE SIGNAGE PER D7r
 - INSTALL 12"x12"x2" CONCRETE STEPPING STONES FOR FIRE RESCUE LADDERS BENEATH BEDROOM ESCAPE WINDOWS
- LEGEND**
- 1-HR WALL
 - 2-HR WALL - SEE 8/A14
 - CLASS 1 - 4" STANDPIPE W/ 2 1/2" OUTLET
 - EXIT SIGN - SEE NOTES F13&14/A1
 - WATER CURTAIN SEE 3/A4, TYP.
 - EXIST. GRADE
 - FINISHED GRADE
 - WEATHER BASED IRRIGATION CONTROLLER SEE SHEET A15
 - ELEVATION MONUMENT TO BE ESTABLISHED AND VERIFIED PRIOR TO FOUNDATION EXCAVATION
 - PH PANIC HARDWARE
- GRADE PLANE**
- PLANNING LOWEST POINT EL. 30.90
- BUILDING 32.48+31.60+30.10+31.18/4=31.34

ZONING FLOOR AREA
(NET FLOOR AREA* W/BALCONIES LESS ELEVATOR, STAIRS, ENCLOSED SHAFTS & OPEN TO SKY OPEN SPACE)

1ST FLR (13,133 SF - 1,333 SF)	11,800 SF
2ND FLR (14,055 SF - 1,339 SF)	12,716 SF
3RD FLR (14,055 SF - 1,339 SF)	12,716 SF
4TH FLR (14,055 SF - 1,339 SF)	12,716 SF
TOTAL	49,948 SF

*= EXCLUDES AREA OF EXTERIOR WALLS

BUILDING FLOOR AREA
(NET FLOOR AREA INCL. BALCONIES, ELEV., STAIRS & ENCLOSED SHAFTS LESS OPEN TO SKY OPEN SPACE)

1ST FLR (13,133 SF - 802 SF)	12,331 SF/200=62 OCCUP
2ND FLR (14,055 SF - 802 SF)	13,253 SF/200=66 OCCUP
3RD FLR (14,055 SF - 802 SF)	13,253 SF/200=66 OCCUP
4TH FLR (14,055 SF - 802 SF)	13,253 SF/200=66 OCCUP
GARAGE AREA	15,548 SF/200=261 OCCUP
TOTAL	67,639 SF

SCHOOL FEE FLOOR AREA
(GROSS FLOOR AREA W/ BALCONIES LESS ELEVATOR, STAIRS, ENCLOSED SHAFTS & OPEN TO SKY OPEN SPACE)

1ST FLR (13,371 SF - 1,333 SF)	12,038 SF/200= 60 OCCUP
2ND FLR (14,299 SF - 1,339 SF)	12,960 SF/200= 65 OCCUP
3RD FLR (14,299 SF - 1,339 SF)	12,960 SF/200= 65 OCCUP
4TH FLR (14,299 SF - 1,339 SF)	12,960 SF/200= 65 OCCUP
TOTAL	50,918 SF

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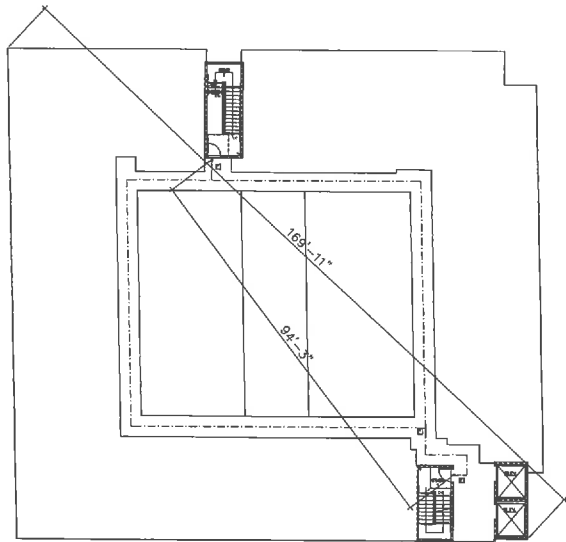
JOB TITLE
1600 VENICE BLVD

SHEET TITLE
SITE PLAN

PROJECT NUMBER

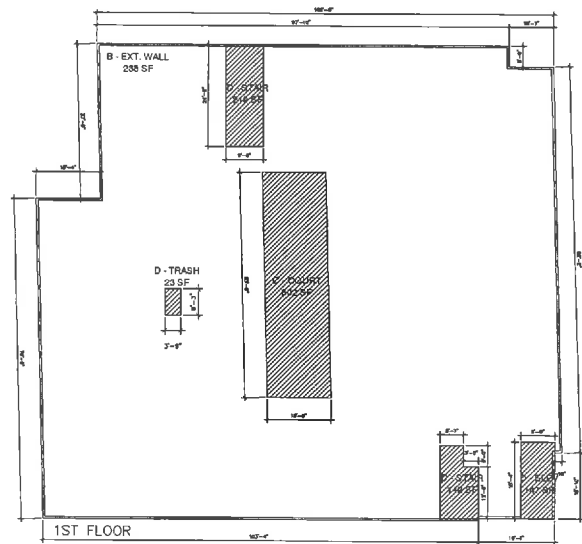
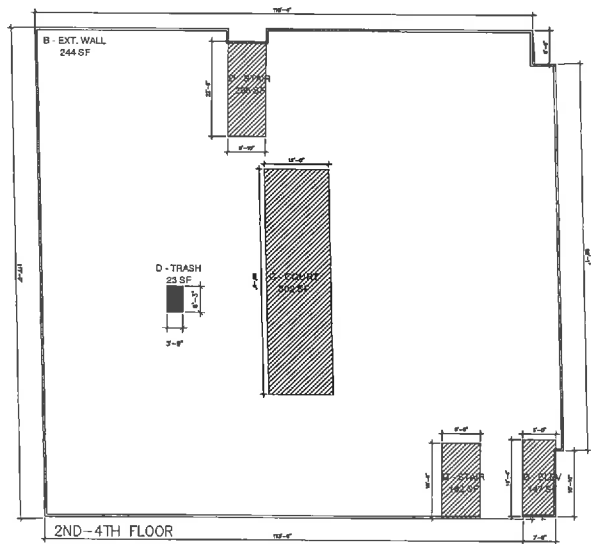
SHEET NUMBER
A2

EXHIBIT "A"
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Case No. DIV. 2019-4426-TOC



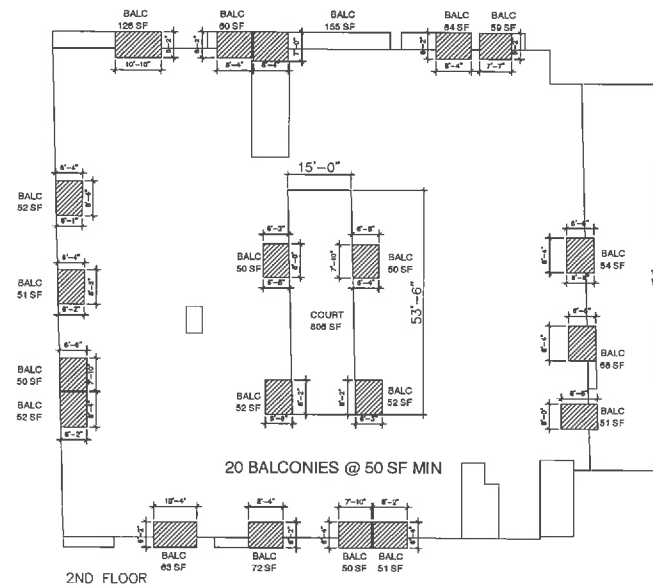
2 EXIT DISTANCE DIAGRAM
NTS

BUILDING DIAGONAL $170' / 3 = 57'$
DISTANCE BETWEEN EXITS = $94'-3" > 55'-8"$

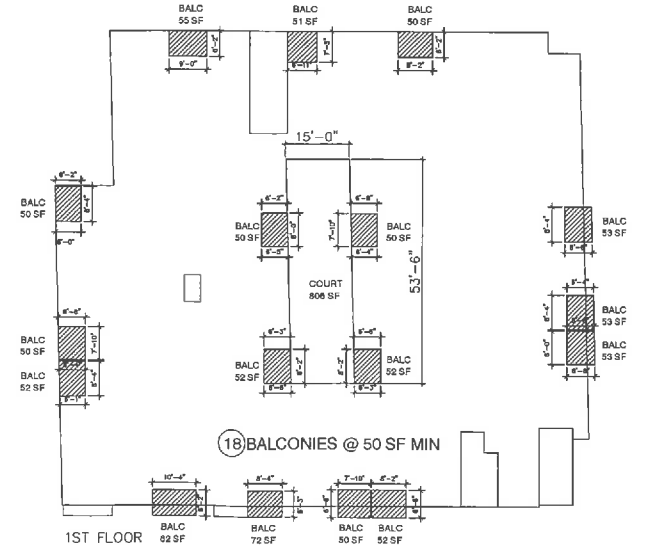


1 FLOOR AREA DIAGRAM
NTS

FLOOR LEVEL	PERMITTED AREA (16' X 125')	AREA OF PATIO/DECK/COVERED PORCH	AREA OF COURT/CLIMATE CONTROLLED AREA	AREA OF TERRACE/MECHANICAL ROOM	AREA OF OTHER PERMITTED USE	TOTAL AREA
1ST FLOOR	12,000	1,000	1,000	1,000	1,000	16,000
2ND FLOOR	12,000	1,000	1,000	1,000	1,000	16,000
3RD FLOOR	12,000	1,000	1,000	1,000	1,000	16,000
4TH FLOOR	12,000	1,000	1,000	1,000	1,000	16,000
TOTAL AREA	48,000	4,000	4,000	4,000	4,000	64,000



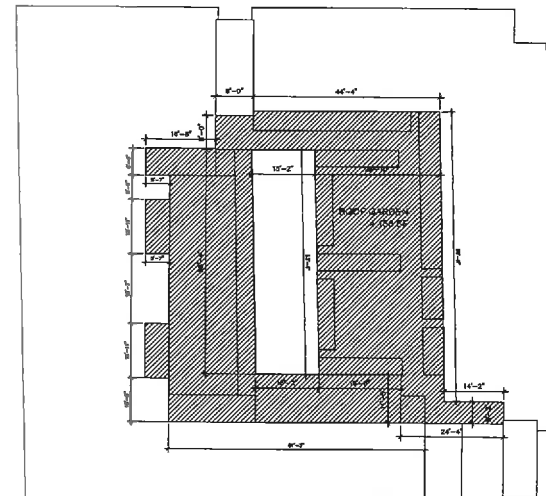
2ND FLOOR



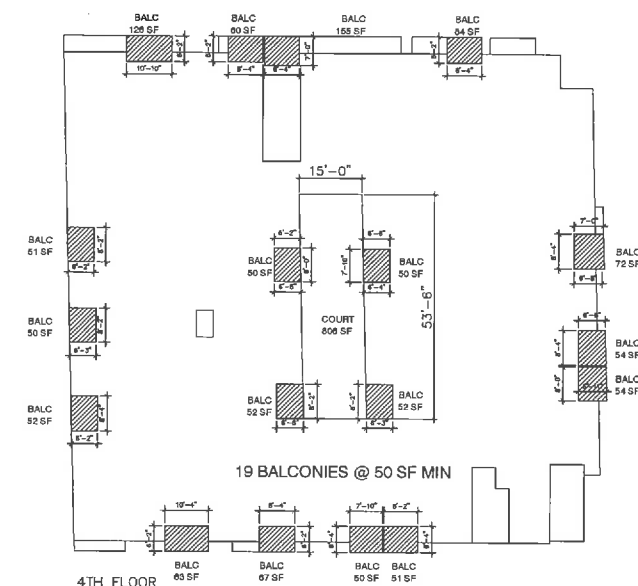
1ST FLOOR

2 OPEN SPACE DIAGRAM
1/16" = 1'-0"

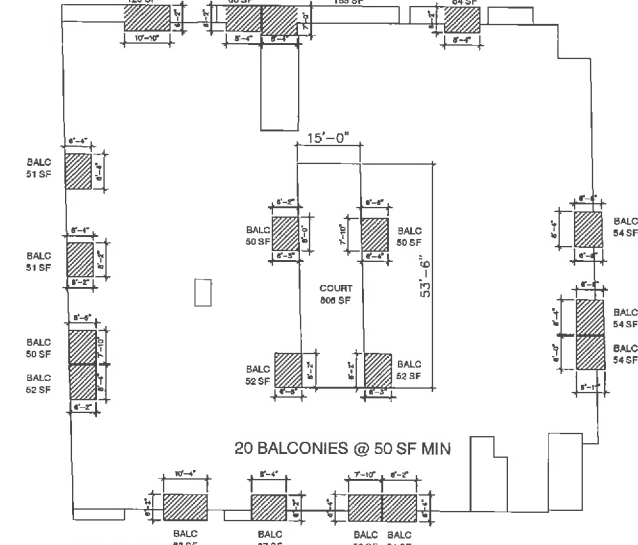
OPEN SPACE REQUIRED (16' X 125') (61 X 100) X 80	6,880 SF
OPEN SPACE PROVIDED	
PRIVATE OPEN SPACE (50 SF X 7')	3,500 SF
COURT YARD	800 SF
ROOF GARDEN	4,156 SF
TOTAL	8,456 SF



ROOF PLAN



4TH FLOOR



3RD FLOOR

EXHIBIT "A"
Page No. 2 of 18
Case No. DW 2019-4920-700

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JOB TITLE
1500 VENICE BLVD



SHEET TITLE
DIAGRAMS

PROJECT NUMBER

SHEET NUMBER

A2.1



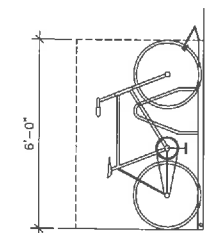
EXHIBIT "A"

Page No. 5 of 18

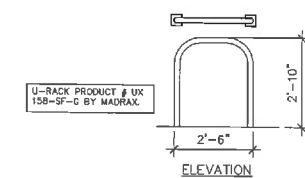
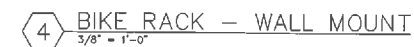
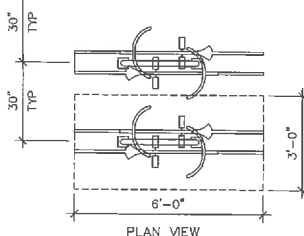
Case No. DM-2019-11922-706

-
- Orthographic projection of a cylinder. The front view is a rectangle with a height of 2'-0" and a width of 4'-0". The top view is a circle with a diameter of 4'-0". The cylinder is oriented horizontally.

SIDE ELEVATIONS



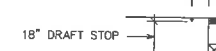
SIDE ELEVATIONS

ELEVATION

PLAN VIEW

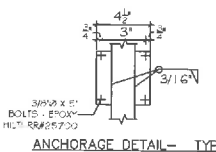
- ## 2 BIKE RACK - FLOOR MOUNT

- SPRINKLER HEAD SHALL BE
6"-12" FROM THE DRAFT STOP

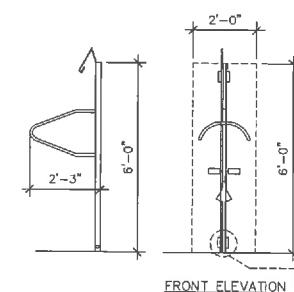


OPENING IN THE EXTERIOR WALL.

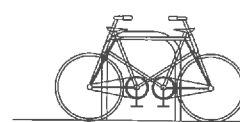
DRAFT STOP TO BE A MINIMUM
OF 18 INCHES MEASURED FROM
THE SPRINKLER'S DEFLECTOR
(SEE EXCEPTION UNDER INSTALLATION)



ANCHORAGE DETAIL- TYPE



FRONT ELEVATION



SIDE ELEVATION

DATE	ISSUED FOR	DATE	REVISIONS
	*	*	*
	*	*	*
	*	*	*

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JOB TITLE
1600 VENICE BLVD

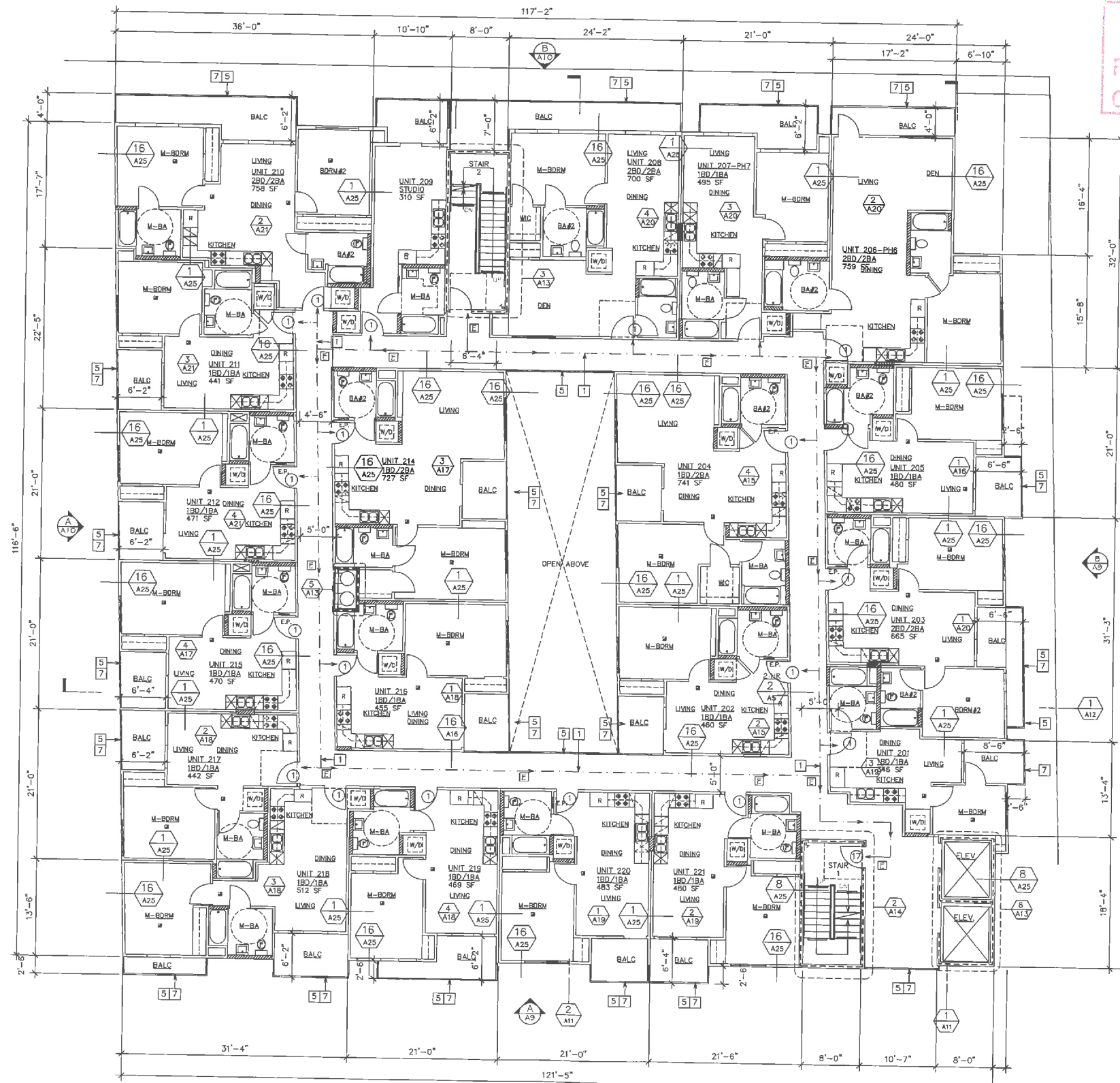


SHEET TITLE
1ST FLOOR PLAN
2ND FLOOR PLAN

PROJECT NUMBER

SHEET NUMBER

A4



1 2ND FLOOR PLAN
1/8"=1'-0"

EXHIBIT "A"
Page No. 6 of 16
Case No. DM-2019-4920-100

REFERENCE NOTES:

- 1 ADA PATH OF TRAVEL
- 2 NON-FILTRATION PLANTER
- 3 CONCRETE STAIRS/STEPS - SEE D4r
11" MIN. TREADS, 4"-7" RISERS, TYP.
- 4 WARNING STRIPE & HANDRAIL
EXTENSIONS AS PER 11A-BA/D4r
- 5 LINE OF BALCONY ABOVE
- 6 6'-0" HIGH W.I. GATE & FENCE,
SEE 2/A2
- 7 42" HIGH RAILING - SEE 14/A25

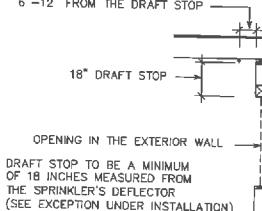
LEGEND

- 1-HR WALL: 4/A14 & 16/A14
- 2-HR WALL - SEE 8/A14
- CLASS 1 - 4" STANDPIPE
W/ 2 1/2" OUTLET
- EXIT SIGN - SEE NOTES
F13&14/A1
- WATER CURTAIN,
SEE 3/A4, TYP
- 100.00 EXIST. GRADE
- 100.00 FINISHED GRADE
- WEATHER BASED IRRIGATION
CONTROLLER SEE SHEET A15
- SMART THERMOSTAT
SEE A16
- HARD WIRE FIRE MARSHALL APPROVED SMOKE
DETECTOR W/ BATTERY BACKUP, LOW BATTERY
SIGNAL, AND WITH APPROVED CARBON MONOXIDE
ALARM
- PH PANIC HARDWARE

ENERGY NOTES:

