



# DEPARTMENT OF CITY PLANNING

## REVISED RECOMMENDATION REPORT

### South Valley Area Planning Commission

**Date:** March 24, 2022  
**Time:** After 4:30 p.m.  
**Place:** In conformity with the Governor's Executive Order N-29-20 (March 17, 2020) and due to concerns over COVID-19, the CPC meeting will be conducted entirely telephonically by Zoom [<https://zoom.us/>]. The meeting's telephone number and access code access number will be provided no later than 72 hours before the meeting on the meeting agenda published at <https://planning.lacity.org/about/commissionsboards-hearings> and/or by contacting [cpc@lacity.org](mailto:cpc@lacity.org)

**Public Hearing:** May 17, 2021  
**Appeal Status:** Appealable to City Council.  
**Expiration Date:** March 24, 2022; subject to Mayor's Tolling Order  
**Multiple Approval:** Yes

**PROJECT LOCATION:** 4801 – 4815 N. Laurel Canyon Boulevard; 12107 – 12111 W. Riverside Drive

**REVISED PROJECT:** The project is the construction, use, and maintenance of a 736 square foot, self-operated car wash tunnel, to be operated from 7 a.m. to 10 p.m. daily, as an accessory use to an existing gas station with convenience store, on an approximately 19,164 square foot commercial corner site. A total of 5 parking spaces will be provided. The car wash tunnel is proposed at the southwest corner of the site, adjacent to a commercial use. The project includes new landscaping around the site, raising existing concrete masonry unit (CMU) walls of six feet in height (west property line) and four feet in height (north property line) by two feet each, the addition of 8 new on-site trees and 3 new street trees, four additional parking spaces, new lighting, two new signs, a new trash enclosure area, new air/water location, the addition of two coin operation vacuum hoses with sound dampeners, and the re-facing of an existing free-standing pole sign. The project includes the demolition of a small storage building (160 square feet), the removal of 9 palm trees (3 mature, 6 small) and grading of less than 500 cubic yards.

### REQUESTED ACTION:

1. Pursuant to CEQA Guidelines Section 15303, an Exemption from CEQA pursuant to CEQA Guidelines Section 15303 and that there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.
2. Pursuant to Los Angeles Municipal Code Section 11.5.7.F, Specific Plan Exceptions to allow the following:

**Case No.:** APCSV-2019-1481-SPE-SPP-CU-ZV  
**CEQA No.:** ENV-2019-1482-CE  
**Incidental Cases:** N/A  
**Related Cases:** N/A  
**Council No.:** 2 – Krekorian  
**Plan Area:** North Hollywood – Valley Village  
**Specific Plan:** Valley Village Specific Plan  
**Certified NC:** Valley Village  
**GPLU:** Neighborhood Office Commercial  
**Zone:** [Q]C2-1VL  
**Applicant:** Nader Hattar, N & D Corporation  
**Representative:** Ken Kang, MK Design

- a. An accessory car wash use, in lieu of the C4 use limitations of Section 5.B. of the Valley Village Specific Plan.
  - b. A front yard of 3% of the total lot area, in lieu of the 5% required by 6.E.1. of the Valley Village Specific Plan.
3. Pursuant to Los Angeles Municipal Code Section 11.5.7.C, a Specific Plan Project Permit Compliance to allow the construction, use, and maintenance of a 736 square foot, self-operated car wash tunnel.
4. Pursuant to Los Angeles Municipal Code Section 12.24.W.27, a Conditional Use to allow a Commercial Corner Development with the following deviations from the standards of Los Angeles Municipal Code Section 12.22.A.23:
  - a. Less than 50% ground floor transparent windows.
  - b. An existing free-standing pole sign.
  - c. Hours of operation for the existing convenience store from 5 a.m. to 11 p.m., Monday through Friday, and 6 a.m. to 11 p.m. Saturday and Sunday.
5. Pursuant to Los Angeles Municipal Code Section 12.27, a Zone Variance from "Q" Conditions of Ordinance No. 165,108 to allow a car wash where otherwise prohibited.

#### RECOMMENDED ACTIONS:

1. **Determine** that based on the whole of the administrative record, the Project is exempt from CEQA pursuant to CEQA Guidelines Section 15303, and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.
2. **Approve**, pursuant to Los Angeles Municipal Code Section 11.5.7.F, Specific Plan Exceptions to allow the following:
  - a. An accessory car wash use in lieu of the C4 use limitations of Section 5.B. of the Valley Village Specific Plan.
  - b. A front yard of 3% of the total lot area in lieu of the 5% required by 6.E.1. of the Valley Village Specific Plan.
3. **Approve**, pursuant to Los Angeles Municipal Code Section 11.5.7.C, a Project Permit Compliance to allow the construction, use, and maintenance of a 736 square foot self-operated car wash tunnel.
4. **Approve**, pursuant to Los Angeles Municipal Code Section 12.24.W.27, a Conditional Use to allow a Commercial Corner Development with the following deviations from the standards of Los Angeles Municipal Code Section 12.22.A.23:
  - a. Less than 50% ground floor transparent windows.
  - b. An existing free-standing pole sign.

- c. Hours of operation for the existing convenience store from 5 a.m. to 11 p.m., Monday through Friday, and 6 a.m. to 11 p.m. Saturday and Sunday.
- 5. **Approve**, pursuant to Los Angeles Municipal Code Section 12.27, a Zone Variance from "Q" Conditions of Ordinance No. 165,108 to allow a car wash where otherwise prohibited.

VINCENT P. BERTONI, AICP  
Director of Planning

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## REVISED PROJECT ANALYSIS

### Project Summary

This project came before the South Valley Area Planning Commission meeting on August 26, 2021. At that meeting, Planning Staff and the applicant requested the case be continued in order to do additional analysis relating to comments regarding potential sound impacts. The case was continued to December 9, 2021. On December 9, 2021 staff asked for additional time to review materials and the project was continued to March 24, 2022.

The project is the construction of a 736 square foot, self-operated car wash tunnel (17 ½ feet maximum height) to be operated from 7 a.m. to 10 p.m. daily, in conjunction with an existing gas station with convenience store, on an approximate 19,164 square foot commercial corner site. A total of 5 parking spaces will be provided. The car wash tunnel is proposed at the southwest corner of the site, adjacent to a commercial use. The car wash tunnel will be approximately 38 feet long and 16 feet wide and will be located at the southwest corner of the site, with the entrance from the rear/north end of the tunnel and exit at the south end facing Riverside Drive. The project includes new landscaping around the site, raising existing concrete masonry unit (CMU) walls of six feet in height (west property line) and four feet in height (north property line) by two feet each, the addition of 8 new on-site trees and 3 new street trees, four additional parking spaces, new lighting, two new signs, a new trash enclosure area, new air/water location and the addition of two coin operation vacuum hoses with sound dampeners. Access to the site will remain as is, which includes four driveway aprons (two from each street frontage). Access to the carwash will be from the rear, north side of the tunnel, and exiting south near the Riverside Drive southwest driveway. The project includes the demolition of a small storage building (160 square feet), the removal of 9 palm trees (3 mature, 6 small) and grading of less than 500 cubic yards. The construction and operation of a 736 square foot self-service drive-through car wash tunnel would primarily serve the applicant as an accessory use typically seen in conjunction with other similar gas stations, including on the immediately adjacent property to the south.

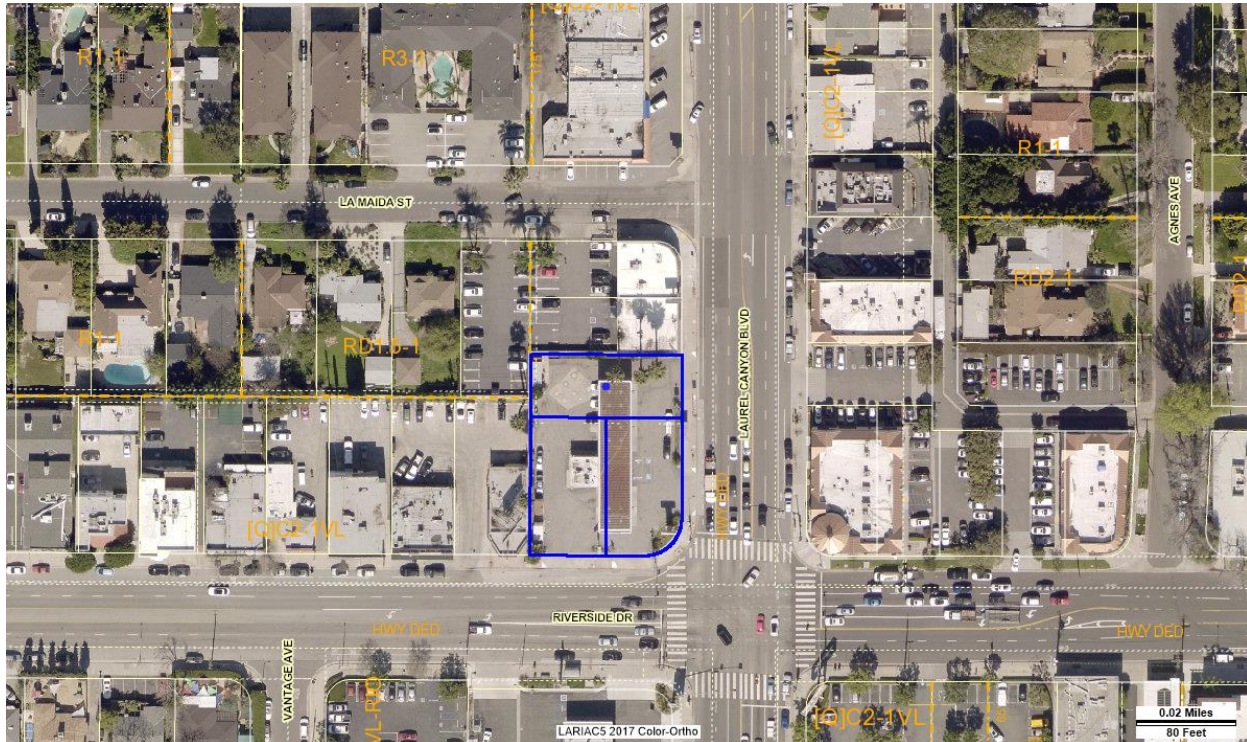
In order to develop the proposed project, the applicant is requesting the following discretionary actions:

- Pursuant to Los Angeles Municipal Code Section 11.5.7.F, Specific Plan Exceptions to allow the following:
  - o An accessory car wash use in lieu of the C4 use limitations of Section 5.B. of the Valley Village Specific Plan.
  - o A front yard of 3% of the total lot area in lieu of the 5% required by 6.E.1. of the Valley Village Specific Plan.
- Pursuant to Los Angeles Municipal Code Section 11.5.7.C, a Project Permit Compliance to allow the construction, use, and maintenance of a 736 square foot, self-operated car wash tunnel.
- Pursuant to Los Angeles Municipal Code Section 12.24.W.27, a Conditional Use to allow a Commercial Corner Development with the following deviations from the standards of Los Angeles Municipal Code Section 12.22.A.23:
  - o Less than 50% ground floor transparent windows.
  - o An existing free-standing pole sign.
  - o Hours of operation for the existing convenience store from 5 a.m. to 11 p.m., Monday through Friday, and 6 a.m. to 11 p.m. Saturday and Sunday.
- Pursuant to Los Angeles Municipal Code Section 12.27, a Zone Variance from "Q" Conditions of Ordinance No. 165,108 to allow a car wash where otherwise prohibited.

## Background

### ***Subject Property***

The proposed development would take place on a commercial corner lot, currently developed with a gas station and convenience store and gas station canopies; the remainder of the site is mostly covered with asphalt except for landscape planter areas along the perimeter of the site which include shrubs as well as 20 non-protected trees comprised of palm trees, junipers, and pygmy palms.



*Figure 1. 2017 Aerial Photography via zimas.lacity.org.*

The proposed project site is an approximately 19,164 square foot, generally flat, and rectangle-shaped area comprised of three lots; see photos above and following. The site is located at the northwest corner of Riverside Drive (running east-west) and Laurel Canyon Boulevard (running north-south) in the North Hollywood – Valley Village Community Plan area. The site is zoned [Q]C2-1VL and is designated for Neighborhood Office Commercial land use under the Community Plan which corresponds to the C1, C1.5, C2, C4, RAS3, RAS4, and P Zones.

The subject site is located in the Valley Village Specific Plan and subject to the [Q] Qualified conditions of Ordinance 165,108 that was enacted in 1989, which states that “The use of the property shall be limited to the uses existing upon the effective date of this ordinance and thereafter to those of the C1.5-1-VL Zone.” Therefore, development of the proposed car wash use requires both a Specific Plan Exception and Zone Variance as detailed below in the “Discussion” section.

The subject property is a level site comprised of 3 lots, located 3.24 kilometers from the Hollywood Fault and is within a Liquefaction Area. The site is located within an urban built up area and is not located on or near a designated sensitive environmental area.

### ***Surrounding Land Uses and Zoning***

Surrounding properties are generally characterized by consistent zoning and land use designations showing a graduated decrease in intensity from the commercial corner of Laurel Canyon Boulevard and Riverside Drive, with commercial development at the corner and along Laurel Canyon, with low-medium / medium density residential development adjacent to commercial uses, and single-family development located further away from commercial corridors.

The subject site is bounded in all four cardinal directions by commercial properties similarly designated for Neighborhood Office Commercial land uses. Surrounding properties are similarly zoned [Q]C2-1VL and developed with one and two story commercial buildings. The west adjoining property is developed with a drive-thru dry cleaners; north adjoining property is developed with a one-story bank and related surface parking lot; south abutting property is developed with a gas station, convenience store and drive-thru car wash; east abutting property is developed with a multi-tenant commercial center which include sit down eateries and a coffee shop; and the southeast abutting corner is developed with a grocery store and large parking lot.



