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Decision Date: January 31, 2024

Last Day to Appeal Decision: February 15, 2024

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6380 Wilshire, LLC  
3470 Wilshire Boulevard  
Los Angeles, CA 90010

Jon Meyer ®  
Craig Lawson & Co., LLC  
3221 Hutchison Avenue  
Los Angeles, CA 90034

CASE NO. ZA-2023-4115-SPP-DI-ZAD-VHCA  
Project Permit Compliance, Director's  
Interpretation, Zoning Administrator's  
Determination  
1023, 1025 & 1055 West 7<sup>th</sup> Street  
Westlake Community Plan  
Zone: C4(CW)-U/6  
C.D: 1 – Eunisses Hernandez  
D.M.: 130-5A207  
CEQA: ENV-2023-4116-CE  
Legal Description: Lots FR 5-7, Lot FR 4  
(Arbs 1,2, and 3), Tract Subdivision of a  
Portion of Lot 1 Block 37 Hancocks  
Survey

Pursuant to California Environmental Quality Act, I hereby DETERMINE:

based on the whole of the administrative record, that the Project is exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines, Section 15332, Class 32, and there is no substantial evidence demonstrating that any exceptions contained in Section 15300.2 of the State CEQA Guidelines regarding location, cumulative impacts, significant effects or unusual circumstances, scenic highways, or hazardous waste sites, or historical resources applies.

Pursuant to Los Angeles Municipal Code Section 12.24 X.1, I hereby APPROVE:

a Zoning Administrator's Determination to allow the following exceptions and incentives for an Adaptive Reuse Project in the C Zone outside of the Downtown Project Area;

Adaptive Reuse Exceptions: Existing Floor Area Exception pursuant to LAMC 12.24 X.1 (b)(2) and 12.22.A.26(j)(1); Existing Yards Exception pursuant to LAMC 12.24 X.1 (b)(2) and 12.22.A.26(j)(3); and Zoning Administrator's exception pursuant to LAMC 12.24 X.1(b)(2) to deviate from the required parking design standards set forth in LAMC 12.21 A.5;

Adaptive Reuse Incentives: Density Incentive pursuant to LAMC 12.24 X.1 (b)(2) and 12.22.A.26(h)(5); Site Plan Review Incentive pursuant to LAMC 12.24 X.1 (b)(2) and 12.22.A.26(j)(3);

Pursuant to LAMC Section 11.5.7, I hereby APPROVE:

A Project Permit Compliance review for compliance with the Central City West Specific Plan (CCSWP); and

Pursuant to LAMC Section 11.5.7.H, I hereby APPROVE:

A Director's Interpretation to determine that the open space standards in Appendix D Section C of the CCWSP, including Appendix D.C.1(a) requiring one (1) on-site tree for every residential unit, do not apply to projects consisting entirely of the Adaptive Reuse of an existing building(s),

upon the following additional terms and conditions:

1. All other use, height and area regulations of the Municipal Code and all other applicable government/regulatory agencies shall be strictly complied with in the development and use of the property, except as such regulations are herein specifically varied or required.
2. The use and development of the property shall be in substantial conformance with the plot plan and floor plan submitted with the application and marked Exhibit "A", except as may be revised as a result of this action.
3. The authorized use shall be conducted at all times with due regard for the character of the surrounding district, and the right is reserved to the Zoning Administrator to impose additional corrective Conditions, if, in the Administrator's opinion, such Conditions are proven necessary for the protection of persons in the neighborhood or occupants of adjacent property.
4. All graffiti on the site shall be removed or painted over to match the color of the surface to which it is applied within 24 hours of its occurrence.
5. A copy of the first page of this grant and all Conditions and/or any subsequent appeal of this grant and its resultant Conditions and/or letters of clarification shall be printed on the building plans submitted to the Department of City Planning and the Department of Building and Safety for purposes of having a building permit issued at any time during the term of this grant.

6. Prior to the effectuation of this grant, a covenant acknowledging and agreeing to comply with all the terms and conditions established herein shall be recorded in the County Recorder's Office. The agreement (standard master covenant and agreement form CP-6770) shall run with the land and shall be binding on any subsequent owners, heirs, or assigns. The agreement with the conditions attached must be submitted to the Department of City Planning for approval before being recorded. After recordation, a certified copy bearing the Recorder's number and date shall be provided for inclusion in case file.
7. **Authorization.** Approved herein is the adaptive reuse of an existing, 32-story, approximately 610,803 square-foot commercial office building located within the C Zone outside of the Downtown Project Area, into a residential building containing a maximum of approximately 609,772 square feet of floor area and a maximum of 700 joint living and work quarters units with the following:
  - a. Existing Floor Area Exception pursuant to LAMC 12.24 X.1 (b)(2), to recognize the conversion of the existing 610,803 square-foot commercial office building into a residential building containing a maximum of approximately 609,772 square feet of floor area.
  - a. Existing Yards Exception pursuant to LAMC 12.24 X.1(b)(2), to allow the existing yards in an Adaptive Reuse development to remain in lieu of present setback requirements.
  - b. Zoning Administrator's parking design exception pursuant to LAMC 12.24 X.1(b)(2) to permit the use and maintenance of the existing parking stall dimensions pursuant to LAMC 12.21. A.5.
  - c. Density Incentive pursuant to LAMC 12.24 X.1 (b)(2) and 12.22.A.26(h)(5), to allow a maximum of 700 joint living and work quarters units.
  - d. Site Plan Review Incentive pursuant to LAMC 12.24 X.1 (b)(2), to be exempt from the requirements for Site Plan Review set forth in Section 16.05.
8. Prior to residential occupancy, one or more signs or symbols of a size and design approved by the Fire Department shall be placed by the applicant at designated locations on the exterior of the Adaptive Reuse Project to indicate the presence of residential uses.
9. The project shall comply with the City of Los Angeles Noise Ordinance Nos. 144,331 and 161,574, and any subsequent ordinances, which prohibit the emission or creation of noise beyond certain levels at adjacent uses unless technically infeasible.
10. Outdoor lighting shall be designed and installed with shielding, so that the light does not overflow into adjacent residential properties.

11. Prior to the sign-off of plans by the Development Services Center, the applicant shall submit the plans for review and approval to the Los Angeles Fire Department (LAFD). Said Department's approval shall be included in the plans submitted to the Development Services Center.

12. **Central City West Specific Plan.** The applicant shall comply with all other provisions of the Central City West Specific Plan.

Note: The subject Director's Interpretation addresses the open space standards in Appendix D Section C of the CCWSP, including Appendix D.C.1(a) requiring one (1) on-site tree for every residential unit. These standards do not apply to the subject Adaptive Reuse project.

13. **Residential Parking.** The project shall not be required to provide any minimum vehicle parking, consistent with the provisions of Assembly Bill (AB) 2097 (2022). The applicant may choose to provide a greater amount of vehicle parking.

14. **Transportation Board.** The applicant shall install a transportation board within the ground level lobby area of the subject building providing residents with available transit lines serving the region and nearby bus lines and station found near the subject site.

15. **Expedited Processing Section Fee.** Prior to the clearance of any conditions, the applicant shall show proof that all fees have been paid to the Department of City Planning, Expedited Processing Section.

16. **INDEMNIFICATION AND REIMBURSEMENT OF LITIGATION COSTS.**

Applicant shall do all of the following:

- i. Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- ii. Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
- iii. Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the applicant and requesting

a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).

- iv. Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- v. If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions include actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the applicant otherwise created by this condition.

**OBSERVANCE OF CONDITIONS - TIME LIMIT - LAPSE OF PRIVILEGES**

All terms and conditions of the approval shall be fulfilled before the use may be established. The instant authorization is further conditional upon the privileges being utilized within three years after the effective date of approval and, if such privileges are not utilized or substantial physical construction work is not begun within said time and carried on diligently to completion, the authorization shall terminate and become void.

**TRANSFERABILITY**

This authorization runs with the land. In the event the property is to be sold, leased, rented or occupied by any person or corporation other than yourself, it is incumbent upon you to advise them regarding the conditions of this grant.

**VIOLATIONS OF THESE CONDITIONS, A MISDEMEANOR**

Section 12.29 of the Los Angeles Municipal Code provides:

“A variance, conditional use, adjustment, public benefit or other quasi-judicial approval, or any conditional approval granted by the Director, pursuant to the authority of this chapter shall become effective upon utilization of any portion of the privilege, and the owner and applicant shall immediately comply with its Conditions. The violation of any valid Condition imposed by the Director, Zoning Administrator, Area Planning Commission, City Planning Commission or City Council in connection with the granting of any action taken pursuant to the authority of this chapter, shall constitute a violation of this chapter and shall be subject to the same penalties as any other violation of this Code.”

Every violation of this determination is punishable as a misdemeanor and shall be punishable by a fine of not more than \$2,500 or by imprisonment in the county jail for a period of not more than six months, or by both such fine and imprisonment.

**NOTICE**

The applicant is further advised that subsequent contact regarding this determination must be with the staff assigned to this case. This would include clarification, verification of condition compliance and plans or building permit applications, etc., and shall be accomplished **BY APPOINTMENT ONLY**, in order to assure that you receive service with a minimum amount of waiting. You should advise any consultant representing you of this requirement as well.

**FINDINGS OF FACT**

After thorough consideration of the statements contained in the application, the plans submitted therewith, and public comments received all of which are by reference made a part hereof, as well as knowledge of the property and surrounding district, I find that the requirements and prerequisites for approving an Adaptive Reuse Project as enumerated in Section 12.24 of the Municipal Code have been established by the following facts.

## **BACKGROUND**

The subject site is a rectangular through lot consisting of seven (7) lots totaling approximately 59,553 square feet. The property has an approximately 355.8 linear feet frontage along West 7<sup>th</sup> Street and approximately 155.7 linear feet along Bixel Street.

The subject property is located within the boundaries of the Westlake Community Plan Area. The Community Plan designates the subject property for Regional Center Commercial land uses corresponding to the C2, C4, P, PB, RAS3 and RAS4 Zones. The subject property is currently zoned C4(CW)-U/6 and is thus consistent with the existing land use designation. The subject property is also located within the Transit Priority Area of the City of Los Angeles, the MTA Right-Of-Way Project Area (ZI-1117), the Freeway Adjacent Advisory Notice for Sensitive Uses (ZI-2427), the Local Emergency Temporary Regulations – Time Limits and Parking Relief (ZI-2498), a Very Low VMT area, a reduced parking area, and the Central City West Specific Plan. Additionally, the subject site is located within an Urban Agriculture Incentive Zone, Outside Flood Zone and located approximately 0.801 km from the Puente Hills Blind Thrust fault.

The site is improved with an existing 32-story, approximately 464 feet tall, 610,803square-foot office building and an adjacent attached 10-level parking structure with 1,110 spaces and approximately 5,188 square feet of floor area for elevator lobbies, that was constructed in 1985.

The proposed project is an adaptive reuse and change of use of the existing office building into a multi-family residential building consisting of approximately 609,772 square feet of floor area and a maximum of 700 joint living and work quarter units. The proposed project will not physically enlarge the existing building footprint and will result in a slight decrease in the overall floor area. The project will maintain and provide the 1,110 vehicle parking spaces within the adjacent existing parking structure.

According to submitted plans, no significant alterations to the exterior facades of the existing commercial building are proposed. The first-floor plan will be converted to accommodate a residential entryway, meeting rooms, manager's office, kitchen/break room, resident coffee lounge, business center, game room, club lounge, mail and reception area, and resident ride share lounge area. The second floor will be converted into residential live work units and one (1) theatre room. Floors three through six will be converted into residential live work units and one (1) fitness room for each floor. Floors seven through 33 will be converted to only residential live work units. The existing penthouse will remain occupied with a mechanical room, fan room, pump room and second pump room. The upper roof level will maintain the existing emergency fire helicopter landing pad.

The project does not propose any removal of protected trees. There are three (3) existing street trees (palm trees) on 7<sup>th</sup> Street and two (2) street trees (palm trees) on Bixel Street to remain. There is approximately 500 square feet of existing landscaping on the ground level along the southwest corner of the parking structure at 7<sup>th</sup> Street and Bixel Street that consists of ornamental hedges and ficuses. No new landscaping is proposed and the existing landscaping will be maintained. The proposed project will add 9 new off-site street trees.

### Surrounding Properties

The project site is located in a high-rise commercial corridor with commercial, hotel, and residential uses. Properties to the north, across the abutting alley, are zoned CW with a land use designation of Regional Center Commercial and developed with a seven-story mixed-use residential building as well as a 15-story commercial building. The 110 Harbor Freeway is adjacent to the subject property to the east. Properties to the south, across 7<sup>th</sup> Street, are zoned CW with a land use designation of Regional Center Commercial and developed with a six-story mixed-use residential building. Properties to the west, across Bixel Street are zoned CW with a land use designation of Regional Center Commercial and developed with surface parking lots, two-story motels, and a vacant lot.

### Streets

7<sup>th</sup> Street, adjoining the subject property to the south, is a designated Avenue II, with a designated right-of-way width of 86 feet.

Bixel Street, adjoining the subject property to the west, is designated Avenue II, with a designated right-of-way width of 100 feet.

Rear Alley, adjoining the subject property to the north, is an alley with a width of approximately 15 feet.

### **Relevant Previous Cases, Affidavits, and Orders on the Subject Property:**

Case No. DIR-2012-2987-SPP: On February 8, 2013, the Director of Planning approved a project permit compliance review to allow the installation of two sets of illuminated wall signs on the top parapet of an existing multi story commercial building in the Central City West Specific Plan Area located at the subject site.

### **Relevant Cases on Surrounding Properties:**

Case No. ZA-2006-3449-ZV-ZAA-SPR: On November 7, 2009, the Zoning Administrator approved approximately 3.275 million gross square feet of residential, office, hotel and retail uses in four high rise buildings with associated parking and open space, the components of which include the following to be built over four phases: up to 836 residential units; up to 480 hotel rooms; up to 48,949 square feet of retail uses; and up to 1,007,056 square feet of office uses located at 899 South Francisco Avenue.

Case No. ZA-2014-2221-ZV-SPR: On September 15, 2014, the Zoning Administrator approved a mixed-use development with 350 hotel rooms, 1,560 residential condos and 74,903 square feet of retail space located at 811, 899 South Francisco Street and 1000, 1004, 1010, 1018, 1020, 1026, 1028, 1030 and 1032 East 8<sup>th</sup> Street.

Case No. ZA-2015-3926-CU-CUB-SPPA-SPP: On September 27, 2017, the Zoning Administrator approved 36-story, 402,939 square foot mixed use building with 422 residential units, 5,597 square feet of commercial ground floor located at 675-685 South Bixel Street, 111 West 7<sup>th</sup> Street, 112-1142 West Ingraham Street.



Case No. APCC-2016-1150-SPE-CU-CUB-CUX-SPP: On October 28, 2016, the Area Planning Commission approved the expansion of an existing, non-conforming hotel use with construction of a new pool deck with a bar and fitness area located at 1250-1256 West 7<sup>th</sup> Street, 713-727 South Hartford Street.

Case No. ZA-2017-2512-CU-CUB-SPR: on August 2, 2018, the Zoning Administrator approved a the on-site sale of a full line of alcoholic beverages; a determination to allow the required passageway pursuant to LAMC Section 12.21C.2(b) to be located on an adjacent lot within the Unified Development Site; and site plan review for a development that creates 50 or more dwelling units located at 945 West 8<sup>th</sup> Street.

### **PUBLIC CORRESPONDENCE**

Prior to the public hearing, Planning staff received a request to receive notices for this project. Any public correspondence in support or opposition of the project. After the public hearing,

### **PUBLIC HEARING**

A public hearing was held before the Zoning Administrator on January 23, 2024, at 9:30 a.m. through ZOOM. The hearing was attended by the applicant's representative, Jim Ries and Jon Meyer, both from Craig Lawson Associates. Also, other members from the public were in attendance.

Mr. Ries made the following statements:

- This is a project for the adaptive reuse of an office building to residential.
- The site is located along 7<sup>th</sup> Street west of downtown LA.
- The site has Bixel to the west, an alley and Wilshire to the north, and the 110 Freeway to the east.
- The site is within the Central City West Specific Plan. It has a C4 zone and has an intense land use designation near transit.
- The site is near the Metro Center.
- This slide shows views of the building with the freeway on the east and parking on the west.
- The project will add 691 new joint living work quarter units, all net new housing units, and no displacement, which is a huge benefit to the city at this point.
- The floor area will remain approximately the same at about 10.24 to 1. Our building height remains the same 463 feet and 32 stories. You will note there are different descriptions that call this a 33-story building because it does not have a 13<sup>th</sup> floor. Thus it's referred to as 32 stories.
- There are 1110 spaces, all existing commercial parking spaces. Because they're existing commercial spaces, they all require a little bit of a deviation to be allowed to be used for residential. We are asking to utilize AB 2097, which says that we will have no required parking. The intent is to provide all of these spaces, but we are asking that we are not required parking.

- Open space is not required for this project from the LAMC, but the project is including about 44,000 square feet, including 11,000 square feet in common amenities and then a series of balconies for almost every unit. There will be up to about 33,000 square feet of private decks.
- For this project, we will need incentives as part of our entitlements. These relate to density to site planning review.
- We need exceptions that relate to floor area of the existing building, the yards of the existing building and parking design, which were previously designed as commercial parking spaces, not residential.
- We also require a directors determination to determine that open space standards in the specific plan should be applied as they are in the LAMC and that means not applied to adaptive reuse projects that don't add floor area and are at least six new units. We are not adding new floor area, so they should not be required.
- We are asking for a project permit compliance, and are highlighting our desire to use AB 2097.
- We're requesting a Class 32 Categorical Exemption.
- The site plan shows the configuration of the parking structure to the West of the site, fronting along Bixel. The residential tower is on the eastern portion of the site.
- Amenity space will be provided to our residents. There will be other reception areas, leasing rooms and back of house typical of an office building on the 2<sup>nd</sup> floor. Our first residential units while also adding an amenity space will occur within floors 3 through 6. As we start moving up the building, we no longer add amenity space but we are adding in balconies for these units on levels seven through 16. Levels 17 through 25 is a slightly different configuration of units, but similar. Then there's levels 26 through 33. There's also a roof plan where nothing is being done to the roof. We are not proposing dramatic changes.
- There are minor changes when we put in those balconies, and minor change on the ground floor as well.
- The garage elevation again shows we are not making any changes to this.
- I want to highlight consistency with current city policy. This is 691 net new units. We are adding without displacing the existing residents. This is consistent with the housing element, the citywide adaptive reuse ordinance and their bases Executive Directive #7.
- We are adding housing in an appropriate location, which is all on the commercial corridor near jobs and transit. This supports the Mobility Plan 2035.
- By redeveloping this building into 691 units, we are maintaining the economic viability of the site, which then also allows us to ensure the physical maintenance of the property and by maintaining the economic viability and promoting the City's economic viability.

During the public comment portion of the public hearing, three people provided public testimony.

The first speaker, Gil with Empty Kitchen stated the importance of this project. These projects are important because they are what are needed for Los Angeles. After the pandemic, it's been very quiet, like a ghost town. A project like this brings up revenue, start construction and do a lot. The project can bring the community back together. He

is in the construction business, and there are a lot of these projects, whether they are constructed or not. People are moving out of state because they can't find projects to work on. This is a huge positive for our county.

The second speaker, Jaime Reyes, stated he is a contractor and partner for West Coast Contracting. He stated support for the project. It will provide good paying jobs to hard working people and create needed housing.

The third speaker, Andres Chavez, stated he lives across from the site at the Medici. He asked about the project's ground level interaction for pedestrians.