1. SEE ENLARGED FLOOR
PLANS FOR ENERGY STAR
APPLIANCES.
2. ENERGY STAR APPLIANCES
MUST BE INSTALLED.

SPRINKLER HEAD SHALL BE
6'-12" FROM THE DRAFT STOP



3 WATER CURTAIN
ELEVATION VIEW

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JOB TITLE
1600 VENICE BLVD

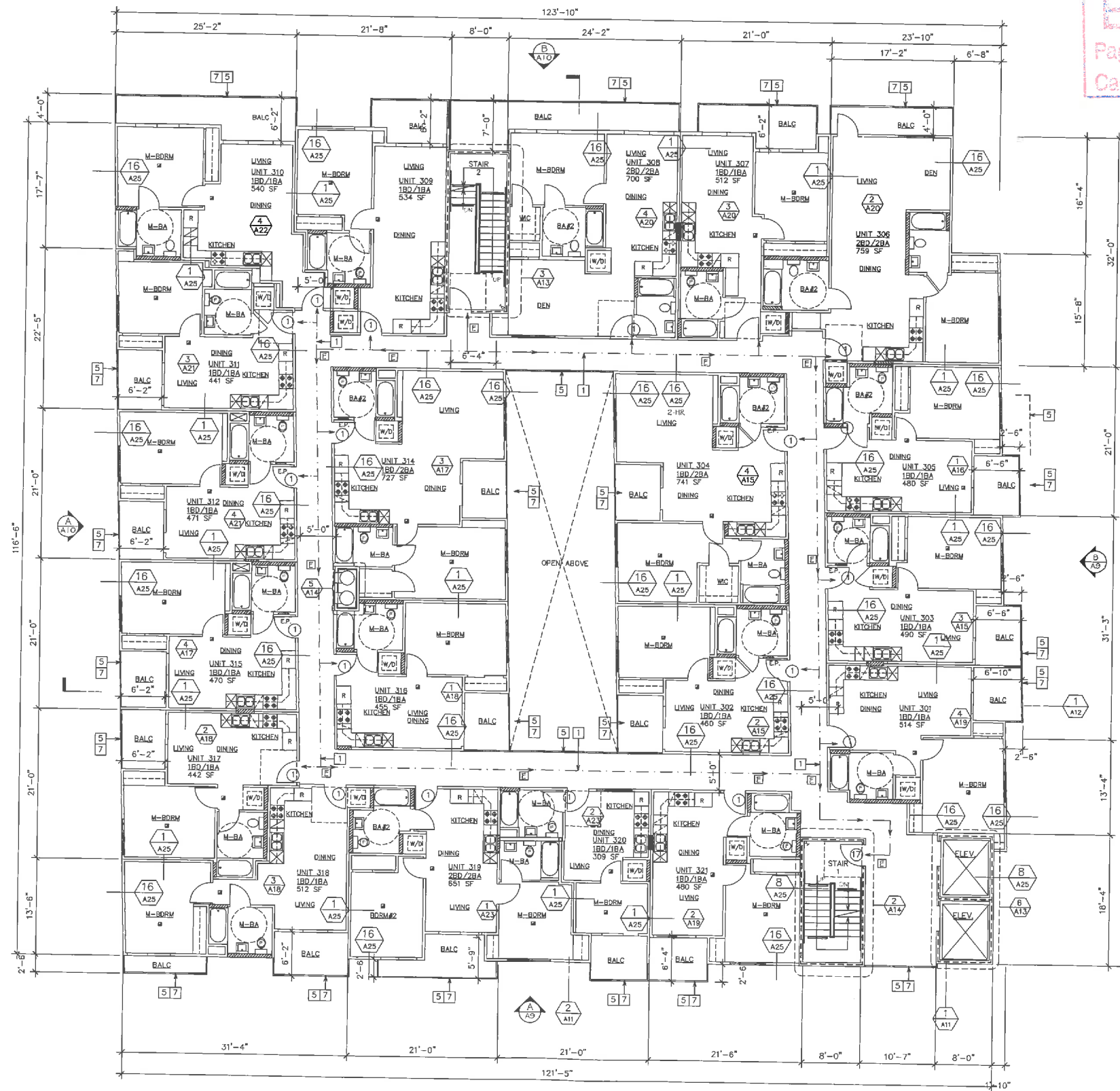
SHEET TITLE
2ND FLOOR PLAN

PROJECT NUMBER

SHEET NUMBER

A5

DATE	ISSUED FOR	DATE	REVISIONS



1 3RD FLOOR PLAN
1/8"=1'-0"

EXHIBIT "A"
Page No. 7 of 16
Case No. D162019-4920-700

REFERENCE NOTES:

- 1 ADA PATH OF TRAVEL
- 2 NON-FILTRATION PLANTER
SEE G2-G3
- 3 CONCRETE STAIRS/STEPS - SEE D4r
11" MIN. TREADS, 4"-7" RISERS, TYP.
- 4 WARNING STRIPE & HANDRAIL
EXTENSIONS AS PER 11A-6A/D4r
- 5 LINE OF BALCONY ABOVE
- 6 6'-0" HIGH W.I. GATE & FENCE,
SEE 2/A2
- 7 42" HIGH RAILING - SEE 14/A25

LEGEND

- 1-HR WALL: 4/A14 & 16/A14
- 2-HR WALL - SEE 8/A14
- CLASS 1 - 4" STANDPIPE
W/ 2 1/2" OUTLET
- EXIT SIGN - SEE NOTES
F13A14/A1
- WATER CURTAIN,
SEE 3/A4, TYP.
- (100.00) EXIST. GRADE
- 100.00 FINISHED GRADE
- WEATHER BASED IRRIGATION
CONTROLLER SEE SHEET A15
- SMART THERMOSTAT
SEE A16
- HARD WIRE FIRE MARSHALL APPROVED SMOKE
DETECTOR W/ BATTERY BACKUP, LOW BATTERY
SIGNAL, AND WITH APPROVED CARBON MONOXIDE
ALARM
- PH PANIC HARDWARE

ENERGY NOTES:

1. SEE ENLARGED FLOOR
PLANS FOR ENERGY STAR
APPLIANCES.
2. ENERGY STAR APPLIANCES
MUST BE INSTALLED.

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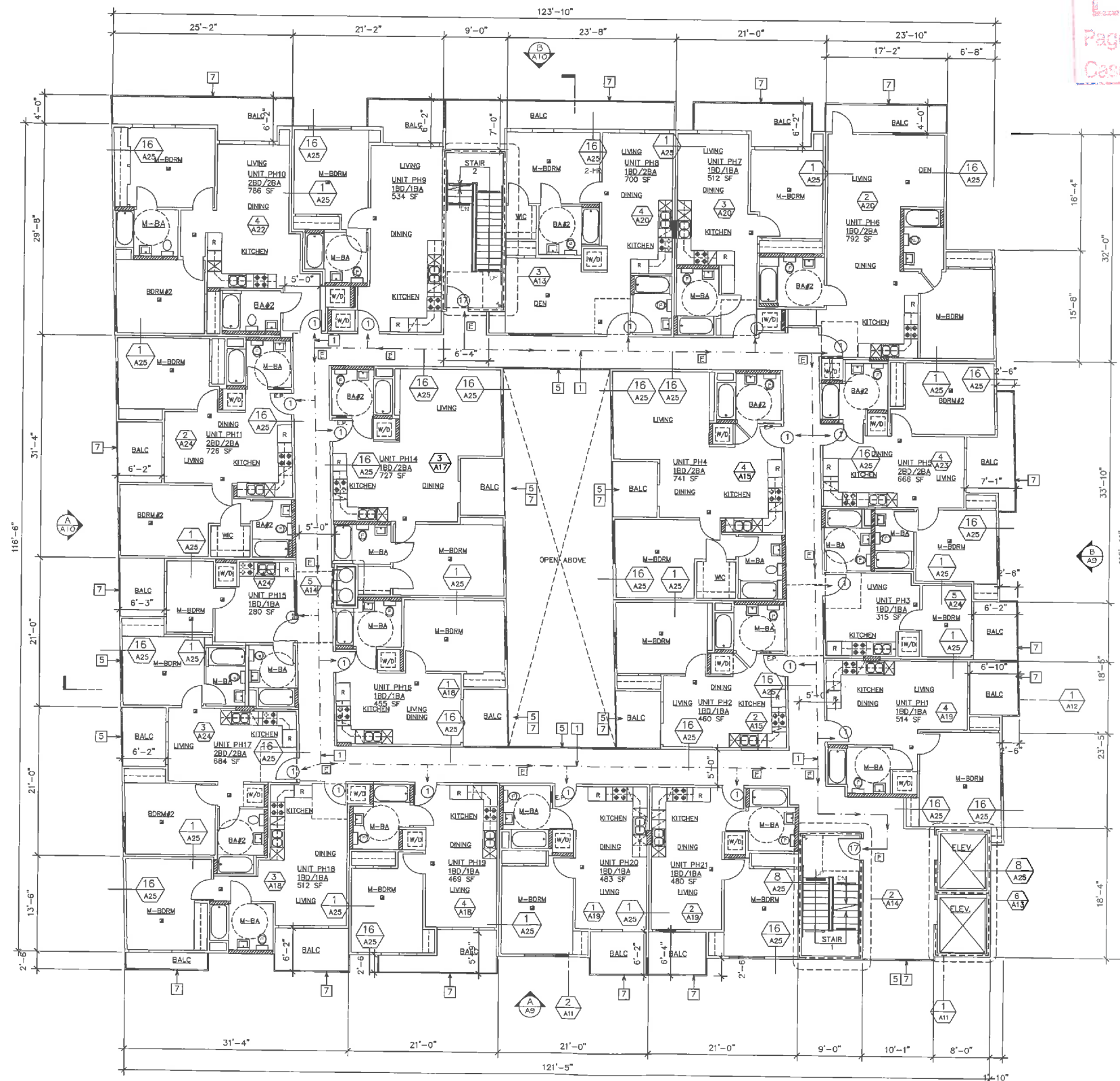
JOB TITLE
1600 VENICE BLVD

SHEET TITLE
3RD FLOOR PLAN

PROJECT NUMBER

SHEET NUMBER

A6



1 4TH FLOOR PLAN
1/8"=1'-0"

EXHIBIT "A"
Page No. 46 of 18
Case No. DM-2019-4920-7a

REFERENCE NOTES:

- 1 ADA PATH OF TRAVEL
- 2 NON-FILTRATION PLANTER
SEE G2-G3
- 3 CONCRETE STAIRS/STEPS - SEE D4r
11" MIN. TREADS, 4"-7" RISERS, TYP.
WARNING STRIPE & HANDRAIL
EXTENSIONS AS PER 11A-5A/D4r
- 4 LINE OF BALCONY ABOVE
- 5 6'-0" HIGH W.I. GATE & FENCE,
SEE 2/A2
- 7 42" HIGH RAILING - SEE 14/A25

LEGEND

- 1-HR WALL: 4/A14 & 16/A14
- 2-HR WALL - SEE 8/A14
- CLASS 1 - 4" STANDPIPE
W/ 2 1/2" OUTLET
- EXIT SIGN - SEE NOTES
F13&14/A1
- WATER CURTAIN,
SEE 3/A4, TYP
- 100.00 EXIST. GRADE
- 100.00 FINISHED GRADE
- WEATHER BASED IRRIGATION
CONTROLLER SEE SHEET A15
- SMART THERMOSTAT
SEE A16
- HARD WIRE FIRE MARSHALL APPROVED SMOKE
DETECTOR W/ BATTERY BACKUP, LOW BATTERY
SIGNAL, AND WITH APPROVED CARBON MONOXIDE
ALARM
- PH PANIC HARDWARE

ENERGY NOTES:

1. SEE ENLARGED FLOOR
PLANS FOR ENERGY STAR
APPLIANCES
2. ENERGY STAR APPLIANCES
MUST BE INSTALLED.

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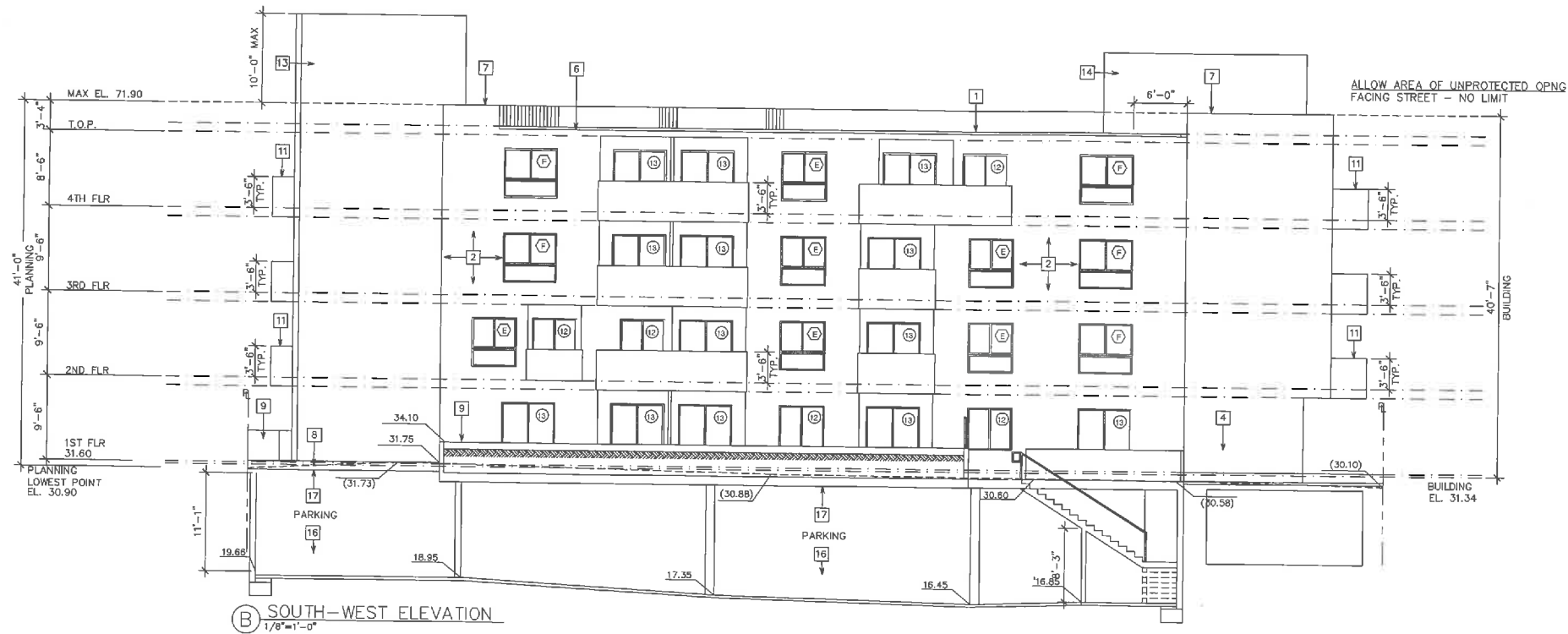
JOB TITLE
1600 VENICE BLVD

SHEET TITLE
4TH FLOOR PLAN

PROJECT NUMBER

SHEET NUMBER

A7



ELEVATION LEGEND:

- 1 BUILT-UP ROOF-CLASS 'A'
- 2 EXTERIOR STUCCO
- 3 DOWNSPOUT
- 4 42" HIGH WALL
- 5 EXTERIOR TILE FINISH
- 6 G.I. GUTTER
- 7 PARAPET WALL
- 8 LINE OF NATURAL GRADE
- 9 PLANTER - SEE G2-G3
- 10 6' HIGH CMU WALL
- 11 42" HIGH RAILING - SEE 14/A25
- 12 6' HIGH W.I. FENCE & GATE - SEE 2/A2
- 13 STAIR TOWER BEYOND
- 14 ELEVATOR TOWER BEYOND
- 15 ROLL-UP DOOR
- 16 CONCRETE SLAB ON GRADE
- 17 CONCRETE DECK (3 HR)
- 18 RETAINING WALL
- 19 STRUCTURAL FOAM -
INSULFAOM ASTM D1621 &
ICC ERS 1778
- 20 BUILDING ADDRESS PROVIDED
AT ENTRANCE OF BUILDING IN
ACCORDANCE TO LAMC 57.09.11.
- 21 FIRE DEPARTMENT CONNECTION.

REFERENCE NOTE:

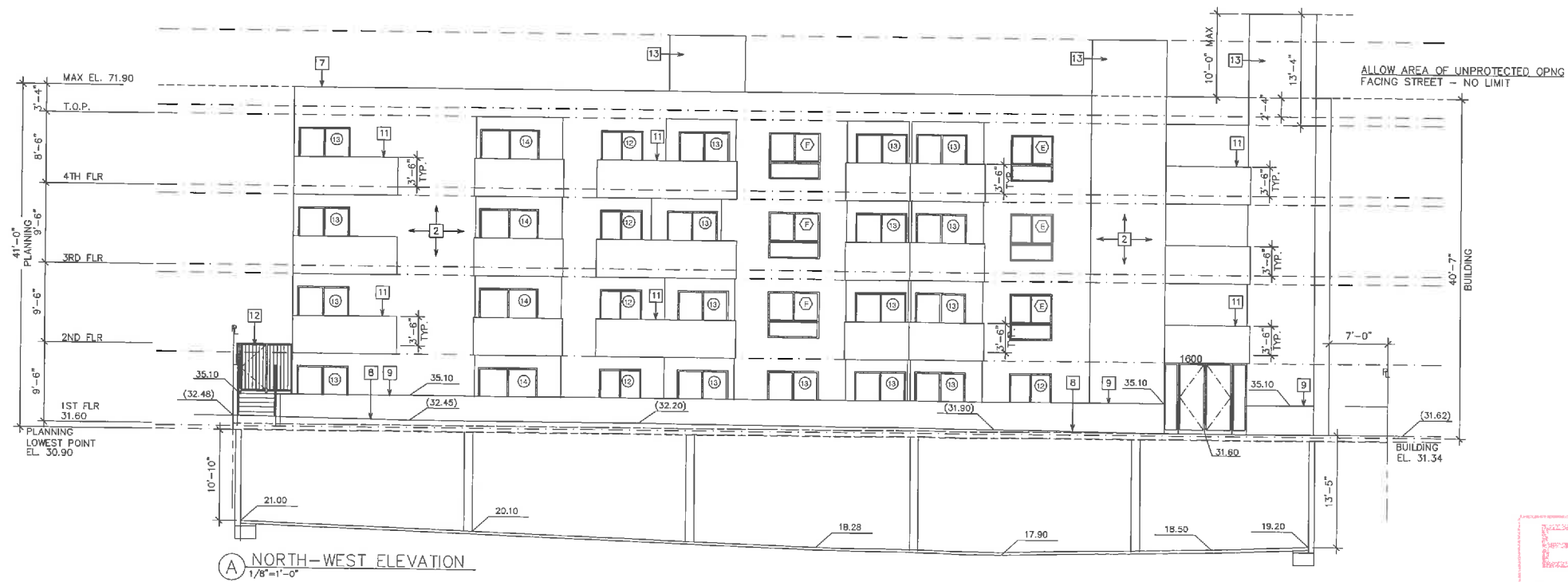
- 100.00 EXIST. GRADE
- 100.00 FINISHED GRADE

NOTE:

1. ALL ROOF DRAINS & DOWNSPOUTS TO FLOW TO
STORMWATER FILTRATION PLANTERS (LID).
SEE SHEETS G2-G3.1

PLANNING
LOWEST POINT EL. 30.90

BUILDING
32.48+31.60+30.10+31.18/4=31.34



GRADE PLANE

PLANNING: EL. 30.90
BUILDING: 32.48 + 31.60 + 30.10
+ 31.18 = 125.36
125.36 / 4 = 31.34

BUILDING HEIGHT

PLANNING: EL. 30.90 + 41.0 = 71.90
BUILDING: 71.90 - 31.34 = 40.56
(MAX. BLDG. HT.)