*Figure 2. General Plan Land Use map via [zimas.lacity.org](http://zimas.lacity.org)*

Surrounding properties away from the commercial corner are a mix of multiple family and single family residential. Residential areas adjacent to commercial areas are primarily designated Medium (primarily zoned R3) and Low Medium II Residential land uses (primarily zoned RD1.5 and R2), with larger developments of two to three stories in the Medium Residential areas and smaller developments of one to two stories in the Low Medium II Residential areas. Located further away from the commercial corner are single-family areas, designated for Low Residential land uses, zoned R1-1, and developed with one story buildings.

Also of note, approximately 1,000 feet to the south of the subject is an entrance to the US Route 101 Ventura Freeway, designated for Public Facilities land uses and zoned PF-1VL; the proximity

to this major freeway has resulted in the development of multiple nearby auto-oriented uses, including multiple other fuel service stations.

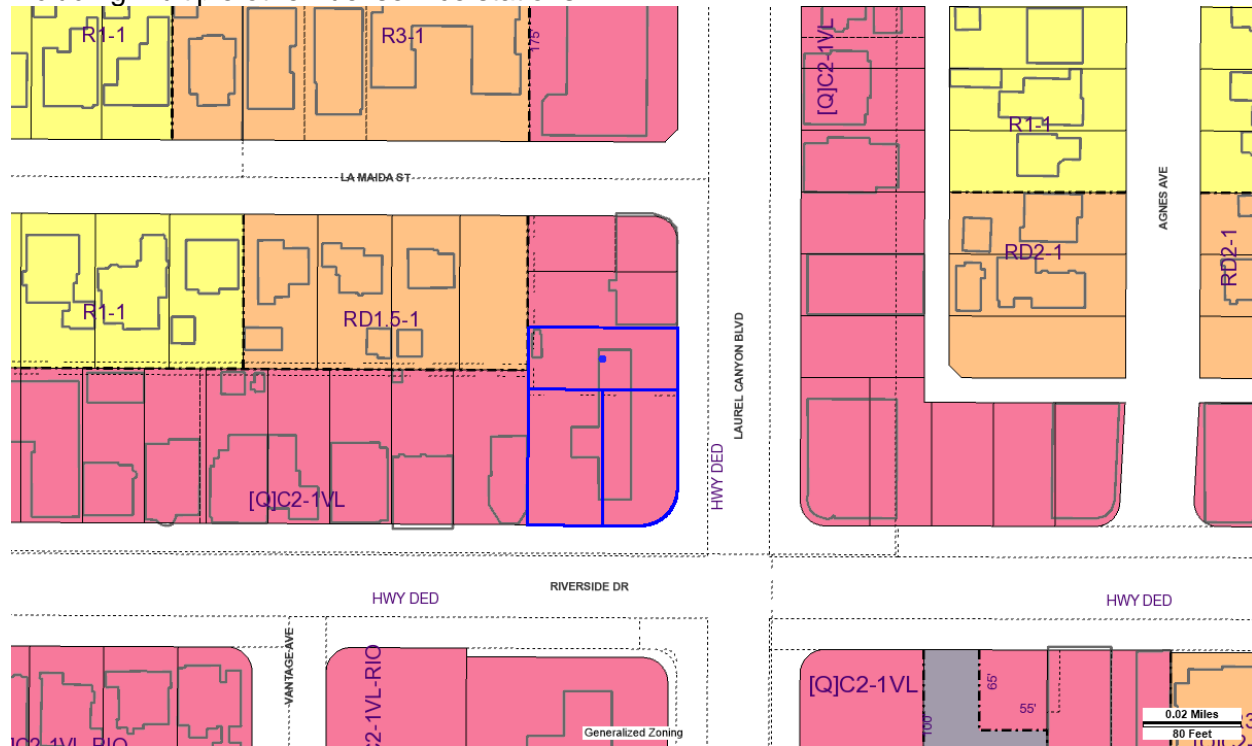


Figure 3. Zoning designation map via zimas.lacity.org

### **Street Designations and General Circulation**

Riverside Drive – Designated Avenue I pursuant to Mobility Plan 2035, designated and fully improved to a width of 100 feet.

Laurel Canyon Boulevard – Designated Avenue I pursuant to Mobility Plan 2035, designated and fully improved to a width of 100 feet.

### **Relevant Cases**

#### *Subject Property*

Ordinance 165,108 (Sub Area 6740) – On September 24, 1989, the City Council Ordinance became effective, effectuating multiple zoning changes, including the adoption of the following [Q] Qualified Condition for the subject site: “The use of the property shall be limited to the uses existing upon the effective date of this ordinance and thereafter to those of the C1.5-1-VL Zone.”

Ordinance 168,613 – On April 4, 1993, the City Council adopted the Valley Village Specific Plan.

Permit Nos. 1972LA44670 – 1972LA44673 – On February 4, 1972, the Department of Building and Safety issued a series of permits for the original development of the service station, including the existing pole sign.

#### *Surrounding Properties*



Case No. ZA-2008-2924-CUB-CU-ZV – On November 12, 2009, the Zoning Administrator approved multiple entitlements related to the addition of a 576 square foot car wash tunnel, including a Variance from Ordinance 165,108 for the property located at 4757 Laurel Canyon Boulevard.

## **Discussion**

### ***Public Hearing***

A public hearing was conducted by the Hearing Officer on May 17, 2021 at 1:00 p.m. as a virtual meeting over phone and Zoom. The hearing was attended by neighborhood residents, and the applicant's representatives. Approximately seven people attended the hearing. Four members of the public spoke in opposition, primarily with concerns in relation to traffic, noise, and the number of car washes in the area. Four written comments were received by staff subsequent to the hearing expressing opposition. Further details are noted under Public Hearing and Communication section of this report.

### ***Revised Project Description and Additional Public Communication***

The case was originally scheduled to be presented before the South Valley Area Planning Commission at its August 26, 2021 meeting date. Prior to the meeting, several comments in opposition were received, which included a petition with seven signatories and two letters challenging the Project's entitlements and environmental analysis. Subsequently, the case was continued in order to conduct additional noise analysis and community outreach. The applicant made modifications to their project to include additional barrier height along the west and north property lines, to include sound dampeners for the car wash vacuums, and to restrict car wash operations to daytime hours. Additionally, the applicant submitted a support petition with 120 signatures and six support letters on their behalf. Conditions, Findings, Analysis, and Exhibits have subsequently revised by staff to reflect the revised Project.

### ***Traffic***

In relation to concerns related to traffic, the proposed function of the car wash tunnel is as an accessory use typically found in conjunction with other service stations, such as the immediately adjacent gas station. Self-service tunnels, which are typically accessory to gas stations, are fundamentally different from a standalone, full-service car wash use relative to their size, staff required, and potential for annoyance to nearby residents by primarily as an additional amenity rather than a standalone use. While the car wash itself may attract additional customers, this in itself does not create a significant impact or an unusual circumstance leading to a significant effect and is more of an ancillary use, secondary to the gas station use that is existing on-site. Traffic generation rates are based on averages for a type of use, rather than the popularity of an individual establishment.

Further, based on the LADOT Vehicle Miles Travelled calculator, no traffic/circulation impacts were identified through the environmental clearance procedure. Furthermore, as a condition of the approval, the driveway, parking, and circulation plans will be reviewed by Los Angeles Department of Transportation prior to the issuance of a building permit.

### ***Noise***

The project must comply with the City of Los Angeles Noise Ordinance No. 144,331 and 161,574 and any subsequent ordinances which prohibit the emission or creation of noise beyond certain levels, during both construction and ongoing operation. The Ordinances cover both operational noise levels (i.e. postconstruction), as well as any noise impact during construction. Section 41.40

of the LAMC regulates noise from demolition and construction activities and prohibits construction activity (including demolition) and repair work, where the use of any power tool, device, or equipment would disturb persons occupying sleeping quarters in any dwelling hotel, apartment, or other place of residence, between the hours of 9:00 p.m. and 7:00 a.m. Monday through Friday, and between 6:00 p.m. and 8:00 a.m. on Saturdays and holidays; all such activities are also prohibited on Sundays. Section 112.05 of the LAMC also specifies the maximum noise level of construction machinery that can be generated in any residential zone of the city or within 500 feet thereof. As the project is required to comply with the above ordinances and regulations, it will not result in any significant noise impacts.

Further, per the CNEL (Community Noise Equivalent Level) Sound Study prepared by MK Design, dated January 7, 2021, "by averaging the peak noise levels (with the Recommended Regulatory Compliance Measures in place) against the measured existing ambient noise levels our site meets the Minimum Ambient Noise Level (Sec 111.01(a))." This study was voluntarily supplemented by the applicant with a Noise Study to supplement and clarify prior studies prepared by Omny Acoustics, dated December 7, 2021, which consisted of a detailed on-site noise survey and propagation modeling. In studying the existing ambient noise levels, the study found that on-site ambient noise levels from adjacent sources exceeded the composite limits generated by Omny based on several sources, including the LAMC and LA County Codes. The report notes that permanent future increases in operational noise when modeled shows full compliance with LAMC noise ordinances. Further, the report states that the Project's CNEL levels are "below existing ambient CNEL levels at all property adjacencies ... [resulting] in a CNEL increase well below CEQA significance thresholds at all locations."

### ***Entitlement Requests***

Establishment of a car wash at the subject site is restricted under both the Valley Village Specific Plan and the existing [Q] conditions onsite. Pursuant to Ordinance 165,108 (Sub Area 6740) adopted in 1989, "the use of the property shall be limited to the uses existing upon the effective date of this ordinance and thereafter to those of the C1.5-1-VL Zone." This ordinance, then, in essence restricts the zoning of this previously-C2 lot to C1.5 for the determination of whether a use is allowed on the lot; car washes are first allowed in the C2 zone and therefore not allowed in the C1.5 zone.

The Valley Village Specific Plan, adopted in 1993, Section 5.B., states that "unless the zoning on a lot is more restrictive than allowed by the C4 Zone, a commercially zoned lot within the Valley Village Specific Plan Area shall be limited to the C4 use limitations in Section 12.16 A 2 of the Code." As the site's zoning is more restrictive (de facto C1.5) than the C4 zone, this section does not apply and further does not supersede the Valley Village Specific Plan, which by its terms only supersedes the underlying zone when it requires "different commercial land uses." However, this Section 5.B. does also regulate land use, specifically automotive-related ones, and the C4 zone would also prohibit the development of a car wash on this site.

As both the [Q] conditions and Specific Plan would limit the proposed use, the applicant must therefore apply for and receive both a Specific Plan Exception and Zone Variance pursuant to LAMC Section 11.5.7.F.1(e), which states in part: "if a specific plan contains a regulation that conflicts with the same type of regulation but with a different standard contained in an applicable provision of Chapter I of this Code and the specific plan does not supersede the Code by its terms, then an applicant seeking relief from those regulations must apply for and receive both an exception to the specific plan and a variance for relief from those Code provisions."

### **Conclusion**

Staff concludes that after a review of the materials, testimony submitted, and attached findings, that the requested deviations from the Valley Village Specific and Los Angeles Municipal Code are appropriate for the site. The current use as a gas station and convenience store were established prior to the establishment of the Specific Plan in 1993. Allowing for customers to receive a car wash at this location further allows the applicant to develop a use which is typically accessory to such gas station uses, which would further allow them to offer a one-stop shop for their customers and offer them desired services while reducing vehicular trips. Staff concludes the project will enhance the site and will not detract from the surrounding community and therefore recommends approval of the request as conditioned

## REVISED CONDITIONS OF APPROVAL

1. **Authorization.** Approved herein is the construction, use, and maintenance of a 736 square foot, self-operated car wash tunnel in conjunction with an existing gas station with convenience store, on an approximate 19,164 square foot commercial corner site and the re-facing and continued use of an existing free-standing pole sign.
2. **Height.** The car wash shall not exceed 17 feet and six inches in height and the free-standing pole sign shall not exceed 18 feet in height.
3. **Plot/Site Plan.** Prior to the issuance of any building permits for the subject project, detailed development plans, including site and elevation plans, and including complete landscape and irrigation plans prepared by a licensed landscape architect or architect, shall be submitted for review by the Department of City Planning for verification of compliance with the imposed conditions. The plans submitted to Building and Safety shall be in substantial conformance with the plans labeled "Exhibit A" dated March 8, 2022, attached to the subject case file. Minor deviations may be allowed in order to comply with provisions of the Municipal Code, the subject conditions, and the intent of the subject permit authorization.
4. **Parking.** Parking shall be provided pursuant to the requirements of the Los Angeles Department of Building and Safety.
5. **Roof-Top Equipment and/or Utilities.** Any air conditioning units and other equipment and/or utilities shall be fully screened from view of any abutting residential properties.
6. **Solar Panels.** Solar panels shall be installed on the roof of all buildings to the maximum extent feasible, and shall be provided as a part of an operational photovoltaic system to be maintained for the life of the project. The project shall comply with the Los Angeles Municipal Green Building Code, Section 99.05.211, to the satisfaction of the Department of Building and Safety.
7. **Graffiti Removal.** All graffiti on the site shall be removed or painted over to match the color of the surface to which it is applied within 24 hours of its occurrence.
8. **Hours of Operation.** Hours of operation for the car wash shall be permitted from 7 a.m. to 10 p.m. daily, and the convenience store from 5 a.m. to 11 p.m., Monday through Friday, and 6 a.m. to 11 p.m. Saturday and Sunday.
9. **Transparent Windows.** As shown on "Exhibit A," less than 50% transparent windows are permitted herein for the car wash. The west elevation shows 46% transparency, and the south elevation shows 20% transparency. Any deviation from "Exhibit A" that further reduces window transparency must be approved by Valley Project Planning.
10. **Deliveries.** No deliveries, loading, or unloading shall occur before 7 a.m. or after 8 p.m. daily. All deliveries shall occur entirely on site and shall not be visible from the adjacent residential development.
11. **Ingress, Egress, and Queuing.** The Department of Transportation shall review and approve the proposed site ingress, egress, and queuing prior to issuance of a building permit.
12. **Noise Attenuation.**

- a. Existing concrete masonry unit (CMU) walls of six feet in height (west property line) and four feet in height (north property line) shall be increased in height by two feet each as demonstrated on Exhibit "A". The walls shall be without openings and shall have a minimum nominal thickness of 6 inches.
- b. Sound dampeners shall be installed on all vacuum equipment sufficient to ensure compliance with City of Los Angeles Noise Ordinance No. 144,331 and 161,574.
- c. Compliance with LAMC Section 13.18 F.2(l) shall be demonstrated.