At the conclusion of the public hearing, the Zoning Administrator stated he would keep the record open since there was a request for all notices by one interested party. The Zoning Administrator also stated that the findings that are required for the entitlements requested can be found in the affirmative and that he was inclined to approve the project. In addition, the project would meet all the provisions for a Class 32 Categorical Exemption for the environmental clearance.

## **DIRECTOR'S INTERPRETATION**

### **Authority**

Pursuant to Section 11.5.7 H. of the Los Angeles Municipal Code (LAMC), the Director of Planning has authority to interpret specific plans when there is a lack of clarity in the meaning of their regulations.

### **Background**

In 1991, the City Council adopted the Central City West Specific Plan (CCWSP). Located immediately west of Downtown Los Angeles, the CCWSP seeks to "*Regulate all development, including use, location, height and density to assure compatibility of uses, and to provide for the consideration of transportation and public facilities, aesthetics, historic preservation, open space and the economic and social well-being of area residents.*" The Plan includes the following Urban Design Guidelines as part of Appendix D for all projects located in the CCWSP:

**CCWSP Appendix D Section C.1(a):** "A minimum of 100 square feet per unit of the required useable Open Space, as provided in Section 12.21.G of the LAMC, shall be provided as Common Open Space."

**CCWSP Appendix D Section C.2:** "There shall be one tree provided on-site for every dwelling unit, each of which shall be a minimum of 12 feet in height and three inches in caliper at the time of planting..."

### **Request**

With the requirements effective, and pursuant to Los Angeles Municipal Code Section 11.5. 7 H., the Applicant has requested an Interpretation from the Director of Planning to determine that the open space standards in Appendix D Section C of the CCWSP, including Appendix D.C.1(a) requiring 100 square feet of common open space and

Appendix D.C.2 requiring one on-site tree for every residential unit, do not apply to projects consisting entirely of the Adaptive Reuse of an existing building(s).

### Discussion

The proposed project involves the adaptive reuse and change of use of the subject existing office building into a multi-family residential building consisting of approximately 609,772 square feet of floor area and a maximum of 700 joint living and work quarter units. The proposed project will not physically enlarge the existing building footprint and will result in a slight decrease in the overall floor area. The project will maintain and provide the 1,110 vehicle parking spaces within the adjacent existing parking structure.

The requirements set forth in Appendix D Section C.1(a) of the CCWSP require a project's total required Open Space be in compliance with LAMC Section 12.21.G, where at least 100 square feet per unit must be provided as Common Open Space. However, pursuant to LAMC Section 12.21.G, the proposed project is not required to provide open space as the project will not result in new construction. Although the proposed project will result in additional units, it will not result in additional floor area. Furthermore, the overall floor area of the subject existing building will decrease from 610,803 to 609,772 square feet in floor area. Thus, the project is not required to provide any open space pursuant to LAMC Section 12.21.G.

Additionally, requirements for on-site trees for residential projects are set forth in LAMC Section 12.21.G.2(a)(3) which also apply to new construction. As mentioned above, the proposed project will not result in additional floor area therefore it cannot be categorized as new construction and does not need to comply with the on-site tree requirements set forth in Appendix D.C.2 of the CCSWP.

The subject project involves the adaptive reuse of an existing building that was originally constructed for commercial office uses, which required no yards or landscape buffers. Accordingly, the existing 32-story building was constructed to the property lines. As such, there is no land for open space and incorporating additional open space would require the removal of portions of the existing building or demolishing the existing building altogether, which would defeat the purpose and benefit of the subject adaptive reuse to convert the existing office commercial building into a residential building.

Although the current version of the Adaptive Reuse Ordinance does not directly address Open Space and on-site tree requirements in Adaptive Reuse Projects, in an apparent recognition that this issue should be clarified, the City is in the process of updating the Adaptive Reuse Ordinance to specifically exempt Adaptive Reuse Projects from providing Open Space or on-site trees. According to Section 2 of the May 2023 Draft Adaptive Reuse Ordinance, LAMC Section 12.22.A.26 is amended to reflect this issue.

### Conclusion

In conclusion, the proposed project will not result in the new ground-up construction of any new buildings or additional floor area and therefore is not required to comply with LAMC and CCSWP open space and on-site tree requirements. The proposed project is

an Adaptive Reuse project which is intended to facilitate the conversion of an existing commercial building into a residential building without adding new floor area. Furthermore, the City of Los Angeles is in the process of addressing these kind or requirements that are not applicable to Adaptive Reuse projects in the Adaptive Reuse Ordinance update. As such, it is appropriate to determine that the open space standards set forth in Appendix D Section C of the CCWSP, including Appendix D.C.1(a) requiring 100 square feet of common open space and Appendix D.C.2 requiring one on-site tree for every residential unit, do not apply to projects consisting entirely of the Adaptive Reuse of an existing building(s).

### **PROJECT PERMIT COMPLIANCE FINDINGS FOR THE CENTRAL CITY WEST SPECIFIC PLAN**

1. That the project substantially complies with the applicable regulations, findings, standards and provisions of the specific plan, as follows:

a. **Density.** The maximum residential density in the C4 zone is 1 unit per 400 square feet of lot area. Based on an estimated lot area of 59,533 square feet, up to 148 dwelling units would be permitted on the Project Site. Pursuant to LAMC Sections 12.24.X.1(b)(2) and 12.22.A.26(h)(2), the Zoning Administrator may grant an incentive to waive the density limits of the underlying zone for an Adaptive Reuse Project. This incentive is requested to permit the Proposed Project's 700 dwelling units. As such, the project is consistent with the density provisions of the Specific Plan, as may be modified by authority of the Zoning Administrator.

b. **Height and Floor Area.** Pursuant to the CCWSP, the project site's U/6 Height Designation limits the buildings height to 1,268 feet above sea level. The existing building is 463 feet seven inches above sea level and does not propose to increase the height of the existing building. The existing office building and adjacent parking structure were originally constructed when the subject site was in Height District 4 which permitted an FAR of 13:1. Subsequently, in 1989 (per Ordinance No. 164,625) the site was changed to Height District 2, which permitted the site an FAR of 6:1. Per the CCWSP, the site is permitted an FAR of 6:1.

The subject properties including the existing office building and adjacent parking structure have a combined FAR of 10.26:1 or 610,803 square feet which was allowed under the Height District 4 designation. Presently, this FAR would not be allowed at the subject site. The proposed adaptive reuse project will not expand the existing footprint of the existing buildings. Pursuant to LAMC Sections 12.24.X.1(b)(2) and 12.22.A.26(j)(1), the Zoning Administrator may grant an exception to permit existing floor area in an adaptive reuse project to remain, even if it exceeds current FAR requirements. This exception is requested to permit the 10.24:1 FAR.

c. **Yards.** The project site is zoned C4(CW)-U/6 and is located at the corner of West 7<sup>th</sup> Street and Bixel Street. The front yard is located along Bixel Street

which has no setback requirements. There are no side yard requirements for commercial uses, however for residential uses a setback of five feet plus one foot for each story over the second floor is required and not to exceed 16 feet. The existing building is 33 stories tall therefore a 17-foot side yard setback would be required at the ground floor. There are no rear yard setback requirements for commercial uses, however, for residential uses a setback of 15 feet plus one foot for each story over the third story is required and not to exceed 20 feet. The existing building is 33 stories high therefore a 20-foot rear yard setback would be typically required at the ground floor.

The existing buildings provided minimal setbacks in all four yards in compliance with commercial use requirements, with the conversion of the subject property from commercial to residential the project will be required to provide the residential yard requirements. Pursuant to LAMC Sections 12.24.X.1(b)(2) and 12.22.A.26(j)(3), the Zoning Administrator may grant an exception to permit the existing yards in an Adaptive Reuse development to remain in lieu of present setback requirements.

- d. **Open Space.** The requirements set forth in Appendix D Section C.1(a) of the CCWSP requires that of a project's total required Open Space in compliance with LAMC Section 12.21.G, at least 100 square feet per unit must be provided as Common Open Space. However, pursuant to LAMC Section 12.21.G, the proposed project is not required to provide open space as the project will not result in new construction. Although the proposed project will result in additional units it will not result in additional floor area. Furthermore, the overall floor area of the subject existing building will decrease from 610,803 to 609,772 square feet in floor area. Thus, the project is not required to provide any open space pursuant to LAMC Section 12.21.G.

Additionally, requirements for on-site trees for residential projects are set forth in LAMC Section 12.21.G.2(a)(3) which also apply to new construction. As mentioned above, the proposed project will not result in any new ground-up construction or additional floor area therefore it cannot be categorized as new construction and does not need to comply with the on-site tree requirements set forth in Appendix D.C.2 of the CCSWP. Moreover, Appendix D.C.2 of the CCSWP would require the proposed project to provide a minimum of 700 on-site trees for a project that would otherwise not require any new trees per LAMC open space requirements. The applicant is requesting a Director's Interpretation to determine that the open space standards set forth in Appendix D Section C of the CCWSP, including Appendix D.C.1(a) requiring 100 square feet of common open space and Appendix D.C.2 requiring 1 on-site tree for every residential unit, do not apply to projects consisting entirely of the Adaptive Reuse of an existing building(s), because it would be impractical to provide additional trees on a site developed entirely with an existing building and it would defeat the purpose of the proposed Adaptive Reuse.

Furthermore, the project does not propose any removal of protected trees. There are three (3) existing street trees (palm trees) on 7th Street and two (2)

street trees (palm trees) on Bixel Street to remain. There is approximately 500 square feet of existing landscaping on the ground level along the southwest corner of the parking structure at 7th Street and Bixel Street that consists of ornamental hedges and ficuses. No new landscaping is proposed and the existing landscaping will be maintained. The proposed project will add 9 new off-site street trees. Therefore, the project is in substantial compliance with the Specific Plan with regards to open space.

- e. **Parking.** The project site located within ½ mile of several Major Transit Stops, therefore it is eligible to utilize AB 2097 which will allow the project to provide zero parking. The existing parking structure currently contains 1,110 vehicle parking spaces which are proposed to be maintained under the subject project. The majority of the existing parking spaces are compact, tandem, or commercial standard stalls with the exception of 34 parking stalls which provide the required minimum dimensions for residential parking stalls. Although, the 1,110 parking stalls are not require, the LAMC prohibits compact and tandem stalls for residential units until at least one residential standard stall is provided for every unit. As such, the applicant is requesting that the Zoning Administrator grant an exception from the requirement that one standard residential stall per unit be provided before any compact, tandem, or commercial stalls can be provided.
- f. **Inclusionary Housing.** Per the CCWSP residential projects must set aside either 15 percent of dwelling units for Low Income households or 8 percent of dwelling units for Very Low Income household, or residential project applicants can pay a fee to the Central City West Housing Trust Fund in lieu of providing the deed restricted affordable housing on-site. The Project Applicant will pay the in-lieu fee to satisfy the CCWSP Inclusionary Housing requirement.

- 2. **That the project incorporates mitigation measures, monitoring measures when necessary, or alternatives identified in the environmental review which would mitigate the negative environmental effects of the project, to the extent physically feasible.**

Mitigation measures are not necessary for the subject project, and there are no potentially significant negative environmental effects associated with the project. It has been determined the project is Categorically Exempt from the environmental review pursuant to State CEQA Guidelines Section 15332 Class 32. There is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.

### **ZONING ADMINISTRATOR DETERMINATION FINDINGS**

In order for an Adaptive Reuse project to be authorized, all of the legally mandated findings delineated in 12.24 X.1 of the LAMC must be made in the affirmative.

3. **The project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city or region.**

The subject site is a rectangular through lot consisting of seven (7) lots totaling approximately 59,553 square feet. The site is bounded by 7<sup>th</sup> Street to the south, Bixel Street to the west, an abutting alley to the north, and the Harbor (110) freeway to the east.

The site is improved with an existing 32-story 605,615 square-foot office building and an adjacent attached 10-level parking structure with 1,110 spaces and approximately 5,188 square feet of floor area for elevator lobbies, that was constructed in 1985.

The project site is located in a high-rise commercial corridor with commercial, hotel, and residential uses. Properties to the north, across the abutting alley, are zoned CW with a land use designation of Regional Center Commercial and developed with a seven-story mixed-use residential building as well as a 15-story commercial building. The 110 Harbor Freeway is adjacent to the subject property to the east. Properties to the south, across 7<sup>th</sup> Street, are zoned CW with a land use designation of Regional Center Commercial and developed with a six-story mixed-use residential building. Properties to the west, across Bixel Street are zoned CW with a land use designation of Regional Center Commercial and developed with surface parking lots, two-story motels, and a vacant lot.

The proposed project is an adaptive reuse and change of use of the existing office building into a multi-family residential building consisting of approximately 609,772 square feet of floor area and a maximum of 700 joint living and work quarter units. The proposed project will not physically enlarge the existing building footprint and will result in a slight decrease in the overall floor area. The project will maintain and provide the 1,110 vehicle parking spaces within the adjacent existing parking structure.

To facilitate the adaptive reuse of the building from office to residential, the applicant has requested, and the Zoning Administrator has granted the following: 1) a Zoning Administrator's Determination to allow an Adaptive Reuse Project in the C Zone outside of the Downtown Project Area, for the conversion of the approximately 610,803 square-foot into a residential building containing a maximum of approximately 609,772 square-feet of residential floor area and a maximum of 700 joint living and work quarters units with the following Adaptive Reuse Project incentives and exceptions: a Density incentive to allow 700 joint living and work quarters units, pursuant to LAMC Section LAMC 12.24.X.1 and 12.22 A.26(h)(2); a Site Plan Review Incentive for the construction, use, and maintenance of a new residential development that results in an increase of 50 or more residential dwelling units; an Existing Floor Area Exception pursuant to LAMC Section 12.22.A.26(j)(1), an Existing Yards Exception pursuant to LAMC 12.22A.26(j)(2), a Zoning Administrator's exception pursuant to LAMC



12.24.X.1(b)(2) to deviate from required parking design standards set forth in LAMC 12.21.A.5.

No significant alterations to the exterior facades of the existing commercial building are proposed. The adaptive reuse of the existing commercial building for residential purposes will maintain existing improvements and reinforce the development patterns of the area that surrounds the subject site. The built environment of the surrounding area will be maintained without significant alteration, and the existing character of the neighborhood will not be degraded or materially altered. Conversion of the existing office space to residential use provides a function that is beneficial to the community, through offering a residential living option with various amenities. The project enhances the community through the adaptive reuse of the existing built environment with a more viable use for the property. Therefore, the proposed project will enhance the built environment in the surrounding neighborhood as well as perform a function that is beneficial to the community.

4. **The project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare and safety.**

The subject site is a rectangular through lot consisting of seven (7) lots totaling approximately 59,553 square feet. The site is bounded by 7<sup>th</sup> Street to the south, Bixel Street to the west, an abutting alley to the north, and the Harbor (110) freeway to the east. The property has an approximately 355.8 linear feet frontage along West 7<sup>th</sup> Street and approximately 155.7 linear feet along Bixel Street.

The site is improved with an existing 32-story 610,803 square-foot office building and an adjacent attached 10-level parking structure with 1,110 spaces and approximately 5,188 square feet of floor area for elevator lobbies, that was constructed in 1985. The project site is located in a high-rise commercial corridor with commercial, hotel, and residential uses.

Properties to the north, across the abutting alley, are zoned CW with a land use designation of Regional Center Commercial and developed with a seven-story mixed-use residential building as well as a 15-story commercial building. The 110 Harbor Freeway is adjacent to the subject property to the east. Properties to the south, across 7<sup>th</sup> Street, are zoned CW with a land use designation of Regional Center Commercial and developed with a six-story mixed-use residential building. Properties to the west, across Bixel Street are zoned CW with a land use designation of Regional Center Commercial and developed with surface parking lots, two-story motels, and a vacant lot.

The project will convert an existing office building to accommodate 691 residential units, with approximately 609,772 square-feet of floor area. There will be no significant alterations to the exterior facades of the existing commercial building are proposed.

The first floor plan will be converted to accommodate a residential entryway, meeting rooms, manager's office, kitchen/break room, resident coffee lounge, business center, game room, club lounge, mail and reception area, and resident ride share lounge area. The second floor will be converted into residential live work units and one (1) theatre room. Floors three through six will be converted into residential live work units and one (1) fitness room for each floor. Floors seven through 32 will be converted to only residential live work units. The existing pent house will remain occupied with a mechanical room, fan room, pump room and second pump room. The upper roof level will maintain the existing emergency fire helicopter landing pad.

The subject site is within a High Quality Transit Area (HQTA), which are areas within one-half mile of a high quality transit corridor and with fixed route bus service with service intervals no longer than 15 minutes during peak commute hours. At the intersection of Figueroa and 7th Street (approximately 1,000 feet southeast of the site), is the 7th/Metro Center Station for Metro rail lines A, B, D, and E. This intersection is also currently served by Metro bus line 51, LADOT DASH lines A, E, F, Santa Clarita Transit bus line 799, and Foothill Transit bus line 699. The project is also eligible for Assembly Bill (AB) 2097 which permits development projects to provide zero vehicle parking spaces. The project proposes to maintain and provide the existing 1,100 parking spaces located within the adjacent 10-story parking structure.

The existing height of the building is approximately 463 feet seven inches; as proposed, no additional height would be added to the building. Los Angeles Municipal Code Section 12.22A.26(j)(2) provides an exception to the maximum building height limitation for the existing height of the building.

The existing office building and adjacent parking structure were originally constructed when the subject site was in Height District 4 which permitted an FAR of 13:1. Subsequently, in 1989 (per Ordinance No. 164,625) the site was changed to Height District 2, which permitted the site an FAR of 6:1. Per the CCWSP, the site is permitted an FAR of 6:1.

The subject properties including the existing office building and adjacent parking structure have a combined FAR of 10.26:1 or 610,803 square feet which was allowed under the Height District 4 designation. Presently, this FAR would not be allowed at the subject site. The proposed adaptive reuse project will not expand the existing footprint of the existing buildings. Pursuant to LAMC Sections 12.24.X.1(b)(2) and 12.22.A.26(j)(1), the Zoning Administrator may grant an exception to permit existing floor area in an adaptive reuse project to remain, even if it exceeds current FAR requirements. This exception is requested to permit the 10.24:1 FAR.

Additionally, per the CCWSP, the project site's U/6 Height Designation limits the buildings height to 1,268 feet above sea level. The existing building is 463 feet seven inches above sea level and does not propose to increase the height of the existing building.

The project site is zoned C4(CW)-U/6 and is located at the corner of West 7<sup>th</sup> Street and Bixel Street. The front yard is located along Bixel Street which has no setback requirements. There are no side yard requirements for commercial uses, however for residential uses a setback of five feet plus one foot for each story over the second floor is required and not to exceed 16 feet. The existing building is 33 stories tall therefore a 17-foot side yard setback would be required at the ground floor. There are no rear yard setback requirements for commercial uses, however, for residential uses a setback of 15 feet plus one foot for each story over the third story is required and not to exceed 20 feet. The existing building is 33 stories high therefore a 20-foot rear yard setback would be typically required at the ground floor.

The existing buildings provided minimal setbacks in all four yards in compliance with commercial use requirements, with the conversion of the subject property from commercial to residential the project will be required to provide the residential yard requirements. Pursuant to LAMC Sections 12.24.X.1(b)(2) and 12.22.A.26(j)(3), the Zoning Administrator may grant an exception to permit the exiting yards in an Adaptive Reuse development to remain, even without present setback requirements.