WINDOW SCHEDULE			
TYPE	WIDTH	HEIGHT	S.F.
A	3'-0"	3'-0"	9 SF
B	5'-0"	5'-0"	25 SF
C	6'-0"	5'-0"	30 SF
D	4'-0"	6'-6"	26 SF
E	5'-0"	5'-6"	27.5 SF
F	6'-0"	5'-6"	33 SF

DOOR SCHEDULE			
TYPE	WIDTH	HEIGHT	S.F.
G	5'-0"	7'-0"	35 SF
H	6'-0"	7'-0"	42 SF
I	7'-0"	7'-0"	49 SF

EXHIBIT A
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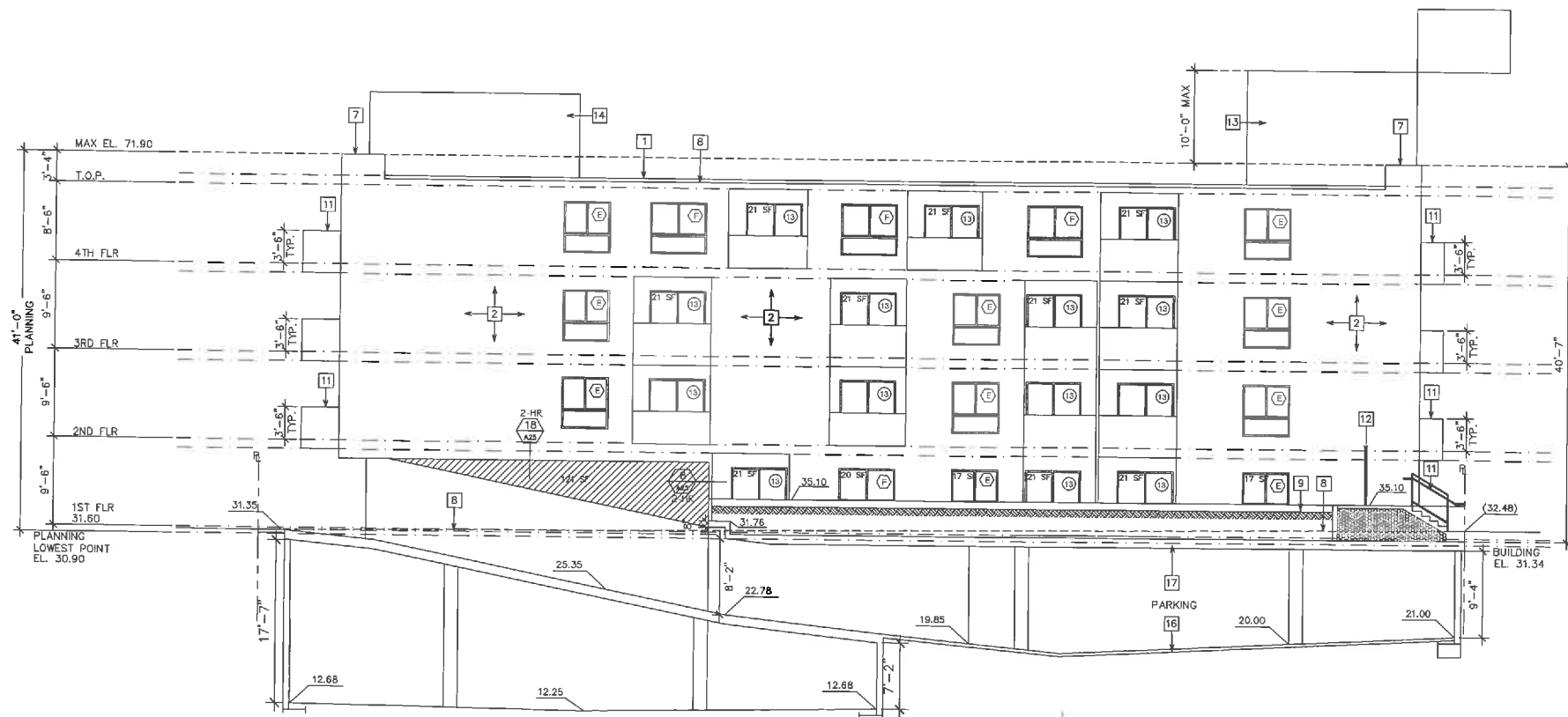
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JOB TITLE
1600 VENICE BLVD

SHEET TITLE
ELEVATIONS

PROJECT NUMBER

SHEET NUMBER
A9



ALLOW AREA OF UNPROTECTED OPNG
7 FT FROM P.L. - 25%

W.A. 116'-6X9'-6" = 1,106.75 SF
A. OF OPNGS 154 SF(W)+63 SF(D) = 217 SF/19.6%

W.A. 116'-6X9'-6" = 1,106.75 SF
A. OF OPNGS 55 SF(W)+84 SF(D) = 139 SF/12.5%

W.A. 116'-6X9'-6" = 1,106.75 SF
A. OF OPNGS 55 SF(W)+84 SF(D) = 139 SF/12.5%

W.A. 116'-6X9'-6" = 1,106.75 SF
A. OF OPNGS 117 SF(W)+121 SF(D) = 238 SF/21.5%

ELEVATION LEGEND:

- BUILT-UP ROOF-CLASS 'A'
- EXTERIOR STUCCO
- DOWNSPOUT
- 42" HIGH WALL
- EXTERIOR TILE FINISH
- G.I. GUTTER
- PARAPET WALL
- LINE OF NATURAL GRADE
- PLANTER - SEE G2-G3
- 6" HIGH CMU WALL
- 42" HIGH RAILING - SEE 14/A25
- 6" HIGH W.I. FENCE & GATE - SEE 2/A2
- STAIR TOWER BEYOND
- ELEVATOR TOWER BEYOND
- ROLL-UP DOOR
- CONCRETE SLAB ON GRADE
- CONCRETE DECK (3 HR)
- RETAINING WALL
- STRUCTURAL FOAM - INSULFOAM ASTM D1621 & ICC ER ERS 1778
- BUILDING ADDRESS PROVIDED AT ENTRANCE OF BUILDING IN ACCORDANCE TO LAMC 57.09.11.
- FIRE DEPARTMENT CONNECTION.

REFERENCE NOTE:
100.00 EXIST. GRADE
100.00 FINISHED GRADE

NOTE:
1. ALL ROOF DRAINS & DOWNSPOUTS TO FLOW TO STORMWATER FILTRATION PLANTERS (LID). SEE SHEETS G2-G3.1

PLANNING
LOWEST POINT EL. 30.90

BUILDING
32.48+31.60+30.10+31.18/4=31.34

WINDOW SCHEDULE			
TYPE	WIDTH	HEIGHT	S.F.
(A)	3'-0"	3'-0"	9 SF
(B)	5'-0"	5'-0"	25 SF
(C)	6'-0"	5'-0"	30 SF
(D)	4'-0"	6'-6"	26 SF
(E)	5'-0"	5'-6"	27.5 SF
(F)	6'-0"	5'-6"	33 SF

DOOR SCHEDULE			
TYPE	WIDTH	HEIGHT	S.F.
(12)	5'-0"	7'-0"	35 SF
(13)	6'-0"	7'-0"	42 SF
(14)	7'-0"	7'-0"	49 SF

ALLOW AREA OF UNPROTECTED OPNG
19 FT FROM ALLEY. - 75%

W.A. 108'-3"X9'-6" = 1,395.41 SF
A. OF OPNGS 33 SF(W)+357 SF(D) = 390 SF/28%

W.A. 108'-3"X9'-6" = 1,395.41 SF
A. OF OPNGS 33 SF(W)+357 SF(D) = 390 SF/28%

W.A. 108'-3"X9'-6" = 1,395.41 SF
A. OF OPNGS 93.5 SF(W)+(231+119=350) SF(D) = 443.5 SF/31.7%

EXHIBIT "A"
Page No. 11 of 16
Case No. DLK-2019-4970-70a

DATE	ISSUED FOR	DATE	REVISIONS

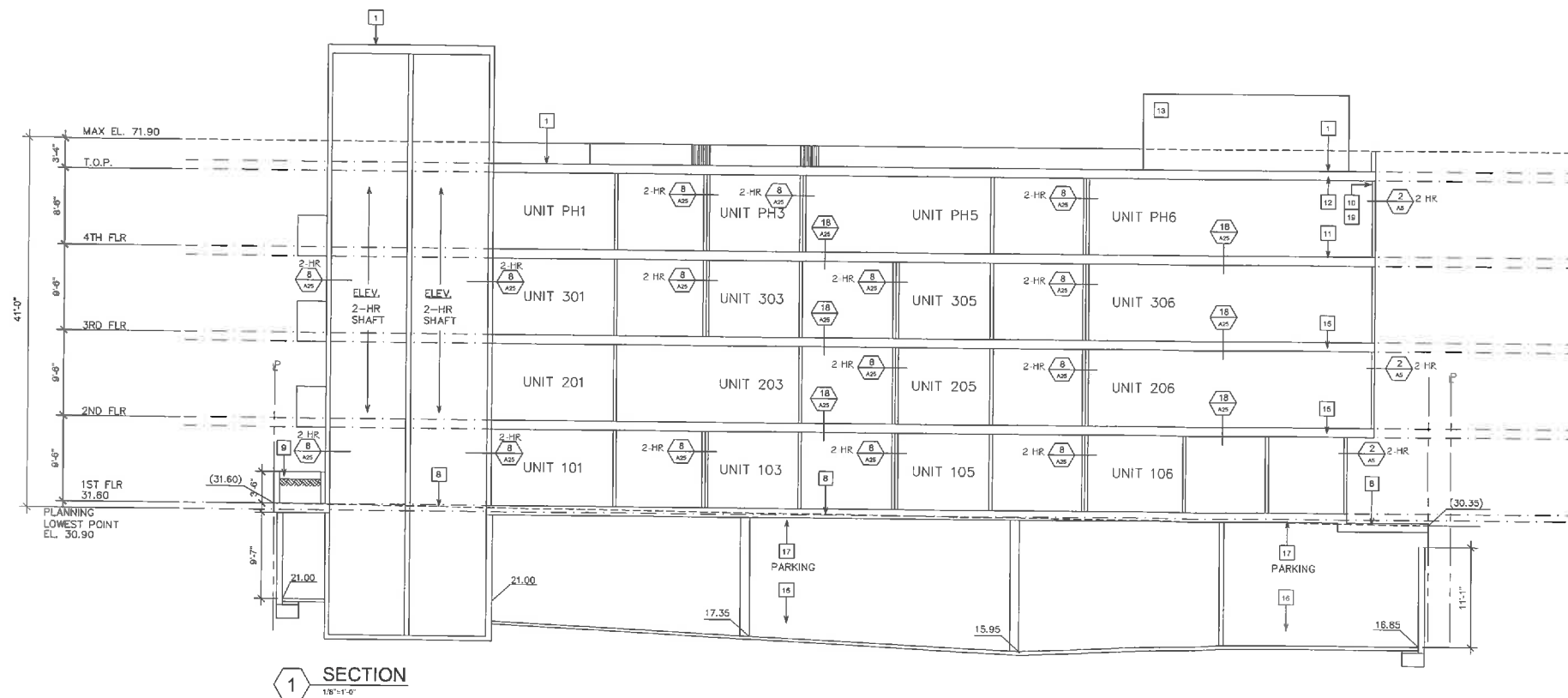
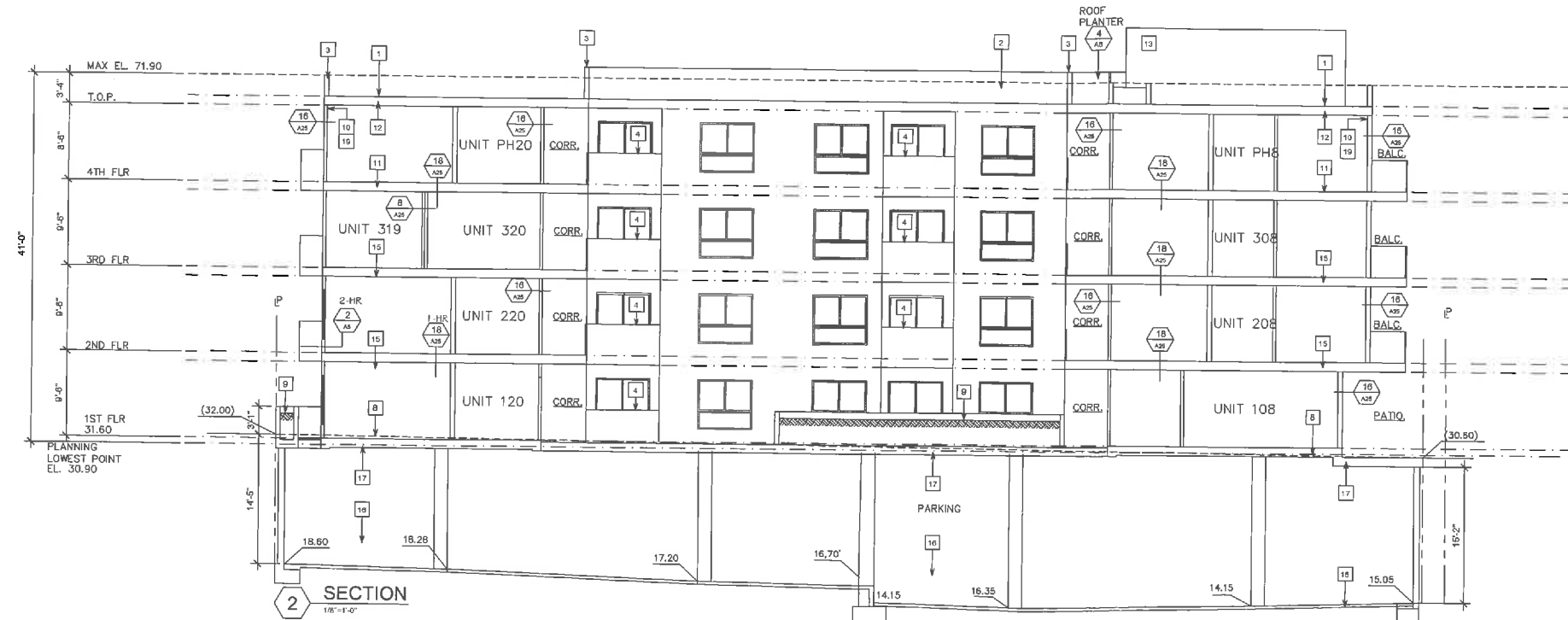
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JOB TITLE
1600 VENICE BLVD

SHEET TITLE
ELEVATIONS

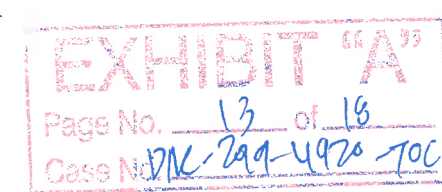
PROJECT NUMBER

SHEET NUMBER
A10



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Case No. PIK 2019-4920-TOC

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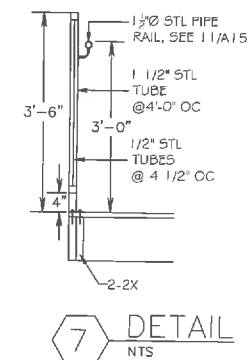
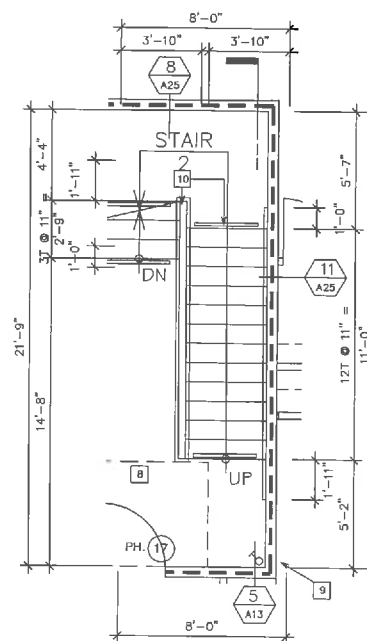
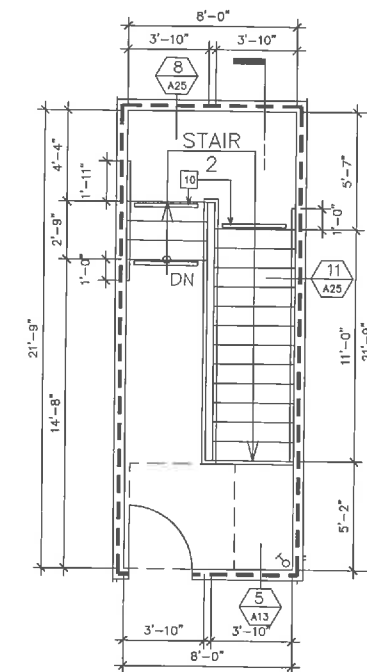
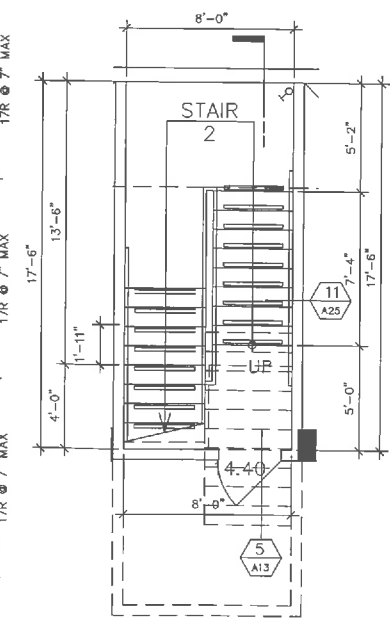
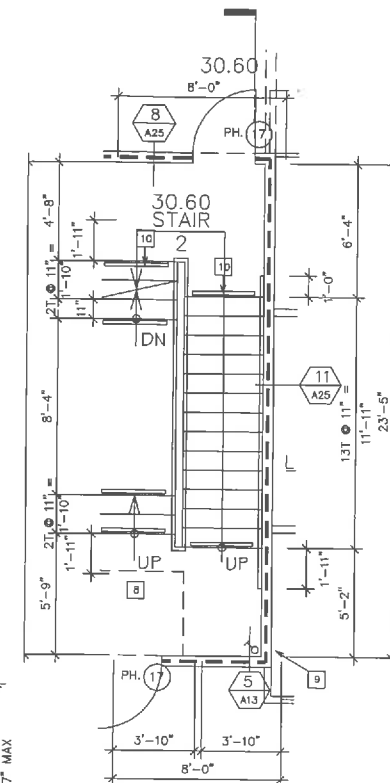
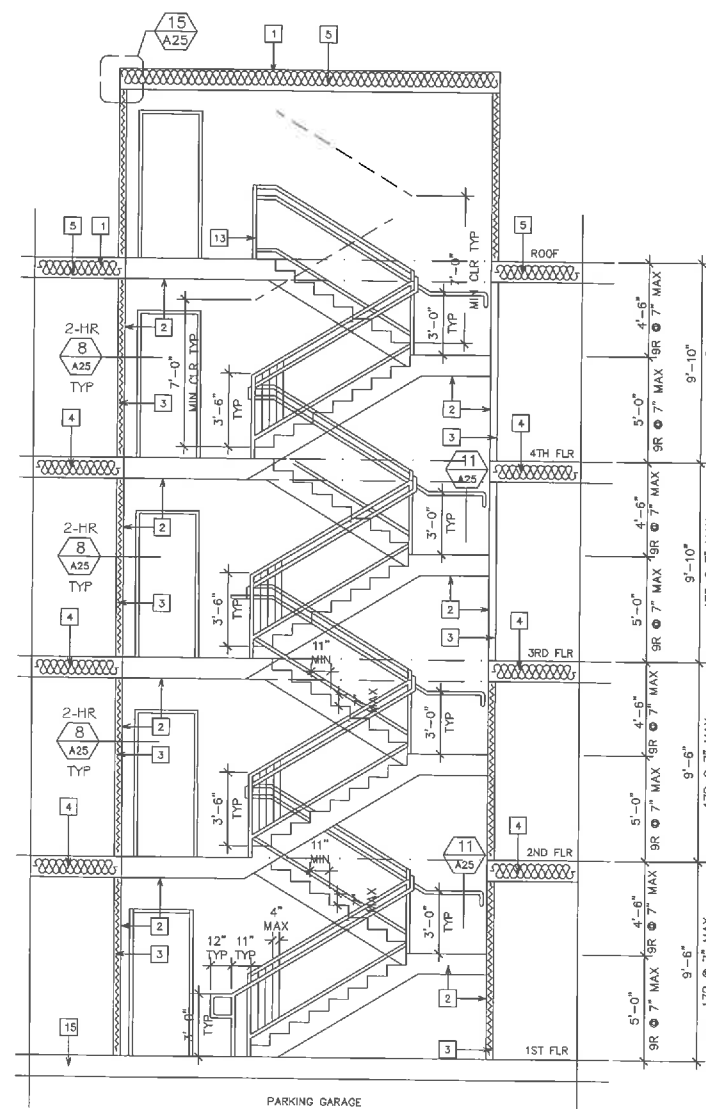
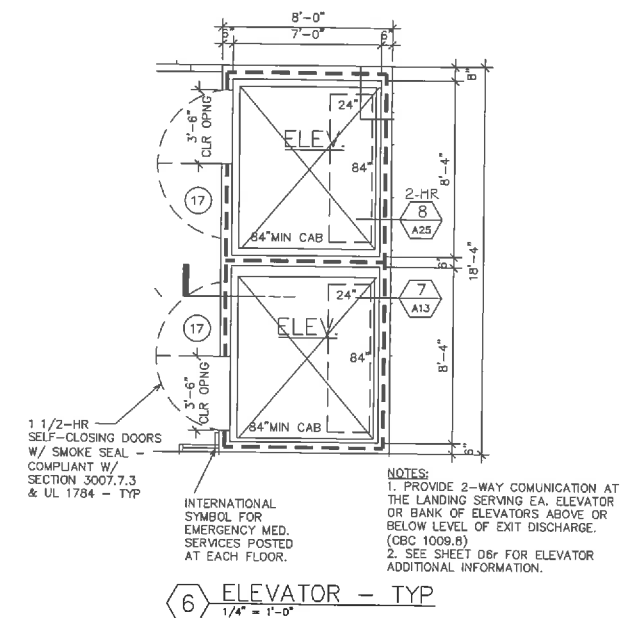
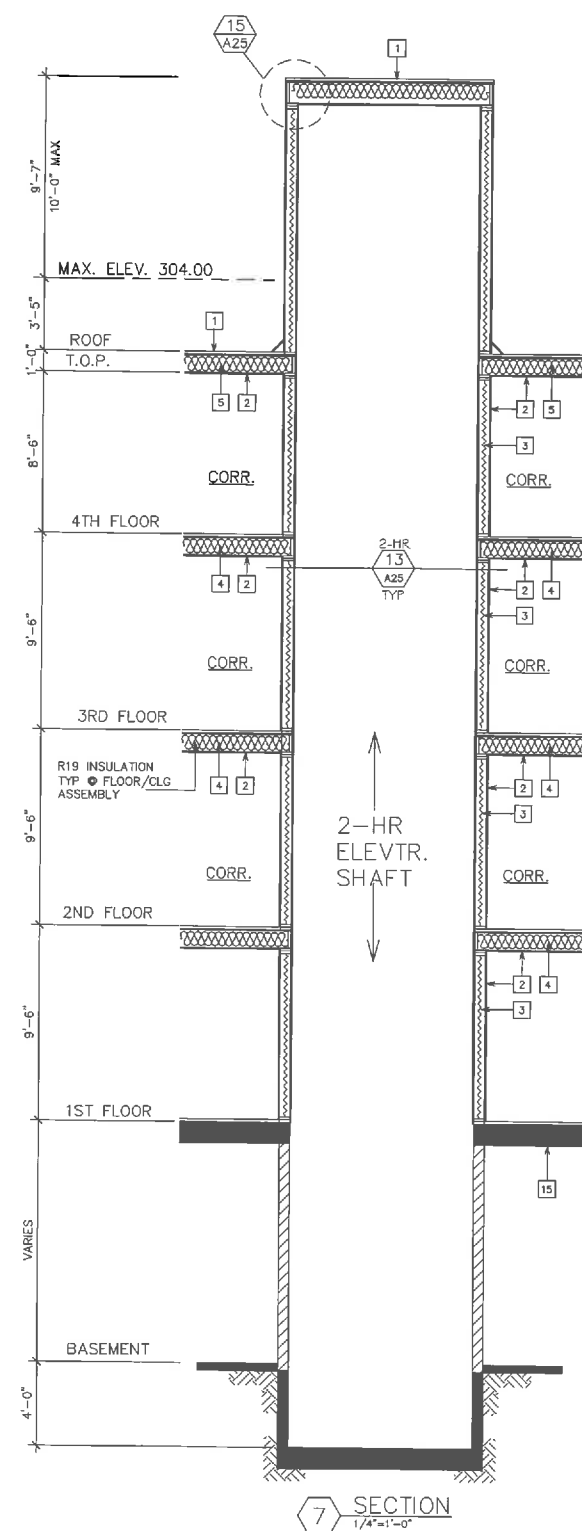