### 13. Valley Village Conditions.

- a. **Use (Exception).** Uses shall be limited to gas station and car wash uses. The following uses are prohibited:
  - i. all ground floor residential uses
  - ii. automobile sales, new
  - iii. bathhouses
  - iv. burglar alarm businesses
  - v. driving schools
  - vi. escort businesses
  - vii. fast-food & drive-through restaurants
  - viii. hotels and motels
  - ix. massage parlors
  - x. mobile home sales, new
  - xi. monuments and tombstones, retail sales
  - xii. off-site advertising signs
  - xiii. recreational vehicle sales, new
  - xiv. rescue missions
  - xv. taxicab businesses
  - xvi. trade schools, universities and barber and beauty colleges
  - xvii. trailer sales, new
- b. **Lighting.** All lighting for the project shall be low-illumination safety lighting of a color similar to incandescent light, which is shielded and directed onto the property on which the project is located.
- c. **Yards for Commercial Projects (Exception).** The project shall comply with the following:
  - i. The area of the front yard shall be no less than three percent of the total area, but shall not extend to a depth of greater than 15 feet.
- d. **Landscape Plan.** Pursuant to Sections 9.A and 9.E. of the Specific Plan, landscaped areas shall be planted with a variety of plant materials which include shrubs, trees and ground cover, and all plants and trees shall be drought-resistant. Artificial plants are prohibited, however, pursuant to California Civil Code 4735 artificial turf is allowable, due to change in State regulations in response to drought conditions.
  - i. The area from the lot line to the building shall be landscaped with live plant materials and/or ground cover, except for required exit-ways and walkways.

- ii. Front, rear and side yards shall be landscaped using similar materials so that the total development creates a consistent landscape theme.
  - iii. A minimum of 50 percent of the total required front and rear yards shall be landscaped.
  - iv. All landscaped areas shall be equipped with an automatic sprinkler or drip irrigation system designed to conserve water.
- e. **On-Site Trees.**
- i. For every non-fruit bearing, mature tree above five feet in height removed, a replacement tree shall be planted on a 1:1 basis for a total of six shade trees on site.
  - ii. Replacement trees shall be at least a 24-inch box size, not less than eight feet in height, with a trunk diameter of not less than two inches, and a minimum branch spread of five feet.
  - iii. All trees shall be in healthy growing condition and all trees shall be drought-resistant.
- f. **Street Trees.** Shade-producing street trees shall be planted at a ratio of at least one tree for each 30 lineal feet of street frontage when no obstructions are present. The minimum size for street trees shall be 10 feet in height and two inches in caliper at the time of planting. Installation of street trees shall be to the satisfaction of the Bureau of Street Services, Urban Forestry Division.
- g. **Certification of Landscape Installation.** Prior to obtaining a Certificate of Occupancy, the project architect, landscape architect, or engineer shall certify in a letter to the Department of City Planning and to the Department of Building and Safety that the approved landscape plan has been implemented.

#### **Administrative Conditions of Approval**

14. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review or approval, plans, etc., as may be required by the subject conditions, shall be provided to the Department of City Planning for placement in the subject file.
15. **LAMC Requirements.** All other use, height and area regulations of the Municipal Code and all other applicable government/regulatory agencies shall be strictly complied with in the development and use of the property, except as such regulations are herein specifically varied or required.
16. **Code Compliance.** The area, height and use regulations of the zone classification of the subject property shall be complied with, except where conditions herein are more restrictive.
17. **Covenant.** Prior to the issuance of any permits relative to this matter, a covenant acknowledging and agreeing to comply with all the terms and conditions established herein shall be recorded in the County Recorder's Office. The agreement (standard master covenant and agreement form CP-6770) shall run with the land and shall be binding on

any subsequent owners, heirs or assigns. The agreement with the conditions attached must be submitted to the Development Services Center for approval before being recorded. After recordation, a certified copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the subject case file.

18. **Definitions.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public officials, legislation or their successors, designees or amendment to any legislation.
19. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning and any designated agency, or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.
20. **Building Plans.** Page 1 of the grant and all the conditions of approval shall be printed on the building plans submitted to the Department of City Planning and Department of Building and Safety.
21. **Project Plan Modifications.** Any corrections and/or modifications to the Project plans made subsequent to this grant that are deemed necessary by the Department of Building and Safety, Housing Department, or other Agency for Code compliance, and which involve a change in site plan, floor area, parking, building height, yards or setbacks, building separations, or lot coverage, shall require a referral of the revised plans back to the Department of City Planning for additional review and final sign-off prior to the issuance of any building permit in connection with said plans. This process may require additional review and/or action by the appropriate decision-making authority, including the Director of Planning, City Planning Commission, Area Planning Commission, or Board.
22. **Indemnification and Reimbursement of Litigation Costs.** The applicant shall do all of the following:
  - a. Defend, indemnify and hold harmless the City from any and all actions against the City, in whole or in part, relating to or arising out of the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
  - b. Reimburse the City for any and all costs incurred in defense of an action related to or arising out of the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
  - c. Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).

- d. Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- e. If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions includes actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.



## REVISED FINDINGS

### Entitlement Findings

#### Specific Plan Exceptions

- 1. That the strict application of the regulations of the specific plan to the subject property would result in practical difficulties or unnecessary hardships inconsistent with the general purpose and intent of the specific plan.**

The proposed project includes the addition of a 736 square-foot drive through self-service car wash tunnel.

Establishment of a car wash at the subject site is restricted under both the Valley Village Specific Plan and the existing [Q] conditions onsite. Pursuant to Ordinance 165,108 (Sub Area 6740) adopted in 1989, "the use of the property shall be limited to the uses existing upon the effective date of this ordinance and thereafter to those of the C1.5-1-VL Zone." This ordinance, then, in essence restricts the zoning of this previously-C2 lot to C1.5 for the determination of whether a use is allowed on the lot; car washes are first allowed in the C2 zone and therefore not allowed in the C1.5 zone.

The Valley Village Specific Plan, adopted in 1993, Section 5.B., states that "unless the zoning on a lot is more restrictive than allowed by the C4 Zone, a commercially zoned lot within the Valley Village Specific Plan Area shall be limited to the C4 use limitations in Section 12.16 A 2 of the Code." As the site's zoning is more restrictive (de facto C1.5) than the C4 zone, this section does not apply and further does not supersede the Valley Village Specific Plan, which by its terms only supersedes the underlying zone when it requires "different commercial land uses." However, this Section 5.B. does also regulate land use, specifically automotive-related ones, and the C4 zone would also prohibit the development of a car wash on this site.

As both the [Q] conditions and Specific Plan would limit the proposed use, the applicant must therefore apply for and receive both a Specific Plan Exception and Zone Variance pursuant to LAMC Section 11.5.7.F.1(e), which states in part: "if a specific plan contains a regulation that conflicts with the same type of regulation but with a different standard contained in an applicable provision of Chapter I of this Code and the specific plan does not supersede the Code by its terms, then an applicant seeking relief from those regulations must apply for and receive both an exception to the specific plan and a variance for relief from those Code provisions."

Specific Plans do not address every nuance that might occur. However, an Exception is a grant of permission to depart from the literal enforcement of the Plan and allow the property to be used in a manner otherwise not permitted provided that the spirit of the ordinance is observed without detrimental impacts to the community.

The strict application of the specific plan creates an unnecessary hardship because it limits the improvement and expansion of an existing legal use to include a service on site which will mitigate total vehicle trips and be desirable to the public convenience.

- 2. That there are exceptional circumstances or conditions applicable to the subject property involved or to the intended use or development of the subject property that do not apply generally to other property in the specific plan area.**

The unique location (subject to a Specific Plan which prohibits redevelopment or expansion of the legally existing, previously-established use) and existing development of the site are special circumstances applicable to the subject property that do not apply generally to other property in the same zone and vicinity. These circumstances include the large size of the site, history of automotive uses, location on a corner, and adjacency to the nearby Freeway.

In essence, the site is uniquely capable of accommodating the proposed car wash and providing needed access and parking for the proposed incidental use without impeding access to or interference with the existing refueling operation.

Granting the Exception acknowledges the special circumstances of the subject property that does not generally apply to other properties in the same zone in the surrounding area.

- 3. That an exception from the specific plan is necessary for the preservation and enjoyment of a substantial property right or use generally possessed by other property within the specific plan area in the same zone and vicinity but which, because of special circumstances and practical difficulties or unnecessary hardships is denied to the property in question.**

The granting of the requested exception is necessary for the enjoyment of a substantial property right or use available to others in the vicinity; specifically, pursuant to case number ZA-2008-2924-CUB-CU-ZV, the immediately adjacent gas station to the south was granted a Zone Variance for a similar self-service car wash project. While similarly located in the Valley Village Specific, the adjacent project was approved and developed without the granting of an Exception.

Therefore, granting of the subject exception is necessary for the preservation and enjoyment of a substantial property right or use generally possessed by other property in the same zone and vicinity but which, because of such special circumstances and practical difficulties or unnecessary hardships, is denied the property in question.

- 4. That the granting of an exception will not be detrimental to the public welfare or injurious to the property or improvements adjacent to or in the vicinity of the subject property.**

Granting the exception will not be materially detrimental because special consideration is given to the impact on residences adjacent to the property, as well as other nearby properties through the imposition of corrective conditions to require thoughtful lighting, landscaping and noise attenuation.

Therefore, granting of the exception will not be materially detrimental to the public welfare, or injurious to the property or improvements in the same zone or vicinity in which the property is located.

- 5. That the granting of an exception will be consistent with the principles, intent and goals of the specific plan and any applicable element of the general plan.**

The proposed project site is an approximately 19,164 square foot, generally flat, and rectangle-shaped area comprised of three lots. The proposed project complies with the following goals, policies, and programs of the Valley Village Specific Plan and the General Plan, and therefore, granting of the request would be consistent:

Valley Village Specific Plan

The proposed land use and front yard exception complies with the following Purposes of the Valley Village Specific Plan:

*To assure orderly, attractive and harmonious multiple residential and commercial developments that are adjacent to the existing single-family developments within the Valley Village area of the North Hollywood Community Plan area.*

*To preserve the quality and existing character of the Valley Village area.*

*To minimize adverse environmental effects of development and promote the general welfare by regulating buildings by height, and bulk, and prohibiting commercial uses which are not compatible to their sites and surroundings, or which cause parking or traffic circulation impacts.*

#### Framework Element

*General Commercial Areas. Objective 3.12: Generally, maintain the uses, density, and character of existing low-intensity commercial districts whose functions serve surrounding neighborhoods...*

The proposed car wash maintains and upgrades an existing low density, commercial gas station and convenience store use that serves the surrounding residential and commercial neighborhood.

*Urban Form and Neighborhood Design. Policy 5.5.3: Formulate and adopt building and site design standards and guidelines to raise the quality of design Citywide.*

The proposed car wash is attractively designed to upgrade and raise the quality of the existing gas station and is further conditioned to screen air conditioning and rooftop equipment from view, ensure graffiti removal, and provide landscaping and irrigation throughout the project site.

#### Land Use Element

The site is zoned [Q]C2-1VL and is designated for Neighborhood Office Commercial land use under the Community Plan which corresponds to the C1, C1.5, C2, C4, RAS3, RAS4, and P Zones. The proposed project is consistent with the following policies of the North Hollywood – Valley Village Community Plan, one of the 35 Community Plans which together comprise the Land Use Element:

*Objective 4a: To promote economic well being and public convenience through allocating and distributing commercial lands for retail, service and office facilities, with adequate off-street parking in quantities and patterns based on accepted planning principles and standards; retaining viable commercial frontages with provision for concentrated development and redesigning underutilized strip commercial zoning to more appropriate uses; and improving the appearance of commercial buildings along the major arteries.*

#### Other Elements

The Health and Wellness, Mobility 2035, and Air Quality Elements include policies to reduce levels of pollution or greenhouse gas emissions. The proposed project would co-locate auto-related uses through the addition of a car wash to an existing service center, helping customers to complete their auto-related needs with fewer trips. The condition

requiring solar panels will support the site's other site electrical uses to help reduce the site's dependence on fossil fuels and carbon generating public utility electrical power. Taken together, these conditions show conformance with the General Plan by reducing the level of pollution or greenhouse gas emissions to the benefit of the neighborhood and City in response to General Plan Health and Wellness Element Policies 5.1 (reduce air pollution), 5.7 (reduce greenhouse gas emissions); Air Quality Element Policy 5.1.2 (shift to non-polluting sources of energy in buildings and operations); Mobility Element Policy 4.1 (expand access to transportation choices).