The project site located within a quarter-mile of several Major Transit Stops, therefore it is eligible to utilize AB 2097 which will allow the project to provide zero parking. The existing parking structure currently contains 1,110 vehicle parking spaces which are proposed to be maintained under the subject project. The majority of the existing parking spaces are compact, tandem, or commercial standard stalls with the exception of 34 parking stalls which provide the required minimum dimensions for residential parking stalls. Although, the 1,110 parking stalls are not required, the LAMC prohibits compact and tandem stalls for residential units until at least one residential standard stall is provided for every unit. As such, the applicant is requesting that the Zoning Administrator grant an exception from the requirement that one standard residential stall per unit be provided before any compact, tandem, or commercial stalls can be provided. The Zoning Administrator imposed a condition to require a transportation information board within the lobby of the subject building. The information board will allow residents of the building to be made aware of available transportation alternatives to travel throughout the region as well as utilize local buses to minimize use of cars and vehicle miles traveled.

The project does not propose any removal of protected trees. There are three existing street palm trees on 7th Street and two street palm trees on Bixel Street to remain. There is approximately 500 square feet of existing landscaping on the ground level along the southwest corner of the parking structure at 7th Street and Bixel Street that consists of ornamental hedges and ficuses. No new landscaping is proposed and the existing landscaping will be maintained. The proposed project will add nine new off-site street trees.

The project is compatible and consistent with the surrounding area. The project will introduce a residential use within a community surrounded by residential uses,

including multi-story mixed-use residential buildings. Moreover, because the project is the adaptive reuse of the existing commercial building and proposes not to alter the height or massing of the existing building, the project will not require significant construction activities that could adversely affect the surrounding community. The project will also result in interior retrofitting work which will improve the viability and seismic safety of the existing building. When occupied, the residential use of the project would remain compatible with the surrounding neighborhood as there is housing and office uses within the vicinity of the site. Therefore, the project's location, size, height, operations and other significant features will be compatible with and not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare and safety.

5. **The project substantially conforms with the purpose, intent and provisions of the General Plan, the applicable community plan, and any specific plan.**

The site is improved with an existing 32-story 605,615 square-foot office building and an adjacent attached 10-level parking structure with 1,110 spaces and approximately 5,188 square feet of floor area for elevator lobbies, that was constructed in 1985. The subject building is eligible for adaptive reuse pursuant to LAMC Section 12.25-A,25.d(ii), where the subject office building is no longer viable. Since the Covid pandemic, building occupancy has significantly decreased. The applicant stated that there have been issues collecting rent, and many remaining tenants have indicated they do not intend to renew their leases upon expiration. According to the applicant, the building is currently 67 percent occupied, which is not economically feasible. Another three percent will be moving out, and 55 percent of the building leases are expiring this year.

The applicant proposes to convert the entire 610,803 commercial office building with into a residential building with a maximum of 691 joint living and work quarters units.

The General Plan is the City's roadmap for future growth and development. The General Plan Elements establish goals, policies, purposes, and programs that provide for the regulatory environment in managing the City, and for addressing environmental concerns and problems. The majority of the policies derived from these elements are implemented in the form of Municipal Code requirements. The City of Los Angeles' General Plan consists of the Framework Elements, seven State-mandated Elements including, Land Use, Mobility, Housing, Conservation, Noise, Safety, and Open Space, and optional elements including Air Quality and Service Systems.

Framework Element

The Framework Element is a strategy for long-term growth which sets a citywide context to guide the update of the Community Plan and Citywide Elements. The Framework Element is a comprehensive, long range document containing purposes, policies, programs for the development of the City of Los Angeles. The

Citywide General Plan Framework text defines policies related to growth and includes policies for land use, housing, urban form/neighborhood design, open space/conservation, economic development, transportation, and infrastructure/public services.

The primary objectives of the policies in the Framework Element are to support the viability of the City's residential neighborhoods and commercial districts, and when growth occurs, to encourage sustainable growth in a number of higher-intensity commercial and mixed-use districts, centers and boulevards and industrial districts particularly in proximity to transportation corridors and transit stations.

The project is consistent with the following General Plan Framework Goals and Policies:

GOAL 3A: A physically balanced distribution of land uses that contributes towards and facilitates the City's long term fiscal and economic viability, revitalization of economically depressed areas, conservation of existing residential neighborhoods, equitable distribution of public resources, conservation of natural resources, provision of adequate infrastructure and public services, reduction of traffic congestion and improvement of air quality, enhancement of recreation and open space opportunities, assurance of environmental justice and a healthful living environment, and achievement of the vision for a more livable city.

Objective 3.2: Provide for the spatial distribution of development that promotes an improved quality of life by facilitating a reduction of vehicular trips, vehicle miles traveled, and air pollution.

Objective 3.4 Encourage new multi-family residential, retail commercial, and office development in the City's neighborhood districts, community, regional, and downtown centers as well as long primary transit corridors/boulevards, while at the same time conserving existing neighborhoods and related districts.

Goal 3C: Multi-family neighborhoods that enhance the quality of life for the City's existing and future residents.

Objective 3.7: Provide for the stability and enhancement of multi-family residential neighborhoods and allow for growth in areas where there is sufficient public infrastructure and services and the residents' quality of life can be maintained or improved.

The proposed adaptive reuse of an existing commercial building along Wilshire Boulevard, resulting in a residential use building, promotes reduced vehicular trips by making additional residential uses available in very close proximity to other commercial uses, with the potential for reducing vehicular trips, vehicle miles traveled, and air pollution, all in conformity with Objective 3.2 and 3.4.

The growth of a multi-family neighborhood within close proximity to Wilshire Boulevard is in proximity to sufficient public infrastructure and services to meet the project's demand. The project also includes streetscape improvements on a currently underutilized site that will enhance the future residents' quality of life. As such, the project is in conformance of Objective 3.7.

### Housing Element

The City's Housing Element for 2021- 2029 was adopted by City Council on November 24, 2021. The Housing Element identifies the City's housing conditions and needs, established the goals, objectives, and policies that are the foundation of the City's housing and growth strategy, and provides an array of programs the City intends to implement to create sustainable, mixed-income neighborhoods across the City. The Housing Element aims to provide affordable housing and amenity-rich, sustainable neighborhoods for its residents, answering the variety of housing needs of its growing population. Specifically, the Housing Element encourages units to accommodate all income groups.

**Goal 1:** A City where housing production results in an ample supply of housing to create more equitable and affordable options that meet existing and projected needs.

The project results in the creation of 691 new dwelling units. The project is compatible with existing development patterns adjacent to the project site, which are characterized by a variety of commercial and residential uses. As such, the proposed project substantially conforms to the purpose of the Housing Element of the General Plan.

### Mobility Element

The Mobility Plan 2035 includes goals that define the City's high-level mobility priorities. The Mobility Element sets forth objectives and policies to establish a citywide strategy to achieve long-term mobility and accessibility within the City of Los Angeles. Among other objectives and policies, the Mobility Plan aims to support ways to reduce vehicle miles traveled (VMT) per capita by increasing the availability of housing options with proximity to transit stations and major bus stops and offering more non-vehicle alternatives, including transit, walking, and bicycling.

Chapter 3 of this plan is entitled "Access for All Angelinos" and includes the discussion topic "A transportation system is only useful insofar as it is accessible and convenient."

**Policy 3.3: Land Use Access and Mix:** Promote equitable land use decisions that result in fewer vehicle trips by providing greater proximity and access to jobs, destinations, and other neighborhood services.

Policy 3.4: Transit Services: Provides all residents, workers and visitors with affordable, efficient, convenient, and attractive transit services.

Policy 3.5: Multi-Modal Features: Provides all residents, workers and visitors the opportunities to maximize multi-modal connectivity and access for transit riders given the close proximity to major transit stops.

The subject site is within a High Quality Transit Area (HQTA), which are areas within one-half mile of a high quality transit corridor and with fixed route bus service with service intervals no longer than 15 minutes during peak commute hours. At the intersection of Figueroa and 7th Street (approximately 1,000 feet southeast of the site), is the 7th/Metro Center Station for Metro rail lines A, B, D, and E. This intersection is also currently served by Metro bus line 51, LADOT DASH lines A, E, F, Santa Clarita Transit bus line 799, and Foothill Transit bus line 699.

The subject site is located in Downtown Los Angeles a major tourist hub and commercial hub in the City of Los Angeles. New residents of the project will be able to avail themselves of the many shopping, dining, entertainment, and cultural opportunities of the area. The need for the automobile will be greatly reduced by the many public transit options along 7<sup>th</sup> Street. The Project would encourage residents to walk, bike or take mass transit, effectively improving sustainable modes of transportation by providing residents opportunities for non-vehicular travel options.

The adaptive reuse project, resulting in a residential use building, with close access to multiple points for public transit, and one-site facilities for bicycle parking, is in conformance with 3.3, 3.4, 3.8, and 5.1.

#### Land Use Element- Westlake Community Plan

The Land Use of the City's General Plan divides the City into 35 Community Plans. The subject property is located within the Westlake Community Plan Area. The Westlake Community Plan was adopted by the City Council in 1985. The Community Plan's purpose is to enhance neighborhood characteristics while providing housing opportunities, improving commercial areas preserving community identity, development around transit, providing economic base, and improving the quality of the built environment. The Land Use Designations and corresponding zones in the Community Plan are implemented through zoning regulations in the Los Angeles Municipal Code (LAMC) including applicable ordinances that are codified in the LAMC. The Community Plan Area Map designates the property for Regional Center Commercial land uses corresponding to the C2, C4, P, PB, RAS3 and RAS4 Zones. The subject property is currently zoned C4(CW)-U/6 and is thus consistent with the existing land use designation. There are no other specific plans, overlays, or interim control ordinances pertaining to the project site.

The Westlake Community Plan text is silent with regards to the adaptive reuse of commercial building and the incentives this request allows. In such cases. the

Zoning Administrator must interpret the intent of the Plans. Specifically, the project addresses the following goals and policies of the Community Plan:

Objective 1: To designate a supply of residential land adequate to provide housing of the types, sizes, and densities required to satisfy the varying needs and desires of all segments of the community's population.

Objective 3: To sequence housing development so as to provide a workable, efficient, and adequate balance between land use, circulation, and service system facilities at all times.

Residential Policy 2: That medium density housing be located near commercial corridors where access to public transportation and shopping services is convenience and where a buffer from or transition between low density housing can be achieved.

Residential Policy 3: That housing for the elderly have convenient access to public transportation, commercial services, recreational and health facilities.

The proposed adaptive reuse of the existing commercial building, which is not eligible or currently listed as a historic resource, into a residential building is in conformance with the policies of the Community Plan. The project will result in the addition of 691 new residential units within the existing commercial building located in a commercial zone along 7<sup>th</sup> Street, which features a number of multi-family, commercial, and mixed-use development in the immediate area, in conformity with the above Goals and Policies. The property is located along 7<sup>th</sup> Street, a major transit, and commercial corridor containing several bus stops.

The subject site is within a High Quality Transit Area (HQTA), which are areas within one-half mile of a high quality transit corridor and with fixed route bus service with service intervals no longer than 15 minutes during peak commute hours. At the intersection of Figueroa and 7th Street (approximately 1,000 feet southeast of the site), is the 7th/Metro Center Station for Metro rail lines A, B, D, and E. This intersection is also currently served by Metro bus line 51, LADOT DASH lines A, E, F, Santa Clarita Transit bus line 799, and Foothill Transit bus line 699. The project is also eligible for Assembly Bill (AB) 2097 which permits development projects to provide zero vehicle parking spaces. The project proposes to maintain and provide the existing 1,100 parking spaces located within the adjacent 10-story parking structure. Therefore, as analyzed above, the project substantially conforms to the purpose, intent, and provisions of the General Plan and the applicable community plan.

## **FINDINGS FOR APPROVAL FOR ADAPTIVE REUSE PROJECTS**

- 6. The Adaptive Reuse Project complies with the standards for dwelling units, guest rooms and joint living and work quarters set forth in LAMC Section 12.22A.26.(i).**



The project proposed 691 new dwelling units, ranging in size from approximately 450 square feet to approximately 750 square feet. The proposed dwelling units are joint living and work quarters units and the minimum proposed unit size is 450 square feet and the average unit size is 750 square feet. As such, the project complies with the Adaptive Reuse Project minimum dwelling unit size of 450 square feet and an overall minimum average of 750 square feet, and therefore, complies with the standards prescribed by LAMC Section 12.22 A.26.(i).

**7. The surrounding area will not be adversely affected by overflow parking or traffic congestion originating or terminating at the site of the Adaptive Reuse Project.**

The project site is improved with a 32-story, approximately 610,803 square-foot, commercial office building with six levels of above grade parking within an adjacent 10-story parking structure that was constructed in 1985. The project will convert the entire office building into a residential building containing a maximum of approximately 609,772 square-feet of residential floor area. The project will result in 691 joint living and work quarters units with 1,100 (non-required) parking stalls. No new driveways or curb cuts are proposed.

The subject site is within a High Quality Transit Area (HQT), which are areas within one-half mile of a high quality transit corridor and with fixed route bus service with service intervals no longer than 15 minutes during peak commute hours. At the intersection of Figueroa and 7th Street (approximately 1,000 feet southeast of the site), is the 7th/Metro Center Station for Metro rail lines A, B, D, and E. This intersection is also currently served by Metro bus line 51, LADOT DASH lines A, E, F, Santa Clarita Transit bus line 799, and Foothill Transit bus line 699.

The project will convert an existing commercial office building with existing vehicle parking into residential units, with the existing vehicle parking to be maintained. As such, the conversion will not represent a significant change in vehicle traffic or parking, and is unlikely to have a significant impact on the surrounding community. The project does not trigger a Vehicle Miles Traveled (VMT) Analysis, nor does it trigger an access, safety, and circulation evaluation or assessment. The project's estimated trip generation and VMT would not require a transportation assessment, according to Los Angeles Department of Transportation, and the project would be presumed to have less-than-significant transportation impacts.

Although the project is not required to provide any vehicle parking pursuant to AB 2097, the project is nonetheless providing 1,110 vehicle parking spaces for residents and guests entirely internally, which will prevent any parking impacts in the surrounding area. Therefore, the surrounding area will not be adversely affected by overflow parking or traffic congestion.

**ADDITIONAL MANDATORY FINDINGS**

8. The National Flood Insurance Program rate maps, which are a part of the Flood Hazard Management Specific Plan adopted by the City Council by Ordinance No. 172,081, have been reviewed and it has been determined that this project is located outside of a flood zone area.

**APPEAL PERIOD - EFFECTIVE DATE**

This grant is not a permit or license and any permits and/or licenses required by law must be obtained from the proper public agency. If any Condition of this grant is violated or not complied with, then the applicant or their successor in interest may be prosecuted for violating these Conditions the same as for any violation of the requirements contained in the Los Angeles Municipal Code (LAMC).

This determination will become effective after the end of appeal period date on the first page of this document unless an appeal is filed with the Department of City Planning. An appeal application must be submitted and paid for before 4:30 PM (PST) on the final day to appeal the determination. Should the final day fall on a weekend or legal City holiday, the time for filing an appeal shall be extended to 4:30 PM (PST) on the next succeeding working day. Appeals should be filed early to ensure the Development Services Center (DSC) staff has adequate time to review and accept the documents, and to allow appellants time to submit payment.

An appeal may be filed utilizing the following options:

**Online Application System (OAS):** The OAS (<https://planning.lacity.org/oas>) allows entitlement appeals to be submitted entirely electronically by allowing an appellant to fill out and submit an appeal application online directly to City Planning’s DSC, and submit fee payment by credit card or e-check.

**Drop off at DSC.** Appeals of this determination can be submitted in-person at the Metro or Van Nuys DSC locations, and payment can be made by credit card or check. City Planning has established drop-off areas at the DSCs with physical boxes where appellants can drop off appeal applications; alternatively, appeal applications can be filed with staff at DSC public counters. Appeal applications must be on the prescribed forms and accompanied by the required fee and a copy of the determination letter. Appeal applications shall be received by the DSC public counter and paid for on or before the above date or the appeal will not be accepted.

Forms are available online at <http://planning.lacity.org/development-services/forms>.  
Public offices are located at:

Metro DSC  
(213) 482-7077  
201 N. Figueroa Street  
Los Angeles, CA 90012  
planning.figcounter@lacity.org

Van Nuys DSC  
(818) 374-5050  
6262 Van Nuys Boulevard  
Van Nuys, CA 91401  
planning.mbc2@lacity.org

West Los Angeles DSC  
(CURRENTLY CLOSED)  
(310) 231-2901  
1828 Sawtelle Boulevard  
West Los Angeles, CA 90025  
planning.westla@lacity.org

City Planning staff may follow up with the appellant via email and/or phone if there are any questions or missing materials in the appeal submission, to ensure that the appeal package is complete and meets the applicable LAMC provisions.

If you seek judicial review of any decision of the City pursuant to California Code of Civil Procedure Section 1094.5, the petition for writ of mandate pursuant to that section must be filed no later than the 90th day following the date on which the City's decision became final pursuant to California Code of Civil Procedure Section 1094.6. There may be other time limits which also affect your ability to seek judicial review.

Verification of condition compliance with building plans and/or building permit applications are done at the City Planning Metro or Valley DSC locations. An in-person or virtual appointment for Condition Clearance can be made through the City's [BuildLA](https://www.lacity.org/buildla) portal ([appointments.lacity.org](https://appointments.lacity.org)). The applicant is further advised to notify any consultant representing you of this requirement as well.



QR Code to  
Online Appeal  
Filing



QR Code to Forms for In-  
Person Appeal Filing



QR Code to BuildLA Appointment  
Portal for Condition Clearance

Inquiries regarding this matter shall be directed to Stephanie Escobar, Planning Staff for Los Angeles City Planning, at (213)978-1492.