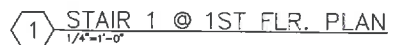
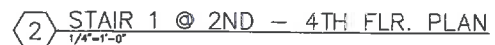
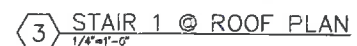
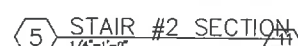
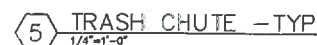
EXHIBIT "A"

Page No. 14 of 18

Case No. DM-209-4970-70

SECTION REFERENCE NOTES

- 1 BUILT-UP ROOF CLASS 'A'
- 2 5/8" GYP. D.W. TYP. 'X'
- 3 R-13 INSUL.
- 4 R-19 INSUL.
- 5 R-30 INSUL.
- 6 MACOAT DECK SYSTEM RR# 25983
- 7 W.I. RAILING
- 8 PROVIDE STAIRWAY ILLUMINATION MIN. 1-FT CANDLE @ TREAD RUNS, TYP
- 9 CLASS I, 4" STANDPIPE W/2 1/2" OUTLET, TYP
- 10 WARNING STRIPE AS PER 11A-6A/D6r, TYP.
- 11 27" HIGH RAILING TO PREVENT ENTRY
- 12 1/2" MAX THRESHOLD W/ BEVEL MAX 1:2, TYP
- 13 42" HIGH GUARDRAIL
- 14 27" HIGH GUARDRAIL AND GATE TO PREVENT ENTRY
- 15 CONC. DECK (3-HR)

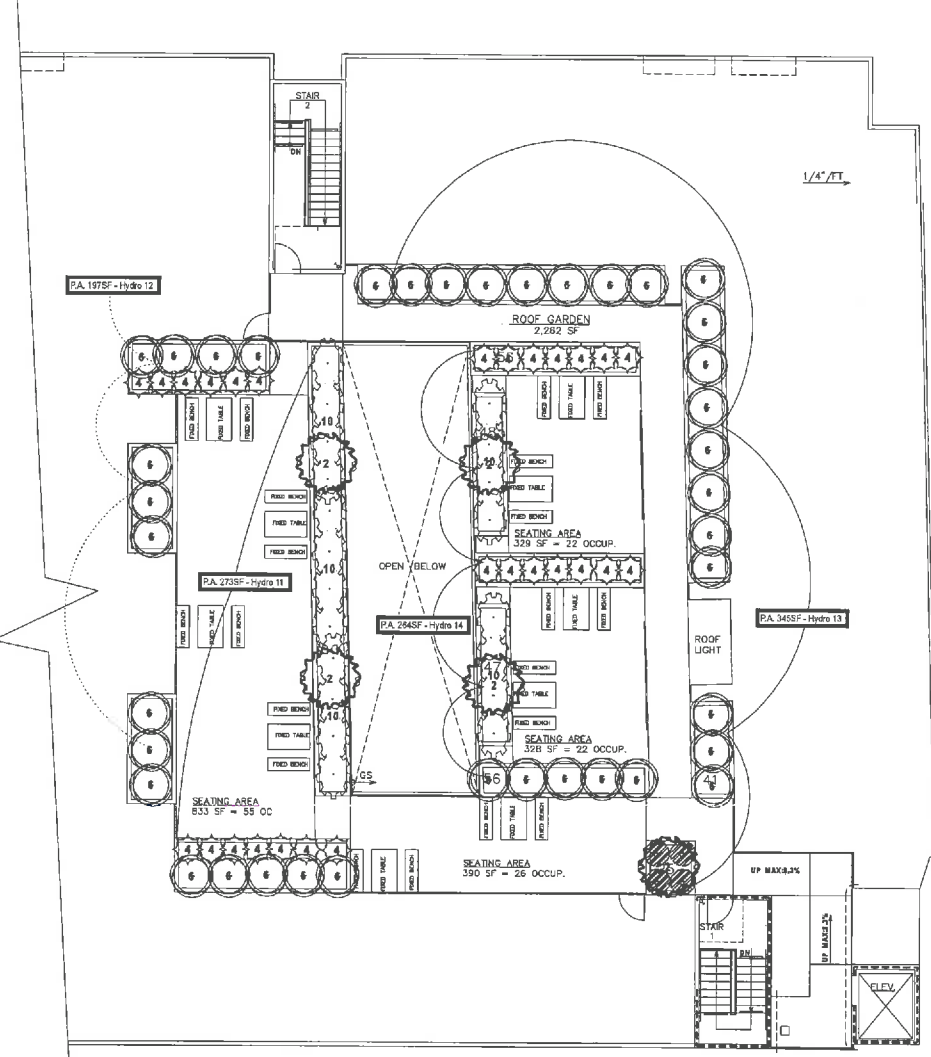


8 SIGN IN STAIRWAY
N.T.S.

Page No. 14 of 18
Case No. DM-2019-4920-70C

OH	CLASS I, 4" STANDPIPE W/2 1/2" OUTLET, TYP
PH	PANIC HARDWARE

A14



Page No. 16 of 16
Case No. DMC 7494930-JCC

PIN: 111B149 1352
TRACT: WALGROVE
BLOCK: 10, LOTS: 1, 2, & 3
MAP REF: M B 7-50



REVISION LOG:
08/01/2019

SUBMITTAL:
NO. DATE:

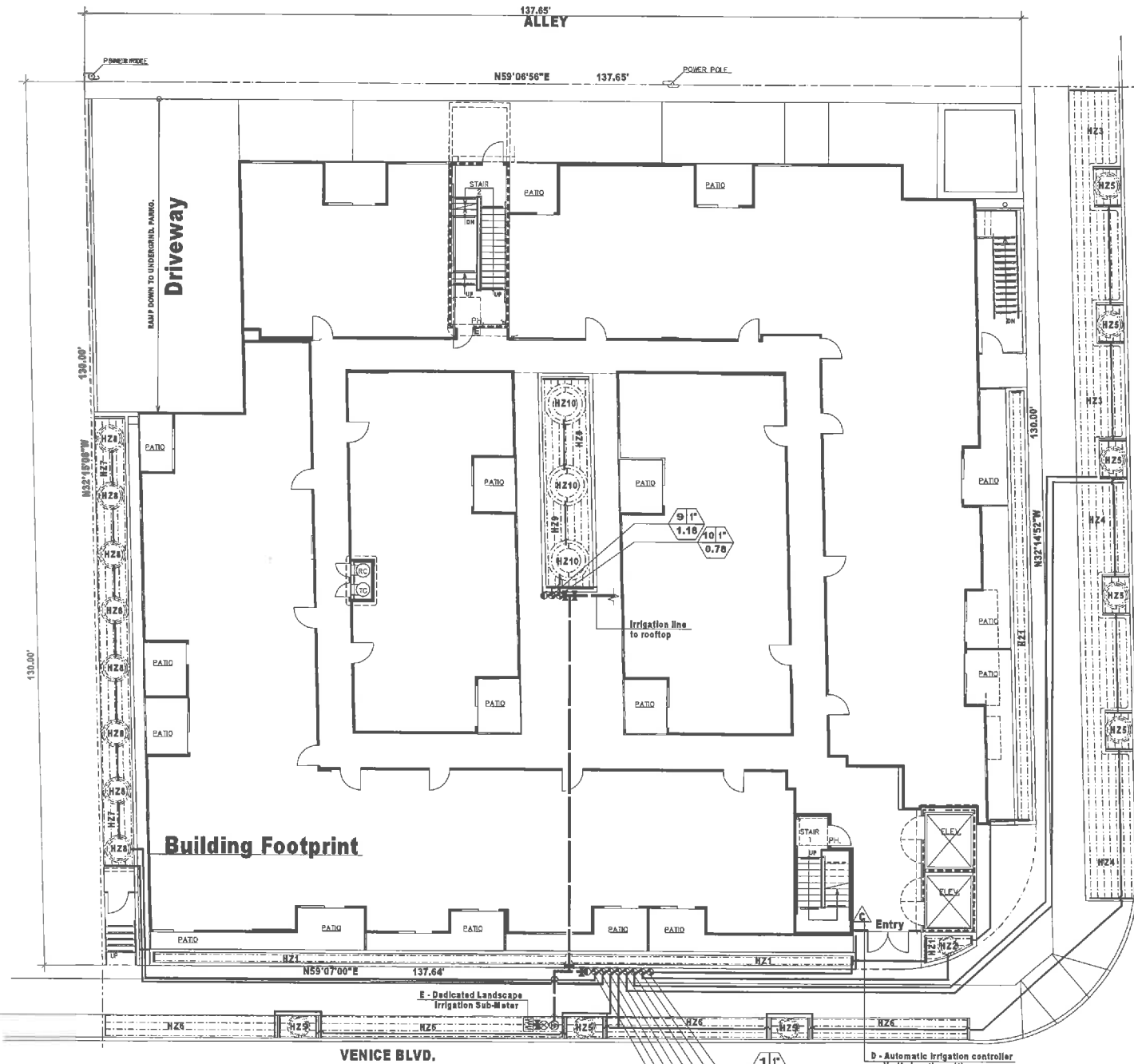
PROJECT ADDRESS:
1600 E. Venice Blvd
Los Angeles, CA 90291

OWNER ADDRESS:
WISEMAN DEVELOPMENT
11601 SANTA MONICA BLVD.
LOS ANGELES, CA 90025
(310) 914 - 5555

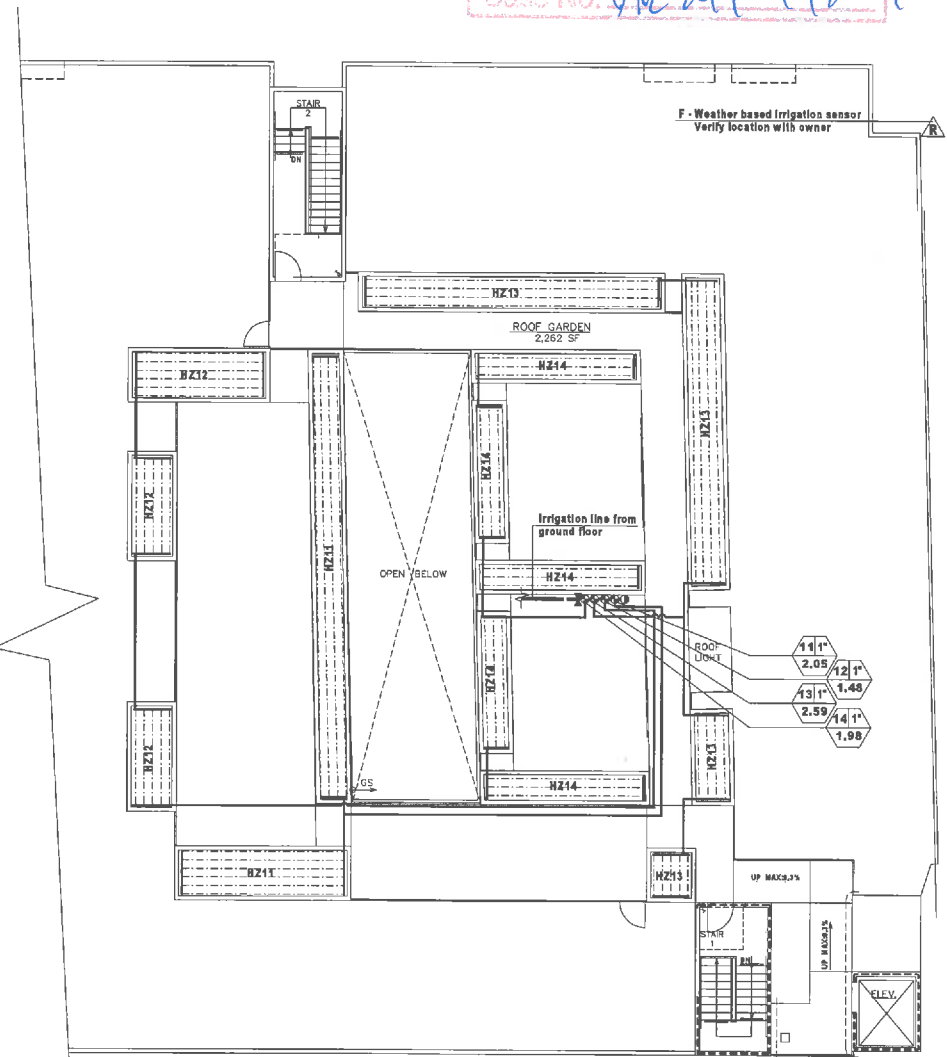
IRRIGATION PLAN

Date: 08/01/2019
Scale: 1" = 10'
Drawn By: PG

Sheet No.
L2
2 of 3



CELENCOE AVE.



ROOF DECK PLAN

HZ = Hydrozone
Zone No. 5
Flow rate in GPM 4.00
Valve size 1"

STATIC PRESSURE AT METER: 81-66 PSI PER MARK PATTERSON, LADWP (213) 367-0673

SYMBOL	DESCRIPTION	PART NO
1	DEDICATED LANDSCAPE IRRIGATION SUB-METER	1"
2	FEBCO BACKFLOW PREVENTER	MODEL 825Y
3	WILKINS PRESSURE REDUCING VALVE	800
4	HUNTER CONTROL ZONE VALVE KIT	PCZ-10-25
5	HUNTER RAIN SENSOR, CONDUIT MOUNT	SOLAR-SYNC
6	HUNTER I-CORE CONTROLLER	IC-600-M, size station of two (IC-600 exp. modules)
7	HOSE BIB - OWNER TO SELECT LOCATION	
8	NIBCO GATE VALVE	SIZE PER LINE
9	SCHED. 40 PVC LATERAL LINE	3/4"
10	SCHED. 40 PVC MAIN LINE	

VALVE NO.	SYM.	DESCRIPTION	PART NO	PRESSURE	GPM	APP. IN/HR	QTY	SPACING	SUBTOTAL	TOTAL GPM
1		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	230	16"	2.30	2.30
2		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	16	16"	0.16	0.16
3		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	267	16"	2.67	2.67
4		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	309	16"	3.09	3.09
5		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	285	16"	2.85	2.85
6		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	203	16"	2.03	2.03
7		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	138	16"	1.38	1.38
8		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	192	16"	1.92	1.92
9		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	118	16"	1.18	1.18
10		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	78	16"	0.78	0.78
11		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	208	16"	2.08	2.08
12		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	148	16"	1.48	1.48
13		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	259	16"	2.59	2.59
14		HUNTER PLO DRIP TUBING	PLO-06-12	30	0.01	0.72	198	16"	1.98	1.98

NOTES:
1. Pressure-regulating devices are required if water pressure is below or exceeds the recommended pressure of the specified irrigation devices.
2. Check valves or anti-siphon valves are required on all sprinkler heads where low-siphon drainage could occur.
3. Recirculating water systems shall be used for water features.
4. Locks shall be installed on all publicly accessible exterior faucets and hose bibs.
5. Piping contractor to install stake line for rooftop and upper floor irrigation.
6. A diagram of the irrigation plan showing hydrozones shall be kept with the irrigation controller for subsequent management purposes.
7. An impact audit report shall be completed at the time of final inspection.
8. I have complied with the criteria of the ordinance and applied them for the efficient use of water in the landscape design plans.
9. I agree to comply with the requirements of the water efficient landscape ordinance and submit a complete Landscape Documentation Package.
10. Signed: 08/01/2019

LEGAL DESCRIPTION:
APN: 4336-006-013, 014, 015
PIN: 1118149 1352
TRACT: WALGROVE
BLOCK: 10, LOTS: 1, 2, & 3
MAP REF: N B 7 - 59

EXHIBIT D
ENVIRONMENTAL CLEARANCE (ENV-2019-4921-CE)
DIR-2019-4920-TOC-1A

CALIFORNIA ENVIRONMENTAL QUALITY ACT NOTICE OF EXEMPTION

(PRC Section 21152; CEQA Guidelines Section 15062)

Filing of this form is optional. If filed, the form shall be filed with the County Clerk, 12400 E. Imperial Highway, Norwalk, CA 90650, pursuant to Public Resources Code Section 21152(b) and CEQA Guidelines Section 15062. Pursuant to Public Resources Code Section 21167 (d), the posting of this notice starts a 35-day statute of limitations on court challenges to reliance on an exemption for the project. Failure to file this notice as provided above, results in the statute of limitations being extended to 180 days.

PARENT CASE NUMBER(S) / REQUESTED ENTITLEMENTS

DIR-2019-4920-TOC

LEAD CITY AGENCY

City of Los Angeles (Department of City Planning)

CASE NUMBER

ENV-2019-4921-CE

PROJECT TITLE

COUNCIL DISTRICT

11

PROJECT LOCATION (Street Address and Cross Streets and/or Attached Map)

☐ Map attached.

1600-1614 East Venice Boulevard

PROJECT DESCRIPTION:

☐ Additional page(s) attached.

Demolition of three residential structures (nine dwelling units) and construction of a 61-foot tall, four-story, 77-unit, 49,948 square-foot apartment building with one level of subterranean parking.

NAME OF APPLICANT / OWNER:

Venice Wave LP

CONTACT PERSON (If different from Applicant/Owner above)

Jeffrey Khau

(AREA CODE) TELEPHONE NUMBER

(213) 473-9987

EXT.

EXEMPT STATUS: (Check all boxes, and include all exemptions, that apply and provide relevant citations.)

STATE CEQA STATUTE & GUIDELINES

☐ STATUTORY EXEMPTION(S)

Public Resources Code Section(s) _____

☒ CATEGORICAL EXEMPTION(S) (State CEQA Guidelines Sec. 15301-15333 / Class 1-Class 33)

CEQA Guideline Section(s) / Class(es) **Section 15332 (Class 32)**

☐ OTHER BASIS FOR EXEMPTION (E.g., CEQA Guidelines Section 15061(b)(3) or (b)(4) or Section 15378(b))

JUSTIFICATION FOR PROJECT EXEMPTION:

☐ Additional page(s) attached

The Planning Department determined that the State CEQA Guidelines designate the subject project as Categorically Exempt under Article 19, Section 15332, Class 32, Case No. ENV-2019-4921-CE. A project qualifies for a Class 32 Categorical Exemption if it is developed on an infill site and meets the following criteria:

a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with the applicable zoning designation and regulations.

The site currently is developed with a 3-unit apartment building, a 4-unit apartment building, and a 2-unit apartment building. The site is zoned [Q]R4-1XL and has a General Plan Land Use designation of High Medium Residential. The project proposes the construction of a three-story residential building containing 77 dwelling units. As proposed, the project is conformance with the General Plan Zoning and Land Use designation.

b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.

The subject site is wholly within the City of Los Angeles. Lots surrounding the subject site are developed with multi-family buildings and single-family residences.

c) The project site has no value as a habitat for endangered, rare, or threatened species.

The site is not a wildland area, and is not inhabited by endangered, rare, or threatened species. The area around the site is highly urbanized and surrounded by commercial and residential uses. NavigateLA shows that the subject site is not located in a Significant Ecological Area.

d) **Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.**

The project will be subject to Regulatory Compliance Measures (RCMs), which require compliance with the City of Los Angeles Noise Ordinance for pollutant discharge, dewatering, and stormwater mitigations; and Best Management Practices for stormwater runoff. More specifically, RCMs include but are not limited to:

- **Regulatory Compliance Measure RC-AQ-1 (Demolition, Grading and Construction Activities): Compliance with provisions of the SCAQMD District Rule 403.** The project shall comply with all applicable standards of the Southern California Air Quality Management District, including the following provisions of District Rule 403:
 - All unpaved demolition and construction areas shall be wetted at least twice daily during excavation and construction, and temporary dust covers shall be used to reduce dust emissions and meet SCAQMD District Rule 403. Wetting could reduce fugitive dust by as much as 50 percent.
 - The construction area shall be kept sufficiently dampened to control dust caused by grading and hauling, and at all times provide reasonable control of dust caused by wind.
 - All clearing, earth moving, or excavation activities shall be discontinued during periods of high winds (i.e., greater than 15 mph), to prevent excessive amounts of dust.
 - All dirt/soil loads shall be secured by trimming, watering or other appropriate means to prevent spillage and dust.
 - All dirt/soil materials transported off-site shall be either sufficiently watered or securely covered to prevent excessive amount of dust.
 - General contractors shall maintain and operate construction equipment to minimize exhaust emissions.
 - Trucks having no current hauling activity shall not idle but be turned off.
- **Regulatory Compliance Measure RC-GEO-1 (Seismic):** The design and construction of the project shall conform to the California Building Code seismic standards as approved by the Department of Building and Safety.
- **Regulatory Compliance Measure RC-NO-1 (Demolition, Grading, and Construction Activities):** The project shall comply with the City of Los Angeles Noise Ordinance and any subsequent ordinances, which prohibit the emission or creation of noise beyond certain levels at adjacent uses unless technically infeasible.

These RCMs will reduce any potential impacts on noise and water quality to less than significant. The creation of noise is limited to certain decibels, restricted to specific hours. A Noise Impact Analysis, prepared by Cadence Environmental, dated September 25, 2019, determined that the proposed development would not exceed the limits stated in the Noise Ordinance. Regarding traffic and air quality impacts, DOT staff determined that the project requires a traffic assessment study. A traffic study dated September 3, 2019 was prepared by Overland Traffic Consultants. The Department of Transportation (LADOT) issued a Traffic Assessment, dated February 4, 2020, indicating the project would generate a net increase of 341 trips. The Traffic Assessment included a VMT analysis that determined the project would not have a significant transportation impact under any of the above thresholds.