### **Specific Plan Project Permit Compliance**

#### **6. The project substantially complies with the applicable regulations, findings, standards, and provisions of the specific plan.**

The proposed project, as conditioned, complies with all applicable development requirements of the Valley Village Specific Plan, as follows:

- a. **Section 5: Zoning and Land Use.** The site is zoned [Q]C2-1VL and has a General Plan Land Use designation of Neighborhood Office Commercial. The project consists of the construction, use, and maintenance of a 736 square foot, self-operated car wash tunnel (17 ½ feet maximum height) in conjunction with an existing gas station with convenience store, on an approximate 19,164 square foot commercial corner site. The proposed project is not consistent with the use restrictions of the Specific Plan and thus has sought a Specific Plan Exception to permit a car wash use where otherwise prohibited.
- b. **Section 6.A.2: Exterior Lighting.** Section 6.A.2 of the Specific Plan requires that all lighting for the project shall be low-illumination safety lighting of a color similar to incandescent light, which is shielded and directed onto the property on which the project is located. As per the Conditions of Approval, the project will comply with the Specific Plan.
- c. **Section 6.B: Building Height.** Section 6.B.2 of the Specific Plan requires that commercial projects within 199 of feet from an RW1 or more restrictive Zone shall require transitional height; the nearest such Zone approximately 290 feet away and thus this does not apply.
- d. **Sections Relating to Single-Family Multi-Family Projects.** Sections 6.B.1, 6.C, 6.D of the Specific Plan relate to residential projects, and do not apply to this commercial project. Section 6.D relates to multi-family projects on lots with a total width of 150 feet or more, which does not apply to this project.
- e. **Section 7: Signage.** Section 7 of the Specific Plan prohibits new commercial off-site sign or sign support structure, or roof sign to be erected in the Specific Plan area. The free-standing pole sign subject to the Conditional Use as part of this request is an existing sign and thus this section does not apply.
- f. **Section 8: Parking.** The project will also provide vehicular and bicycle parking as required by LAMC Section 12.21. As such, the project is in conformance with Section 8 of the Specific Plan.
- g. **Section 9. Landscape Standards.**

- i. Section 9.A: General Requirements.** Pursuant to Section 9.A of the Specific Plan, a landscape plan prepared by a person licensed to prepare landscape plans under state law will be submitted for review and approval to the Department of City Planning, Development Service Center. Landscaped areas will be planted with a variety of plant materials which include shrubs, trees and ground cover, and all plants and trees will be drought-resistant. Artificial plants are prohibited. All landscaped areas will be equipped with an automatic sprinkler or drip irrigation system designed to conserve water.
  - ii. Section 9.B: Buffer Standards.** Pursuant to Section 9B of the Specific Plan, there shall be a five-foot buffer of plant material wherever a multiple-family or commercial project is adjacent to an RW1 or more restrictively zoned lot or use. This does not apply to the subject property.
  - iii. Section 9C: Existing Trees and Replacement Trees.** Pursuant to Section 9C of the Specific Plan, existing non-fruit bearing mature trees that are at least five feet in height shall be incorporated into a multiple-family or commercial project, except within the building area and driveway access. For every tree removed due to subterranean parking facilities, a replacement tree shall be planted on a 1:1 basis. Replacement trees shall be at least a 24-inch box size, not less than eight feet in height, with a trunk diameter of not less than two inches, and a minimum branch spread of five feet. All trees shall be in healthy growing condition. The project has been conditioned to replace removed trees on a 1:1 basis.
  - iv. Section 9D: Street Trees.** Pursuant to Section 9D of the Specific Plan, shade-producing street trees will be planted at a ratio of at least one tree for each 30 lineal feet of street frontage when no obstructions are present. The minimum size for street trees will be a minimum of 10 feet in height and two inches in caliper at the time of planting. Installation of street trees will be to the satisfaction of the Bureau of Street Services, Urban Forestry Division. As such, the project complies with Section 9.D of the Specific Plan.
  - v. Section 9E: Landscaping Requirements.** Pursuant to Section 9E of the Specific Plan, the area from the lot line to the building shall be landscaped with live plant materials and/or ground cover, except for required exit-ways, walkways and driveways. The front, rear and side yards except the areas used for walkways, driveways, or parking will be landscaped. A minimum of 50 percent of the total required front and rear yards will be landscaped. As such, the project complies with Section 9.E of the Specific Plan.
- 7. The project incorporates mitigation measures, monitoring measures when necessary, or alternatives identified in the environmental review, which would mitigate the negative environmental effects of the project, to the extent physically feasible.**

Mitigation measures are not necessary for the subject project, and there are no potentially significant negative environmental effects associated with the project. Based on the whole of the administrative record, the Project is exempt from CEQA pursuant to State CEQA Guidelines, Article 19, Section(s) 15303, Class 3], and there is no substantial evidence demonstrating that an exception to a categorical exemption applies pursuant to CEQA Guidelines, Section 15300.2 applies.

The applicant is requesting a Conditional Use to deviate from Mini-Shopping Center/Commercial Corner and Automotive Use exception regulations of the Los Angeles Municipal Code to allow the following deviations:

- Less than 50% ground floor transparent windows.
- The re-facing and continued use of an existing free-standing pole sign that would otherwise require a Conditional Use Permit.
- Hours of operation for the existing convenience store from 5 a.m. to 11 p.m., Monday through Friday, and 6 a.m. to 11 p.m. Saturday and Sunday.

**8. The project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city or region.**

The project is the construction, use, and maintenance of a 736 square foot, self-operated car wash tunnel (17 ½ feet maximum height) in conjunction with an existing gas station with convenience store, on an approximate 19,164 square foot commercial corner site.

Surrounding properties are generally characterized by consistent zoning and land use designations showing a graduated decrease in intensity from the commercial corner of Laurel Canyon Boulevard and Riverside Drive, with commercial development at the corner and along Laurel Canyon, with low-medium / medium density residential development adjacent to commercial uses, and single-family development located further away from commercial corridors.

The subject site is bounded in all four cardinal directions by commercial properties similarly designated for Neighborhood Office Commercial land uses. Surrounding properties are similarly zoned [Q]C2-1VL and developed with one and two story commercial buildings. The west adjoining property is developed with a drive-thru dry cleaners; north adjoining property is developed with a one story bank and related surface parking lot; south abutting property is developed with a gas station, convenience store and drive-thru car wash; east abutting property is developed with a multi-tenant commercial center which include sit down eateries and a coffee shop; and the southeast abutting corner is developed with a grocery store and large parking lot.

Surrounding properties away from the commercial corner are a mix of multiple family and single family residential. Residential areas adjacent to commercial areas are primarily designated Medium (primarily zoned R3) and Low Medium II Residential land uses (primarily zoned RD1.5 and R2), with larger developments of two to three stories in the Medium Residential areas and smaller developments of one to two stories in the Low Medium II Residential areas. Located further away from the commercial corner are single-family areas, designated for Low Residential land uses, zoned R1-1, and developed with one story buildings.

Also of note, approximately 1,000 feet to the south of the subject is an entrance to the US Route 101 Ventura Freeway, designated for Public Facilities land uses and zoned PF-1VL; the proximity to this major freeway has resulted in the development of multiple nearby auto-oriented uses, including multiple other fuel service stations.

Given the project's proximate location to other similarly automotive-oriented uses, the rehabilitation and renovation of the existing gas station that would take place concurrently, the well-designed appearance of the proposed car wash, and the benefit provided to the

community by co-locating adjacent uses, the project as proposed will provide an enhancement to the nearby community.

Therefore, the project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city or region

**9. The project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare and safety.**

The subject site is bounded in all four cardinal directions by commercial properties similarly designated for Neighborhood Office Commercial land uses. Surrounding properties are similarly zoned [Q]C2-1VL and developed with one and two story commercial buildings. The west adjoining property is developed with a drive-thru dry cleaners; north adjoining property is developed with a one-story bank and related surface parking lot; south abutting property is developed with a gas station, convenience store and drive-thru car wash; east abutting property is developed with a multi-tenant commercial center which include sit down eateries and a coffee shop; and the southeast abutting corner is developed with a grocery store and large parking lot.

Surrounding properties away from the commercial corner are a mix of multiple family and single family residential. Residential areas adjacent to commercial areas are primarily designated Medium (primarily zoned R3) and Low Medium II Residential land uses (primarily zoned RD1.5 and R2), with larger developments of two to three stories in the Medium Residential areas and smaller developments of one to two stories in the Low Medium II Residential areas. Located further away from the commercial corner are single-family areas, designated for Low Residential land uses, zoned R1-1, and developed with one story buildings.

As described above and below, multiple corrective Conditions of Approval have been applied to ensure the project will be compatible with its surroundings and the City's land use and planning goals.

Therefore, the project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare and safety.

**10. The project substantially conforms with the purpose, intent and provisions of the General Plan, the applicable community plan, and any specific plan.**

The proposed project site is an approximately 19,164 square foot, generally flat, and rectangle-shaped area comprised of three lots. The proposed project complies with the following goals, policies, and programs of the General Plan, and therefore, substantially conforms:

Framework Element

*General Commercial Areas. Objective 3.12: Generally, maintain the uses, density, and character of existing low-intensity commercial districts whose functions serve surrounding neighborhoods...*

The proposed car wash maintains and upgrades an existing low density, commercial gas station and convenience store use that serves the surrounding residential and commercial neighborhood.

*Urban Form and Neighborhood Design. Policy 5.5.3: Formulate and adopt building and site design standards and guidelines to raise the quality of design Citywide.*

The proposed car wash is attractively designed to upgrade and raise the quality of the existing gas station and is further conditioned to screen air conditioning and rooftop equipment from view, ensure graffiti removal, and provide landscaping and irrigation throughout the project site.

### Land Use Element

The site is zoned [Q]C2-1VL and is designated for Neighborhood Office Commercial land use under the Community Plan which corresponds to the C1, C1.5, C2, C4, RAS3, RAS4, and P Zones. The proposed project is consistent with the following policies of the North Hollywood – Valley Village Community Plan, one of the 35 Community Plans which together comprise the Land Use Element:

*Objective 4a: To promote economic well being and public convenience through allocating and distributing commercial lands for retail, service and office facilities, with adequate off-street parking in quantities and patterns based on accepted planning principles and standards; retaining viable commercial frontages with provision for concentrated development and redesigning underutilized strip commercial zoning to more appropriate uses; and improving the appearance of commercial buildings along the major arteries.*

### Other Elements

The Health and Wellness, Mobility 2035, and Air Quality Elements include policies to reduce levels of pollution or greenhouse gas emissions. The proposed project would co-locate auto-related uses through the addition of a car wash to an existing service center, helping customers to complete their auto-related needs with fewer trips. The condition requiring solar panels will support the site's other site electrical uses to help reduce the site's dependence on fossil fuels and carbon generating public utility electrical power. Taken together, these conditions show conformance with the General Plan by reducing the level of pollution or greenhouse gas emissions to the benefit of the neighborhood and City in response to General Plan Health and Wellness Element Policies 5.1 (reduce air pollution), 5.7 (reduce greenhouse gas emissions); Air Quality Element Policy 5.1.2 (shift to non-polluting sources of energy in buildings and operations); Mobility Element Policy 4.1 (expand access to transportation choices).

- 11. That based on data provided by the City Department of Transportation or by a licensed traffic engineer, that ingress to and egress from the project will not create a traffic hazard or cause significant traffic congestion or disruption of vehicular circulation on adjacent streets.**

In relation to concerns related to traffic, the proposed function of the car wash tunnel is as an accessory use typically found in conjunction with other service stations, such as the immediately adjacent gas station. Self-service tunnels are fundamentally different from a full-service car wash use relative to their size and potential for annoyance to nearby residents, as well as further serving primarily as an additional amenity rather than a standalone use. While the car wash itself may attract additional customers, this in itself does not create a significant impact or an unusual circumstance leading to a significant



effect. Traffic generation rates are based on averages for a type of use, rather than the popularity of an individual establishment.

Further, based on the LADOT Vehicle Miles Travelled calculator, no traffic/circulation impacts were identified through the environmental clearance procedure. Furthermore, as a condition of the approval, the driveway, parking, and circulation plans will be reviewed by Los Angeles Department of Transportation prior to the issuance of a building permit.

**12. That project approval will not create or add to a detrimental concentration of Mini-Shopping Centers or Commercial Corner Developments in the vicinity of the proposed project.**

The proposed project is the addition of an accessory use to an existing commercial corner development and would therefore not create or add to a detrimental concentration of such developments.

### **Zone Variance**

**13. The strict application of the provisions of the Zoning Ordinance would result in practical difficulties or unnecessary hardships inconsistent with the general purpose and intent of the zoning regulations.**

The proposed project includes the addition of a 736 square-foot drive through automobile laundry. In 1989, City Council adopted Ordinance No. 165,108 (SA 6740) which states, "[the use of the property shall be limited to the uses existing upon effective date of this ordinance and thereafter to those uses and provisions of the C1.5-1-VL.]" The existing fueling station and the proposed addition are not permitted in the C1.5 Zone Classification. The existing fueling station and convenience store have non-conforming rights and are allowed to continue. The project proponent has applied for a Zone Variance to expand a C2 use (Section 12.14-A,9 of the Los Angeles Municipal Code allows the following in the C2 Zone Classification, "Automotive laundry or wash rack, provided the automotive laundry or wash rack is in compliance with all of the development standards and operating conditions set forth in Section 12.22 A.28. of this Code.").

Regulations in the Zoning and Planning Code are deemed necessary in order to encourage the most appropriate use of land; to conserve and stabilize the value of property; to provide adequate open spaces for light and air, and to prevent and fight fires; to prevent undue concentration of population; to lessen congestion on streets; to facilitate adequate provisions for community utilities and facilities such as transportation, water, sewerage, schools, parks and other public requirements; and to promote health, safety, and the general welfare all in accordance with the comprehensive plan." (Section 12.02, Purpose, Los Angeles Municipal Code)

Ordinances do not address every situation that might occur. However, a Variance is a grant of permission to depart from the strict enforcement of a zoning ordinance and allow the property to be used in a manner otherwise not permitted provided that the spirit of the ordinance is observed and substantial justice is done without detrimental impacts to the community.