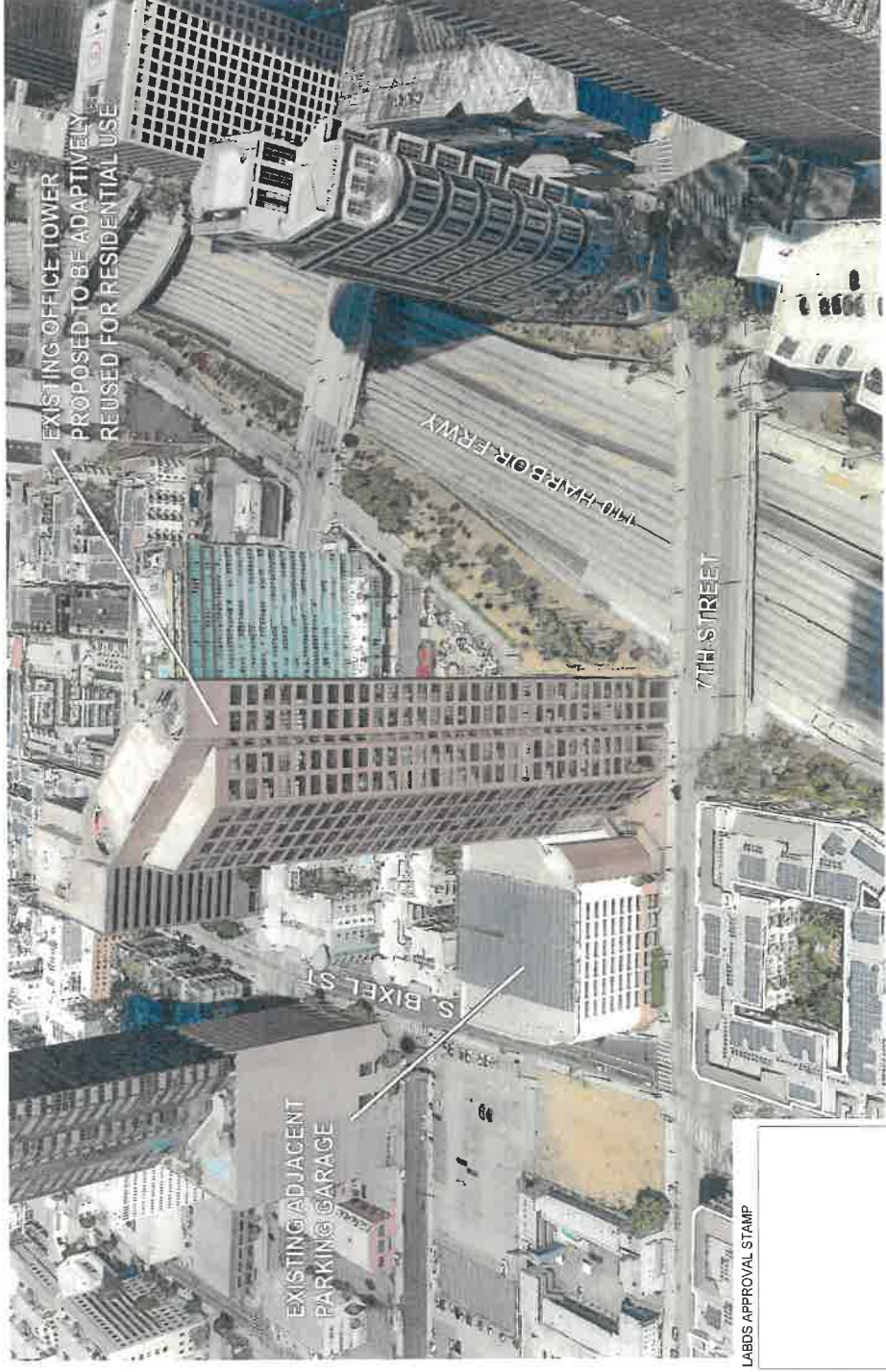


HENRY CHU  
Associate Zoning Administrator

HC:MS:SE:ss

cc: Councilmember Eunisses Hernandez  
First Council District  
Adjoining Property Owners

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SHEET TOTAL: 31	



COVER SHEET  
0.00

1055 7TH STREET, LOS ANGELES, CA

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

10/13/2023

SCHEMATIC DESIGN

EXHIBIT "A"  
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Case No. LA-2023-4115-SF-DI-2AD

AF

**EXHIBIT 'A'**  
 Page No. 2 of 31  
 Case No. 1A-2023-415

**PROPOSED PROJECT INFORMATION**

**PROJECT ADDRESS:**  
 1055 7TH ST, LOS ANGELES, CA 90017

**APN:** 18020191

**PROPOSED PROJECT:**  
 REUSE OF EXISTING OFFICE TOWER TO BE RESIDENTIAL UNITS (JOINT LIVING AND WORK QUARTERS UNITS)

**OWNER:** 1055 WEST 7TH STREET, LLC  
 LOS ANGELES, CA 90010

**ZONE:** CA-PT-10  
 CENTRAL CITY WEST SPECIFIC PLAZONE

**NEIGHBORHOOD COUNCIL:** DOWNTOWN LOS ANGELES COMMUNITY COUNCIL DISTRICT 2D

**GENERAL PLAN USE:** REGIONAL CENTER COMMERCIAL

**ADAPTIVE REUSE AREA ADOPTED:** ADAPTIVE REUSE AREA ADOPTED

**EXISTING AND PROPOSED USES:**  
 EXISTING: OFFICE TOWER (TYPE 1)  
 PROPOSED: CONCRETE AND TIE PROTECTED STEEL SPRING BRIDGE OCCUPIED FLOORS - NO 13TH FLOOR PLUS 7 LEVELS OF REPAIR MECHANICAL GARAGE EXISTING SEPARATE IN LEVEL TYPE 1 MECHANICAL GARAGE EXISTING STRUCTURE-TYPICAL TYPE 1 SPINNAZLED.

**PROPOSED PROJECT LAND USE SUMMARY**

Category	Existing	Proposed	Change
Residential Units	0	681	+681
Commercial Units	0	0	0
Office Units	0	0	0
Garage Spaces	0	0	0
Other	0	0	0
<b>TOTAL</b>	<b>0</b>	<b>681</b>	<b>+681</b>

**EXISTING PARKING SUMMARY**

Level	Existing	Proposed	Change
Level 1	15	15	0
Level 2	15	15	0
Level 3	15	15	0
Level 4	15	15	0
Level 5	15	15	0
Level 6	15	15	0
Level 7	15	15	0
Level 8	15	15	0
Level 9	15	15	0
Level 10	15	15	0
Level 11	15	15	0
Level 12	15	15	0
Level 13	15	15	0
Level 14	15	15	0
Level 15	15	15	0
Level 16	15	15	0
Level 17	15	15	0
Level 18	15	15	0
Level 19	15	15	0
Level 20	15	15	0
Level 21	15	15	0
Level 22	15	15	0
Level 23	15	15	0
Level 24	15	15	0
Level 25	15	15	0
Level 26	15	15	0
Level 27	15	15	0
Level 28	15	15	0
Level 29	15	15	0
Level 30	15	15	0
Level 31	15	15	0
Level 32	15	15	0
Level 33	15	15	0
Level 34	15	15	0
Level 35	15	15	0
Level 36	15	15	0
Level 37	15	15	0
Level 38	15	15	0
Level 39	15	15	0
Level 40	15	15	0
Level 41	15	15	0
Level 42	15	15	0
Level 43	15	15	0
Level 44	15	15	0
Level 45	15	15	0
Level 46	15	15	0
Level 47	15	15	0
Level 48	15	15	0
Level 49	15	15	0
Level 50	15	15	0
Level 51	15	15	0
Level 52	15	15	0
Level 53	15	15	0
Level 54	15	15	0
Level 55	15	15	0
Level 56	15	15	0
Level 57	15	15	0
Level 58	15	15	0
Level 59	15	15	0
Level 60	15	15	0
Level 61	15	15	0
Level 62	15	15	0
Level 63	15	15	0
Level 64	15	15	0
Level 65	15	15	0
Level 66	15	15	0
Level 67	15	15	0
Level 68	15	15	0
Level 69	15	15	0
Level 70	15	15	0
Level 71	15	15	0
Level 72	15	15	0
Level 73	15	15	0
Level 74	15	15	0
Level 75	15	15	0
Level 76	15	15	0
Level 77	15	15	0
Level 78	15	15	0
Level 79	15	15	0
Level 80	15	15	0
Level 81	15	15	0
Level 82	15	15	0
Level 83	15	15	0
Level 84	15	15	0
Level 85	15	15	0
Level 86	15	15	0
Level 87	15	15	0
Level 88	15	15	0
Level 89	15	15	0
Level 90	15	15	0
Level 91	15	15	0
Level 92	15	15	0
Level 93	15	15	0
Level 94	15	15	0
Level 95	15	15	0
Level 96	15	15	0
Level 97	15	15	0
Level 98	15	15	0
Level 99	15	15	0
Level 100	15	15	0
<b>TOTAL</b>	<b>1,500</b>	<b>1,500</b>	<b>0</b>

**EXISTING TOWER FAR SUMMARY**

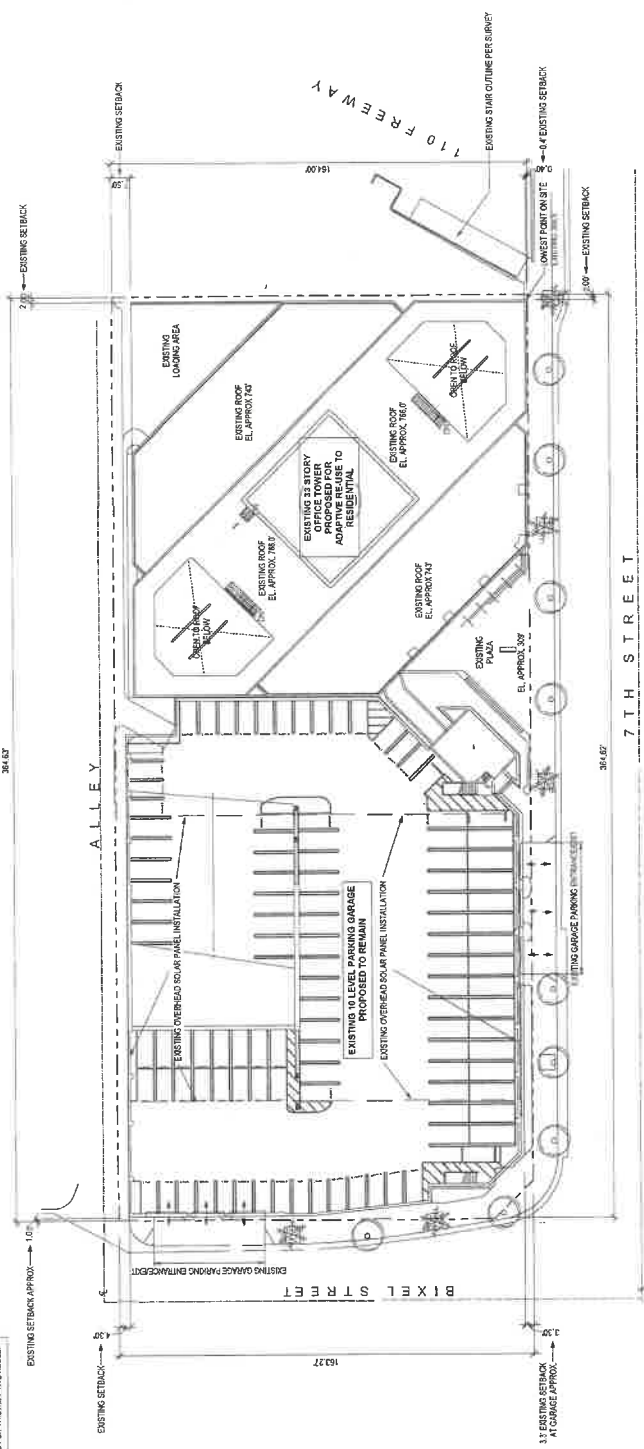
Level	Existing	Proposed	Change
Level 1	15,000 SF	15,000 SF	0
Level 2	15,000 SF	15,000 SF	0
Level 3	15,000 SF	15,000 SF	0
Level 4	15,000 SF	15,000 SF	0
Level 5	15,000 SF	15,000 SF	0
Level 6	15,000 SF	15,000 SF	0
Level 7	15,000 SF	15,000 SF	0
Level 8	15,000 SF	15,000 SF	0
Level 9	15,000 SF	15,000 SF	0
Level 10	15,000 SF	15,000 SF	0
Level 11	15,000 SF	15,000 SF	0
Level 12	15,000 SF	15,000 SF	0
Level 13	15,000 SF	15,000 SF	0
Level 14	15,000 SF	15,000 SF	0
Level 15	15,000 SF	15,000 SF	0
Level 16	15,000 SF	15,000 SF	0
Level 17	15,000 SF	15,000 SF	0
Level 18	15,000 SF	15,000 SF	0
Level 19	15,000 SF	15,000 SF	0
Level 20	15,000 SF	15,000 SF	0
Level 21	15,000 SF	15,000 SF	0
Level 22	15,000 SF	15,000 SF	0
Level 23	15,000 SF	15,000 SF	0
Level 24	15,000 SF	15,000 SF	0
Level 25	15,000 SF	15,000 SF	0
Level 26	15,000 SF	15,000 SF	0
Level 27	15,000 SF	15,000 SF	0
Level 28	15,000 SF	15,000 SF	0
Level 29	15,000 SF	15,000 SF	0
Level 30	15,000 SF	15,000 SF	0
Level 31	15,000 SF	15,000 SF	0
Level 32	15,000 SF	15,000 SF	0
Level 33	15,000 SF	15,000 SF	0
Level 34	15,000 SF	15,000 SF	0
Level 35	15,000 SF	15,000 SF	0
Level 36	15,000 SF	15,000 SF	0
Level 37	15,000 SF	15,000 SF	0
Level 38	15,000 SF	15,000 SF	0
Level 39	15,000 SF	15,000 SF	0
Level 40	15,000 SF	15,000 SF	0
Level 41	15,000 SF	15,000 SF	0
Level 42	15,000 SF	15,000 SF	0
Level 43	15,000 SF	15,000 SF	0
Level 44	15,000 SF	15,000 SF	0
Level 45	15,000 SF	15,000 SF	0
Level 46	15,000 SF	15,000 SF	0
Level 47	15,000 SF	15,000 SF	0
Level 48	15,000 SF	15,000 SF	0
Level 49	15,000 SF	15,000 SF	0
Level 50	15,000 SF	15,000 SF	0
Level 51	15,000 SF	15,000 SF	0
Level 52	15,000 SF	15,000 SF	0
Level 53	15,000 SF	15,000 SF	0
Level 54	15,000 SF	15,000 SF	0
Level 55	15,000 SF	15,000 SF	0
Level 56	15,000 SF	15,000 SF	0
Level 57	15,000 SF	15,000 SF	0
Level 58	15,000 SF	15,000 SF	0
Level 59	15,000 SF	15,000 SF	0
Level 60	15,000 SF	15,000 SF	0
Level 61	15,000 SF	15,000 SF	0
Level 62	15,000 SF	15,000 SF	0
Level 63	15,000 SF	15,000 SF	0
Level 64	15,000 SF	15,000 SF	0
Level 65	15,000 SF	15,000 SF	0
Level 66	15,000 SF	15,000 SF	0
Level 67	15,000 SF	15,000 SF	0
Level 68	15,000 SF	15,000 SF	0
Level 69	15,000 SF	15,000 SF	0
Level 70	15,000 SF	15,000 SF	0
Level 71	15,000 SF	15,000 SF	0
Level 72	15,000 SF	15,000 SF	0
Level 73	15,000 SF	15,000 SF	0
Level 74	15,000 SF	15,000 SF	0
Level 75	15,000 SF	15,000 SF	0
Level 76	15,000 SF	15,000 SF	0
Level 77	15,000 SF	15,000 SF	0
Level 78	15,000 SF	15,000 SF	0
Level 79	15,000 SF	15,000 SF	0
Level 80	15,000 SF	15,000 SF	0
Level 81	15,000 SF	15,000 SF	0
Level 82	15,000 SF	15,000 SF	0
Level 83	15,000 SF	15,000 SF	0
Level 84	15,000 SF	15,000 SF	0
Level 85	15,000 SF	15,000 SF	0
Level 86	15,000 SF	15,000 SF	0
Level 87	15,000 SF	15,000 SF	0
Level 88	15,000 SF	15,000 SF	0
Level 89	15,000 SF	15,000 SF	0
Level 90	15,000 SF	15,000 SF	0
Level 91	15,000 SF	15,000 SF	0
Level 92	15,000 SF	15,000 SF	0
Level 93	15,000 SF	15,000 SF	0
Level 94	15,000 SF	15,000 SF	0
Level 95	15,000 SF	15,000 SF	0
Level 96	15,000 SF	15,000 SF	0
Level 97	15,000 SF	15,000 SF	0
Level 98	15,000 SF	15,000 SF	0
Level 99	15,000 SF	15,000 SF	0
Level 100	15,000 SF	15,000 SF	0
<b>TOTAL</b>	<b>1,500,000 SF</b>	<b>1,500,000 SF</b>	<b>0</b>

**EXISTING GARAGE FAR SUMMARY**

Level	Existing	Proposed	Change
Level 1	15,000 SF	15,000 SF	0
Level 2	15,000 SF	15,000 SF	0
Level 3	15,000 SF	15,000 SF	0
Level 4	15,000 SF	15,000 SF	0
Level 5	15,000 SF	15,000 SF	0
Level 6	15,000 SF	15,000 SF	0
Level 7	15,000 SF	15,000 SF	0
Level 8	15,000 SF	15,000 SF	0
Level 9	15,000 SF	15,000 SF	0
Level 10	15,000 SF	15,000 SF	0
Level 11	15,000 SF	15,000 SF	0
Level 12	15,000 SF	15,000 SF	0
Level 13	15,000 SF	15,000 SF	0
Level 14	15,000 SF	15,000 SF	0
Level 15	15,000 SF	15,000 SF	0
Level 16	15,000 SF	15,000 SF	0
Level 17	15,000 SF	15,000 SF	0
Level 18	15,000 SF	15,000 SF	0
Level 19	15,000 SF	15,000 SF	0
Level 20	15,000 SF	15,000 SF	0
Level 21	15,000 SF	15,000 SF	0
Level 22	15,000 SF	15,000 SF	0
Level 23	15,000 SF	15,000 SF	0
Level 24	15,000 SF	15,000 SF	0
Level 25	15,000 SF	15,000 SF	0
Level 26	15,000 SF	15,000 SF	0
Level 27	15,000 SF	15,000 SF	0
Level 28	15,000 SF	15,000 SF	0
Level 29	15,000 SF	15,000 SF	0
Level 30	15,000 SF	15,000 SF	0
Level 31	15,000 SF	15,000 SF	0
Level 32	15,000 SF	15,000 SF	0
Level 33	15,000 SF	15,000 SF	0
Level 34	15,000 SF	15,000 SF	0
Level 35	15,000 SF	15,000 SF	0
Level 36	15,000 SF	15,000 SF	0
Level 37	15,000 SF	15,000 SF	0
Level 38	15,000 SF	15,000 SF	0
Level 39	15,000 SF	15,000 SF	0



**NOTE:**  
 1. THESE DRAWINGS ARE PRELIMINARY AND ARE SUBJECT TO CHANGE WITHOUT NOTICE.  
 2. THE DESIGNER HAS CONDUCTED VISUAL VERIFICATION OF THE EXISTING CONDITIONS AND HAS FOUND THEM TO BE AS SHOWN ON THESE DRAWINGS.  
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LABS APPROVAL STAMP