The project will not conflict with any adopted policies, plans, or programs regarding public transit, bicycle facilities, or pedestrian facilities. Therefore, the project will not have any significant impacts to traffic. Likewise, air quality will not worsen as a result of the proposed project. Interim thresholds were developed by DCP staff based on CalEEMod model runs relying on reasonable assumptions, consulting with AQMD staff, and surveying published air quality studies for which criteria air pollutants did not exceed the established SCAQMD construction and operational thresholds. The Air Quality Impact Analysis prepared by Cadence Environmental Consultants, dated September 24, 2019, concluded the project would not result in impacts to air quality. Regarding water quality, the proposed project will utilize existing municipal water sources through the Department of Water and Power (LADWP). The project is subject to the City's Low Impact Development Ordinance (Ordinance 181,889), which requires the capture and retention of storm water through onsite filtration and treatment. As such, the project has been designed to meet the City's Low Impact Development (LID) requirements.

- e) The project site will be adequately served by all public utilities and services given that the property is located in an urban tract with water supply, sewage and waste disposal infrastructure, and power lines installed. Venice Boulevard and Glencoe Avenue are improved with existing utilities that service the lots in the area. Both can be accessed by emergency vehicles. The project includes a street dedication to widen the pedestrian right-of-way. The project will also repair and replace any broken or off-grade asphalt, sidewalk, curb, or gutter. The project shall comply with any street light requirements required by the Bureau of Street Lighting. The proposed project will not result in significant impacts on the capacity of existing utilities and services.

The project is a transit-oriented development on a site within an urbanized area and meets the criteria outlined above. Therefore the project qualifies for a Class 32 Categorical Exemption. Furthermore, the Exceptions outlined in the State CEQA Guidelines Section 15300.2 do not apply to the project:

- a) **Cumulative Impact.** There is not a succession of known projects of the same type located in the same place as the subject project. No comparable pending projects (TOC or Density Bonus) have been identified within a 500-foot radius of the subject site. One TOC project located approximately 0.33 miles away at 1015 East Venice Boulevard (DIR-2017-4421-TOC) was approved on September 7, 2018. Another TOC project, located at 1808-1816 South Lincoln Boulevard (DIR-2019-1133-TOC), was approved on February 20, 2020. Since these projects qualify for a Class 32 Categorical Exemption and are subject to Regulatory Compliance Measures, no cumulative impacts are anticipated. Further, the proposed 77-unit residence does not exceed thresholds identified for impacts to the area (i.e. traffic, noise, etc.) and will not result in significant cumulative impacts.
- b) **Significant Effect Due to Unusual Circumstances.** A categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances. The project

proposes a multi-family building in an area zoned and designated for such use. Adjacent lots are developed with multi- and single-family residences. No unusual circumstances are present or foreseeable.

- c) Scenic Highways. The project site is not located on or near a designated state scenic highway. The only State Scenic Highway within the City of Los Angeles is the Topanga Canton State Scenic Highway, State Route 27, which travels through a portion of Topanga State Park.
- d) Hazardous Waste Sites. The project site is not identified as a hazardous waste site or is on any list compiled pursuant to Section 65962.5 of the Government Code.
- e) Historical Resources. The project site has not been identified as a historic resource by local or state agencies, and the project site has not been determined to be eligible for listing in the National Register of Historic Places, California Register of Historical Resources, the Los Angeles Historic-Cultural Monuments Register, and/or any local register. The site was not found to be a potential historic resource based on the City's HistoricPlacesLA website or SurveyLA, the citywide survey of Los Angeles. The City does not treat the site as a historic resource. Based on this, the project will not result in a substantial adverse change to the significance of a historic resource.

☒ None of the exceptions in CEQA Guidelines Section 15300.2 to the categorical exemption(s) apply to the Project.

☐ The project is identified in one or more of the list of activities in the City of Los Angeles CEQA Guidelines as cited in the justification.

IF FILED BY APPLICANT, ATTACH CERTIFIED DOCUMENT ISSUED BY THE CITY PLANNING DEPARTMENT STATING THAT THE DEPARTMENT HAS FOUND THE PROJECT TO BE EXEMPT.

If different from the applicant, the identity of the person undertaking the project.

CITY STAFF USE ONLY:

CITY STAFF NAME AND SIGNATURE

Juliet Oh

STAFF TITLE

Senior City Planner

ENTITLEMENTS APPROVED

Transit Oriented Communities (TOC) Affordable Housing Incentive Program Compliance Review, Community Design Overlay (CDO) Plan Review

FEE:

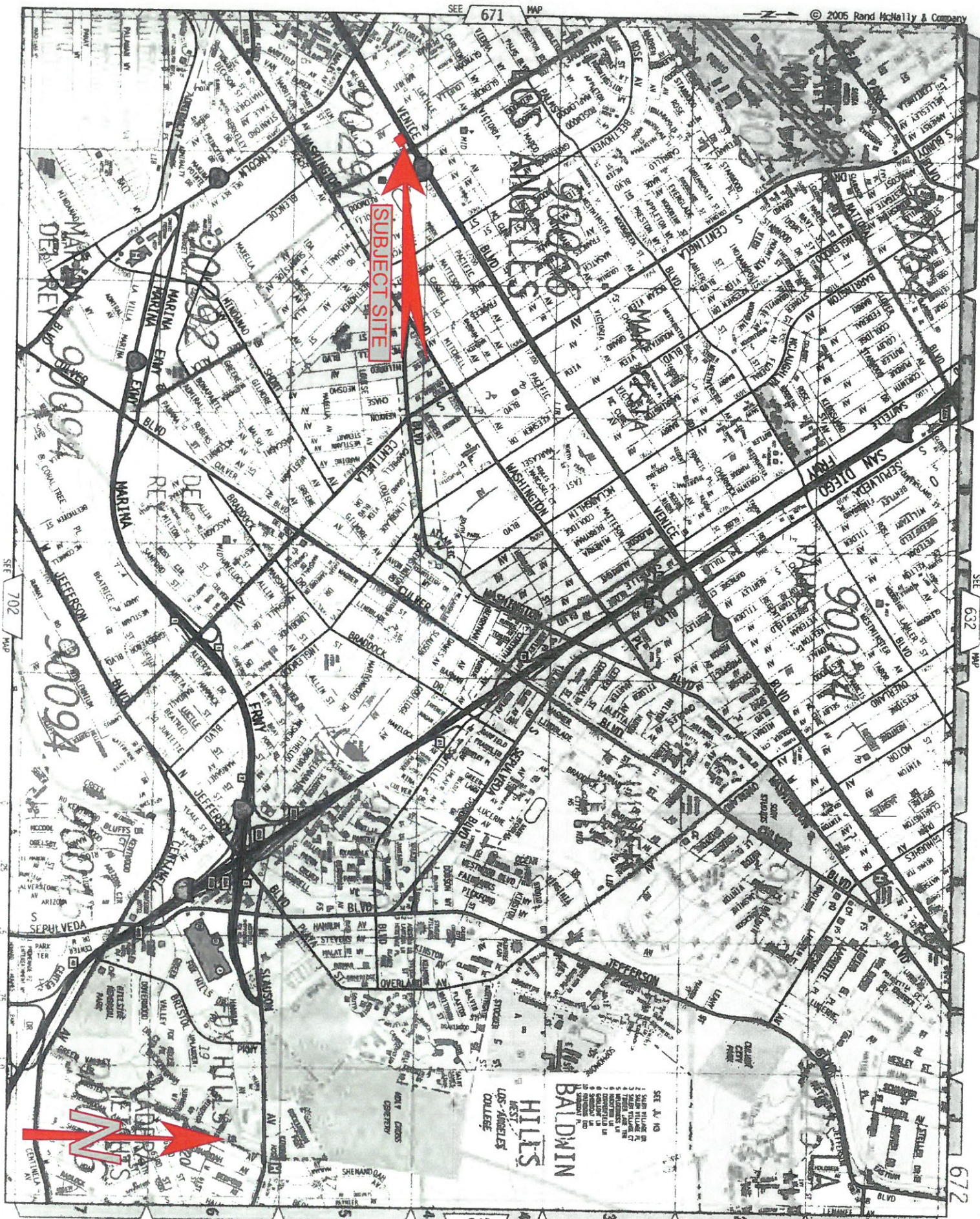
RECEIPT NO.

REC'D. BY (DCP DSC STAFF NAME)

DISTRIBUTION: County Clerk, Agency Record

Rev. 3-27-2019

EXHIBIT E
MAPS, ZIMAS PARCEL PROFILE REPORT,
AND SITE PHOTOS
DIR-2019-4920-TOC-1A





City of Los Angeles Department of City Planning

8/19/2019 PARCEL PROFILE REPORT

PROPERTY ADDRESSES

1614 E VENICE BLVD
1610 E VENICE BLVD

ZIP CODES

90291

RECENT ACTIVITY

ADM-2018-7025-TOC
ADM-2019-1208-TOC
DIR-2019-4920-TOC
ENV-2019-4921-EAF
PAR-2019-3781-TOC

CASE NUMBERS

CPC-2018-7548-CPU
CPC-2014-1456-SP
CPC-2005-8252-CA
CPC-1992-446-HD
CPC-1992-318-ZC
CPC-1984-226
ORD-186104
ORD-183497
ORD-169327
ORD-168999
ENV-2014-1458-EIR-SE-CE
ENV-2005-8253-ND

Address/Legal Information

PIN Number	111B149 1332
Lot/Parcel Area (Calculated)	5,963.4 (sq ft)
Thomas Brothers Grid	PAGE 672 - GRID A5
Assessor Parcel No. (APN)	4236006015
Tract	WALGROVE TRACT
Map Reference	M B 7-50
Block	10
Lot	3
Arb (Lot Cut Reference)	None
Map Sheet	111B149

Jurisdictional Information

Community Plan Area	Venice
Area Planning Commission	West Los Angeles
Neighborhood Council	Venice
Council District	CD 11 - Mike Bonin
Census Tract #	2738.00
LADBS District Office	West Los Angeles

Planning and Zoning Information

Special Notes	None
Zoning	[Q]R4-1XL
Zoning Information (ZI)	ZI-2452 Transit Priority Area in the City of Los Angeles ZI-1874 Los Angeles Coastal Transportation Corridor ZI-2479 Livable Boulevards
General Plan Land Use	High Medium Residential
General Plan Note(s)	Yes
Hillside Area (Zoning Code)	No
Specific Plan Area	Los Angeles Coastal Transportation Corridor
Subarea	None
Special Land Use / Zoning	None
Design Review Board	No
Historic Preservation Review	No
Historic Preservation Overlay Zone	None
Other Historic Designations	None
Other Historic Survey Information	None
Mills Act Contract	None
CDO: Community Design Overlay	None
CPIO: Community Plan Imp. Overlay	None
Subarea	None
CUGU: Clean Up-Green Up	None
HCR: Hillside Construction Regulation	No
NSO: Neighborhood Stabilization Overlay	No
POD: Pedestrian Oriented Districts	None
RFA: Residential Floor Area District	None
RIO: River Implementation Overlay	No
SN: Sign District	No
Streetscape	Livable Boulevards

This report is subject to the terms and conditions as set forth on the website. For more details, please refer to the terms and conditions at zimas.lacity.org
(*) - APN Area is provided "as is" from the Los Angeles County's Public Works, Flood Control, Benefit Assessment.

Adaptive Reuse Incentive Area	None
Affordable Housing Linkage Fee	
Residential Market Area	High
Non-Residential Market Area	High
Transit Oriented Communities (TOC)	Tier 1
CRA - Community Redevelopment Agency	None
Central City Parking	No
Downtown Parking	No
Building Line	None
500 Ft School Zone	Active: Venice Senior High School Active: Venice Senior High School (Foreign Language Magnet) Active: Venice Senior High School (STEMM Magnet)
500 Ft Park Zone	No
Assessor Information	
Assessor Parcel No. (APN)	4236006015
Ownership (Assessor)	
Owner1	STUHLBARG, EMILY CO TR
Address	0 PO BOX 252012 LOS ANGELES CA 90025
Ownership (Bureau of Engineering, Land Records)	
Owner	1600 VENICE LLC
Address	PO BOX 252012 LOS ANGELES CA 90025
Owner	HEMRICK, ROSALINDA
Address	2488 GLYNDON AVE VENICE CA 90291
Owner	STUHLBARG, EMILY NORENE, RICHARD A
Address	2214 TORRANCE BLVD STE 200 TORRANCE CA 90501
APN Area (Co. Public Works)*	0.137 (ac)
Use Code	0200 - Residential - Double, Duplex, or Two Units - 4 Stories or Less
Assessed Land Val.	\$43,269
Assessed Improvement Val.	\$11,184
Last Owner Change	08/10/2011
Last Sale Amount	\$500,005
Tax Rate Area	67
Deed Ref No. (City Clerk)	1076180 1-853 0201707 0201706
Building 1	
Year Built	1914
Building Class	D3D
Number of Units	1
Number of Bedrooms	2
Number of Bathrooms	1
Building Square Footage	1,240.0 (sq ft)
Building 2	
Year Built	1939
Building Class	D35C
Number of Units	1
Number of Bedrooms	1
Number of Bathrooms	1
Building Square Footage	360.0 (sq ft)
Building 3	No data for building 3
Building 4	No data for building 4

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 (*) - APN Area is provided "as is" from the Los Angeles County's Public Works, Flood Control, Benefit Assessment.

Building 5 No data for building 5

Additional Information

Airport Hazard	None
Coastal Zone	None
Farmland	Area Not Mapped
Urban Agriculture Incentive Zone	YES
Very High Fire Hazard Severity Zone	No
Fire District No. 1	No
Flood Zone	None
Watercourse	No
Hazardous Waste / Border Zone Properties	No
Methane Hazard Site	None
High Wind Velocity Areas	No
Special Grading Area (BOE Basic Grid Map A-13372)	No
Wells	None

Seismic Hazards

Active Fault Near-Source Zone

Nearest Fault (Distance in km)	5.1492912
Nearest Fault (Name)	Santa Monica Fault
Region	Transverse Ranges and Los Angeles Basin
Fault Type	B
Slip Rate (mm/year)	1.00000000
Slip Geometry	Left Lateral - Reverse - Oblique
Slip Type	Moderately / Poorly Constrained
Down Dip Width (km)	13.00000000
Rupture Top	0.00000000
Rupture Bottom	13.00000000
Dip Angle (degrees)	-75.00000000
Maximum Magnitude	6.60000000

Alquist-Priolo Fault Zone	No
Landslide	No
Liquefaction	No
Preliminary Fault Rupture Study Area	No
Tsunami Inundation Zone	No

Economic Development Areas

Business Improvement District	None
Hubzone	Not Qualified
Opportunity Zone	No
Promise Zone	None
State Enterprise Zone	None

Housing

Direct all Inquiries to	Housing+Community Investment Department
Telephone	(866) 557-7368
Website	http://hcidla.lacity.org
Rent Stabilization Ordinance (RSO)	Yes
Ellis Act Property	No

Public Safety

Police Information

Bureau	West
Division / Station	Pacific
Reporting District	1443

Fire Information

Bureau	West
Battalion	4

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District / Fire Station
Red Flag Restricted Parking

63
No

This report is subject to the terms and conditions as set forth on the website. For more details, please refer to the terms and conditions at zimas.lacity.org
(*) - APN Area is provided "as is" from the Los Angeles County's Public Works, Flood Control, Benefit Assessment.

CASE SUMMARIES

Note: Information for case summaries is retrieved from the Planning Department's Plan Case Tracking System (PCTS) database.

Case Number:	CPC-2018-7548-CPU
Required Action(s):	CPU-COMMUNITY PLAN UPDATE
Project Descriptions(s):	ADOPT COMMUNITY PLAN POLICY DOCUMENT, GENERAL PLAN AMENDMENTS, AND ZONE CHANGES TO APPLY RE-CODE LA ZONING.
Case Number:	CPC-2014-1456-SP
Required Action(s):	SP-SPECIFIC PLAN (INCLUDING AMENDMENTS)
Project Descriptions(s):	SPECIFIC PLAN AMENDMENT
Case Number:	CPC-2005-8252-CA
Required Action(s):	CA-CODE AMENDMENT
Project Descriptions(s):	AN ORDINANCE ESTABLISHING PERMANENT REGULATIONS IMPLEMENTING THE MELLO ACT IN THE COASTAL ZONE.
Case Number:	CPC-1992-446-HD
Required Action(s):	HD-HEIGHT DISTRICT
Project Descriptions(s):	ZC & HD ENCOMPASSING THE REGULATORY PROVISIONS OF ORD NO 166,513, THE EAST VENICE MULTI-FAMILY ICO TO CONTROL MULTI-FAMILY RESIDENTIAL DEVELOPMENT ON THE MULTI-FAMILY & COMMERCIAL ZONED PROPERTIES IN THE EAST VENICE & WEST MAR VISTA NEIGHBORHOODS, & REGULATE HEIGHTS IN SINGLE-FAMILY AREAS OF THOSE NEIGHBORHOODS
Case Number:	CPC-1992-318-ZC
Required Action(s):	ZC-ZONE CHANGE
Project Descriptions(s):	ZONE CHANGES ENCOMPASSING THE REGULATORY PROVISIONS OF ORD. NO. 166,513, THE EAST VENICE MULTI-FAMILY INTERIM CONTROL ORDINANCE TO CONTROL MULTI-FAMILY RESIDENTIAL DEVELOPMENT ON THE MULTI-FAMILY AND COMMERCIAL ZONED PROPERTIES IN THE EAST VENICE AND WEST MAR VISTA NEIGHBORHOODS, AND TO REGULATE HEIGHTS IN SINGLE-FAMILY AREAS OF THOSE NEIGHBORHOODS
Case Number:	CPC-1984-226
Required Action(s):	Data Not Available
Project Descriptions(s):	AMENDMENT TO THE COASTAL TRANSPORTATION CORRIDOR SPECIFIC PLAN THE PROPERTY LOCATION IS GENERALLY BOUNDED ON THE EAST BY THE SAN DIEGO FWY; ON THE SOUTH BY THE CITY BOUNDARY OF THE CITY OF EL SEGUNDO; THE NORTH BY THE CITY BOUNDARY OF THE CITY OF SANTA MONICA AND ON THE WEST BY THE PACIFIC OCEAN PROPOSED PROJECT BROAD AMENDMENTS TO THE COASTAL TRANSPORTATION CORRIDOR SPECIFIC PLAN
Case Number:	ENV-2014-1458-EIR-SE-CE
Required Action(s):	SE-STATUTORY EXEMPTIONS CE-CATEGORICAL EXEMPTION EIR-ENVIRONMENTAL IMPACT REPORT
Project Descriptions(s):	ENVIRONMENTAL IMPACT REPORT
Case Number:	ENV-2005-8253-ND
Required Action(s):	ND-NEGATIVE DECLARATION
Project Descriptions(s):	AN ORDINANCE ESTABLISHING PERMANENT REGULATIONS IMPLEMENTING THE MELLO ACT IN THE COASTAL ZONE.

DATA NOT AVAILABLE

ORD-186104

ORD-183497

ORD-169327

ORD-168999

08/19/2019

LARIAC5 2017 Color-Ortho

ZIMAS INTRANET

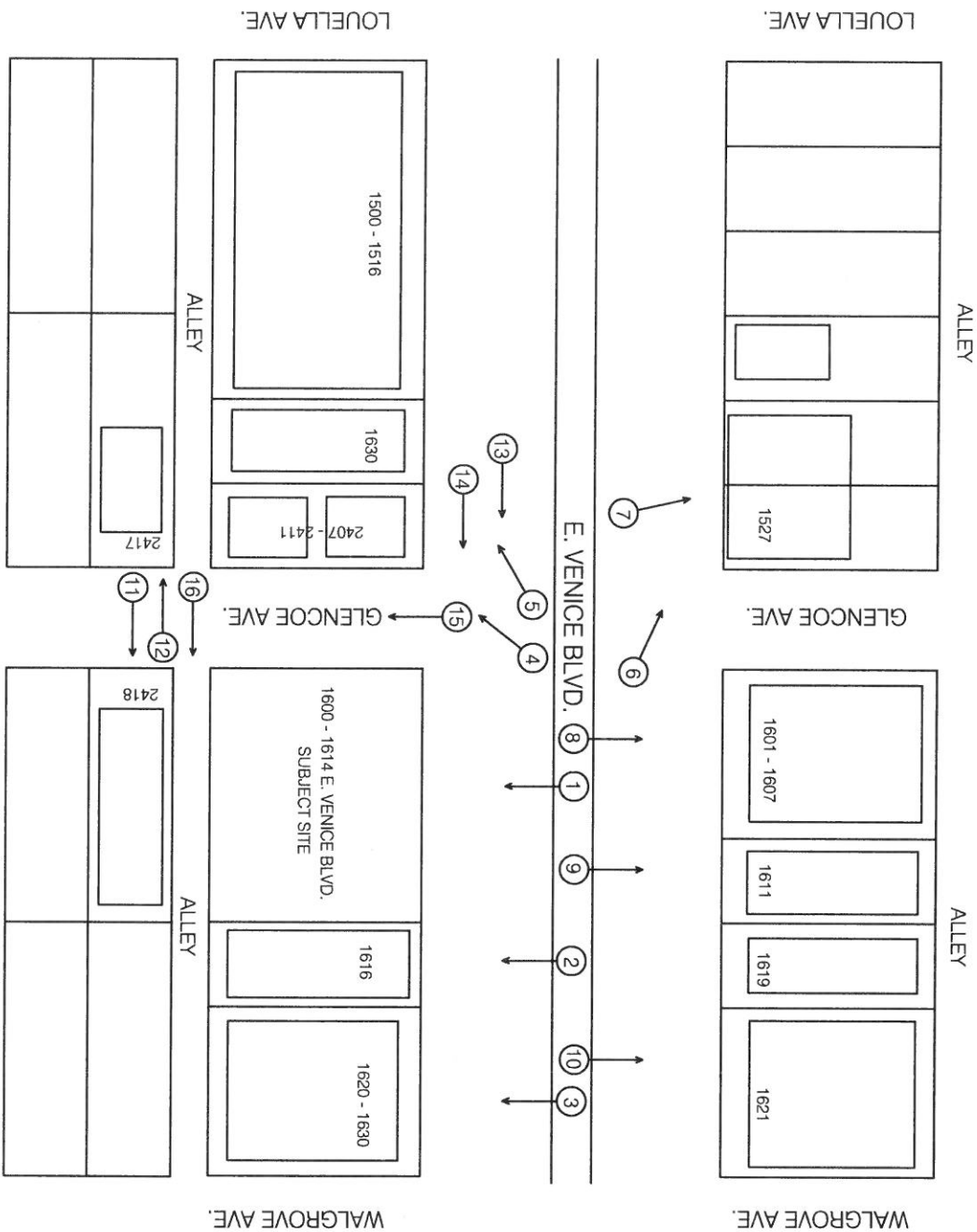


Address: 1614 E VENICE BLVD
APN: 4236006015
PIN #: 111B149 1332

Tract: WALGROVE TRACT
Block: 10
Lot: 3
Arb: None

Zoning: [Q]R4-1XL
General Plan: High Medium Residential

PICTURE INDEX



PROJECT ADDRESS:
1600 E. VENICE BLVD.
LOS ANGELES, CA. 90291





(1) 1600-1614 E. Venice blvd (Subject site)



(2) 1616 E. Venice blvd. (East adjacent property)



(3) 1620 -1630 E. Venice blvd. (Multi-residential bldg. on the further East)



(4) 2407 -2411 Glencoe Ave. (Duplex on the West across subject site at Glencoe Ave.)