The Zone Variance is justified as the current use as a gas station and convenience store were established prior to the [Q] conditions being imposed in 1989. Allowing for customers to receive a car wash at this location further allows the applicant to develop a use which is typically accessory to such gas station uses, which would further allow them to offer a

one-stop shop for their customers and offer them desired services while reducing vehicular trips.

The strict application of the [Q] creates an unnecessary hardship because it limits the improvement and expansion of an existing legal use to include a service on site which will mitigate total vehicle trips and be desirable to the public convenience.

**14. There are special circumstances applicable to the subject property such as size, shape, topography, location or surroundings that do not apply generally to other property in the same zone and vicinity.**

The unique location (subject to an areawide Ordinance which prohibits redevelopment or expansion of the existing use through the imposition of [Q] Qualified conditions) and existing development of the site are special circumstances applicable to the subject property that do not apply generally to other property in the same zone and vicinity. These circumstances include the large size of the site, history of automotive uses, location on a corner, and adjacency to the nearby Freeway.

In essence, the site is uniquely capable of accommodating the proposed car wash and providing needed access and parking for the proposed incidental use without impeding access to or interference with the existing refueling operation.

Granting the Variance acknowledges the special circumstances of the subject property that does not generally apply to other properties in the same zone in the surrounding area.

**15. Such variance is necessary for the preservation and enjoyment of a substantial property right or use generally possessed by other property in the same zone and vicinity but which, because of such special circumstances and practical difficulties or unnecessary hardships, is denied the property in question.**

The granting of the requested variance is necessary for the enjoyment of a substantial property right or use available to others in the vicinity; specifically, pursuant to case number ZA-2008-2924-CUB-CU-ZV, the immediately adjacent gas station to the south was similarly granted a Zone Variance from the same Ordinance for a similar self-service car wash project. To deny such a similar request would deprive the applicant of a substantial property right available to the most equivalent nearby site and project.

Therefore, granting of such variance is necessary for the preservation and enjoyment of a substantial property right or use generally possessed by other property in the same zone and vicinity but which, because of such special circumstances and practical difficulties or unnecessary hardships, is denied the property in question.

**16. The granting of such variance will not be materially detrimental to the public welfare or injurious to the property or improvements in the same zone or vicinity in which the property is located.**

Granting the variance will not be materially detrimental because special consideration is given to the impact on residences adjacent to the property, as well as other nearby properties through the imposition of corrective conditions to require thoughtful lighting, landscaping and noise attenuation.

Therefore, granting of the variance will not be materially detrimental to the public welfare, or injurious to the property or improvements in the same zone or vicinity in which the property is located.

**17. The granting of the variance will not adversely affect any element of the General Plan.**

The proposed project site is an approximately 19,164 square foot, generally flat, and rectangle-shaped area comprised of three lots. The proposed project complies with the following goals, policies, and programs of the General Plan, and therefore, granting of the request would not adversely affect any element of the General Plan:

Framework Element

*General Commercial Areas. Objective 3.12: Generally, maintain the uses, density, and character of existing low-intensity commercial districts whose functions serve surrounding neighborhoods...*

The proposed car wash maintains and upgrades an existing low density, commercial gas station and convenience store use that serves the surrounding residential and commercial neighborhood.

*Urban Form and Neighborhood Design. Policy 5.5.3: Formulate and adopt building and site design standards and guidelines to raise the quality of design Citywide.*

The proposed car wash is attractively designed to upgrade and raise the quality of the existing gas station and is further conditioned to screen air conditioning and rooftop equipment from view, ensure graffiti removal, and provide landscaping and irrigation throughout the project site.

Land Use Element

The site is zoned [Q]C2-1VL and is designated for Neighborhood Office Commercial land use under the Community Plan which corresponds to the C1, C1.5, C2, C4, RAS3, RAS4, and P Zones. The proposed project is consistent with the following policies of the North Hollywood – Valley Village Community Plan, one of the 35 Community Plans which together comprise the Land Use Element:

*Objective 4a: To promote economic well being and public convenience through allocating and distributing commercial lands for retail, service and office facilities, with adequate off-street parking in quantities and patterns based on accepted planning principles and standards; retaining viable commercial frontages with provision for concentrated development and redesigning underutilized strip commercial zoning to more appropriate uses; and improving the appearance of commercial buildings along the major arteries.*

Other Elements

The Health and Wellness, Mobility 2035, and Air Quality Elements include policies to reduce levels of pollution or greenhouse gas emissions. The proposed project would co-locate auto-related uses through the addition of a car wash to an existing service center, helping customers to complete their auto-related needs with fewer trips. The condition requiring solar panels will support the site's other site electrical uses to help reduce the site's dependence on fossil fuels and carbon generating public utility electrical power. Taken together, these conditions show conformance with the General Plan by reducing the level of pollution or greenhouse gas emissions to the benefit of the neighborhood and City in response to General Plan Health and Wellness Element Policies 5.1 (reduce air pollution), 5.7 (reduce greenhouse gas emissions); Air Quality Element Policy 5.1.2 (shift

to non-polluting sources of energy in buildings and operations); Mobility Element Policy 4.1 (expand access to transportation choices).

**CEQA EXEMPTION**

DETERMINE that based on the whole of the administrative record, the Project is exempt from CEQA pursuant to State CEQA Guidelines, Section 15303 (Class 3), and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.

## **PUBLIC HEARING AND COMMUNICATIONS**

A public hearing was conducted by the Hearing Officer on May 17, 2021 at 1:00 p.m. as a virtual meeting over phone and Zoom. The hearing was attended by neighborhood residents, and the applicant's representatives. Approximately seven people attended the hearing. Four members of the public spoke in opposition, primarily with concerns in relation to traffic, noise, and the number of car washes in the area. Four written comments were received by staff subsequent to the hearing expressing opposition.

### **Summary of Public Hearing Testimony**

#### **Applicant's Presentation**

##### **Applicant's Representative – Ken Kang**

- Site is an existing gas station on 4801 Laurel, zoned [Q]C2-1VL, located at the northwest corner of Laurel and Riverside
- Station has served community for over 40 years
- Current owners have owned the property for over 10 years
- Located on commercial zoning with other commercial properties nearby
- Existing gas station with car was across Riverside
- Laurel Canyon has nearby access to freeways south and west
- Given nearby adjacencies and highway location, functions more as a highway-oriented use
- Existing site has small canopy, convenience store, and storage building (to be demolished)
- Site designed in a "modern style" with concrete, metal, and glass
- Pole signage will remain; otherwise, repairing and replacing
- Design and uses would be very similar to adjacent site
- 736 square foot car wash proposed
- Remodel existing store façade with ceramic wood tile
- Remodel trash enclosure
- Remodel existing pole sign
- Bringing in sky lights to store for natural light
- Canopy also to be upgraded with wood tiling
- Small text modifications to signs
- New wash tunnels would windows, pop-outs, and other design features
- Freeway adjacency and business history, with highway-oriented use type
- Car wash noise attenuation includes an increased height wall and vines
- Provides an attractive alternative in keeping with the adjacent property

#### **Comments in Opposition**

##### **Eddie Bilovi, local resident**

- Don't understand why there are so many car washes in the area
- Couldn't they just expand convenience store
- Will add traffic

##### **Kevin Williams, local resident**

- Too many car washes are here already
- Q conditions and plan are there to prevent nonconforming uses
- Noise travels and is a concern

Nicole Miller, local resident

- Calling to voice opposition
- Too many car washes; feels like one on every corner, would rather see something else

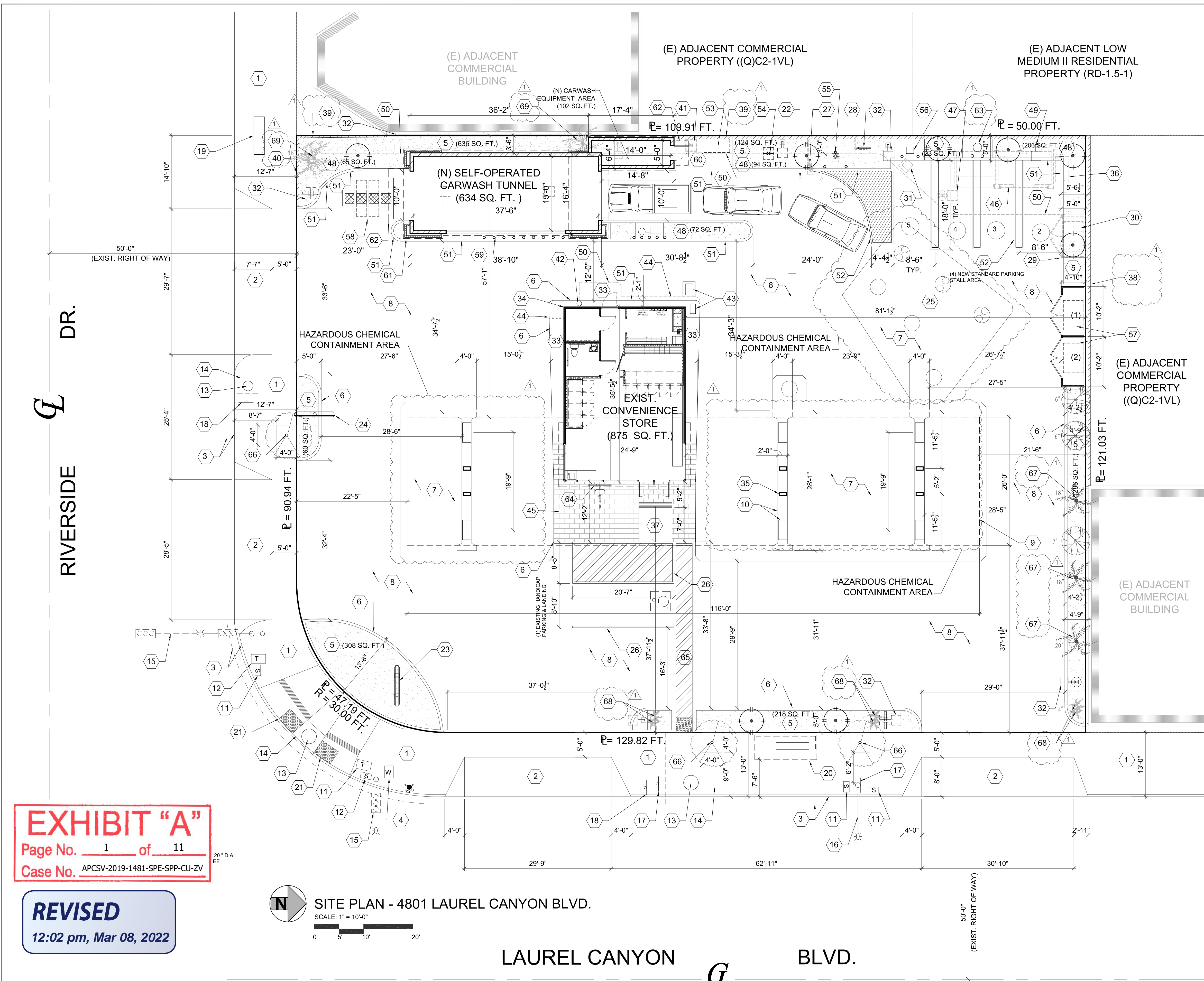
Nathan Colger, local resident

- Don't support more car washes here; not in the best interest of the community
- Additional construction noise from additions and renovations
- Concerns about car wash noise and traffic

**Applicant's Response**Applicant's Representative – Ken Kang

- Neighborhood concerns around noise which resulted in sound study, showing compliance with LAMC Noise Ordinance
- Car wash across the street is very busy; addition of a new car wash would help alleviate lines
- Project incorporates other energy-saving features; would recycle 70% of water used

Exhibit A – Project Plans



**EXHIBIT "A"**  
 Page No. 1 of 11  
 Case No. APCSV-2019-1481-SPE-SPP-CU-ZV

**REVISED**  
 12:02 pm, Mar 08, 2022

**SITE PLAN - 4801 LAUREL CANYON BLVD.**  
 SCALE: 1" = 10'-0"  
 0 5 10 20'

**LAUREL CANYON BLVD.**

**LEGEND**

- NOTES**
- # NOTES
  - # PARKING SPACE NUMBER
- SITE NOTES**
- 1 EXIST. CONC. SIDE WALK
  - 2 EXIST. CONC. DRIVEWAY
  - 3 EXIST. CONC. CURB & GUTTER
  - 4 EXIST. WATER METER
  - 5 EXIST. LANDSCAPE PLANTER
  - 6 EXIST. 6" CONC. CURB - TYP.
  - 7 EXIST. CONC. PAVING
  - 8 EXIST. ASPHALTIC CONC. PAVING
  - 9 EXIST. MTL. FUELING CANOPY (26' X 116')
  - 10 EXIST. FUELING DISPENSER W/ 6" HIGH CONC. ISLAND - TYP.
  - 11 EXIST. TRAFFIC SIGNAL PULL VAULT
  - 12 EXIST. STREET LIGHT PULL VAULT
  - 13 EXIST. EXIST. MANHOLE
  - 14 EXIST. CATCH BASIN
  - 15 EXIST. TRAFFIC SIGNAL & STREET LIGHT POLE
  - 16 EXIST. STREET LIGHT POLE