SEE SURVEY FOR FURTHER INFORMATION SHEET 1.04

PLOT PLAN  
 1.02

1055 7TH STREET, LOS ANGELES, CA

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS SCHEMATIC DESIGN

**EXHIBIT "A"**  
 Page No. 3 of 31  
 Case No. TA-2023-415

19132023





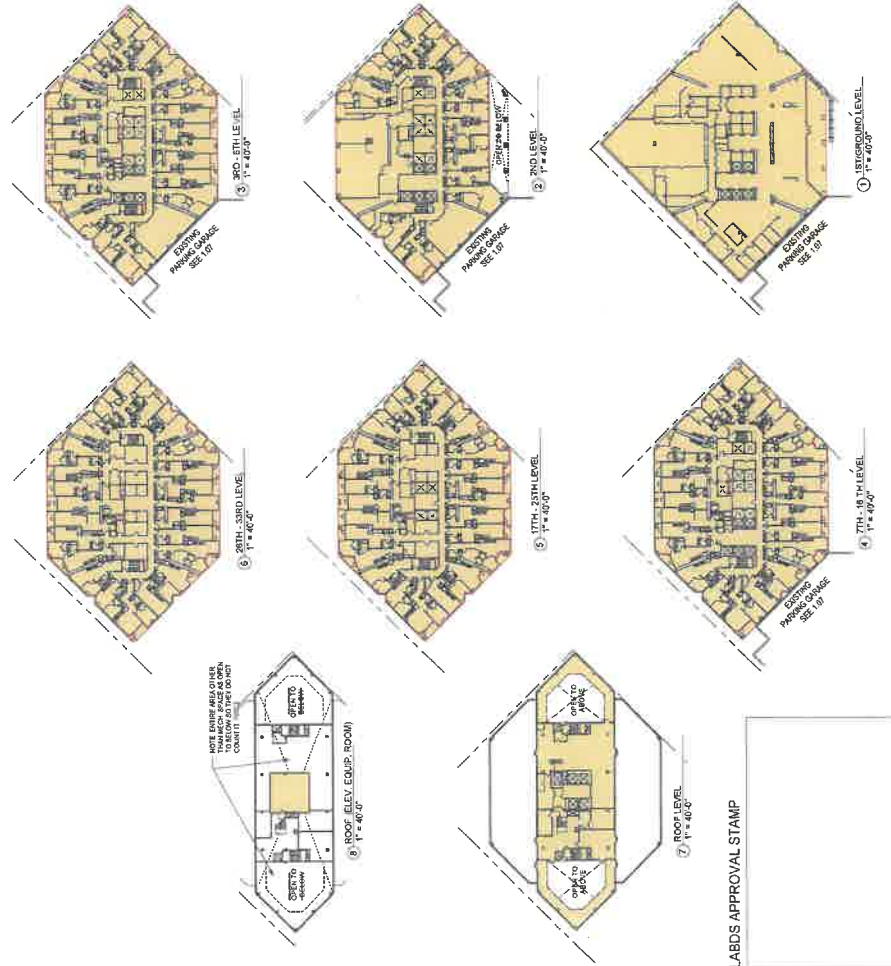
**BUILDING AREA SUMMARY - TOWER**

BUILDING AREA NON-PARKING	
15TH FLOOR LEVEL	21,718 SF
RESIDENTIAL USES COVERED EXTERIOR	1,200 SF
RESIDENTIAL USES COVERED EXTERIOR	7,028 SF
20TH LEVEL	19,009 SF
RESIDENTIAL USES COVERED EXTERIOR	300 SF
RESIDENTIAL USES COVERED EXTERIOR	18,477 SF
21ST LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
22ND LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
23RD LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
24TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
25TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
26TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
27TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
28TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
29TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
30TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
31ST LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
32ND LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
33RD LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
34TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
35TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
36TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
37TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
38TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
39TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
40TH LEVEL	18,478 SF
RESIDENTIAL USES COVERED EXTERIOR	1,144 SF
RESIDENTIAL USES COVERED EXTERIOR	2,620 SF
ROOF	8,022 SF
ROOF ELEV. EQUIP. ROOM	1,028 SF
RESIDENTIAL USES	1,028 SF
TOTAL TOWER BUILDING AREA	670,492 SF

**KEY**  
 NOTE: ONLY COVERED AREAS ARE INCLUDED IN AREA CALC.  
 MARKING USES  
 NON-PARKING USES



**BUILDING AREA SUMMARY**  
**1.06**



**1055 7TH STREET, LOS ANGELES, CA**  
**PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS**  
**SCHEMATIC DESIGN**

10/13/2023

**EXHIBIT 'A'**  
 Page No. 5 of 31  
 Case No. 24-2003-4115

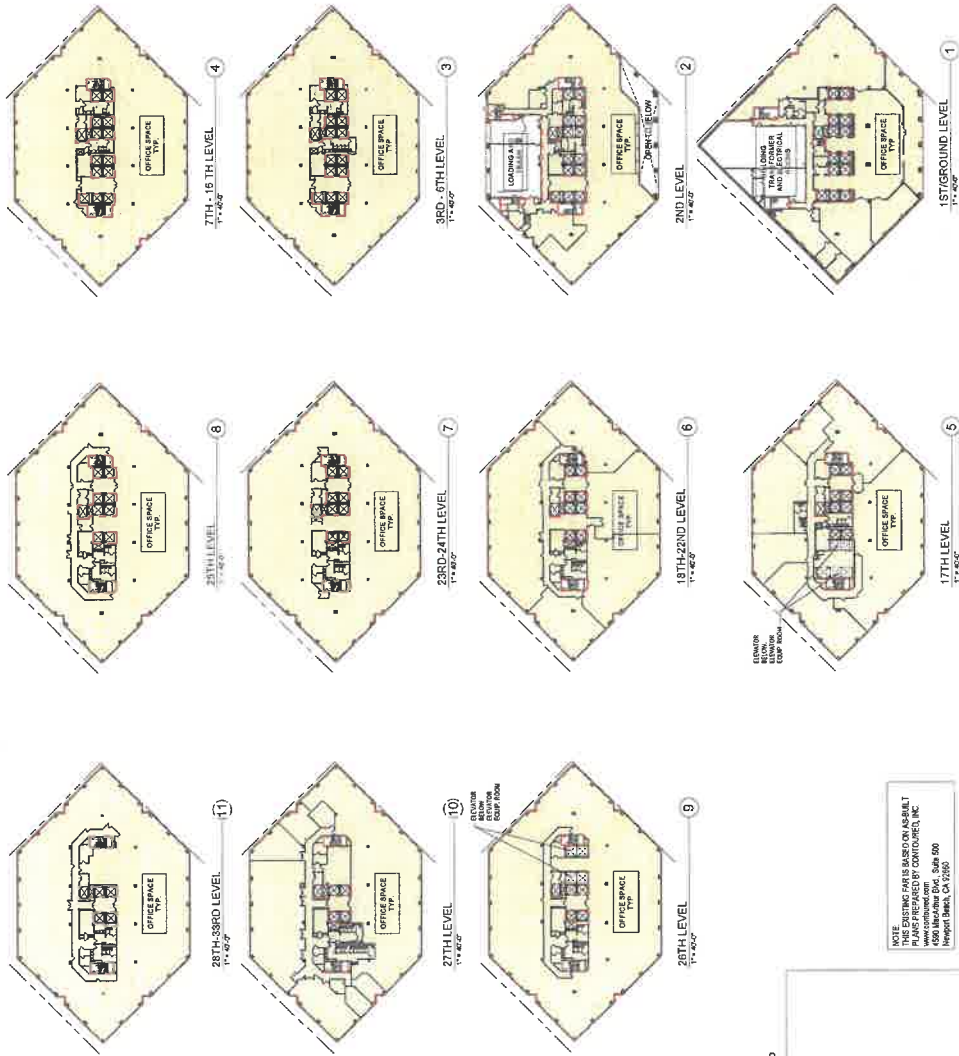








EXISTING TOWER FAR SUMMARY	
1ST FLOOR LEVEL	COMMERCIAL 16,208 SF
2ND LEVEL	COMMERCIAL 14,033 SF
3RD LEVEL	SERVICE 2,807 SF
4TH LEVEL	COMMERCIAL 16,208 SF
5TH LEVEL	COMMERCIAL 16,208 SF
6TH LEVEL	COMMERCIAL 16,208 SF
7TH LEVEL	COMMERCIAL 16,208 SF
8TH LEVEL	COMMERCIAL 16,208 SF
9TH LEVEL	COMMERCIAL 16,208 SF
10TH LEVEL	COMMERCIAL 16,208 SF
11TH LEVEL	COMMERCIAL 16,208 SF
12TH LEVEL	COMMERCIAL 16,208 SF
13TH LEVEL	COMMERCIAL 16,208 SF
14TH LEVEL	COMMERCIAL 16,208 SF
15TH LEVEL	COMMERCIAL 16,208 SF
16TH LEVEL	COMMERCIAL 16,208 SF
17TH LEVEL	COMMERCIAL 16,208 SF
18TH LEVEL	COMMERCIAL 16,208 SF
19TH LEVEL	COMMERCIAL 16,208 SF
20TH LEVEL	COMMERCIAL 16,208 SF
21ST LEVEL	COMMERCIAL 16,208 SF
22ND LEVEL	COMMERCIAL 16,208 SF
23RD LEVEL	COMMERCIAL 16,208 SF
24TH LEVEL	COMMERCIAL 16,208 SF
25TH LEVEL	COMMERCIAL 16,208 SF
26TH LEVEL	COMMERCIAL 16,208 SF
27TH LEVEL	COMMERCIAL 16,208 SF
28TH LEVEL	COMMERCIAL 16,208 SF
29TH LEVEL	COMMERCIAL 16,208 SF
30TH LEVEL	COMMERCIAL 16,208 SF
31ST LEVEL	COMMERCIAL 16,208 SF
32ND LEVEL	COMMERCIAL 16,208 SF
33RD LEVEL	COMMERCIAL 16,208 SF
TOTAL EXISTING TOWER FAR	606,675 SF



EXISTING GARAGE FAR SUMMARY	
GARAGE - B1	ELEVATOR LOBBY 514 SF
GARAGE - P1	ELEVATOR LOBBY 509 SF
GARAGE - P2	ELEVATOR LOBBY 514 SF
GARAGE - P3	ELEVATOR LOBBY 514 SF
GARAGE - P4	ELEVATOR LOBBY 514 SF
GARAGE - P5	ELEVATOR LOBBY 514 SF
GARAGE - P6	ELEVATOR LOBBY 514 SF
GARAGE - P7	ELEVATOR LOBBY 514 SF
GARAGE - P8	ELEVATOR LOBBY 514 SF
GARAGE - P9	ELEVATOR LOBBY 514 SF
TOTAL EXISTING GARAGE FAR	5,189 SF

EXISTING TOWER FAR	606,675 SF
EXISTING GARAGE FAR	5,189 SF
TOTAL EXISTING TOWER & GARAGE FAR	610,863 SF

COLOR LEGEND

□ COMMERCIAL

□ SERVICE

FLOOR AREA (DARKER BY 0.10 IN. 100.000 SF. 0.10000) THE AREA IN SQUARE FEET COMPARED WITH THE EXISTING WALLS OF BUILDING, BUT NOT INCLUDING THE AREA OF OPENING EQUIPMENT OR MACHINERY, PARKING AREAS WITH ASSOCIATED DRIVEWAYS AND RAMP, SPACE REQUIRED TO INSTALL PARKING SPACES FOR THE EXISTING AND STORAGE OF VEHICLES.

EXISTING TOWER FAR SUMMARY & DIAGRAMS

1.09

1055 7TH STREET, LOS ANGELES, CA

SCHEMATIC DESIGN

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

10/13/2023

LABS APPROVAL STAMP



NOTE:  
THE EXISTING TOWER IS A CLASS 1  
PLANS PREPARED BY CONTORON, INC.  
www.contoron.com  
44th 500  
Newport Beach, CA 92660

EXHIBIT of 31  
Page No. 20-23-116  
Case No. AD



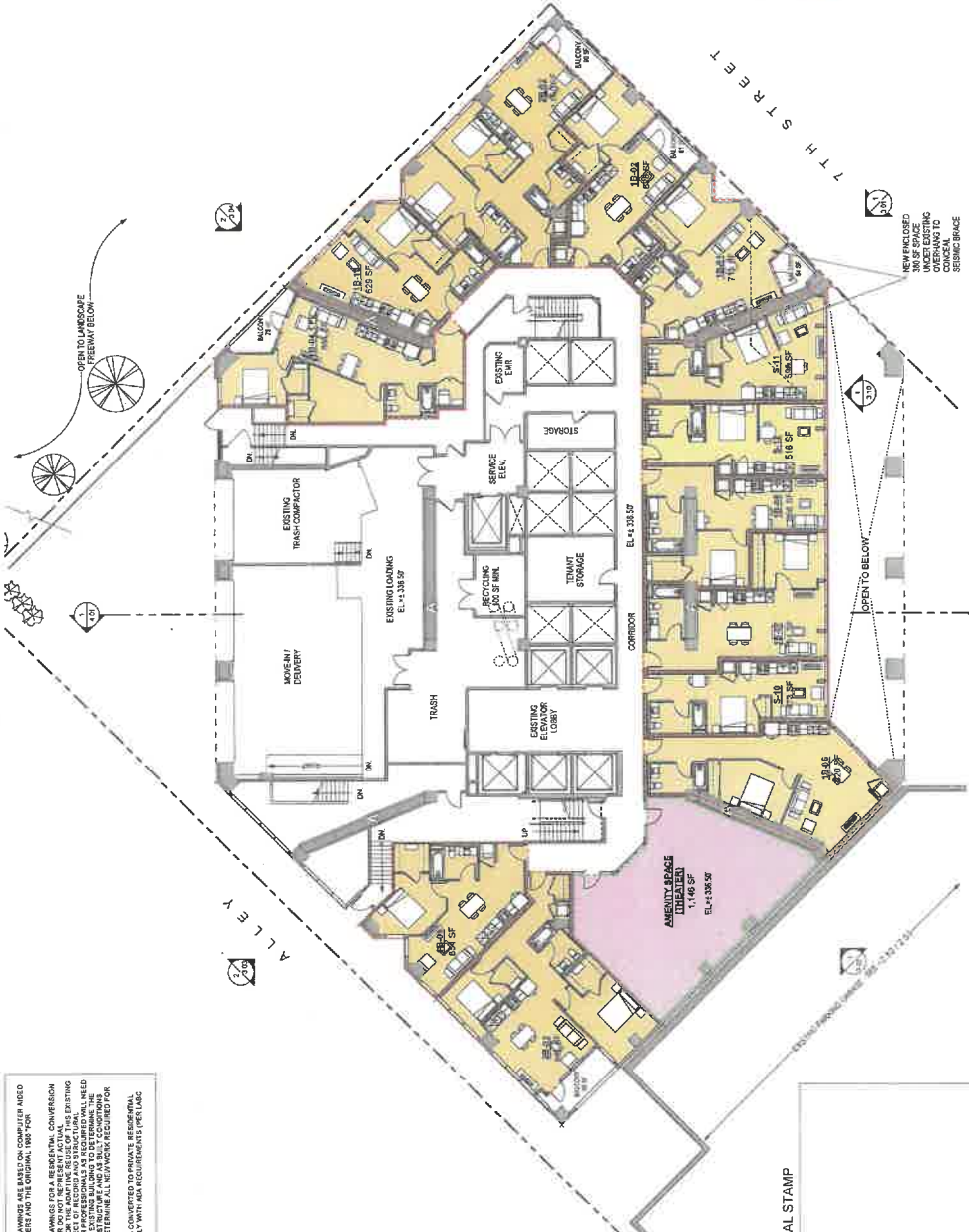




THESE SCHEMATIC DESIGN DRAWINGS ARE BASED ON COMPLETELY AS-BUILT RECORD DRAWINGS AND THE ORIGINAL 1980-FOR CONSTRUCTION PLANS.

THESE SCHEMATIC DESIGN DRAWINGS FOR A RESIDENTIAL CONVERSION PROJECT SHALL BE PROVIDED ON THE ADAPTIVE REUSE OF THE EXISTING BUILDING AREAS WITH THE PROFESSIONAL'S RECORD AND ALL HELD RESPONSIBILITY OF THE BUILDING STRUCTURE AND ALL BUILDING CONDITIONS SHALL BE DETERMINED TO DETERMINE ALL NECESSARY RECORDS FOR THE ADAPTIVE REUSE.

ALL EXISTING BUILDING AREAS CONVERTED TO PRIVATE RESIDENTIAL USE SHALL COMPLY WITH ALL RECORDING'S PER L.A.C. 174 AND L.A.M.C. 12.14.



- NEW BEARING WALLS PER NATIVE REUSE STRUCTURE ENGINEER
- EXISTING BEARING WALLS
- INDICATES BOUNDARY OF LINKS OF AREA NOTED ON PLAN
- INDICATES BOUNDARY OF BALCONY AS NOTED ON PLAN
- INDICATES BOUNDARY OF ADAPTIVE REUSE AS NOTED ON PLAN



FLOOR PLAN (LEVEL 02)  
2.02

LABBS APPROVAL STAMP

RECYCLING CHUTES SHALL BE CLEARLY MARKED 'RECYCLING ONLY' AT EIGHT FOOT CEILING (8'0")

101M2023

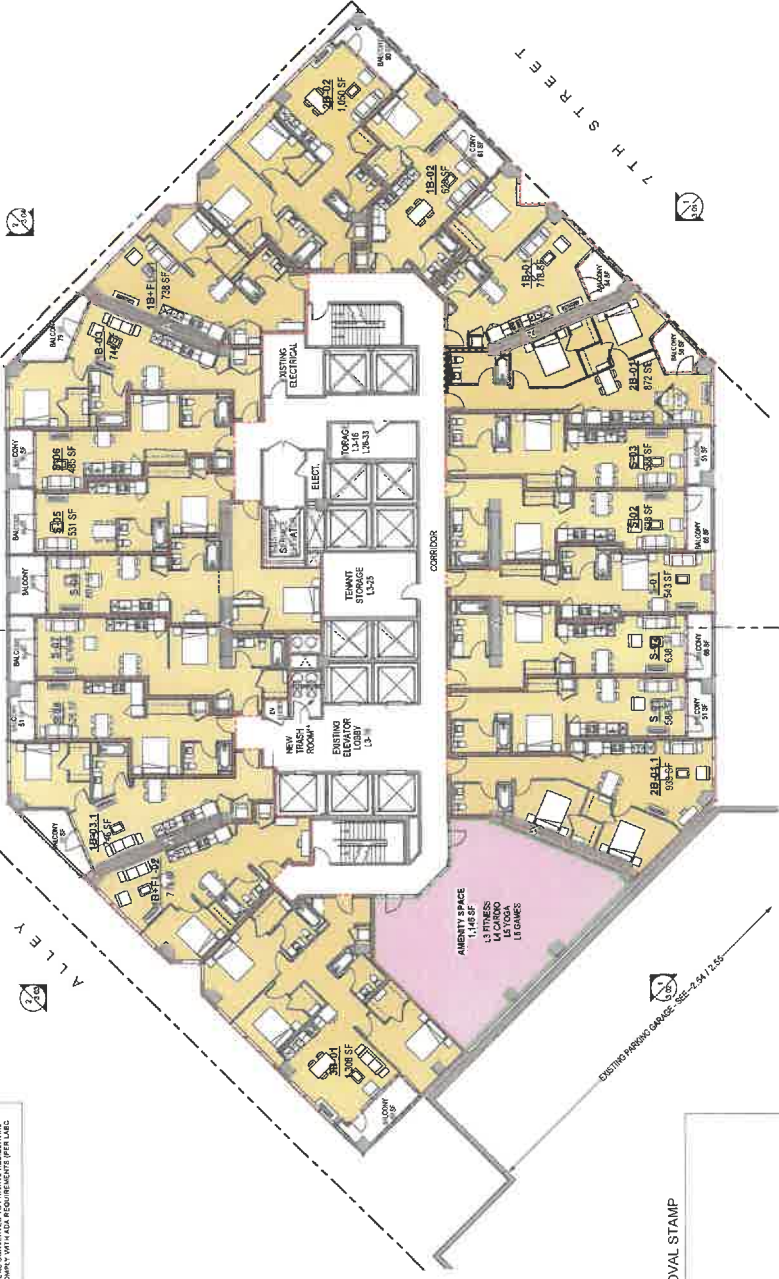
1055 7TH STREET, LOS ANGELES, CA  
SCHEMATIC DESIGN

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

EXHIBIT A  
Page No. 11 of 31  
Case No. 2A-2023-4115

AD

NOTE: THIS PLAN, SPECIFICATIONS, DIMENSIONS AND ALL INFORMATION CONTAINED HEREIN IS THE PROPERTY OF THE ARCHITECT AND IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN. ANY REUSE OR MODIFICATION OF THIS DOCUMENT WITHOUT THE WRITTEN CONSENT OF THE ARCHITECT IS STRICTLY PROHIBITED. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY ERRORS OR OMISSIONS IN THIS DOCUMENT. THE ARCHITECT'S RESPONSIBILITY IS LIMITED TO THE DESIGN AND CONSTRUCTION OF THE PROJECT AS SHOWN ON THESE PLANS. THE ARCHITECT DOES NOT WARRANT THE ACCURACY OF ANY INFORMATION PROVIDED BY OTHERS AND IS NOT RESPONSIBLE FOR ANY CONSEQUENCES ARISING FROM THE USE OF SUCH INFORMATION. THE ARCHITECT'S LIABILITY IS LIMITED TO THE DESIGN AND CONSTRUCTION OF THE PROJECT AS SHOWN ON THESE PLANS. THE ARCHITECT DOES NOT WARRANT THE ACCURACY OF ANY INFORMATION PROVIDED BY OTHERS AND IS NOT RESPONSIBLE FOR ANY CONSEQUENCES ARISING FROM THE USE OF SUCH INFORMATION.