(5) 1500 – 1530 E. Venice Blvd. (Multi-residential bldgs. Further West)



(6) View across Venice blvd further West



(7) 1527 Venice Blvd. (Mini Mart Across Venice Blvd.)



(8) 1601-1605 E. Venice Blvd. (Apartment bldg. t across subject site)



(9) 1611 E. Venice Blvd. (Apartment across Subject site)



(10) 1621 E. Venice Blvd (Apartment across the subject site)



(11) 2418 Glencoe Ave (Single Family home across the alley of the subject site)



(12) 2417 Glencoe Ave. (Across the subject site at Glencoe Ave.



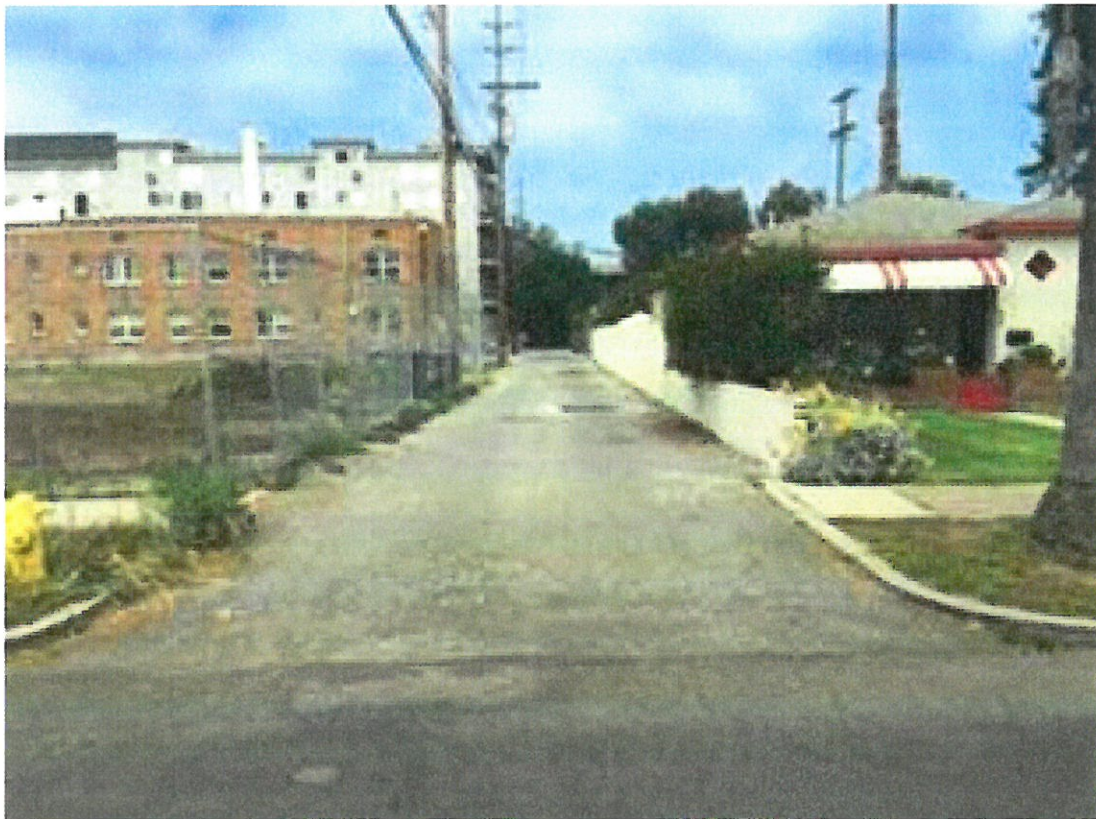
(13) View of E. Venice Blvd in-front of subject site



(14) Another view of Venice Blvd



(15) View of Glencoe Ave. at the side of the subject site



(16) View of the Alley at the back of the subject site

EXHIBIT F
AGENCY CORRESPONDENCE
DIR-2019-4920-TOC-1A

PAR-2019-3781-TOC



REFERRAL FORMS:

TRANSIT-ORIENTED COMMUNITIES - REFERRAL FORM
LOS ANGELES CITY PLANNING DEPARTMENT

This form is to serve as a referral to the Department of City Planning Development Services Center for Affordable Housing case filing purposes (in addition to the required Department of City Planning Application and any other necessary documentation) and as a referral to HCIDLA, CRA, Building and Safety, or other City agency for project status and entitlement need purposes. This form shall be completed by the applicant and reviewed and signed by Department of City Planning staff prior to filing an application for a case or building permit. Any modifications to the content(s) of this form after its authorization by the Department of City Planning staff is prohibited. The Department of City Planning reserves the right to require an updated form for the project if more than 180 days have transpired since the approval date, or as necessary, to reflect project modifications, policy changes and/or amendments to the LAMC, local laws, and State laws.

CITY STAFF USE ONLY

Referral To: <input checked="" type="checkbox"/> Planning DSC - Filing <input type="checkbox"/> HCIDLA <input type="checkbox"/> DBS <input type="checkbox"/> Funding <input type="checkbox"/> SB35 <input type="checkbox"/> Other:	
NOTES: Pending LADBS zoning letter confirmation of Venice Blvd as front yard project planner to evaluate recent demolition for CEQA process.	
Planning Staff Name and Title Navali Martinez, City Planning Associate	Planning Staff Signature [Signature]
Date Approved 8/8/19	Expiration Date 2/3/2020

I. Project Information – To be completed by applicant

1. PROJECT LOCATION/ ZONING

Project Address: 1600 E. VENICE BLVD.
Applicant Name and Phone/Email: ISAAC COHANZAD (310)-914-5555, ISAAC@WISEMANRESIDENTIAL.COM
Assessor Parcel Number(s): 4236-006-013, ...014, & ...015
Community Plan: VENICE Number of Lots: 3 Lot Size: 17,891 s.f.
Existing Zone: IQR4-1XL Land Use Designation: HIGH MEDIUM RESIDENTIAL
☐ Specific Plan ☐ HPOZ ☐ DRB ☐ Enterprise Zone ☐ CRA ☐ CPIO
☒ Q-condition/ D-limitation/ T-classification (please specify): ORD-169327
☒ Other pertinent zoning information (please specify): Livable Boulevards Streetscape
☒ Location of Major Transit Stop (please specify the intersection or metro stop):
VENICE BLVD / LINCOLN BLVD (Local line 33 / R733)

II. Project Eligibility – To be completed by DCP Housing Services Unit Staff

2. TRANSPORTATION QUALIFIERS

Qualifier #1 (rail name & stop, ferry terminal or bus #): Local line 33 + R733
Service Interval # 1: 12.4 [420 min / # of trips]²
Service Interval # 2: 12.4 [420 min / # of trips]
Qualifier #2 (rail name & stop, ferry terminal or bus #): Santa Monica Rapid Line 3
Service Interval # 1: 13.12 [420 min / # of trips]
Service Interval # 2: 12.72 [420 min / # of trips]
TOC Tier³: ☐ Tier 1 ☒ Tier 2 ☐ Tier 3 ☐ Tier 4 Planning Staff Initials: NM

¹ Per AB 744, A Major Transit Stop means a site containing an existing rail transit station, a ferry terminal served by either a bus or rail transit service, or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods. It also includes major transit stops that are included in the applicable regional transportation plan.
² This figure (420 minutes) is based on the total number of minutes during the peak hours of 6 am to 9 am as well as 3 pm to 7 pm.
³ If project is 100% affordable, it is eligible for the designated Tier to be increased by one.

III. Project Information (if applicant is requesting additional incentives) – To be completed by applicant

3. DESCRIPTION OF PROPOSED PROJECT

Proposed 77 unit apartment bldg, 4 story over subterranean garage with 60% density bonus under TOC TIER 2 [Base density of 48 units], setting aside 7 units for Extremely Low Income household (9%), with 2 -on menu items
a) Front Yard setback to be aligned at the adjacent property (5ft). b) Allowable Height to be 41 ft.

4. EXISTING USE

A. Describe Existing Development: 1600- Venice was a 3 unit apartment, 1604 was a 4 unit apartment bldg and 1610 was a duplex

Characteristic of existing use Dwelling Unit (DU), Commercial/ Industrial, or Other	Existing # of Units or Non-Residential SF	Existing # of Units or Non-Residential SF To Be Demolished	Proposed* # of Units or Non-Residential SF
Guest Rooms			
Studio	1	1	1
One Bedroom	5	5	61
Two Bedrooms	3	3	15
Three Bedrooms			
_____ Bedrooms			
Non-Residential Square Feet			
Other:			

B. Previous Cases Filed

	(1)	(2)	(3)
Case Number(s):	ADM-2019-1208-TOC		
Date Filed:			
Date Approved:	03/20/2019		
End of Appeal Period:			
Environmental No.	NA		

5. TYPE OF APPLICATION

- ☐ Transit-Oriented Communities (per TOC Guidelines) with **Base Incentives** filed in conjunction with another discretionary approval.
- ☒ Transit-Oriented Communities (per TOC Guidelines) with **Additional Incentives** (please specify, max of three):
 - 1) Front Yard Reduction (to align to adjacent building = 5 ft)
 - 2) Height to be 41'ft.
 - 3)
- ☐ If applicable, projects adhering to the Labor Standards in LAMC 11.5.11 may be granted two more **Additional Incentives** as listed in the TOC Guidelines (please specify):
 - 4)
 - 5)
- ☐ Site Plan Review per LAMC Sec: 16.05
- ☐ Specific Plan Project Permit Compliance per LAMC Sec. 11.5.7.C
- ☐ Community Design Overlay per LAMC Sec. 13.08
- ☐ Coastal Development Permit per LAMC Sec. 12.20.2 or 12.20.2.1
- ☐ Tract or Parcel Map per LAMC Sec. 17.00 or 17.50
- ☐ Other entitlements requested (please specify):

* Replacement units, per AB 2556, shall be equivalent to the number of units and number of bedrooms of the existing development.

6. ENVIRONMENTAL REVIEW

☐ Environmental Review Not Required – Project is Ministerial.⁵ Please Explain: _____

☒ Not filed

☐ Filed (indicate case number): _____

7. HOUSING DEVELOPMENT PROJECT TYPE (please check all that apply):

- | | | |
|--|---|---|
| <input type="checkbox"/> For Sale | <input type="checkbox"/> Moderate Income | <input type="checkbox"/> Other (please describe): _____ |
| <input checked="" type="checkbox"/> For Rent | <input checked="" type="checkbox"/> Market Rate | _____ |
| <input checked="" type="checkbox"/> Extremely Low Income | <input type="checkbox"/> Mixed Use | _____ |
| <input type="checkbox"/> Very Low Income | <input type="checkbox"/> Senior | _____ |
| <input type="checkbox"/> Low Income | <input type="checkbox"/> Chronically Homeless | _____ |

8. DENSITY CALCULATION**A. Base Density: Maximum density allowable per zoning**

Lot size	<u>18,922 (gross)</u>	s.f. (a)
Minimum area per dwelling unit	<u>400</u>	s.f. of lot area per unit (b)
Units allowed by right (per LAMC)	<u>47</u>	units (c) [c = a/b, round down to whole number]
Base Density	<u>48</u>	units (d) [d = a/b, round up to whole number]

B. Maximum Allowable Density Bonus:

77 units (e)
 [e = d x 1.5 (Tier 1), 1.6 (Tier 2), 1.7 (Tier 3), or 1.8 (Tier 4);
 in RD Zones d x 1.35 (Tiers 1 and 2), 1.4 (Tier 3) or 1.45 (Tier 4);
 round up to whole number]

C. Proposed Project: Please indicate total number of Units requested as well as breakdown by levels of affordability set by each category (HCD or HUD). For information on HCD and HUD levels of affordability please contact the Housing and Community Investment Department of Los Angeles (HCIDLA) at (213) 808-8843 or hcidla.lacity.org.⁶

	<u>Total</u>	<u>HCD (State)</u>	<u>HUD (TCAC)</u>
Market Rate	<u>70</u>	<u>N/A</u>	<u>N/A</u>
Managers Unit(s) - Market Rate	<u></u>	<u>N/A</u>	<u>N/A</u>
Extremely Low Income	<u>7</u>	<u>7</u>	<u></u>
Very Low Income	<u></u>	<u></u>	<u></u>
Low Income	<u></u>	<u></u>	<u></u>
Moderate Income	<u></u>	<u></u>	<u></u>
 TOTAL # of Units Proposed	 <u>77</u> (f)		
TOTAL # of Affordable Housing Units	<u>7</u> (g)		
 Number of Density Increase Units	 <u>30</u> (h) [If f>c, then h=f-c; if f<c, then h=0]		
Percent Density Increase Requested	<u>60</u> (i) [i = 100 x (f/d - 1)]		
Percent of Affordable Set Aside	<u>9%</u> (j) [j = g/f, round down to a whole number]		

Other Notes on Units: _____

⁵ Ministerial Projects (aka, "By-Right") do not require any discretionary Planning approvals.

⁶ HCD (State) = Published affordability levels per California Department of Housing and Community Development. HUD (TCAC) = Published affordability levels per the United States Department of Housing and Urban Development.

9. **SITE PLAN REVIEW CALCULATION** An application for Site Plan Review may be required for projects that meet any of the Site Plan Review thresholds as outlined in LAMC Section 16.05.C. unless otherwise exempted per Section 16.05.D. For Transit Oriented Communities projects involving bonus units, please use the formula provided below to determine if the project meets the Site Plan Review threshold for unit count. If project meets the threshold(s) but qualifies under the exemption criteria per Section 16.05.D please confirm exemption with Department of City Planning's DSC Housing Unit.

47 units allowed by right (permitted by LAMC) – 9 existing units = 38 units

- ☐ YES, Site Plan Review is required, if proposed by right units minus existing units is equal to or greater than 50⁷
☒ NO, Site Plan Review is not required, if Base Density units minus existing units is less than 50
☐ Exempt (*please specify*): _____

10. INCENTIVES

A. Base Incentives (*Please check all that apply*)

- ☒ (1) Floor Area Ratio⁸:

	FAR (whichever is greater)
Tier 1	40% or 2.75:1 in commercial zone
Tier 2	45% or 3.25:1 in commercial zone
Tier 3	50% or 3.75:1 in commercial zone
Tier 4	55% or 4.25:1 in commercial zone
RD Zones or Specific Plans/Overlay Districts that Regulate FAR	45%, unless Tier 1
If Base FAR < 1.25:1	2.75:1
Greater Downtown Housing Incentive Area⁹	40%

	<u>Required (per LAMC)</u>	<u>Proposed (per TOC)</u>
Final Floor Area Ratio ¹⁰	<u>3 : 1</u>	<u>3.75 : 1</u>

- ☒ (2) Parking Reductions Allowed

Minimum Parking Requirements		
	Residential	Ground Floor Commercial
Tier 1	0.5 spaces per bedroom	10% Reduction
Tier 2	1 space per unit	20% Reduction
Tier 3	0.5 space per unit	30% Reduction
Tier 4	No parking requirements	40% Reduction
100% Affordable Housing	No parking requirements	

Total number of bedrooms	<u>92</u>
Total number of residential units	<u>77</u>
Non-residential Parking per code	<u>0</u>

	<u>Required (per LAMC)</u>	<u>Proposed (per TOC)</u>
Final Residential Parking	<u>123</u>	<u>47</u>
Final Non-Residential Parking	<u>0</u>	<u>0</u>

Other Parking Notes: _____

⁷ Site Plan Review may also be required if other characteristics of the project exceeds the thresholds listed in Sec. 16.05 of the LAMC.

⁸ Refer to TOC Guidelines Section VI.1.b. for exceptions

⁹ Calculated per LAMC 12.22 A.29(c)(1)

¹⁰ Refer to TOC Guidelines Section VI.1.b. for exceptions

B. Qualification for Additional Incentives: *(Please check only one)*

Minimum Required Restricted Affordable Housing Units, calculated as a percentage of the base density allowed on the date of the application.

Incentives	% Extremely Low Income	% Very Low Income	% Low Income
One	<input type="checkbox"/> 4%	<input type="checkbox"/> 5%	<input type="checkbox"/> 10%
Two	<input checked="" type="checkbox"/> 7%	<input type="checkbox"/> 10%	<input type="checkbox"/> 20%
Three	<input type="checkbox"/> 11%	<input type="checkbox"/> 15%	<input type="checkbox"/> 30%

C. Additional Incentives *(Please check selected incentives as qualified according to Section 9B)*

- Required (per LAMC)** **Proposed (per TOC)**
- ☒ (1) Yard/Setback *(each yard counts as 1 incentive in Tiers 1 and 2; two yards count as 1 in Tiers 3 and 4)*
- ☐ RAS 3 Yards (only for commercial zones – please specify numbers below, but only check this box)
- ☒ Front 15 ft 5 ft
- ☐ Rear _____
- ☐ Side (1) _____
- ☐ Side (2) _____

	Side and Rear Yards
Tier 1	25%
Tier 2	30%
Tier 3	30% or depth of two yards
Tier 4	35% or depth of two yards
When Abutting R1 or More Restrictive Zones	No Reductions Allowed

- ☐ (2) Lot Coverage _____
- ☐ (3) Lot Width _____
- ☒ (4) Height/ # of Stories 30 ft / 3 stories 41 ft / 4 stories

	Height
Tier 1	11 feet for one story
Tier 2	11 feet for one story
Tier 3	22 feet for two stories
Tier 4	33 feet for three stories
Lots with Height Limits of 45 feet or less	Second and third additional stories must be stepped-back at least 15 feet from any frontage

Transitional Height (check one): ☐ Per LAMC ☐ Per TOC Guidelines¹¹ ☐ Not Applicable

- ☐ (5) Open Space _____
- ☐ (6) Density Calculation _____
- ☐ (7) Averaging *(all count as 1 incentive – mark as many as needed)*
- FAR ☐
- Density ☐
- Parking ☐
- Open Space ☐
- Vehicular Access ☐
- ☐ (8) Public Facility Zone _____

TOTAL # of Additional Incentives Requested: 2

Other Incentive Notes: _____

¹¹ Please provide elevations that show the 45 degree angle as allowed by the TOC guidelines to determine the allowed height.

11. COVENANT:

All Transit Oriented Communities projects are required to prepare and record an Affordability Covenant to the satisfaction of the Los Angeles Housing and Community Investment Department's Occupancy Monitoring Unit **before** a building permit can be issued. Please contact the Housing and Community Investment Department of Los Angeles (HCIDLA) at (213) 808-8843 or hcidla.lacity.org

12. REPLACEMENT UNITS:

AB 2222, as amended by AB 2556, requires that density bonus eligible projects replace any pre-existing affordable housing units on the project site. Replacement units include the following: *(Answer the following with "yes" if any of these items apply to what is **currently existing** on the site or "no" if they do not. Write in N/A if the item is not applicable to your project)*

- A. Units subject to a recorded covenant, ordinance, or law that restricts rents to levels affordable to persons and families of lower or very low income? YES
- B. Units occupied by lower or very low income households below 80% AMI per California Department of Housing and Community Development Department levels not already listed above? NO
- C. Units subject to the Rent Stabilization Ordinance not already listed above? NO
- D. Units that have been vacated or demolished in the last 5 years? YES
- E. Per AB 2556, are the number of replacement units and number of bedrooms equivalent to that being demolished (as shown on Existing Development Table on page 2 above)? YES


Disclaimer: This review is based on the information and plans provided by the applicant at the time of submittal of this form. Applicants are advised to verify any zoning issues such as height, parking, setback, and any other applicable zoning requirements with Building and Safety.



Eric Garcetti, Mayor
Rushmore D. Cervantes, General Manager

DATE: June 11, 2019

TO: Venice Wave LP, a California limited partnership, Tropic Star Corporation, a California corporation, & 1600 Venice LLC, a California limited liability company, Owners

FROM: Marites Cunanan, Senior Management Analyst I 
Los Angeles Housing and Community Investment Department

SUBJECT: **AB 2556 (TOC) Determination for
1600-1610 E. Venice Blvd., Los Angeles, CA 90291**

Based on the Affordable Unit Determination Application submitted by Don Tolentino on behalf of Venice Wave LP, a California limited partnership, Tropic Star Corporation, a California corporation, & 1600 Venice LLC, a California limited liability company (Owners), the Los Angeles Housing + Community Investment Department (HCIDLA) has determined that nine (9) units (as detailed below) are subject to replacement under AB 2556 (formerly AB 2222).

Information about the existing property for the five years prior to the date of the application is required in order to make a determination. HCIDLA received the Affordable Unit Determination Application on or about May 6, 2019, so HCIDLA must collect data from May 2014 to May 2019.