- 17 EXIST. STREET - TRAFFIC SIGNAGE
- 18 EXIST. STREET BUS SIGNAGE
- 19 EXIST. EXIST. BUS STOP BENCH
- 20 EXIST. EXIST. BUS STOP STRUCTURE
- 21 EXIST. STREET INTERSECTION ADA HANDICAP RAMP
- 22 EXIST. 6" DIA. MTL. BOLLARD - TYP.
- 23 EXIST. MAIN FREESTANDING POLE MOBILE / PRICE SIGN 1- REFER TO SHT. C03
- 24 EXIST. POLE PRICE SIGN 2 - REFER TO SHT. C03
- 25 EXIST. UNDERGROUND STORAGE FUEL TANKS W/ CONC. PAD
- 26 EXIST. PARKING PAINT STRIPING (4" WIDE) - TYP.
- 27 EXIST. FUEL VAPOR CARBON CANISTER W/ MTL. ENCLOSURE
- 28 EXIST. MTL. FUEL VENT - TYP.
- 29 EXIST. MTL. TRASH ENCLOSURE AREA (4'-10" X 8'-8")
- 30 EXIST. MTL. TRASH DUMPSTER
- 31 EXIST. AIR / WATER TO BE RELOCATED
- 32 EXIST. L.E.D. ON SITE PARKING LIGHTING-TYP
- 33 EXIST. CONC. WALK WAY
- 34 EXIST. BUILDING (24'-9" X 35'-5 1/2")
- 35 EXIST. (8' X 17') CANOPY MTL. STEEL COLUMN - TYP.
- 36 EXIST. MAIN SWITCH ELEC. CABINET TO BE RELOCATED
- 37 EXIST. HANDICAP ACCESS RAMP

- 38 EXIST. 6" CMU BOUNDARY WALL - 4' HIGH NORTH SIDE OF (NORTH SIDE OF PROP.) BOUNDARY WALL TO BE RAISED TO MIN. OF 6' HIGH - TYP.
- 39 EXIST. 6' HIGH - 8" CMU BOUNDARY WALL (WEST SIDE OF PROP.), BOUNDARY WALL TO BE RAISED MIN. OF 8' HIGH - TYP.
- 40 EXIST. 4' HIGH / 6" CMU BOUNDARY WALL
- 41 EXIST. IRRIGATION BACK FLOW DEVICE W/ MTL. CAGE ENCLOSURE TO BE RELOCATED
- 42 EXIST. SEWER VALVE
- 43 EXIST. WATER VALVE/CONTROLS
- 44 EXIST. 3" DRAIN
- 45 EXIST. 12" X 24" TILE PAVING
- 46 EXIST. STORAGE STRUCTURE TO BE REMOVED
- 47 EXIST. L.E.D. ON SITE PARKING LIGHTING TO BE REMOVED
- 48 NEW LANDSCAPE PLANTER
- 49 NEW L.E.D. ON SITE PARKING LIGHTING
- 50 EXIST. 6" CONC. CURB TO BE REMOVED -TYP.
- 51 NEW 6" CONC. CURB - TYP.
- 52 NEW PARKING AND SITE PAINT STRIPING - TYP.
- 53 NEW MAIN SWITCH ELEC. CABINET LOCATION
- 54 NEW IRRIGATION BACK FLOW DEVICE W/ MTL. CAGE ENCLOSURE LOCATION

- 55 NEW LOCATION FOR THE EXIST. FUEL VAPOR CARBON CANISTER W/ MTL. ENCLOSURE
- 56 NEW AIR / WATER LOCATION
- 57 NEW MTL. TRASH ENCLOSURE W/ EXTERIOR TILE - LIGHT WOOD GRAIN FINISH - ARIZONA TILE: SAV WOOD- 2 X (4'-10" X 10'-2") W/ TRASH (1) & RECYCLING (2) DUMPSTER-REFER TO SHT. C01
- 58 NEW 1,500 GAL. CARWASH CLARIFIER
- 59 NEW 6" DIA. MTL. BOLLARDS - TYP.
- 60 NEW CONC. WALK WAY
- 61 NEW ROOF LINE
- 62 NEW MTL. AWNING / WALL CANOPY (GREY/METALLIC FINISH) W/ 4" DIA. ULTRA RT 4 LED W/ NICKEL TRIM FINISH - DOWN LIGHTING @ 20' O.C. - TYP.
- 63 NEW COIN OPERATED - 2 HOSE VACUUM W/ A SOUND DAMPENERS SYSTEM
- 64 NEW (3) BIKE TUBE STEEL SERPINE RACK, PAINTED GREEN
- 65 NEW PEDESTRIAN WALK WAY
- 66 NEW STREET TREES (MIN. SIZE: 10' IN HEIGHT AND 2" CALIPER AT THE TIME OF PLANNING) / TREES TO BE IN ACCORDANCE WITH THE CITY OF LOS ANGELES LANDSCAPE ORDINANCE 170.978 & APPROVED BY THE STREET TREE DIVISION OF THE BUREAU OF STREET MAINTENANCE)
- 67 EXIST. MATURE PALM TREES TO REMAIN
- 68 EXIST. PALM TREES TO BE REMOVED, NOT MATURE AND NOT ALLOWED
- 69 EXIST. MATURE PALM TREES TO BE REMOVED FOR NEW CARWASH BUILDING AND FOR BETTER VIEW OF DEDICATED PEDESTRIAN AREA AT THE SOUTH OF THE PROPERTY

**GENERAL NOTES**

1. SITE PAVING PER PLANS.
2. EXIST. ASPHALTIC CONCRETE, NOT SHOWN TO BE ALTERED, REQUIRES PERMISSION FROM PUBLIC WORKS PRIOR TO ANY RESURFACING ACTIVITIES.
3. 9' X 18" HANDICAP STALL TO HAVE REQUIRED STRIPING AND SIGNAGE WITH 1/2" PER FOOT MAXIMUM SLOPE IN ANY DIRECTION.
4. EXISTING DRIVEWAYS COMPLY WITH CITY OF LOS ANGELES.
5. EXISTING PERIMETER MASONRY WALLS SHALL BE REPAIRED AS NECESSARY.
6. ALL NEW UTILITY SERVICES SHALL BE UNDERGROUND.
7. THESE PLANS ARE COMPLIANT W/ ADA & CBC ACCESSIBILITY STANDARDS.
8. MINIMUM VERTICAL CLEARANCE OF 25' AT THE STAGING AREA FOR BIN SERVICE CLEARANCE. MINIMUM 13' VERTICAL CLEARANCE FOR SCOOP TRUCK.
9. ALL DRIVEWAY AND STAGING AREAS MUST BE ABLE TO SUSTAIN A MINIMUM GROSS WEIGHT OF 60,000 LBS. PER VEHICLE.
10. ALL STAGING AREAS ARE TO BE ONSITE. NO STREET STAGING IS PERMITTED
11. VISUAL CLEARANCE TO HAVE 7.5' TRIANGULAR CLEARANCE ON BOTH END OF DRIVEWAYS & AT INTERSECTIONS, IT IS 15' TRIANGULAR CLEARANCE WITH VERTICAL CLEARANCE OF OBSTRUCTIONS NO HIGHER THAN 3 FEET.
12. ALL WORK WITHIN THE PUBLIC RIGHT OF WAY REQUIRES A SEPARATE PUBLICWORKS/ENGINEERING DEPARTMENT ENCROACHMENT PERMIT.
13. GENERAL CONTRACTOR TO BE RESPONSIBLE FOR MAINTAINING AND ENFORCING SAFETY STANDARDS, CONDITIONS AND EQUIPMENT AS REQUIRED BY OSHA.

**GENERAL INFORMATION**

**PROPERTY OWNER:**  
 N & D OIL CORP.  
 ATT: HATTAR, NADER  
 9431 BRANDON CT.  
 NORTH RIDGE, CA 91225  
 TEL: (818) 980-1366  
 EMAIL: NadersMobil@gmail.com

**PROJECT OWNER / APPLICANT:**  
 N & D OIL CORP.  
 ATT: HATTAR, NADER  
 4801 N. LAUREL CANYON BLVD.  
 VALLEY VILLAGE, CA 91607  
 TEL: (818) 980-1366  
 EMAIL: NadersMobil@gmail.com

**REPRESENTATIVES / DESIGNERS:**  
 MK DESIGN  
 KANG & IAN MARR  
 535 W. ALLEN AVE., UNIT 23  
 SAN DIMAS, CA 91773  
 TEL: KEN: 626-374-3834  
 Email:kangdesign@hotmail.com  
 TEL: IAN: 619-913-2751  
 Email: iamarr@hotmail.com

**SITE-ASSESSOR'S PARCEL NUMBER:**  
 APN: 2356-008-021

**PROJECT SITE ADDRESS:**  
 4801 N. LAUREL CANYON BLVD.  
 VALLEY VILLAGE, CA 91607

**TOTAL LOT AREA:** 19,164.3 SQ. FT. / 0.44 ACRES

**LEGAL DESCRIPTION:**  
 LOTS 62, 63, AND 64 OF TRACT NO. 7808, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 115 PAGE 44 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 2356-008-021  
 TRACT: TR 7808

**ZONING:** (Q) C-2 -1VL  
 (COMMERCIAL)

**LANDUSE:** NEIGHBORHOOD OFFICE COMMERCIAL

**SPECIFIC PLAN AREA:** VALLEY VILLAGE

**BUILDING CODE USED:** 2016 C.B.C.

**TYPE OF CONSTRUCTION:** Type V B

**OCCUPANCY TYPE:** M

**EXISTING NUMBER OF STORIES:** 1  
 (EXISTING BUILDING MAX. HEIGHT: 21'-11" +/-)

**EXISTING NUMBER OF STORIES:** 1  
 EXISTING BUILDING MAX. HEIGHT: 10'-11" +/-  
 EXISTING BUILDING AVG. HEIGHT: 10'-11" +/-

**EXISTING STRUCTURES SITE COVERAGE:**  
 CONVENIENCE STORE: 875+/- SQ. FT.

**FUEL CANOPY 1 (26'-0" X 116'-0"):** 3,016+/- SQ. FT.

**TRASH ENCLOSURE (4'-10" X 8'-8"):** 42+/- SQ. FT.

**EXISTING STRUCTURES TOTAL:** 3,933+/- SQ. FT.

**EXIST. LANDSCAPING:**  
 TOTAL EXIST. LANDSCAPE: 1,448.00 SQ. FT./  
 19,164.30 SQ. FT.  
 TOTAL EXISTING LANDSCAPE PERCENTAGE PROVIDED: .075= 8%

**TOTAL NEW LANDSCAPE PERCENTAGE PROPOSED:** 434 SQ. FT. / 2.3%

**TOTAL LANDSCAPE:** 1,882 SQ. FT. / 19,164.30 SQ. FT. = 0.098 = 10%

**PARKING REQUIRED:**  
 EXIST. CONVENIENCE STORE (875 SQ. FT.)  
 EXIST. C-STORE PARKING REQUIRED 2 BY SAME AS BEFORE 1985 C-STORE PARKING AGREEMENT-2  
 TOTAL STALLS REQUIRED: 2 STALLS

**EXIST. STANDARD PARKING:** 2  
**EXIST. HANDICAP STALL:** 1  
**TOTAL EXIST. PARKING STALLS PROVIDED:** 3  
**NEW PARKING REQUIRED:** 736 SQ. FT./250 = 2.94 = 3

**NEW PARKING REQUIRED: EXIST. REQUIRED FOR C-STORE AND SERVICE STATION (1985 C-STORE PARKING AGREEMENT):** 2  
**NEW SMALL CARWASH TUNNEL AND EQUIPMENT STORAGE PARKING REQUIRED:** 3

**NEW TOTAL PARKING REQUIRED:** 5  
**NEW TOTAL PARKING PROVIDED:** 5

**PROPOSED PROJECT:**  
 A NEW SMALL SELF OPERATED EXPRESS CARWASH (634 SQ. FT.) + CARWASH STORAGE (102 SQ. FT.). MINOR SITE MODIFICATIONS (MODIFICATION TO EXISTING LANDSCAPE AND PARKING). IN ADDITION, NEW EXTERIOR TILE FINISH AT THE CURRENT BRICK VENEER FINISH AREA, ONLY, TO THE EXISTING C-STORE (875 SQ. FT.) TO MATCH THE PROPOSED CARWASH FINISH. IN ADDITION, NEW CARWASH SIGNAGE AT THE ENTRY AND EXIT OF THE TUNNEL (50 SQ. FT. TOTAL), AND THE EXISTING SIGNAGE TOTAL IS 253.4 SQ. FT.; THUS, THE TOTAL IS 303.4 SQ. FT. AND NEW TRASH/RECYCLING ENCLOSURE (98 SQ. FT.), AND ITS EXTERIOR FINISH TO MATCH BUILDING (LIGHT WOOD GRAIN TILE). IN ADDITION, EXISTING FUEL CANOPY TO HAVE NEW FINISH (LIGHT WOOD GRAIN TILE) AT THE EXISTING COLUMNS TO MATCH BUILDINGS. THE PROJECT REQUIRES A PROJECT PERMIT COMPLIANCE (VALLEY VILLAGE SPECIFIC PLAN), CONDITIONAL USE PERMIT FOR CARWASH, AND A COMMERCIAL CORNER/MINI SHOPPING CENTER CONDITIONAL USE PERMIT.

**NEW STRUCTURES:**  
 \* SELF OPERATED CARWASH (634 SQ. FT.)  
 + CARWASH STOR. (102 SQ. FT.): 736 SQ. FT.  
 \* NEW TRASH ENCLOSURE: 98 SQ. FT.  
**TOTAL:** 834 SQ. FT.