- DASHED BRICKS PER 'ADAPTIVE REUSE' STRUCTURE ENGINEER OF RECORD
- DASHED BRICKS PER 'ADAPTIVE REUSE' STRUCTURE ENGINEER OF RECORD
- DASHED BRICKS PER 'ADAPTIVE REUSE' STRUCTURE ENGINEER OF RECORD
- DASHED BRICKS PER 'ADAPTIVE REUSE' STRUCTURE ENGINEER OF RECORD
- DASHED BRICKS PER 'ADAPTIVE REUSE' STRUCTURE ENGINEER OF RECORD



FLOOR PLAN (LEVELS 03 - 06)

2.03

# 1055 7TH STREET, LOS ANGELES, CA

S SCHEMATIC DESIGN

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

LABDS APPROVAL STAMP



\*RECYCLING CHUTES SHALL BE CLEARLY MARKED 'RECYCLING ONLY' AT EACH POINT OF ENTRY. (174)

10/13/2023

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 Case No. 24-2021-41  
 JAV

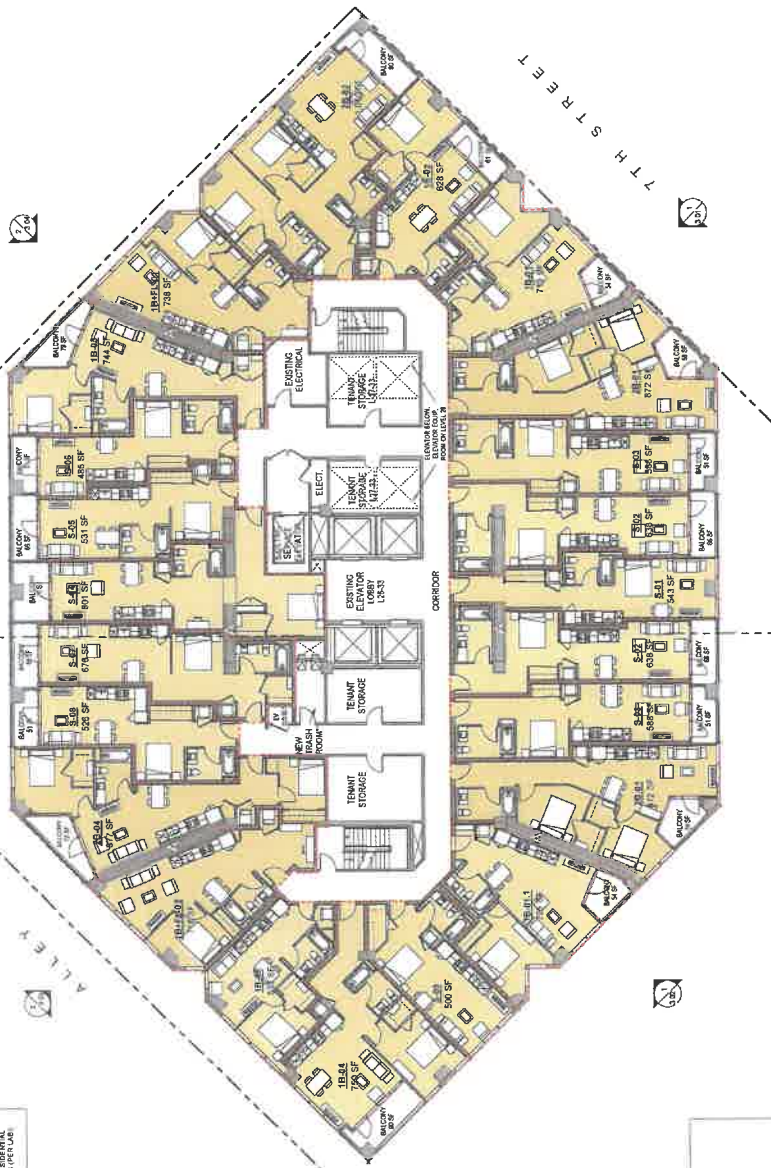








NOTE: SCHEMATIC DESIGN DRAWINGS ARE MADE BY COMPUTER AIDED DESIGN (CAD) SOFTWARE. THESE SCHEMATIC DESIGN DRAWINGS ARE MADE BY COMPUTER AIDED DESIGN (CAD) SOFTWARE. THESE SCHEMATIC DESIGN DRAWINGS ARE MADE BY COMPUTER AIDED DESIGN (CAD) SOFTWARE. THESE SCHEMATIC DESIGN DRAWINGS ARE MADE BY COMPUTER AIDED DESIGN (CAD) SOFTWARE.



KEY:  
 - NEW BUILDING WALLS PER "ADAPTIVE REUSE" STRUCTURE ENGINEER  
 - EXISTING WALLS PER "ADAPTIVE REUSE" STRUCTURE ENGINEER  
 - INDICATES BOUNDARY OF UNIT AS ANNOTATED ON PLAN  
 - INDICATES BOUNDARY OF BALCONY AS ANNOTATED ON PLAN  
 - INDICATES BOUNDARY OF AREA NOTED ON PLAN



FLOOR PLANS (LEVELS 26 - 33)

2.06

1055 7TH STREET, LOS ANGELES, CA

SCHEMATIC DESIGN

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

LABS APPROVAL STAMP



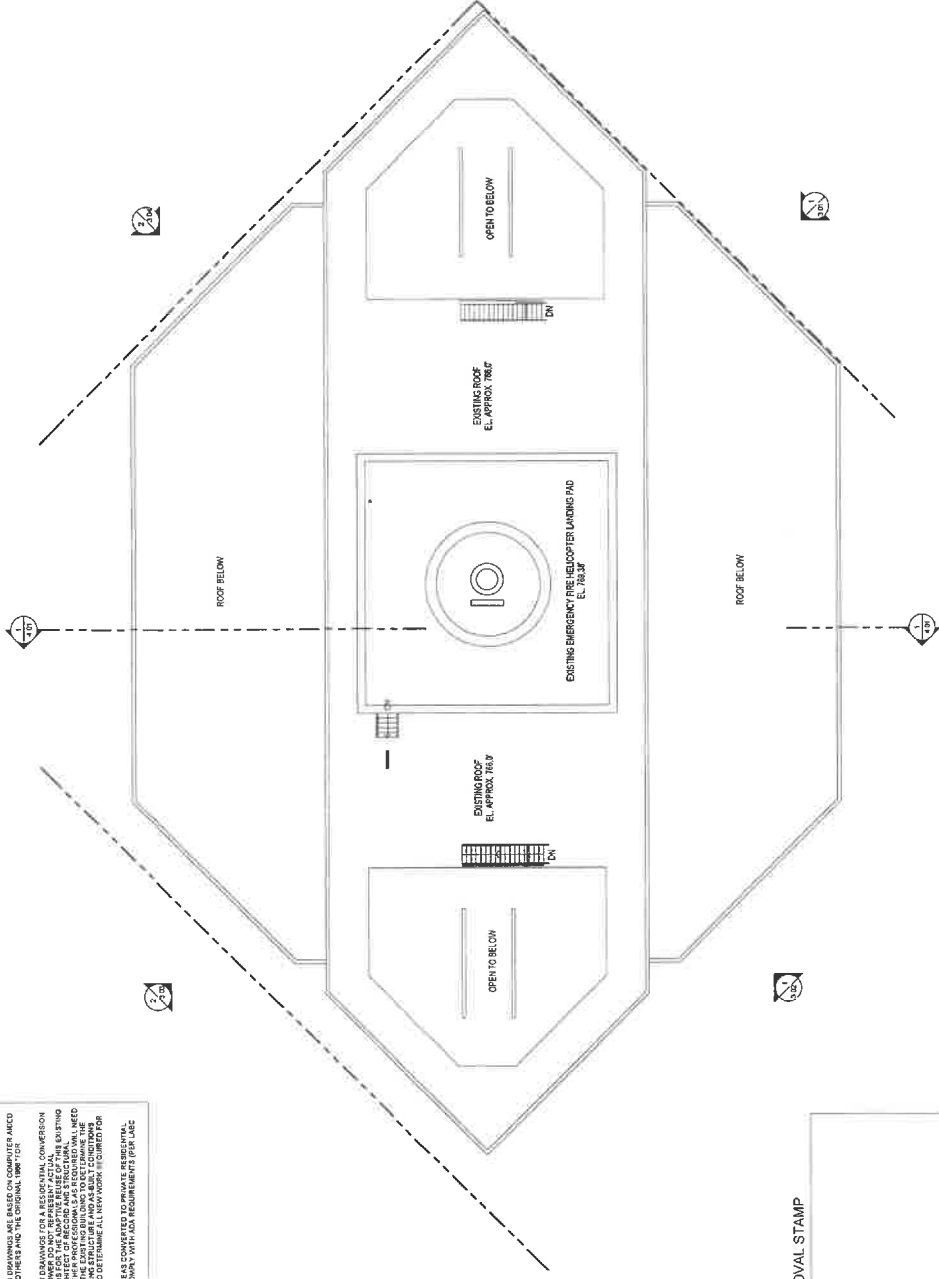
RECYCLING CHUTES SHALL BE CLEARLY MARKED "RECYCLING ONLY" AT EVERY POINT OF ENTRY (TYP)

10/15/2023

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 AD



NOTE: THESE SCHEMATIC DESIGN DRAWINGS ARE BASED ON COMPUTER AIDED CONSTRUCTION PLANS OF THE ORIGINAL 1987 FOR THESE SCHEMATIC DESIGN DRAWINGS FOR A RESIDENTIAL CONVERSION ARCHITECTURAL CHANGES FOR THE ADJUSTED RELEASE OF THE EXISTING ENGINEER AND ARCHITECT PROFESSIONALS AS TO THE ORIGINAL NEEDS OF THE BUILDING. THE EXISTING STRUCTURE AND AS-BUILT CONDITIONS WILL BE REVIEWED TO DETERMINE ALL NEW WORK REQUIRED FOR THIS ADAPTIVE RE-USE. ALL EXISTING BUILDING AREAS CONVERTED TO PRIVATE RESIDENTIAL USE AND MEET WITH ALL REQUIREMENTS (PER LASC 174 AND LASC 170A.1)



LABDS APPROVAL STAMP



UPPER ROOF PLAN

2.08

1055 7TH STREET, LOS ANGELES, CA

SCHEMATIC DESIGN

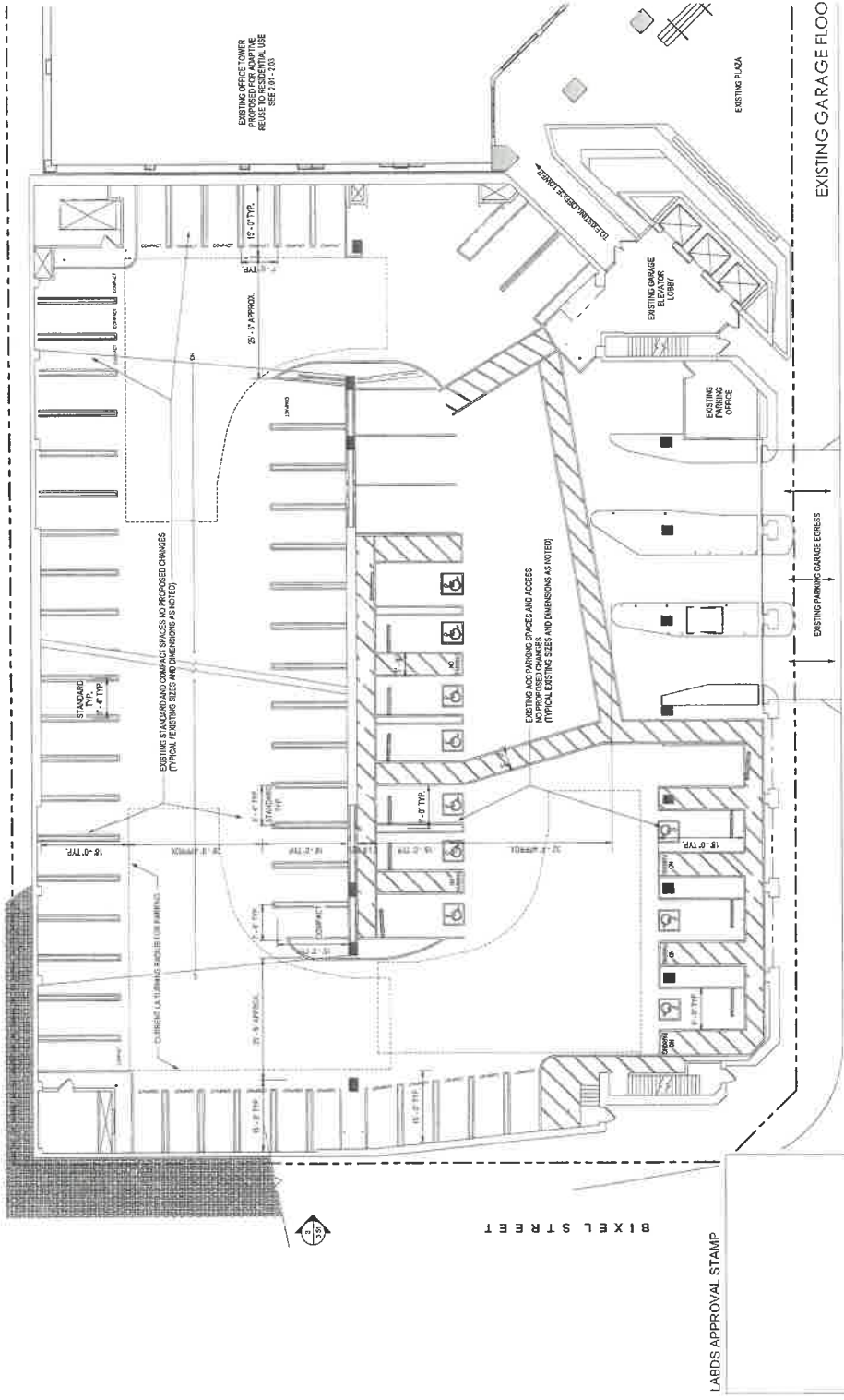
PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

08/13/2023

Sheet No. 17 of 31  
 Date: 2/10/23-4/11  
 AD

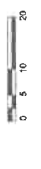


ALLEY



EXISTING PARKING LEVEL P1	
STANDARD (COMMERCIAL SIZE)	29
ACCESSIBLE	10
MISCELLANEOUS	06
TOTAL	45

NOTE: THERE IS NO REQUIRED RESIDENTIAL PARKING FOR THE EXISTING OFFICE TOWER TO BE ADAPTED FOR RESIDENTIAL USE (PER ABOVE). THE EXISTING COMMERCIAL SPACES PROVIDING COMMERCIAL INFORMATION FOR FURTHER INFORMATION.



EXISTING GARAGE FLOOR PLAN (LEVEL P1)

2.51

# 1055 7TH STREET, LOS ANGELES, CA

SCHEMATIC DESIGN

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

NOTES: THESE PAVING DIMENSIONS ARE BASED ON COMPACTIVE BASE DIMENSIONS ASSUMED IN OTHERS PARTS OF THE ORIGINAL UNIT FOR CONSTRUCTION DOCUMENTS. ALL PARKING SPACE NUMBERS AS WELL AS SPACE AND AISLE DIMENSIONS ARE APPROXIMATED BASED ON INFORMATION PROVIDED BY OTHERS.

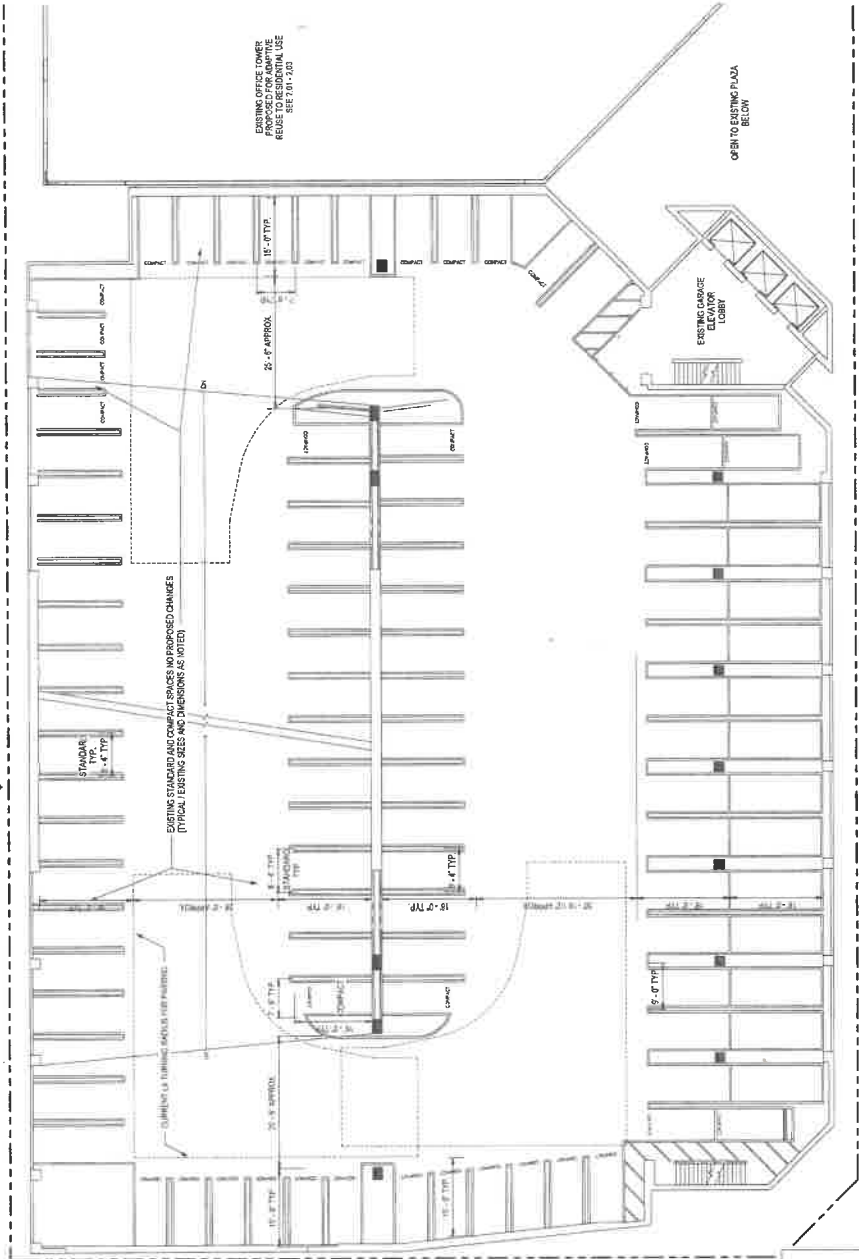
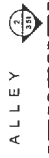
10/13/2023

LABDS APPROVAL STAMP

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2A-2023-41







<b>EXISTING PARKING LEVEL P 4</b>	
STANDARD (COMMERCIAL) SIZE	54
COMPACT	33
TANKEN	16
MISC. VEHICLES	10A
<b>TOTAL</b>	<b>113A</b>
<b>EXISTING PARKING LEVEL P 6</b>	
STANDARD (COMMERCIAL) SIZE	54
COMPACT	33
TANKEN	16
MISC. VEHICLES	10A
<b>TOTAL</b>	<b>113A</b>
<b>EXISTING PARKING LEVEL P 8</b>	
STANDARD (COMMERCIAL) SIZE	54
COMPACT	33
TANKEN	16
MISC. VEHICLES	10A
<b>TOTAL</b>	<b>113A</b>
<b>EXISTING PARKING LEVEL P 9</b>	
STANDARD (COMMERCIAL) SIZE	54
COMPACT	33
TANKEN	16
MISC. VEHICLES	10A
<b>TOTAL</b>	<b>113A</b>



EXISTING GARAGE FLOOR PLAN (LEVEL P4-P9)

7 TH STREET



2.54

NOTE: THESE EXISTING PARKING CHANGE DRAWINGS ARE BASED ON COMPUTER AIDED DRAWINGS PREPARED BY THE ARCHITECT. DIMENSIONS ARE APPROXIMATED BASED ON INFORMATION PROVIDED BY OTHERS. WELL AS SPACE AND AREA DIMENSIONS ARE APPROXIMATED BASED ON INFORMATION PROVIDED BY OTHERS.