Venice Wave LP, a California limited partnership & 1600 Venice LLC, a California limited liability company (Owner) acquired the property commonly known as:

1600 E Venice Blvd. under APN # 4236-006-013, Lot 1 on April 26, 2019 per Grant Deed.

1604 E Venice Blvd. under APN # 4236-006-014, Lot 2 on April 26, 2019 per Grant Deed.

1610 E Venice Blvd. under APN # 4236-006-015, Lot 3 on March 7, 2019 per Grant Deed.

Per Google Earth images, Department of City Planning (ZIMAS), County Assessor Parcel Information (LUPAMS), First American Datatree database, Billing Information Management System (BIMS) database, Code, Compliance and Rent Information System (CRIS) database, Internet Search, and the Rent Stabilization Ordinance (RSO) Unit, the property commonly known as 1600 E. Venice Blvd has a use code of "0300-Three Units".

Per Google Earth images, Department of City Planning (ZIMAS), County Assessor Parcel Information (LUPAMS), First American Datatree database, Billing Information Management System (BIMS) database, Code, Compliance and Rent Information System (CRIS) database, Internet Search, and the Rent Stabilization Ordinance (RSO) Unit, the property commonly known as 1604 E. Venice Blvd has a use code of "0400-Four Units".

Per Google Earth images, Department of City Planning (ZIMAS), County Assessor Parcel Information (LUPAMS), First American Datatree database, Billing Information Management System (BIMS) database, Code, Compliance and Rent Information System (CRIS) database, Internet Search, and the Rent Stabilization Ordinance (RSO) Unit, the property commonly known as 1610 E. Venice Blvd has a use code of "0200-Two Units".

The Los Angeles Department of Building and Safety database indicates that the Owner has applied for Demolition Permit #19019-30000-01159, #19019-30000-01157, #19019-30000-01329, and #19019-30000-01327, issued April 29, 2019 for the project. The Los Angeles Department of Building and Safety database indicates that the Owner has not applied for a Building Permit for the project.

Per the Affordable Unit Determination Application received by HCIDLA on or about May 6, 2019, the Owner plans to construct a seventy-seven (77) unit residential building, pursuant to Transit Oriented Communities (TOC) guidelines.

ADDRESS	BEDROOM TYPE
1600 E. Venice Blvd Unit #1	One bedroom
1600 E. Venice Blvd Unit #2	Two bedroom
1600 E. Venice Blvd Unit #3	Two bedroom
1604 E. Venice Blvd Unit #1	One bedroom
1604 E. Venice Blvd Unit #2	One bedroom
1604 E. Venice Blvd Unit #3	One bedroom
1604 E. Venice Blvd Unit #4	One bedroom
1610 E. Venice Blvd Unit#1	Two bedroom
1610 E. Venice Blvd Unit#2	Studio

No income documents were provided for these unit(s). Pursuant to AB 2556, where incomes of existing or former tenants are unknown, the required percentage of affordability is determined by the percentage of extremely low, very low, and low income rents in the jurisdiction as shown in the HUD Comprehensive Housing Affordability Strategy (CHAS) database. At present, the CHAS database shows 31% Extremely Low (Below 31% Area Median Income [AMI]), 19% Very Low ([31% to 50% AMI]), and 18% Low ([51% to 80% AMI]) renter households for Los Angeles (for a total of 68%). The balance of these unit(s) (i.e. 32%) are presumed to have been occupied by persons and families above-lower income.

Number of Existing RSO Units within five (5) years of Owner's application = 9
Number of Affordable Replacement Units required per CHAS: 7 (see table)

9 Units x 68%	7 Units
31% Extremely Low	3 Units
19% Very Low	2 Units
18% Low	2 Units
Market Rate RSO units	2 Unit

Number of Unit(s) presumed to be above-lower income subject to replacement = 2

For Rental:

Pursuant to CHAS, seven (7) unit(s) need to be replaced with equivalent type, with three (3) units restricted to Extremely Low Income Households, two (2) units restricted to Very Low Income Households, and two (2) units to Low Income Households. For the two (2) remaining units presumed to have been occupied by an above-lower income person or household, as permitted by California Government Code §65915(c)(3)(C)(ii), the City has opted to require that those unit(s) be replaced in compliance with the City's Rent Stabilization Ordinance (RSO).

Please note that all the new units may be subject to RSO requirements unless an RSO Exemption is filed and approved by the RSO Section. This determination is provisional and subject to verification by the RSO Section.

This AB 2556 determination only applies if the proposed project is a rental TOC project and NOT condominiums. In the event the project changes to condominiums, the owner needs to request an AB 2556 amendment to reflect

100% replacement of the units. In addition, if the project is changed from TOC to Density Bonus or vice-versa, an AB 2556 amendment will also be required.

If you have any questions about this Determination, please contact Edmund Wong of the Los Angeles Housing Community and Investment Department at (213) 808-8441, or edmund.wong@lacity.org.

///

****WARNING****

LOT TIES AND EXISTING PRE-1978 SINGLE FAMILY DWELLING ON ONE LOT

ISSUE:	Is a LOT TIE required for the NEW project?
IF NO:	Owner's existing Rent Stablization (RSO) replacement obligation, if any, remains the SAME as above.
IF YES:	Owner's existing RSO replacement obligation, if any, will INCREASE by one and the new project will also be subject to the RSO, unless the existing single family dwelling is demolished before the lots are tied.

cc: Los Angeles Housing and Community Investment Department File
Venice Wave LP, a California limited partnership, Tropic Star Corporation, a California corporation, &
1600 Venice LLC, a California limited liability company, Owner
Ulises Gonzalez, Case Management Section, City Planning Department


MAC:ew

CITY OF LOS ANGELES
INTER-DEPARTMENTAL CORRESPONDENCE

1600 E. Venice Boulevard
DOT Case No. CTC19-108368

Date: February 4, 2020

To: Luciralia Ibarra, Senior City Planner
Department of City Planning

From: 
Hamed Sandoghdar, Transportation Engineer
Department of Transportation

Subject: **TRAFFIC ASSESSMENT FOR THE PROPOSED RESIDENTIAL PROJECT TO BE LOCATED AT
1600 EAST VENICE BOULEVARD**

Pursuant to the Coastal Transportation Corridor Specific Plan (CTCSP) Ordinance No. 168,999, the Department of Transportation (DOT) has completed its review of the traffic impact analysis prepared by Overland Traffic Consultants, Inc. received on September 16, 2019 for the proposed residential project to be located at 1600 East Venice Boulevard. Based on DOT's traffic impact criteria, the study included the detailed analysis of two signalized intersections. After a review of the pertinent data, DOT has determined that the analysis conducted adequately describes the project-related impact of the proposed development.

PROJECT DESCRIPTION

The project proposes to construct a four story apartment building on a 17,891 square feet lot located at 1600 East Venice Boulevard. The residential development will consist of 77 apartment units of which seven (7) units will be designated as affordable units. The existing site consists of seven apartment units and two single family homes which will be demolished for the new development. The project is to provide a total of 47 parking spaces and 82 bicycle parking spaces. Vehicular access will be provided via a single driveway from the alley south of Venice Boulevard. The project is expected to be completed by 2021.

DISCUSSION AND FINDINGS

Trip Generation

The project is estimated to generate a net increase of 341 daily trips, a net increase of 25 a.m. peak hour trips and, a net increase of 27 p.m. peak hour trips. The trip generation estimates are based on the trip rate requirements of the Coastal Transportation Corridor Specific Plan (CTCSP), and the rates published by the Institute of Transportation Engineers (ITE) Trip Generation, 10th Edition, 2017. A copy of the report trip generation table (Table 2) can be found in **Attachment "A"**.

Traffic Impact

Traffic impact analysis was conducted at three intersections surrounding the proposed project site. Based on DOT's traffic impact criteria¹, the proposed development will not create a significant traffic impact at any of the signalized intersections as shown in the report's summary of volume-to-capacity (V/C) ratios and levels of service (LOS) table (Tables 5 and 6). A copy of the LOS summary tables are provided as **Attachment "B"** to this report.

¹ Per LADOT Traffic Study Policies and Procedures, a significant impact is identified as an increase in the Critical Movement Analysis (CMA) value, due to project related traffic, of 0.010 or more when the final ("with project") Level of Service (LOS) is LOS E or F; an increase of 0.020 or more when the final LOS is LOS D; or an increase of 0.040 or more when the final LOS is LOS C.

Congestion Management Program (CMP)

In accordance with the state-mandated Congestion Management Program (CMP), an increase in the freeway volume by 150 vehicles per hour during the a.m. or p.m. peak hours in any direction requires further analysis. A substantial change in freeway segments is defined as an increase or decrease of 2% in the demand capacity ratio when at LOS F. For purposes of CMP intersections, an increase of 50 vehicles or more during the a.m. or p.m. peak hour requires further analysis. Since the total number of trips during the a.m. or p.m. peak is less than 50, no further analysis is needed.

PROJECT REQUIREMENTS**A. Application Fee**

Pursuant to Section 5.C.2.(b) of the CTCSP, the applicant is responsible for remitting payment to all applicable application / traffic study review fees as required. Applicant has submitted all appropriate application fees including the expedited traffic study review fee which was submitted on August 7, 2019.

B. Covenant and Agreement

Pursuant to Section 5.B of the CTCSP, the owner(s) of the property must sign and record a Covenant and Agreement prior to issuance of any building permit, acknowledging the contents and limitations of the Specific Plan is a form designed to run with the land.

C. Highway Dedication And Street Widening Requirements

Pursuant to Section 5.E of the CTCSP, and in order to mitigate potential access and circulation impacts, the applicant may be required to make highway dedications and improvements. The applicant shall consult the Bureau of Engineering (BOE) for any highway dedication or street widening requirements. These requirements must be guaranteed before the issuance of any building permit through the B-permit process of the BOE. They must be constructed and completed prior to the issuance of any certificate of occupancy to the satisfaction of DOT and BOE.

D. Pedestrian Connectivity

The applicant shall consult with the City's Planning Department for any additional requirements pertaining to pedestrian walkability and connectivity, as described in the Walkability Checklist.

E. Construction Impacts

DOT recommends that a construction work site traffic control plan be submitted to DOT for review and approval prior to the start of any construction work. The plan should show the location of any roadway or sidewalk closures, traffic detours, haul routes, hours of operation, protective devices, warning signs and access to abutting properties. DOT also recommends that all construction related traffic be restricted to off-peak hours.

F. Site Access and Internal Circulation

The proposed site plan is acceptable to DOT; however, review of the study does not constitute approval of the driveway dimensions and internal circulation schemes. Those require separate review and approval and should be coordinated with DOT's West LA/Coastal Development Review Section (7166 W Manchester Ave, @ 213-485-1062). In order to minimize potential building design changes, the applicant should contact DOT for driveway width and internal circulation requirements so that such traffic flow considerations are designed and incorporated early into the building and parking layout plans. All new driveways should be Case 2 driveways

and any security gates should be a minimum 20 feet from the property line. All truck loading and unloading should take place on site with no vehicles backing into the project from public streets via any of the project driveways.

G. Development Review Fees

An ordinance adding Section 19.15 to the Los Angeles Municipal Code relative to application fees paid to DOT for permit issuance activities was adopted by the Los Angeles City Council in 2009 and updated in 2014. This ordinance identifies specific fees for traffic study review, condition clearance, and permit issuance. The applicant shall comply with any applicable fees per this ordinance.

DOT ASSESSMENT APPEAL PROCESS

Pursuant to Section 9.A of the CTCSP, an applicant or any other interested person adversely affected by the proposed project who disputes any determination made by DOT pursuant to this Ordinance may appeal to the General Manager of DOT. This appeal must be filed within a 15 day period following the applicant's receipt date of this letter of determination. The appeal shall set forth specifically the basis of the appeal and the reasons why the determination should be reversed or modified.

If you have any questions, please contact Pedro Ayala, of my staff, at (213) 485-1062.

HS:pa

Attachments

c: Jason Douglas, Krista Kline, Council District No. 11
Rudy Guevara, DOT
Mike Patonai, Kevin Azarmahan, BOE
Jerry Overland, Overland Traffic Consultants, Inc


Overland Traffic Consultants, Inc.
Project Traffic Generation

Traffic - generating characteristics have been studied by the Institute of Transportation Engineers (ITE) and LADOT. The project trip rates used in this assessment are shown in Table 1 (data on traffic rates see Attachment E).

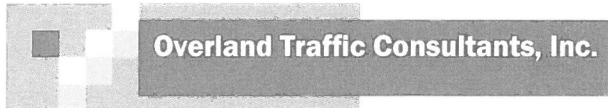
Table 1
Project Trip Generation Rates

<u>Description</u>	<u>ITE Code</u>	<u>Daily Traffic</u>	<u>AM Peak Hour</u>			<u>PM Peak Hour</u>		
			<u>Total</u>	<u>In</u>	<u>Out</u>	<u>Total</u>	<u>In</u>	<u>Out</u>
Single-Family	210	9.44	0.74	25%	75%	0.99	63%	37%
Multifamily Housing Low-Rise	220	7.32	0.46	23%	77%	0.56	63%	37%
Multifamily Housing Mid-Rise	221	5.44	0.36	26%	74%	0.44	61%	39%
Affordable Housing	LADOT	4.08	0.50	40%	60%	0.34	55%	45%

Using these rates, the net project traffic added is estimated at 341 daily trips with 25 morning peak hour and 27 additional afternoon peak hour trips. Table 2 contains the traffic generation estimates for the project.

Table 2
Project Trip Generation

Description	Size	Daily Traffic	AM Peak Hour			PM Peak Hour		
			Total	In	Out	Total	In	Out
Proposed Project								
Apartment (ite 221)	70 units	381	25	6	19	31	19	12
Affordable	7 units	29	4	2	2	2	1	1
Subtotal Proposed		410	29	8	21	33	20	13
Existing to be Removed								
Apartment (ite 220)	7 units	51	3	1	2	4	3	1
Single-Family (ite 210)	2 units	19	1	0	1	2	1	1
Subtotal Existing		69	4	1	3	6	4	2
Total (Proposed - Existing)		341	25	7	18	27	16	11



By applying the CMA procedures, the V/C ratios and the corresponding LOS for “existing + project” traffic conditions were determined for each intersection. The V/C intersection ratios and the corresponding LOS values for the “Existing + Project Traffic Impact Analysis” are summarized in Table 5.

Table 5
Existing + Project Traffic Conditions

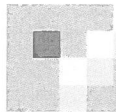
No.	Intersection	Peak Hour	Existing		Existing With Project		
			CMA	LOS	CMA	LOS	Impact
1.	Venice Bl. & Walgrove Av.	AM	0.793	C	0.802	D	+0.009
		PM	0.709	C	0.718	C	+0.009
2.	Venice Bl. & Lincoln Bl.	AM	1.016	F	1.019	F	+0.003
		PM	0.913	E	0.916	E	+0.003

As shown in the table above, the change in traffic flow generated by the proposed project would not exceed the City of Los Angeles LOS significant traffic impact thresholds.

Analysis of Future Traffic Conditions

Future 2021 traffic volume projections have been developed to analyze the traffic conditions after completion of other planned land developments including the proposed project. The following steps have been taken to develop the future cumulative traffic volume estimates with the results illustrated in Figure 6:

- (a) Existing traffic + ambient traffic growth (1% per year)
- (b) Traffic in (a) + related project traffic (without project scenario);
- (c) Traffic in (b) with the proposed project traffic (with project scenario);
- (d) Traffic in (c) + any proposed traffic mitigation, if necessary.



Overland Traffic Consultants, Inc.

The future analysis includes five (5) other development projects located within the study area that are either under construction or planned. As part of this analysis, the related project information was obtained from the City of Los Angeles Department of Transportation and on - line research. The locations of the related projects are shown in and described in Attachment F.

Table 6
Future Traffic Conditions

<u>No.</u>	<u>Intersection</u>	<u>Peak Hour</u>	<u>Future Without</u>		<u>Future With Project</u>		
			<u>CMA</u>	<u>LOS</u>	<u>CMA</u>	<u>LOS</u>	<u>Impact</u>
1.	Venice Bl. & Walgrove Av.	AM	0.826	D	0.834	D	+0.008
		PM	0.736	C	0.745	C	+0.009
2.	Venice Bl. & Lincoln Bl.	AM	1.066	F	1.068	F	+0.002
		PM	0.952	E	0.956	E	+0.004

As shown in the table above, the change in traffic flow generated by the proposed project would not exceed the City of Los Angeles LOS significant traffic impact thresholds. CMA worksheets are provided in Attachment G.

Summary

The proposed project will not create any significant traffic impacts and, therefore no traffic mitigation measures are necessary.

Call me if you have questions.

Sincerely,


Jerry T. Overland

Attachments

EXHIBIT G
PUBLIC CORRESPONDENCE
DIR-2019-4920-TOC-1A



Juliet Oh <juliet.oh@lacity.org>

1600-1614 East Venice Boulevard, Case: DIR-2019-4920-TOC, ENV-2019-4921-CE, PAR 2019-3781

Ira Koslow <president@venicenc.org>

Fri, Aug 21, 2020 at 12:53 PM

To: Jeff Khau <jeff.khau@lacity.org>, Jason Douglas <jason.p.douglas@lacity.org>

Cc: Councilmember Bonin <Councilmember.Bonin@lacity.org>, Len Nguyen <len.nguyen@lacity.org>, Nisa Kove <nisa.kove@lacity.org>, Juliet Oh <juliet.oh@lacity.org>, Sierra Riego <sierra.riego@lacity.org>, vince.bertoni@lacity.org, venicenc@empowerla.org, Mikecohanzad@gmail.com, LUPC <lupc@venicenc.org>

Project Description:

- TOC TIER 2 PROJECT FOR THE CONSTRUCTION OF A NEW 77 UNIT APT. BUILDING REQUESTING 2 ADDITIONAL INCENTIVES FOR AN INCREASE IN HEIGHT AND A FRONT YARD SETBACK REDUCTION.
- PROPOSED TOC TIER 2 PROJECT FOR THE CONSTRUCTION OF A NEW 77 UNIT APARTMENT BUILDING REQUESTING TWO (2) ADDITIONAL INCENTIVES FOR AN 11-FOOT INCREASE IN HEIGHT AND A FRONT YARD SETBACK REDUCTION TO ALLOW 5 FEET IN LIEU OF THE REQUIRED 15 FEET.

Dear Jeffrey Khau and Jason Douglas:

Please be advised that at a regular public meeting of the Venice Neighborhood Council (VNC) Board of Officers held on July 21, 2020 the following Land Use Planning Committee (LUPC) Consent Calendar denial motion was unanimously approved.

MOTION:

The VNC *recommends* denial of the project as presented unless the developer incorporates the following changes:

1. No variance on front yard setback. It must be maintained at 15 ft.
2. Parking units must be increased at a 1:1 stall per unit
3. No roof deck and the developer must work to provide alternate green space.
4. Increase the number of affordable units and provide full replacement of the 9 demolished units and replacement of affordable bedrooms at a 1:1 ratio
5. Require a covenant prohibiting short term rentals in the building

A fully executed copy of the denial motion is attached.

Respectfully submitted,

Ira Koslow
President
Venice Neighborhood Council

Ira Koslow
President
Venice Neighborhood Council
www.venicenc.org



Juliet Oh <juliet.oh@lacity.org>

Item 10A: VOTE NO on 1600-1614 East Venice

Jed Pauker <jed@jed.net>

Tue, Jul 21, 2020 at 5:03 PM

To: Board of Officers <board@venicenc.org>

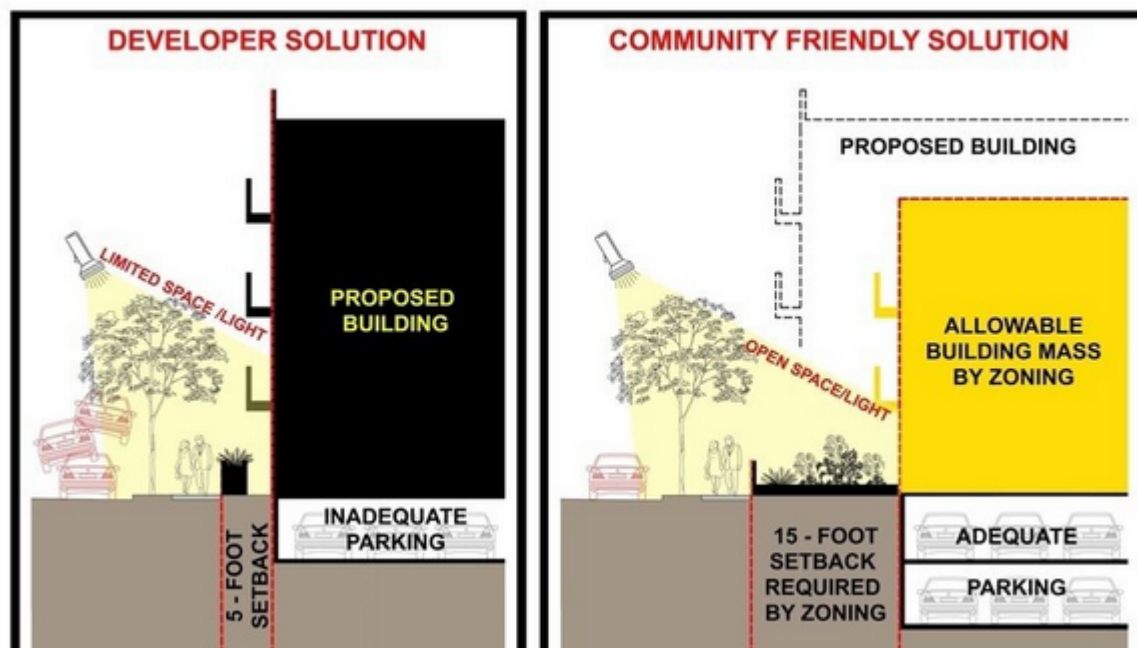
Dear VNC Board members,

Our pandemic appears to have triggered something of a development splurge.

Please reject this project as proposed. How much will rent cost? Too much. How much parking will there be? Too little (and "bicycle parking" is an unmanageable distraction from today's reality). Even LUPC had trouble approving this detrimentality. You rightly have a smidgen more latitude in the matter. I urge you to use it

Will the former residents be returned at their prior rent? No. The only condition today under which such a project might be reasonable is that 90% of units are a combination of low- and very-low income affordable rather than the token minimum 9% affordable listed. Only then should the developer dare to request an entitlement.