**NEW LANDSCAPING AREA:**  
 \* PROPOSED NEW AREA: 437.00 SQ. FT.  
 \* EXISTING AREA: 1,655.00 SQ. FT.  
**TOTAL AREA:** 2,092.00 SQ. FT. /  
 19,164.30 SQ. FT.  
**TOTAL LANDSCAPE PERCENTAGE PROVIDED:** .109 = 11%  
 (TOTAL DOES NOT INCLUDE LANDSCAPING AT THE RIGHT OF WAY AREA)

**REMOVAL OF 9 PALM TREES (3 MATURE & 6 SMALL PALM TREES) & 3 MATURE PALM TREES TO REMAIN.**

**NEW PARKING REQUIRED:**  
 \* EXIST. C-STORE PARKING REQUIRED 2 BY SAME AS BEFORE 1985 C-STORE PARKING AGREEMENT: 2 STALLS REQUIRED  
 \* NEW CARWASH BAY (634 SQ. FT.) + CARWASH STORAGE (102 SQ. FT.): 736/250 = 2.94 = 3 STALLS

**TOTAL STALLS REQUIRED: 5 STALLS**

**NEW PARKING:** 4 STANDARD STALLS  
**EXIST. HANDICAP STALL:** 1  
**TOTAL: 5 PARKING STALLS PROVIDED**

**LOT COVERAGE:**  
 \* NEW AND EXISTING STRUCTURES: 3,989.00 SQ. FT./  
 \* LOT SIZE: 19,164.30 SQ. FT.  
**TOTAL LOT COVERAGE:** .21 = 21%

**ESTIMATED TOTAL SIGNAGE:**

**POLE SIGNAGE:**  
 EXIST. POLE SIGNAGE (1): 129.2 SQ. FT.  
 EXIST. POLE SIGNAGE (2): 91.5 SQ. FT.  
 TOTAL POLE SIGN SQ. FT. FOR BOTH SIDES: 220.7 SQ. FT.

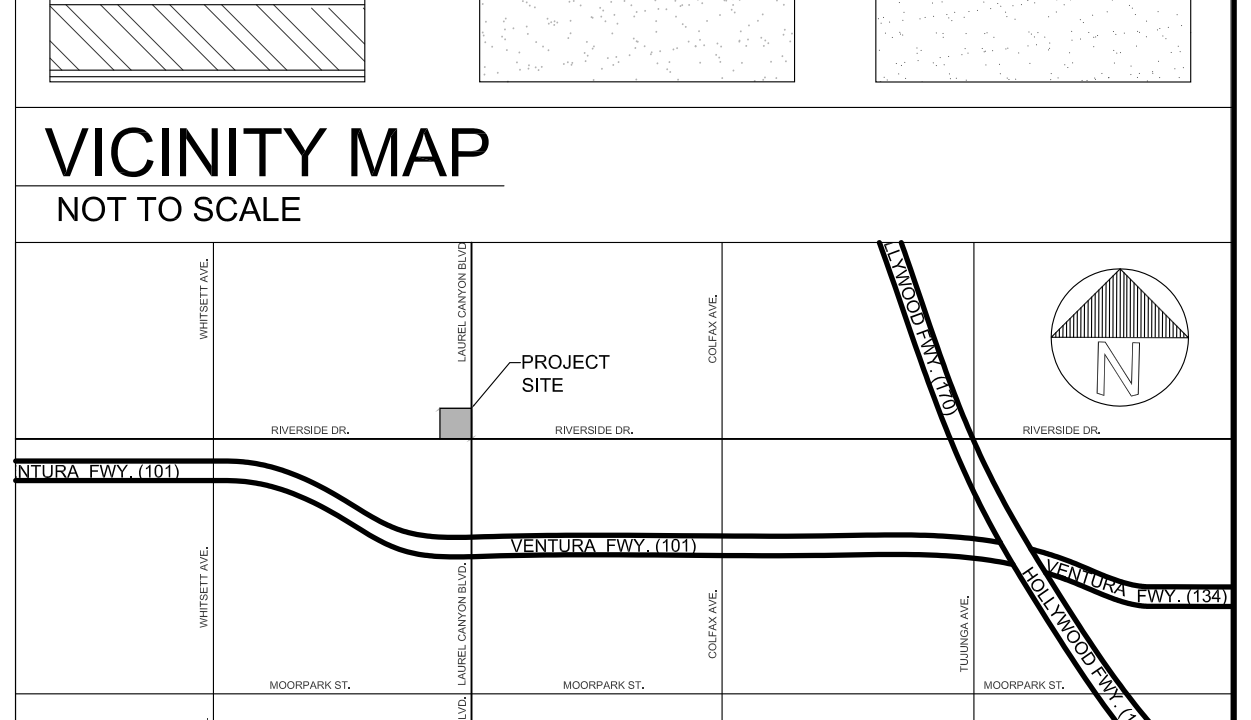
**EXIST. CANOPY MOBIL HALLMARK SIGNAGE:**  
 EAST SIGNAGE: 11.7 SQ. FT.  
 WEST SIGNAGE: 11.7 SQ. FT.  
 SOUTH SIGNAGE: 11.7 SQ. FT.  
**TOTAL SQUARE FOOTAGE:** 35.1 SQ. FT.

**EXIST. CONVENIENCE STORE ENTRY SIGNAGE:**  
 MOBIL SIGNAGE (EAST SIDE OF BUILDING FACADE): 11.0 SQ. FT.  
**TOTAL EXISTING SIGNAGE AREA:** 266.8 SQ. FT.

**NEW SIGNAGE:**  
 SOUTH/CARWASH CHANNEL EXIT SIGNAGE: 25.0 SQ. FT.  
 NORTH/CARWASH CHANNEL ENTRY SIGNAGE: 25.0 SQ. FT.  
**TOTAL NEW SIGNAGE SQUARE FOOTAGE:** 50.0 SQ. FT.

**TOTAL SIGNAGE AREA:** 266.8 + 50.0 = 316.8 SQ. FT.

**LEGEND**  
 FIRE SPRINKLERED NO KNOWN EASEMENTS



1	8-11-19	KK
1	11-9-21	KK

**NADER'S MOBIL GAS STATION REMODEL / CARWASH PROJECT**  
 4801 N. LAUREL CANYON BLVD., VALLEY VILLAGE, CA 91607


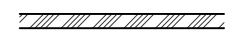


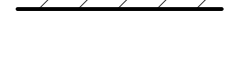

**MK Design**  
 535 W. ALLEN AVE., UNIT 23, SAN DIMAS, CA 91773

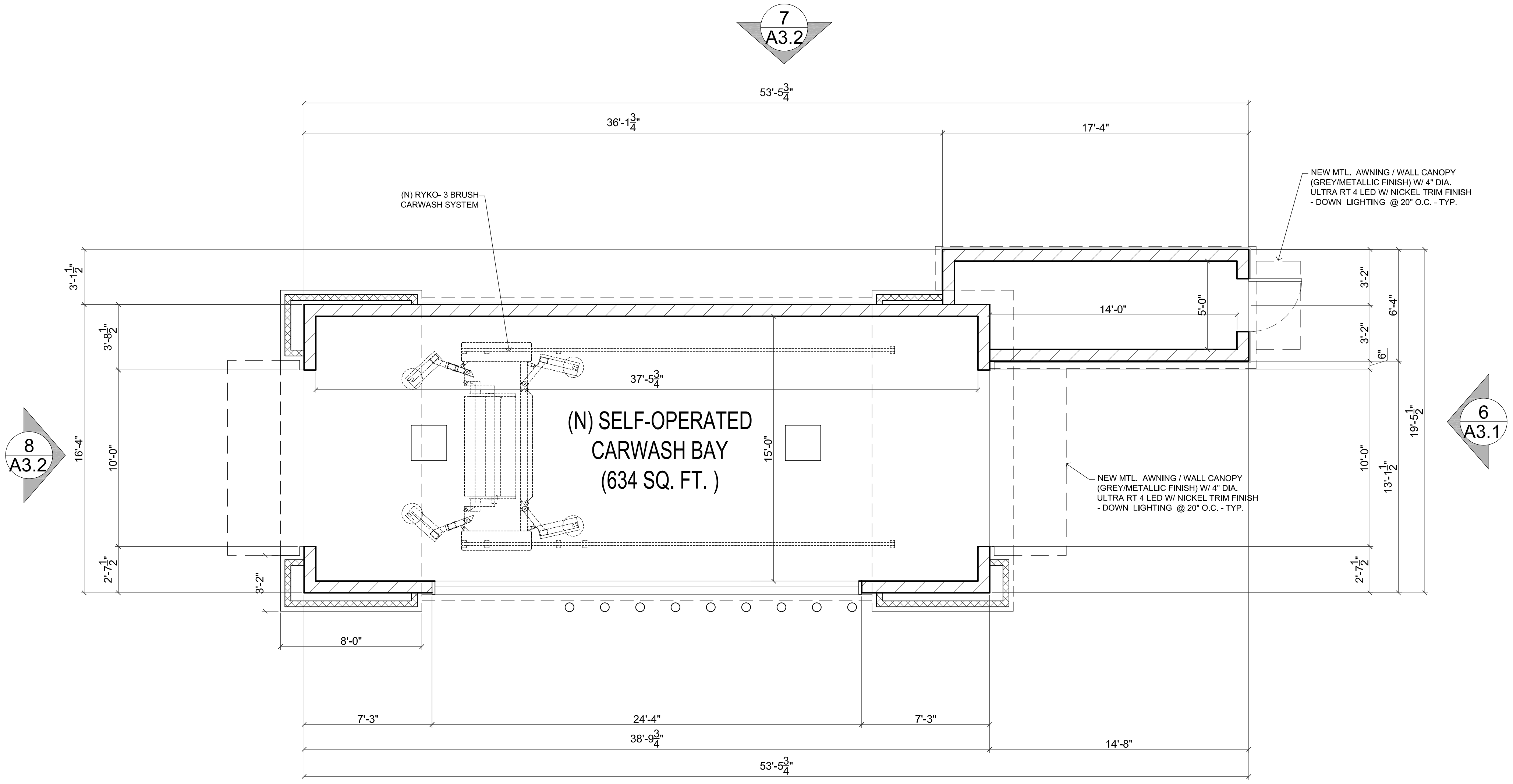
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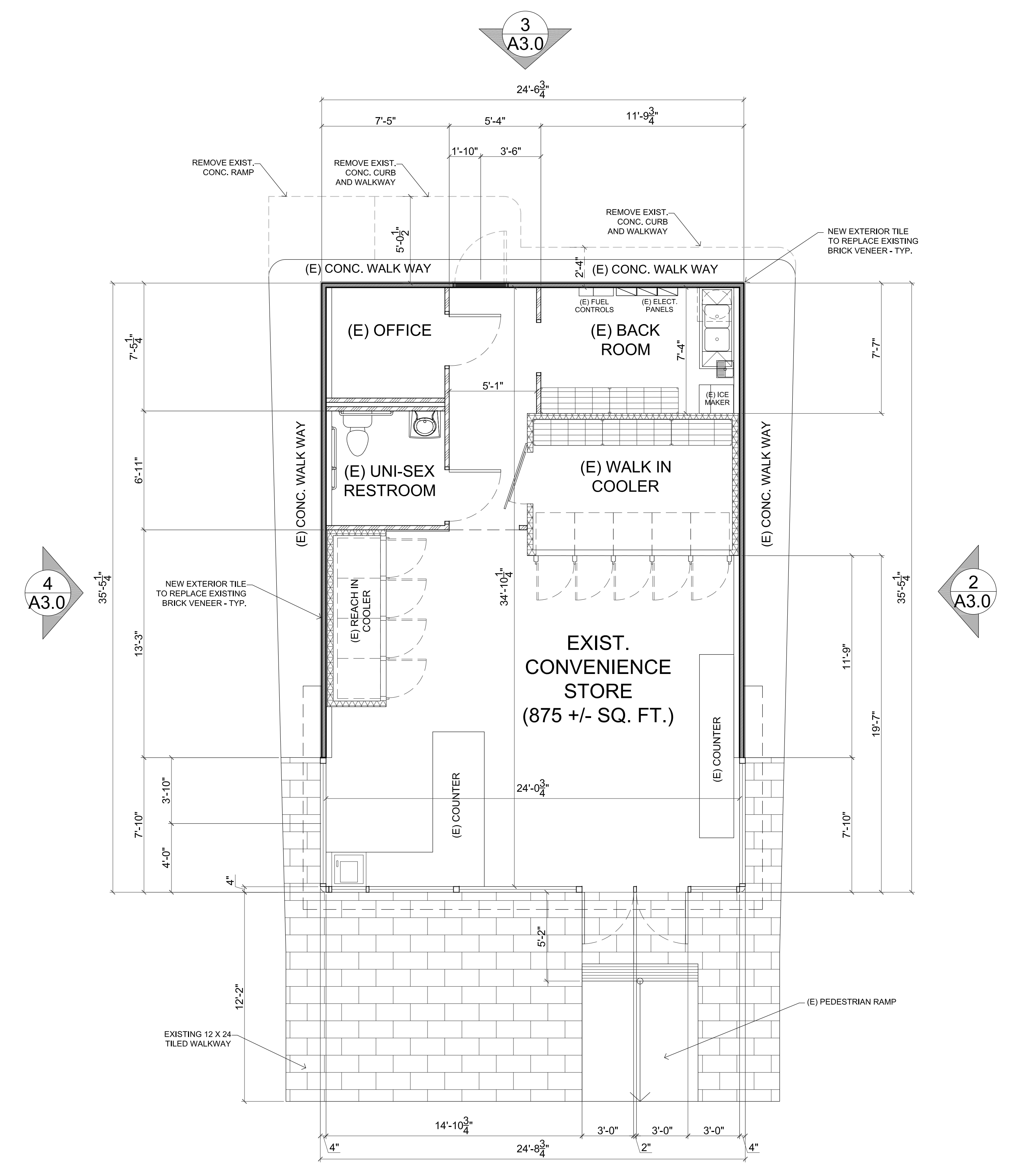
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**LEGEND:**