1055 7TH STREET, LOS ANGELES, CA

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

19/12/2023

SCHEMATIC DESIGN

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AP

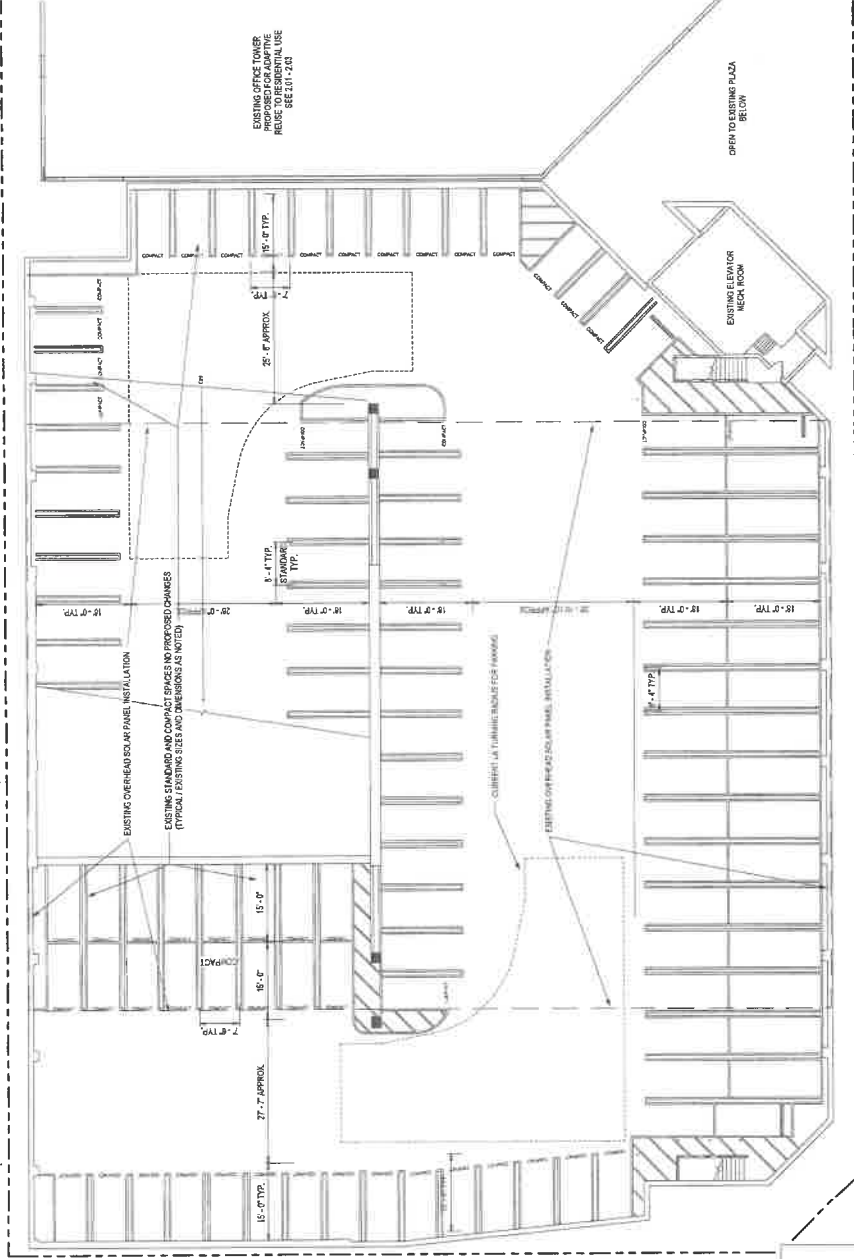
LABDS APPROVAL STAMP

B I X E L S T R E E T





ALLEY



EXISTING PARKING P10 TOWER	
LEVEL	P10
LAND COMMERCIAL SIZE	2
COMPACT	25
STANDARD	10
TOTAL	35

NOTE: THERE IS NO REQUIRED REPAIRS TO THE EXISTING OFFICE TOWER TO BE RE-USED FOR RESIDENTIAL USE (PER 1110 EXISTING COMMERCIAL SPACES FOR ADAPTIVE RE-USE INFORMATION)

0 5 10 20

EXISTING GARAGE ROOF PLAN (LEVEL P10)

2.55

# 1055 7TH STREET, LOS ANGELES, CA

SCHEMATIC DESIGN

7TH STREET

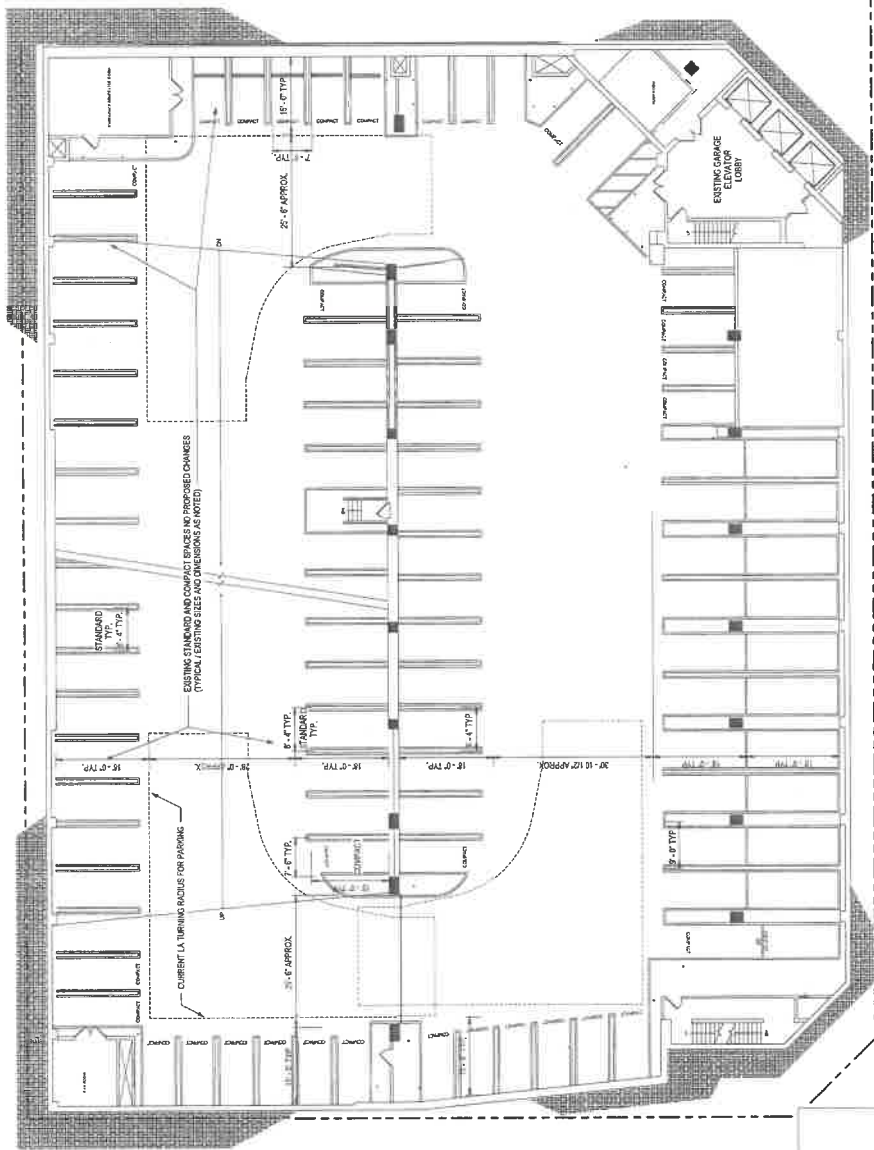
NOTE: THE PARKING CHANGES AND ACCESS TO COMPACT SPACES PREPARED BY OTHERS AND THE ORIGINAL ARCHITECTURE DOCUMENTS. ALL PARKING SPACE NUMBERS ARE APPROXIMATE AND DIMENSIONS ARE APPROXIMATED BASED ON INFORMATION PROVIDED BY OTHERS.

10/13/2023

LABS APPROVAL STAMP

B I X E L S T R E E T

02 of 21  
24-2021-4115  
AED



EXISTING PARKING LEVEL B1	
STANDARD COMMERCIAL SIZE	52
COMPACT	21
TOTAL	73
STANDARD COMMERCIAL SIZE	52
COMPACT	21
TOTAL	73

NOTE: THERE IS NO REQUIRED SETBACK FOR THE ADAPTIVE RE-USE OF THE EXISTING OFFICE TOWER TO THE EXISTING GARAGE (PER AR 209). THIS INCLUDES PER THE EXISTING COMMERCIAL SPACES AND 11% EXISTING COMMERCIAL SPACES FOR FURTHER INFORMATION.



EXISTING GARAGE FLOOR PLAN (LEVEL B1)

2.56

1055 7TH STREET, LOS ANGELES, CA

SCHEMATIC DESIGN

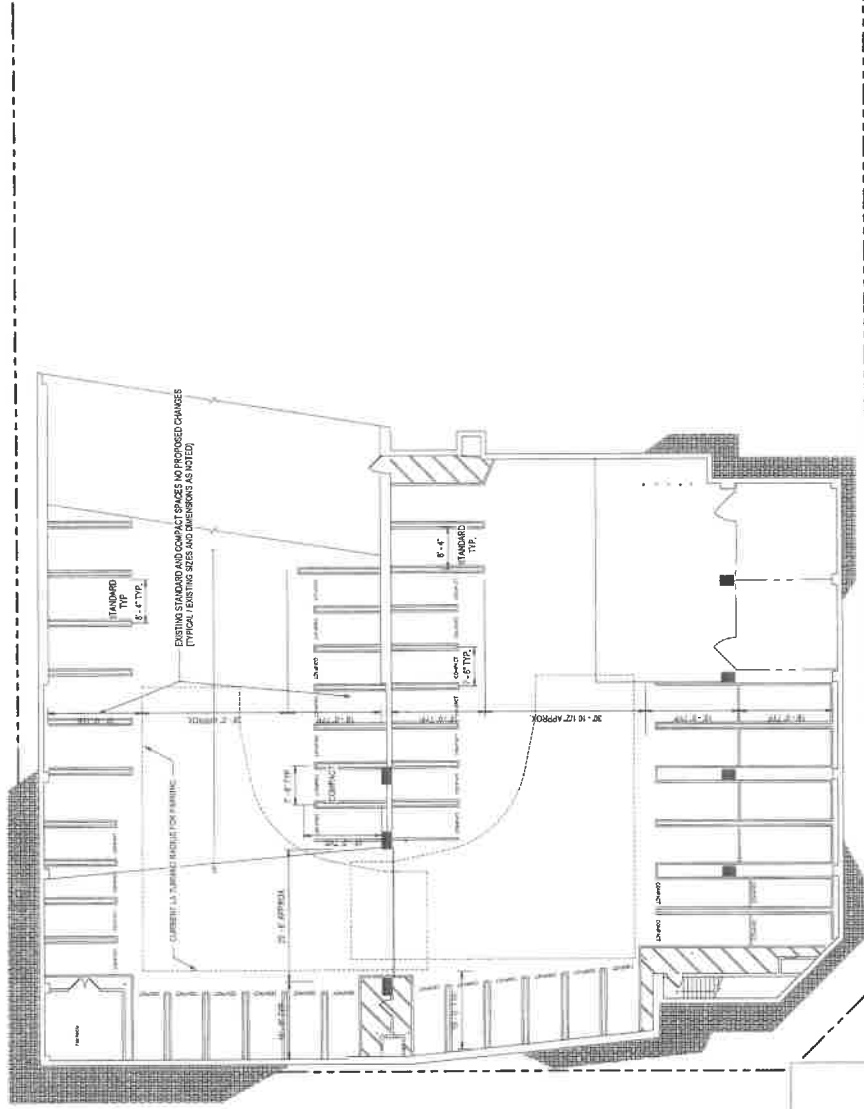
PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

NOTE: EXISTING ARCHITECTURAL CONDITIONS ARE BASED ON COMPUTER GENERATED DIMENSIONS PREPARED BY OTHERS AND THE ORIGINAL SET OF CONSTRUCTION DOCUMENTS. ALL PARKING SPACE NUMBERS AS WELL AS SPACE AND USE DIMENSIONS ARE APPROXIMATED BASED ON INFORMATION PROVIDED BY OTHERS.

10/11/2023

LABDS APPROVAL STAMP

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23 of 31  
24-2023  
AL



EXISTING PARKING LEVEL B2	
EXISTING COMMERCIAL SPACES	10
EXISTING COMPACT SPACES	22
TOTAL	32

NOTE: THERE IS NO REQUIRED ADAPTIVE RE-USE PLAN FOR THE EXISTING OFFICE TOWER TO BE ADAPTED FOR OFFICE USE (PER ADAPTIVE RE-USE PLAN). THE EXISTING COMMERCIAL SPACES WILL BE ADAPTED FOR OFFICE USE (PER ADAPTIVE RE-USE PLAN).



EXISTING GARAGE FLOOR PLAN (LEVEL B2)

2.57

# 1055 7TH STREET, LOS ANGELES, CA

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

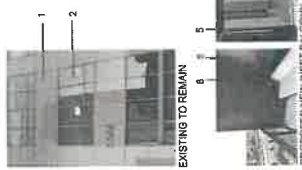
SCHEMATIC DESIGN

NOTE: THIS PLAN IS BASED ON THE EXISTING AS-BUILT CONDITIONS. ALL DIMENSIONS ARE APPROXIMATE AND BASED ON INFORMATION PROVIDED BY OTHERS AND THE ORIGINAL AS-BUILT DOCUMENTS. ALL PARKING SPACE NUMBERS AS WELL AS SPACE AND USE DIMENSIONS ARE APPROXIMATED BASED ON INFORMATION PROVIDED BY OTHERS.

10/9/2023

LABDS APPROVAL STAMP

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2023-4115  
AD



EXISTING TO REMAIN

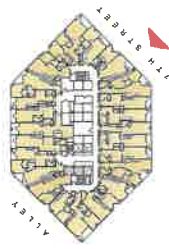
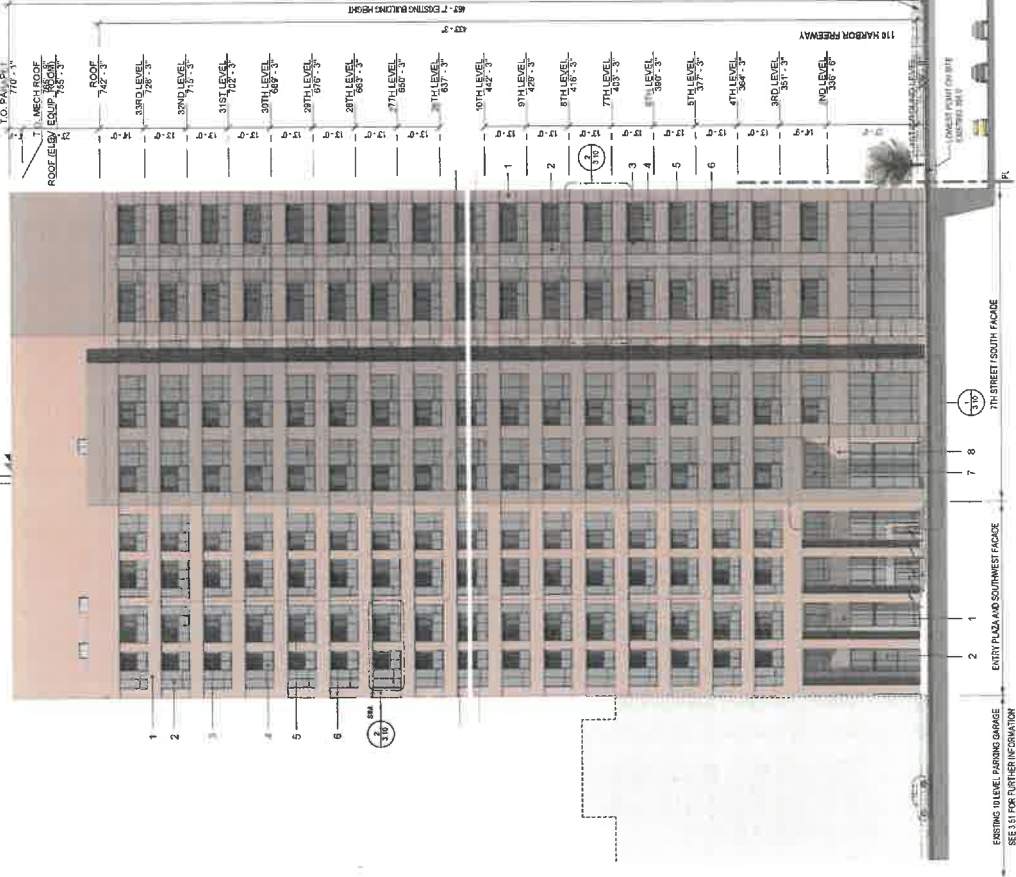
PROPOSED NEW INSET BALCONY

**MATERIAL KEY**

- 1- EXISTING STONE VENEER TO REMAIN TYPICAL ENTIRE
- 2- EXISTING WINDOW WALL TO REMAIN TYPICAL ENTIRE BUILDINGS EXCEPT AT NEW BALCONY INSETS
- 3- NEW STRUCTURAL MULLION TO MATCH EXISTING WALL. WE ADDED WHERE EXISTING WINDOW WALL IS REMOVED AT NEW BALCONY INSETS. BAL WITH METAL SUPERSTRUC AND GALL TO MATCH EXISTING WINDOW WALL MULLION IN COLOR AND FINISH
- 4- NEW WINDOW WALL AND GLASS DOOR AT INTERIOR FACES TO MATCH EXISTING WINDOW WALL MULLION IN COLOR AND FINISH
- 5- NEW METAL PANEL VENEER AT INTERIOR SOLID WALLS OF NEW BALCONY INSETS TO MATCH EXISTING WINDOW WALL MULLION IN COLOR AND FINISH
- 6- NEW FOR BELIEVED STONE VENEER TO MATCH EXISTING AT NEW ENCLOSED GROUND AND SECOND LEVEL AREA CREATED BY NEW INTERNAL STAIRING SPACE

**NOTE:** ALL HEIGHTS, SQUARE FEET, AND OTHER BUILDING PROGRAM CONSTRUCTION DOCUMENTS AND COMPUTER AIDED DRAWINGS PREPARED BY OTHERS. ALL DIMENSIONS SHOWN ARE NOT VERIFIED ON ACTUAL BUILT CONDITIONS. THIS CONCEPTUAL DESIGN FOR A RESIDENTIAL CONVERSION OF AN EXISTING OFFICE TOWER DOES NOT REPRESENT ACTUAL ARCHITECTURAL DRAWINGS FOR THE ADAPTIVE REUSE OF EXISTING STRUCTURE AND ASPIRANT CONSTRUCTION. THIS DESIGN IS INTENDED TO BE USED ALONG WITH OTHER PROFESSIONAL AS REQUIRED WILL NEED TO ANALYZE AND ASSESS THE EXISTING STRUCTURE AND ASPIRANT CONSTRUCTION FOR SUCH A REUSE AND TO DETERMINE ALL NECESSARY REPAIRS AND REINFORCEMENTS TO BE MADE TO THE EXISTING STRUCTURE TO CORRECT AND REPAIR DEFICIENCIES PER LOCAL AND STATE (CALIFORNIA) CODES. THESE REPAIRS AND REINFORCEMENTS DO NOT NEED TO BE SHOWN ON THESE DRAWINGS.