It's easy to get lost in the details of any project. Were LUPC still providing written meeting transcriptions for you to review, we could all readily review and easily digest human observations and considerations on projects like this. As things stand, perhaps this image, from a current petition, may help:



Have you had a chance to review the appeal? The bottom line is that this is another project demanding that your attention be closely focused on the needs of the community - in this case, East Venetians and fellow Angelenos at large - over those of simple commerce, which has only one setting: "Grow." Your job is not to say "How much?" Your job is to be the guardrail.

Please DENY this project as presented.

Thank you,

Jed Pauker

2017 - date: Founding Board Member, Venice Resistance; Member, Indivisible CA-33 and more

2016: Member, VNC Board Public Safety Committee
2014-2016: VNC Communications Officer/Chair, Communications Ad Hoc Committee
2010-2012: VNC Board Community Officer
2010: Co-Chair, VNC Board Food Truck Task Force
2008: Chair, VNC Board Fence and Hedges Ad Hoc Committee
2007-2008: LUPC Fences and Hedges Task Force
2007 - 2017: Member, VNC Outreach Committee
2006 - 2010: VNC Land Use and Planning Committee


INITIAL SUBMISSIONS

The following submissions by the public are in compliance with the Commission Rules and Operating Procedures (ROPs), Rule 4.3a. The Commission's ROPs can be accessed at <http://planning.lacity.org>, by selecting "Commissions, Boards & Hearings" and selecting the specific Commission.

The following submissions are not integrated or addressed in the Staff Report but have been distributed to the Commission.

Material which does not comply with the submission rules is not distributed to the Commission.

ENABLE BOOKMARKS ONLINE:

**If you are using Explorer, you will need to enable the Acrobat toolbar  to see the bookmarks on the left side of the screen.

If you are using Chrome, the bookmarks are on the upper right-side of the screen. If you do not want to use the bookmarks, simply scroll through the file.

If you have any questions, please contact the Commission Office at (213) 978-1300.



Please uphold the appeal of 1600 Venice Blvd. CASE No. DIR-2019-4920-TOC-1A

David Moriarty <davidjmorarty@gmail.com>

Thu, Dec 3, 2020 at 8:08 PM

To: Esther.Serrato@lacity.org, cpc@lacity.org

Cc: councilmember.bonin@lacity.org, jason.p.douglas@lacity.org, Chair-LUPC@venicenc.org, evna.venice@gmail.com

CASE NO. DIR-2019-4920-TOC-1A.

Dear Honorable Planning Commissioners:

THANK YOU for hearing this appeal. I want you to know that I STRONGLY support a denial of the project at [1600-1614 East Venice Boulevard](#) (CASE NO. DIR-2019-4920-TOC-1A), I believe the project's major flaws will have a terrible and lasting impact on our Venice neighborhood. I live directly across the street and have so for 17 years.

I have watched our community grow exponentially. Developers need to be compliant with the communities as a whole moving forward because Venice isn't getting any smaller as far as population.

Backwards thinking. All other apartment residences, except one, along Venice Blvd. between Lincoln and Walgrove have at least a 15-foot wide front yard, as required by code. The proposed, 41-foot tall building is the only one that crowds the sidewalk, other than the adjacent apartment building, which was built in 1928.

This project undermines City plans to improve the quality and livability of our neighborhood. City Planning's decision to allow the developer to shrink the front yard from 15-feet to 5- feet wide, removes approximately 1,200 square feet of front yard area. This decision directly conflicts with:

- Mayor Garcetti's Green Streets Initiative to "activate public spaces" and "enhance Neighborhood Character"
- Mike Bonin's Westside Mobility Plan - Livable Boulevards Streetscape Plan identifies Venice Blvd. as one of the four Westside corridors slated for beautification and improvement. It promises to help "fund efforts to green, beautify, and make major corridors safer and more inviting for pedestrians".
- **This project provides ZERO NEW AFFORDABLE HOUSING.** The developer demolished 9 rent-stabilized (RSO) units; the approved building has 7 Affordable + 2 RSO. The City's rationale for giving Transit Oriented Community (TOC) incentives (setbacks, height, parking, etc.) is to provide affordable housing near transit. The project replaces what was lost, but adds no additional units for Venice.

Three apartments (unit # 320, #PH3 and #Ph15) in the developer's plans (Exhibit A in the Determination Letter) have living rooms with **no windows to the exterior**, only to interior hallways. The same units are called out as 1-bedroom, their sizes are 280-309-315 square feet. Please condition the project to make sure any units used to satisfy the affordability requirements have exterior windows in living rooms as well as bedrooms. The 3 units identified above, shall not be the one selected to house the low-income eligible households.

And please condition the project to be sure the total number of rooms in the affordable units at least matches the number of rooms in the demolished units they're replacing.

Only **43 PARKING STALLS** for 77 units? The spill-over onto street parking will be substantial! With limited available street parking in the neighborhood, due to various restrictions and the lack of on-site parking for some of the older apartment buildings in the immediate area, the project pits neighbor against neighbor for street parking.

Thank you for your time and attention on this appeal.
David J. Moriarty Venice Blvd. Resident

November 3, 2020

BOARD MEMBERS

Lawrence Szabo

Kate Scanlon-Double

David Ewing

Paola Pini

William Wood

City Planning Commissioners

Los Angeles

200 N. Spring Street, Room 720

Los Angeles, CA 90012

RE: Serious Concerns about Appeal Process for Case No. DIR-2019-4920-TOC-1A

Dear City Planning Commissioners,

The East Venice Neighborhood Association was surprised and troubled by the list of appellants that you named in this case on your “Notice of Public Hearing.”

When we inquired on behalf of Venice neighbors about the proposed project at 1600-1614 Venice Bld. and appeal procedures, we were assured by Jason Chan at jason.chan@lacity.org that because the 12 neighbors were choosing to use the same appeal justification that they could all file in one appeal.

When the “Notice of Public Hearing” arrived and we found only one neighbor Mickey Ramos listed as Appellant One, we asked City Planning Associate Esther Serrato, to explain why the other 11 were left off the notice. She informed us that they each had needed to appeal the case separately. This is not what we were told by Jason Chan.

The effect of this misleading information reduces the amount of time provided the 11 residents to respond during the hearing to the timed status of public comment rather than appellants. Please look into this and provide the neighbors with their intended status and privileged time to present their objections to the case at the hearing this Thursday, Nov. 5. Their identification of eligibility was left out of the case Staff Report though it was filed with the appeal on May 5, 2020. It is attached here.

We want a successful project. Our community is growing, and we need more affordable housing. That’s a fact! The community will support a project that benefits both existing and future residents and improves the quality of the neighborhood as a whole.

As you know, the appeal applications and justifications are provided in Exhibit A of the Staff Report linked here: <https://planning.lacity.org/odocument/740f4287-e752-46c3-b647-791a78a8f163/DIR-2019-4920.pdf>..

We hope you will respond and support this request. For your information a list of our objections to the project are below.

Sincerely,

Lawrence Szabo
President, East Venice Neighborhood Association
EVNA.Venice @gmail.com

cc.: Councilmember Mike Bonin (councilmember.bonin@lacity.org); Jason Douglas (jason.p.douglas@lacity.org); Nisa Kove (nisakove@lacity.org) Ira Koslow (president@venicenc.org); Alix Gucovsky (chair-lupc@venicenc.org)

Objections to the planned project at 1600-1614 Venice Blvd.

- 1. The developer bypassed community engagement**, declining multiple offers from the Venice Neighborhood Council (VNC) to present the proposed project to the community. Only after LA City Planning approved the project did they present to the VNC Land Use and Planning Committee.
- 2. The City is not protecting the neighboring residents** from unscrupulous demolition and construction practices; it is also not upgrading the over-taxed and deteriorating infrastructure to handle new demand.
- 3. This project provides ZERO NEW AFFORDABLE HOUSING.** The developer demolished 9 rent-stabilized (RSO) units; the approved building has 7 Affordable + 2 RSO. The City's rationale for giving Transit Oriented Community (TOC) incentives (setbacks, height, parking, etc.) is to provide affordable housing near transit. The project replaces what was lost, but adds no additional units for Venice.
- 4. We're building our way into a deeper problem, not out of it.** The building is 90 % market-rate, replacing three buildings that were 100% rent stabilized, at a time when half of LA renters are rent-burdened and about a third are severely rent-burdened.
- 5. Families are underserved** - only 8 units out of 77 are 2-bedroom. The rest are 1-bedroom or studios.
- 6. The developer is not transparent.** The City must hold the developer accountable to provide the same amount of bedrooms in the affordable and 2 RSO units as there were in the original buildings: 12 bedrooms.

7. The project pits neighbor against neighbor for street parking. Only 43 parking stalls for 77 units. The spill-over onto street parking will be substantial!

8. Exceeds the zoning height by 11 feet.

9. This project undermines City plans to improve the quality and livability of our neighborhood. City Planning's decision to allow the developer to shrink the front yard from 15-feet to 5- feet wide disappears approximately 1,200 square feet of front yard area. This decision directly conflicts with:

- Mayor Garcetti's Green Streets Initiative to "activate public spaces" and "enhance Neighborhood Character"
- Mike Bonin's Westside Mobility Plan - Livable Boulevards Streetscape Plan identifies Venice Blvd. as one of the four Westside corridors slated for beautification and improvement. It promises to help "fund efforts to green, beautify, and make major corridors safer and more inviting for pedestrians".

10. Backwards planning. All the other residences, except one built in 1928, along Venice Blvd. between Lincoln and Walgrove have at least a 15-foot wide front yard. The proposed, 41-foot tall building is the only one that crowds the sidewalk.

11. The building's exterior on Venice and Glencoe is carelessly thought out:

- The combination of the proposed 42-inch tall planters at ground level and the protruding second floor balconies, create the visual equivalent of a 13-foot tall wall along the sidewalk.
- This 121-foot long building up against the sidewalk creates an oppressing experience for pedestrians, the opposite of "inviting". This is not in alignment with Mike Bonin's Westside Mobility Plan.
- For security reasons, ground floor tenants will likely erect barriers in front of their patios, further reducing the distance between sidewalk and private space to 2.5 feet.



Please uphold the appeal of 1600 Venice Blvd. CASE No. DIR-2019-4920-TOC-1A

Michael Gantz <gantzmike@yahoo.com>

Mon, Dec 7, 2020 at 2:33 PM

To: "Esther.Serrato@lacity.org" <Esther.Serrato@lacity.org>, "cpc@lacity.org" <cpc@lacity.org>

Cc: "councilmember.bonin@lacity.org" <councilmember.bonin@lacity.org>, "jason.p.douglas@lacity.org"

<jason.p.douglas@lacity.org>, "Chair-LUPC@VeniceNC.org" <Chair-LUPC@venicenc.org>, "evna.venice@gmail.com" <evna.venice@gmail.com>

Dear Honorable Planning Commissioners,

First, I would like to THANK YOU all for taking the time and hearing this appeal.

I want you to know that I STRONGLY support a denial of the project at [1600-1614 East Venice Boulevard](#) (CASE NO. DIR-2019-4920-TOC-1A).

I believe the project's major flaws will have a terrible impact on our Venice neighborhood.

Backwards thinking. All other apartment residences, except one, along Venice Blvd. between Lincoln and Walgrove have at least a 15-foot wide front yard, as required by code. The proposed, 41-foot tall building is the only one that crowds the sidewalk, other than the adjacent apartment building, which was built in 1928.

This project undermines City plans to improve the quality and livability of our neighborhood. City Planning's decision to allow the developer to shrink the front yard from 15-feet to 5-feet wide, removes approximately 1,200 square feet of front yard area. This decision directly conflicts with:

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Three apartments (unit # 320, #PH3 and #Ph15) in the developer's plans (Exhibit A in the Determination Letter) have living rooms with **no windows to the exterior**, only to interior hallways. The same units are called out as 1-bedroom, their sizes are 280-309-315 square feet. Please condition the project to make sure any units used to satisfy the affordability requirements have exterior windows in living rooms as well as bedrooms. The 3 units identified above, shall not be the one selected to house the low-income eligible households.

And please condition the project to be sure the total number of rooms in the affordable units at least matches the number of rooms in the demolished units they're replacing.

Only **43 PARKING STALLS** for 77 units. The spill-over onto street parking will be substantial!

With limited available street parking in the neighborhood, due to various restrictions and the lack of on-site parking for some of the older apartment buildings in the immediate area, the project pits neighbor against neighbor for street parking.

Thank you again for your time,

Mike
Venice resident,
[2411 Glencoe Ave.](#)



Uphold Appeal of 1600 Venice Blvd. CASE No. DIR-2019-4920-TOC-1A

Ronald Douglas <rnelsondouglas@gmail.com>

Mon, Dec 7, 2020 at 3:57 PM

To: Esther.Serrato@lacity.org, cpc@lacity.org

Cc: councilmember.bonin@lacity.org, jason.p.douglas@lacity.org, Chair-LUPC@venicenc.org, evna.venice@gmail.com

Respected Members of the Body:

As a neighbor, and one that lives directly beside the proposed lot, in the building built in 1928, I can personally attest to the rampant parking issue we already have in our neighborhood.

Let alone the objections to plans that you would expect Councilmember Bonin to make, considering the Venice Beautification projects, the parking and infrastructure disparities we already have cannot be sidestepped or overlooked.

The fact that under the guise of affordable commuter living, a developer is doing the bare minimum to make our neighborhood livable should concern you as it does us.

Our requests, our pleas are not unreasonable: a proper setback to allow walking livable space from a busy "highway" (at least classified as such), and an ADEQUATE amount of parking for every unit in the building they propose (since they are only allotting for HALF of their units), let alone proper reports on the effect to the existing neighborhood, its infrastructure, and the residents therein.

All disregarded to make a few extra dollars in a city, nay state where property value is still going up despite a pandemic?

Please do your part here and hold this developer accountable. Set the standard that the council values its RESIDENTS just as much as its developers' INTERESTS.

Just because we don't HAVE to protect our communities from overdevelopment or unfair practices, don't mean we SHOULDN'T do so.

I urge you to uphold this appeal, hold this dusky developer both responsible and accountable for their plans, and make some concessions for not only their prospective neighbors, but their fellow Californians and fellow citizens.

Respectfully,

Ronald Douglas
Venice Resident

6 attachments



IMG_2568.HEIC
2111K

IMG_2567.HEIC
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IMG_2566.HEIC
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IMG_2565.HEIC
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2475K



Please uphold the appeal of 1600 Venice Blvd. CASE No. DIR-2019-4920-TOC-1A

Ronald Douglas <rnelsondouglas@gmail.com>

Mon, Dec 7, 2020 at 3:35 PM

To: Esther.Serrato@lacity.org, cpc@lacity.org

Cc: councilmember.bonin@lacity.org, jason.p.douglas@lacity.org, Chair-LUPC@venicenc.org, evna.venice@gmail.com

CASE NO. DIR-2019-4920-TOC-1A.

Dear Honorable Planning Commissioners:

THANK YOU for hearing this appeal. I want you to know that I **STRONGLY** support a denial of the project at [1600-1614 East Venice Boulevard](#) (CASE NO. DIR-2019-4920-TOC-1A), I believe the project's major flaws will have a terrible impact on our Venice neighborhood.

Backwards thinking. All other apartment residences, except one, along Venice Blvd. between Lincoln and Walgrove have at least a 15-foot wide front yard, as required by code. The proposed, 41-foot tall building is the only one that crowds the sidewalk, other than the adjacent apartment building, which was built in 1928.

This project undermines City plans to improve the quality and livability of our neighborhood. City Planning's decision to allow the developer to shrink the front yard from 15-feet to 5- feet wide, removes approximately 1,200 square feet of front yard area. This decision directly conflicts with:

- Mayor Garcetti's Green Streets Initiative to "activate public spaces" and "enhance Neighborhood Character"
- Mike Bonin's Westside Mobility Plan - Livable Boulevards Streetscape Plan identifies Venice Blvd. as one of the four Westside corridors slated for beautification and improvement. It promises to help "fund efforts to green, beautify, and make major corridors safer and more inviting for pedestrians".

This project provides ZERO NEW AFFORDABLE HOUSING. The developer demolished 9 rent-stabilized (RSO) units; the approved building has 7 Affordable + 2 RSO. The City's rationale for giving Transit Oriented Community (TOC) incentives (setbacks, height, parking, etc.) is to provide affordable housing near transit. The project replaces what was lost, but adds no additional units for Venice.

Three apartments (unit # 320, #PH3 and #Ph15) in the developer's plans (Exhibit A in the Determination Letter) have living rooms with **no windows to the exterior**, only to interior hallways. The same units are called out as 1-bedroom, their sizes are 280-309-315 square feet. Please condition the project to make sure any units used to satisfy the affordability requirements have exterior windows in living rooms as well as bedrooms. The 3 units identified above, shall not be the one selected to house the low-income eligible households.

And please condition the project to be sure the total number of rooms in the affordable units at least matches the number of rooms in the demolished units they're replacing.

Only **43 PARKING STALLS** for 77 units. The spill-over onto street parking will be substantial!

With limited available street parking in the neighborhood, due to various restrictions and the lack of on-site parking for some of the older apartment buildings in the immediate area, the project pits neighbor against neighbor for street parking.

Yours truly,

Ronald Douglas
Venice resident

6 attachments



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CASE NO. DIR-2019-4920-TOC-1A

Robyn McNutt <robyn.mcnutt@gmail.com>

Mon, Dec 7, 2020 at 4:16 PM

To: Esther.Serrato@lacity.org, cpc@lacity.org, jason.p.douglas@lacity.org, Chair-LUPC@venicenc.org, evna.venice@gmail.com, councilmember.bonin@lacity.org

CASE NO. DIR-2019-4920-TOC-1A.

Dear Honorable Planning Commissioners:

THANK YOU for hearing this appeal. I want you to know that I STRONGLY support a denial of the project at [1600-1614 East Venice Boulevard](#) (CASE NO. DIR-2019-4920-TOC-1A), I believe the project's major flaws will have a terrible impact on our Venice neighborhood.

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Robyn McNutt

"Fight for the things that you care about, but do it in a way that will lead others to join you." ~ RBG 1933 - 2020



Planning CPC <cpc@lacity.org>

1600 Venice Blvd. CASE No. DIR-2019-4920-TOC-1A

1 message

Wood, William <wwood2005@lawnet.ucla.edu>

Mon, Dec 7, 2020 at 3:47 PM

To: Esther.Serrato@lacity.org, cpc@lacity.org

Cc: councilmember.bonin@lacity.org, Jason Douglas <jason.p.douglas@lacity.org>, Chair-LUPC@venicenc.org

Dear Los Angeles City Planning Commissioners,

I write to urge you to reject the project as proposed for [1600 Venice Boulevard](#). I previously submitted comments on November 2, which I include below for reference.

During the November 5 hearing, I listened to Commissioners including Commissioner Mack and Commissioner Leung discuss and emphasize equity and diversity as key values for the City of Los Angeles and the City Planning Commission. I hope the whole Commission shares their perspective and priorities.

As my comments below illustrate, my and other Venice residents' opposition to this project is not NIMBYism. Indeed, I think there should be more density and affordability with housing (and associated retail etc) in Venice and elsewhere in the City. But I am adamantly opposed to projects like this one that do not benefit the community or the City but rather developers who seek to benefit from things like the TOC laws which Los Angeles voters passed thinking they would promote the creation of more affordable housing.

As I noted in my earlier comments, the project as proposed would squander a great opportunity to build more affordable housing for families in a community where their children could walk to school, and for people who would actually use the public transportation the TOC-associated density bonuses were intended for.

Please give the neighborhood and our future neighbors a project that benefits working families and restores some of the economic, racial, cultural, and other diversity that has been too rapidly disappearing from Venice because of projects like the one proposed.

Respectfully,
William Wood
[2001 Penmar Avenue #3](#)
[Venice CA 90291](#)

Dear Los Angeles City Planning Commissioners,

I write to urge you to reject the project as proposed for 1600 Venice Boulevard. I am a decades-long resident of East Venice who drives,

bikes, walks, and rides the bus along this stretch of Venice Boulevard. I am not opposed to more housing in Venice, but I oppose this project. Venice, like all areas of Los Angeles, needs more affordable housing, not projects like this.

Only 8 of the proposed 77 units are 2-bedroom units (the rest are 1-bedroom or studio apartments). With an elementary (Walgrove) and middle school (Mark Twain) within walking distance on Walgrove and Venice High School east on Venice Boulevard across Walgrove, any proposed developments in this area should be geared towards families who can send their children to these schools, whose enrollment has been declining due to Venice's changing demographics, which are in turn due to gentrification and projects like these.

Three rent-stabilized buildings were destroyed for this proposed project. The proposed project is 90% market-rate, with only 9 units which just replace 9 RSO units that were demolished. This proposed 77-unit project provides no net increase in affordable housing units and, measured by the number of (bed)rooms, actually decreases the amount of affordable housing. That is disgraceful. Venice needs housing that working and middle-income families can afford.

Given its location along a major bus route on Venice Boulevard and within walking distance to bus lines on Lincoln Boulevard (and in a location where another bus line, the Big Blue Bus 2, used to run), this proposed project provides an opportunity to build housing that people who use public transportation can actually afford, and to provide good housing for people who use public transportation, as was voters' intent when they approved Measure JJJ and the idea of Transportation Oriented Corridors. But this project, like many others proposed for East Venice and other parts of the City, seeks to benefit from density bonuses and other outside-of-the-norm approvals from the City intended to provide affordable housing near public transportation and vice versa (public transportation near affordable housing) without promoting or achieving that goal.

The overwhelming majority of people who rent market-rate apartments in Venice will not ride public transportation but will drive. (One needs to look no further than Lincoln Place for an example.) Yet only 43 parking spaces are proposed for a 77-unit building, which would greatly increase the strain on neighborhood parking and traffic.

Please do a better job of taking care of the people of Los Angeles -- and not developers. Give the neighborhood and our future neighbors a project that benefits working families and restores some of the economic, racial, cultural, and other diversity that has been too rapidly disappearing from Venice.

Thank you for your time and consideration.

Respectfully,

William Wood
[2001 Penmar Avenue #3](#)
[Venice CA 90291](#)