LABDS APPROVAL STAMP



KEYPLAN

0 6 12 18 24  
SOUTH ELEVATION

3.01

**1055 7TH STREET, LOS ANGELES, CA**

SCHEMATIC DESIGN

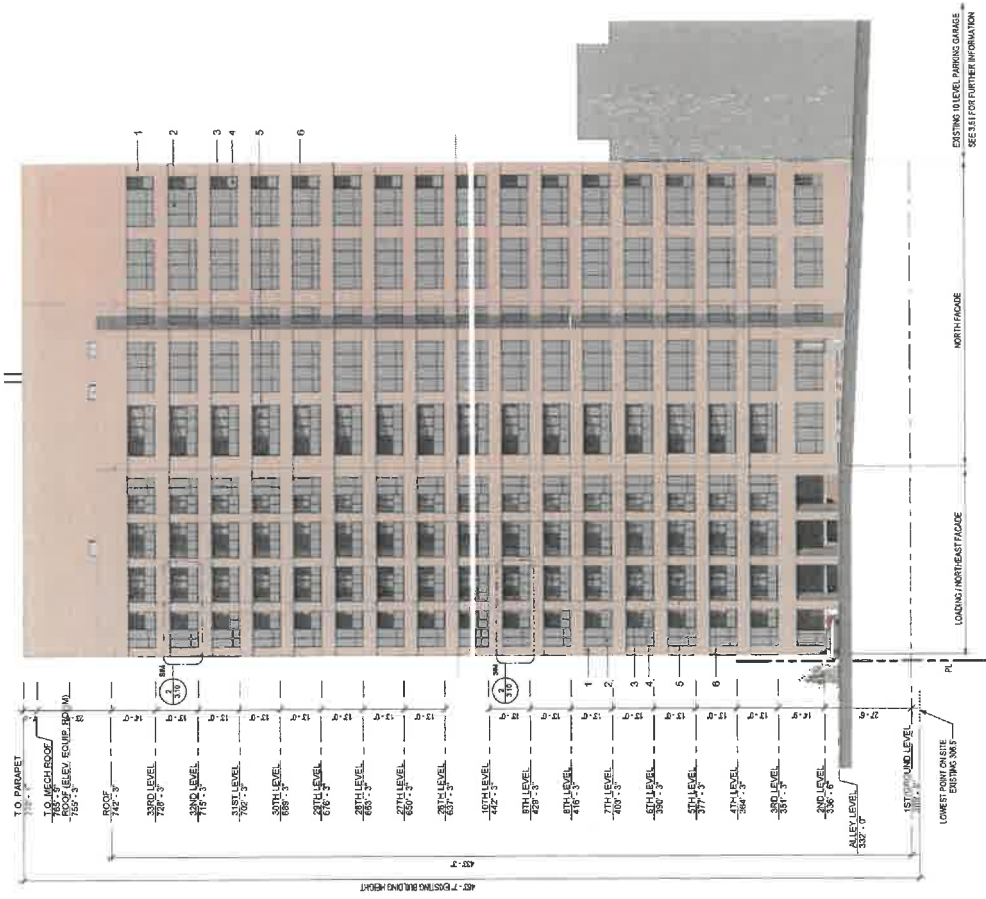
PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS

10/13/2023

25.31  
2A.1003.4  
AD

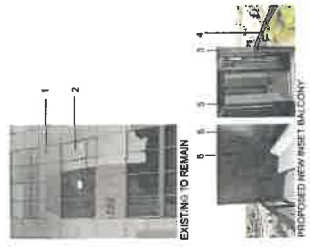


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 24.2023.411  
 100  
 AD



1055 7TH STREET, LOS ANGELES, CA  
 SCHEMATIC DESIGN

NORTH ELEVATION  
 3.03



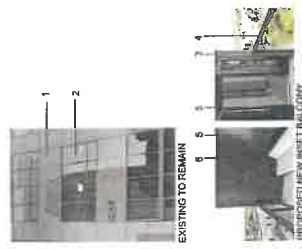
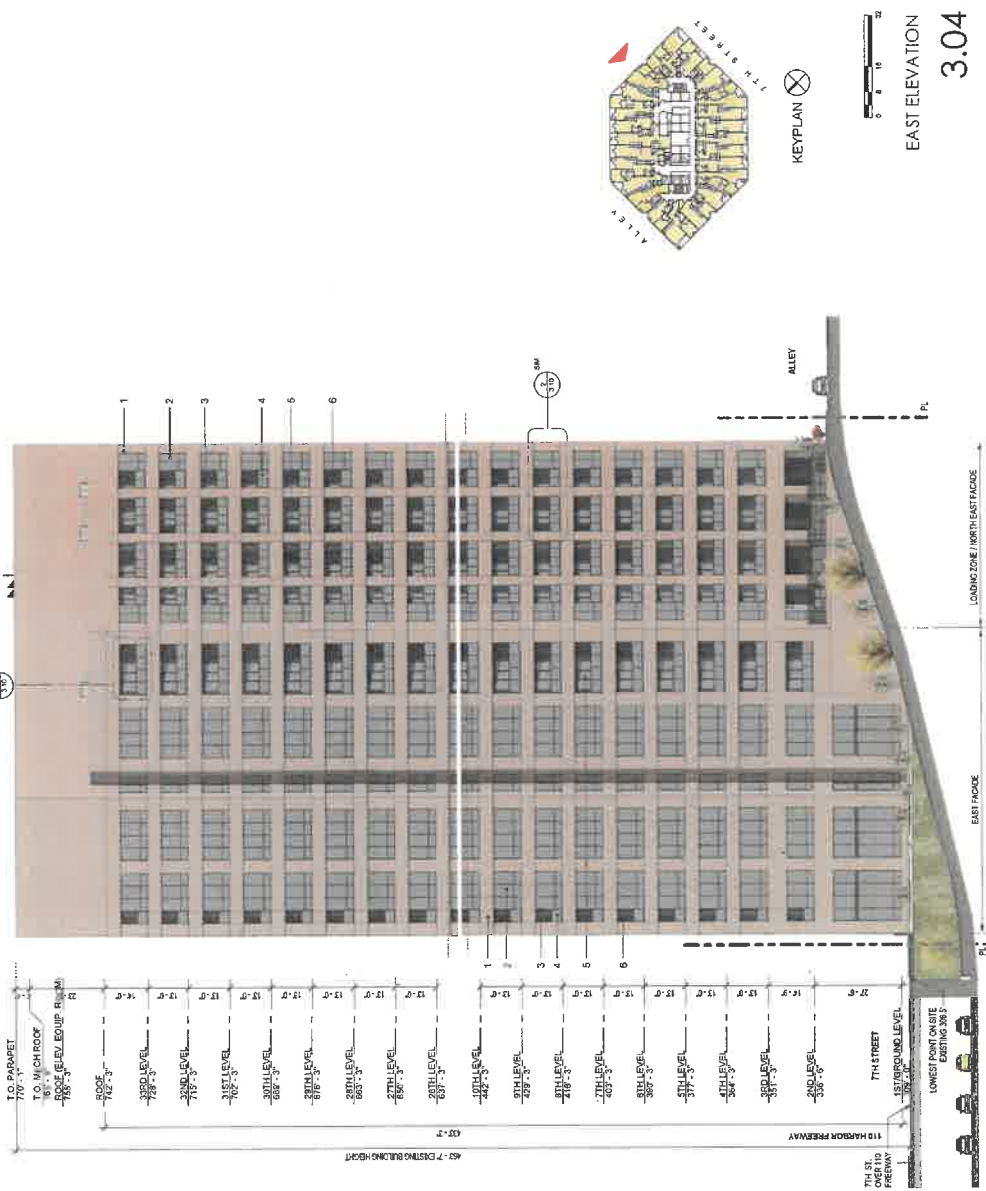
- MATERIAL KEY**
- EXISTING STONE VENEER TO REMAIN TYPICAL ENTIRE BUILDING
  - EXISTING WINDOW WALL TO BE REMOVED
  - NEW STRUCTURAL MULLION TO MATCH EXISTING WALL BE ADDED WHERE EXISTING WINDOW WALL IS REMOVED AT NEW BALCONIES
  - NEW WINDOW WALL AND GLASS DOOR AT INTERIOR FACE
  - NEW METAL PANEL VENEER AT INTERIOR SOLID WALLS OF WINDOW WALL MULLION
  - NEW WINDOW WALL AT NEW ENCLOSED GROUND AND SECOND LEVEL AREA CREATED BY NEW INTERNAL SEISMIC FRAME
  - NEW (OR REUSED) STONE VENEER TO MATCH EXISTING AT ALL EXISTING WINDOW WALLS AND LEVELS CREATED BY NEW INTERNAL SEISMIC FRAME

**NOTE:** CONCEPTUAL ELEVATIONS BASED ON THE EXISTING BUILDING'S ORIGINAL CONSTRUCTION DOCUMENTS AND/OR COMPUTER AIDED DRAWINGS PREPARED BY OTHERS. ALL DIMENSIONS SHOWN ARE NOT VERIFIED ON ACTUAL BUILT CONDITIONS. THIS CONCEPTUAL DESIGN FOR A RESIDENTIAL CONVERSION OF AN EXISTING OFFICE TOWER IS FOR INFORMATION ONLY. THE DESIGNER DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PROVIDED HEREIN. THE DESIGNER'S LIABILITY IS LIMITED TO THE DESIGN SERVICES PROVIDED. THE DESIGNER DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PROVIDED HEREIN. THE DESIGNER'S LIABILITY IS LIMITED TO THE DESIGN SERVICES PROVIDED. THE DESIGNER DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PROVIDED HEREIN. THE DESIGNER'S LIABILITY IS LIMITED TO THE DESIGN SERVICES PROVIDED.

LABDS APPROVAL STAMP

10/13/2023

28.31  
2003.415  
HE



EXISTING TO REMAIN

PROPOSED NEW INSET BALCONY

- MATERIAL KEY**
- EXISTING STONE VENEER TO REMAIN TYPICAL ENTIRE BUILDING
  - EXISTING WINDOW WALL TO REMAIN TYPICAL ENTIRE BUILDING EXCEPT AT NEW BALCONY WALLS
  - NEW STRUCTURAL MULLION TO MATCH EXISTING WALL BE ADDED WHERE EXISTING WINDOW WALL IS REMOVED AT NEW BALCONY
  - NEW METAL PANEL VENEER AND BAL TO MATCH EXISTING WINDOW WALL MULLION IN COLOR AND FINISH
  - NEW WINDOW WALL AND GLASS DOOR AT INTERIOR FACES TO MATCH EXISTING WINDOW WALL MULLION IN COLOR AND FINISH AND COLOR OF TILING WINDOW WALL
  - NEW METAL PANEL VENEER AT INTERIOR GLASS WALLS OF WINDOW WALL TO MATCH EXISTING WINDOW WALL MULLION IN COLOR AND FINISH
  - NEW WINDOW WALL AT NEW ENC. CREED CHINA AND SECOND LEVEL AREA CREATED BY NEW INTERNAL SECOND LEVEL TO MATCH EXISTING WINDOW WALL IN COLOR AND FINISH
  - NEW COR REUSED STONE VENEER TO MATCH EXISTING AT NEW INTERNAL SECOND LEVEL AREA CREATED BY NEW INTERNAL SECOND LEVEL

NOTE: CONCEPTUAL ELEVATIONS BASED ON THE EXISTING BUILDINGS ORIGINAL CONSTRUCTION OCCURRENCE AND ON COMPARESED DRAWINGS PREPARED BY OTHERS. ALL DIMENSIONS SHOWN ARE NOT TO BE USED FOR ACTUAL BUILT CONDITIONS. THE CONCEPTUAL DESIGN FOR A RESIDENTIAL CONVERSION OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS FOR THE EXISTING OF THE BUILDING. AN ARCHITECT OF RECORD AND STRUCTURAL ENGINEER ALONG WITH OTHER PROFESSIONALS AS REQUIRED WILL BE USED TO ANALYZE AND VERIFY THE FEASIBILITY OF THE CONVERSION AND TO DETERMINE THE NECESSARY STRUCTURAL REPAIRS. ALL NEW WORK REQUIRED FOR THE ADAPTIVE REUSE. ALL EXISTING BUILDING AREAS CONVERTED TO PRIVATE RESIDENTIAL USE IS TO NOT NEED TO COMPLY WITH ADA REQUIREMENTS (PER LABS 11A AND LABS 119A-11).

LABS APPROVAL STAMP

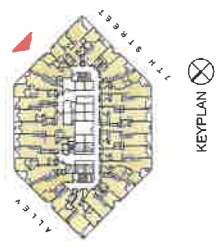


10/13/2023

1055 7TH STREET, LOS ANGELES, CA

SCHEMATIC DESIGN

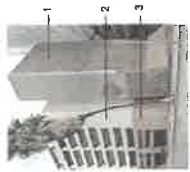
EAST ELEVATION  
3.04



PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS



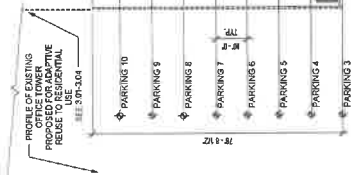




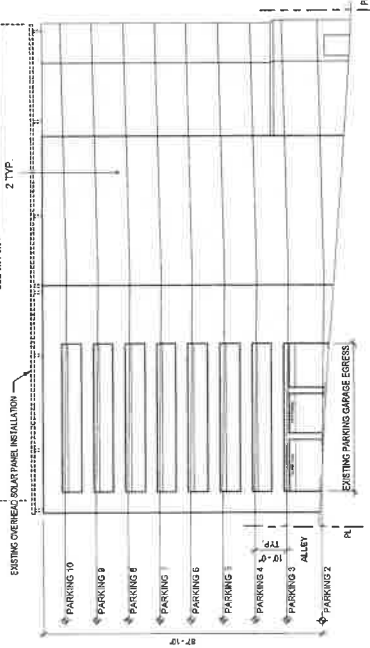
EXISTING TO REMAIN

MATERIAL KEY

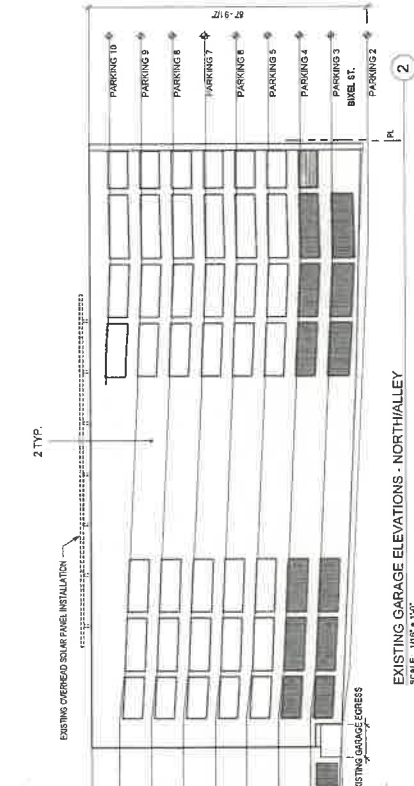
- 1 - EXISTING STONE VENEER TO REMAIN
- 2 - EXISTING PAINTED FORMER PLACEMENT CONCRETE TO REMAIN - EXISTING FIELD PAINT COURT TO REMAIN - LIGHT BROWN
- 3 - EXISTING PAINTED FORMER PLACEMENT CONCRETE TO REMAIN - BROWN SIMILAR TO STONE VENEER



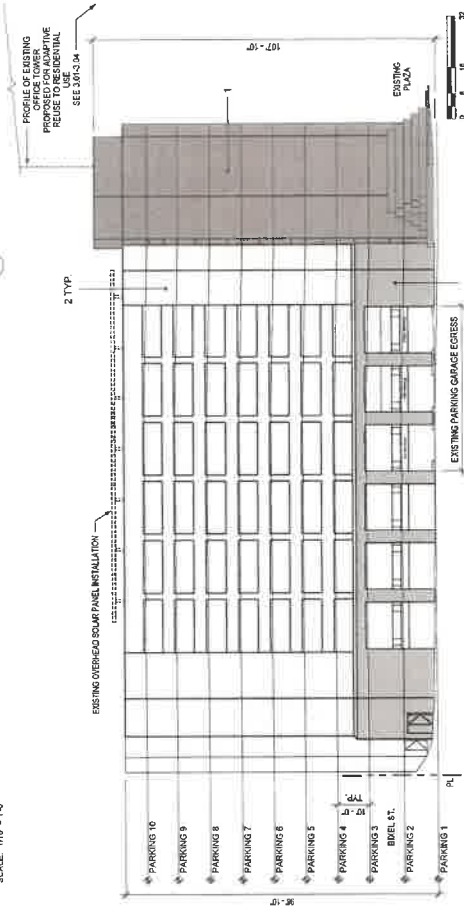
PROFILE OF EXISTING OFFICE TOWER  
RELIEF TO BE ESSENTIAL USE  
SEE 3.01-3.04



EXISTING GARAGE ELEVATIONS - NORTH/HALLEY  
SCALE: 1/16" = 1'-0"



EXISTING GARAGE ELEVATIONS - SOUTH/7TH STREET  
SCALE: 1/16" = 1'-0"



EXISTING GARAGE ELE... WEST/BIXEL ST.  
SCALE: 1/16" = 1'-0"

LABDS APPROVAL STAMP

3

NOTE  
EXISTING PARKING GARAGE EGRESS ARE BASED ON COMPUTER AIDED DRAWING PREPARED BY ARCHITECT. EXISTING PARKING GARAGE EGRESS ARE APPROXIMATELY AS SHOWN AND SHALL BE APPROXIMATELY AS SHOWN. APPROXIMATELY AS SHOWN SHALL BE APPROXIMATELY AS SHOWN. APPROXIMATELY AS SHOWN SHALL BE APPROXIMATELY AS SHOWN.

10/13/2023

EXISTING GARAGE ELEVATIONS 3.51

1055 7TH STREET, LOS ANGELES, CA

PROPOSED ADAPTIVE RE-USE OF AN EXISTING OFFICE TOWER TO JOINT LIVING AND WORK QUARTERS SCHEMATIC DESIGN

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