-  EXIST. EXTERIOR 2 X 3 MTL. BEARING WALLS
-  EXIST. INTERIOR WALLS
-  EXIST. NON-BEARING / WALK IN COOLER / FREEZER PREFABRICATED INSULATED WALL
-  REMOVE EXISTING DOOR AND FILL IN TO THE EXISTING EXTERIOR WALL TO MATCH ENTIRE WALL
-  REMOVE EXISTING DOOR AND FILL IN TO THE EXISTING EXTERIOR WALL TO MATCH ENTIRE WALL
-  REMOVE EXISTING DOOR AND FILL IN TO THE EXISTING EXTERIOR WALL TO MATCH ENTIRE WALL



**PROPOSED NEW CARWASH - FLOOR PLAN**  
 SCALE: 1/4" = 1'-0"



**REMODEL EXIST. CONVENIENCE STORE FLOOR PLAN**  
 SCALE: 1/4" = 1'-0"

### LEGEND

### MATERIAL

- E-01 EXIST. MTL. EXTERIOR WALL ACM PANELS - TYP.
- E-02 EXIST. MTL. EXTERIOR WALL W/ A BRICK VANNER FINISH TO BE REPLACED WITH NEW EXTERIOR TILE - LIGHT WOOD GRAIN FINISH - ARIZONA TILE: SAV WOOD- MIELE (8 X 32) - TYP.
- E-03 EXIST. 3068 MTL. DOOR TO BE REMOVED AND HAVE THE NEW FINISH TO MATCH REST OF THE EXTERIOR WALL FINISH
- E-04 EXIST. FRONT ENTRY PAIR (2) 3070 ALUMINIUM DOOR W/ 1/4" TEMPERED GLASS & 10" KICK PLATE
- E-05 EXIST. ELECT. GUTTER BOX & CONDUITS
- E-06 EXIST. 1/4" COMMERCIAL TEMPERED GLASS-TYP.
- E-07 EXIST. MTL. WIRE TEMPERED GLASS - TYP.
- E-08 EXIST. MTL. CASHIERS SECURITY TRANSACTION DRAWER W/ MTL. POLISHED FINISH
- E-09 EXIST. SIGNAGE-MOBIL HALL MARK SIGNAGE
- E-10 EXIST. 6" CONC. CURB - TYP.
- E-11 EXIST. FIRE EXTINGUISHER BOX
- E-12 EXIST. KNOX BOX FOR FIRE DEPARTMENT
- E-13 EXIST. MTL. TRIM & MTL. FRAMING - TYP.
- E-14 EXIST. PEDESTRIAN CONC. RAMP - 1:20 SLOPE
- E-15 EXIST. ROOF ACCESS LADDER
- E-16 EXIST. ROOF DRAINAGE OVERFLOW SCUPPER - TYP.
- E-17 EXIST. AWNING W/ FLORESENT LIGHT EXTERIOR DOWN LIGHTING
- E-18 EXIST. LITE BUILDING MOBIL HALLMARK SIGNAGE (11.0 SQ. FT.)
- E-19 EXIST. 2" MTL. FLASHING
- E-20 EXIST. EMERGENCY FUEL SHUT OFF
- E-21 EXIST. EMERGENCY FUEL ALARM
- E-22 EXIST. AIR VENT- WHITE PLASTIC FINISH

### COLORS

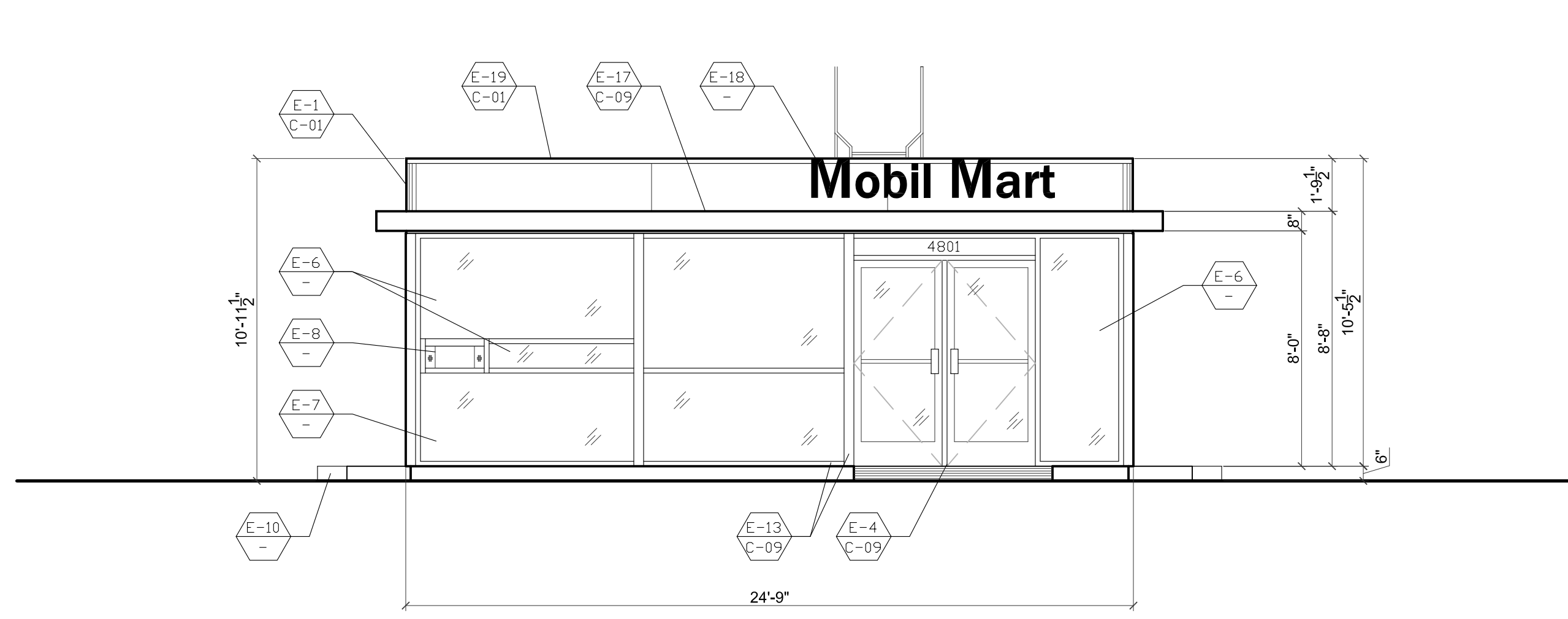
- C-01 (E) MOBIL WHITE ENAMEL- EGGSHELL WHITE (P8); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-02 (E) MOBIL BLUE (P23); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-03 (E) MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - SEMI-GLOSS PAINT
- C-04 (E) DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - SEMI-GLOSS PAINT
- C-05 (E) YELLOW ENAMEL
- C-06 (E) MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - ACRYLIC FLAT PAINT
- C-07 (E) DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - ACRYLIC FLAT PAINT
- C-08 (E) LIGHT BEIGE- DUNN EDWARDS: DRY DUNE (DE 6114) - ACRYLIC FLAT PAINT
- C-09 (E) BLACK ENAMEL
- C-10 (E) MOBIL GRAY (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-11 (E) MOBIL DARK GRAY- GUN METAL (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-12 (E) MOBIL RED: JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT

### SYMBOL

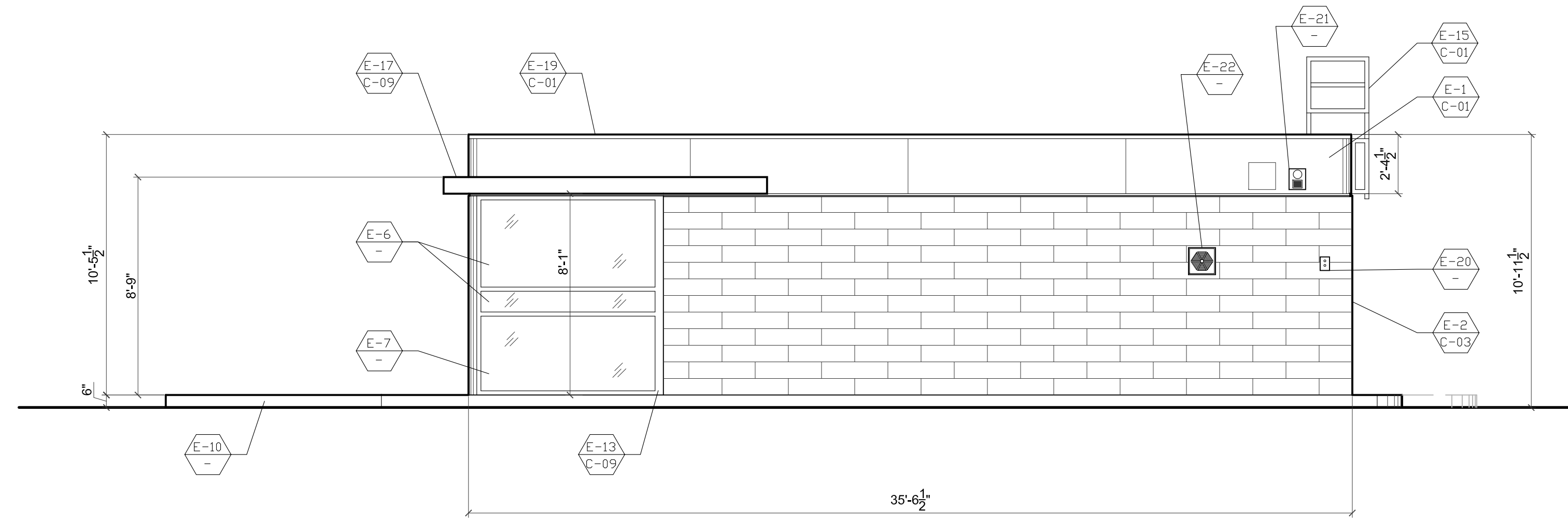
-  COLOR
-  SHT. NUMBER
- (E) EXISTING
- (N) NEW

### NOTE:

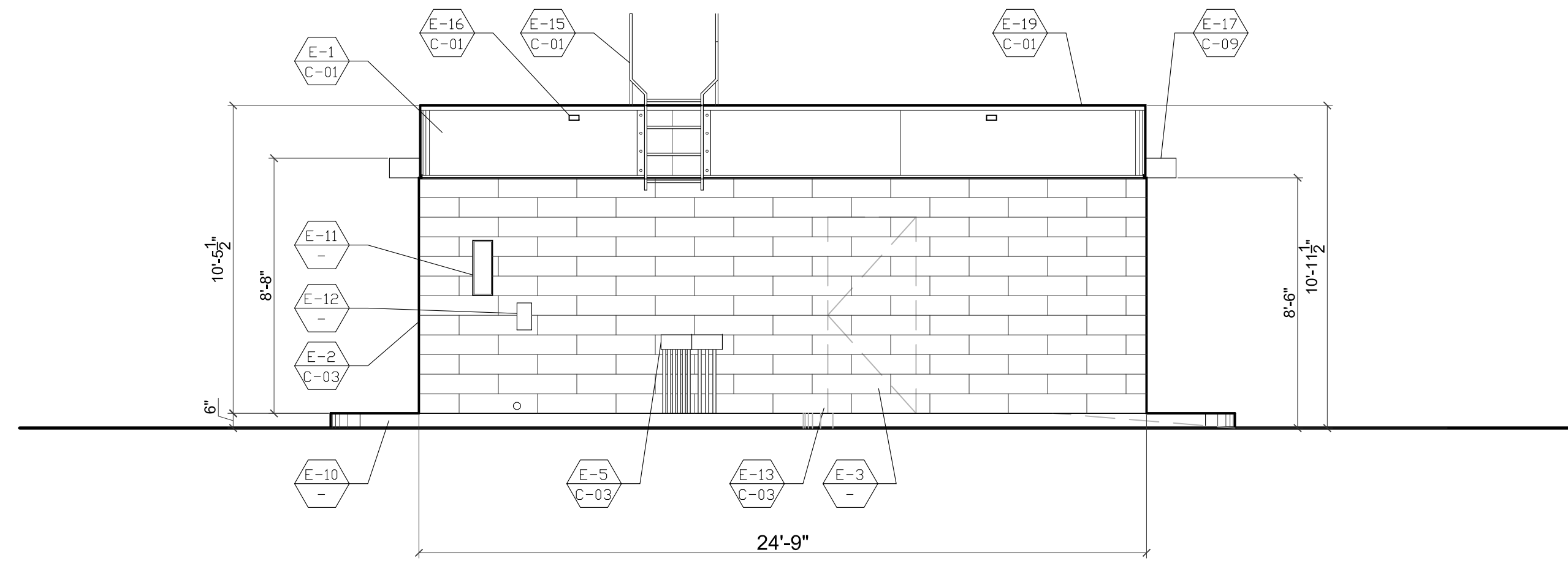
- EXIST. PAINTED MTL. PANELS WILL BE REPAINTED AT THE EXISTING C-STORE/SERVICE BUILDING.
- EXIST. BRICK VENEER TO BE REPLACED WITH NEW EXTERIOR TILE- SAV WOOD - MATCHING THE NEW CARWASH.
- NEW COLORS, MATERIALS, AND TEXTURES OF PROPOSED CARWASH BUILDING WILL MATCH AND COORDINATE WITH THE EXISTING CONVENIENCE STORE / SERVICE BUILDING.
- THE FUEL VAPOR CANISTER WILL BE RELOCATED AND ALL PIPING WILL BE PAINTED TO MATCH EXTERIOR OF THE CONVENIENCE STORE BUILDING.



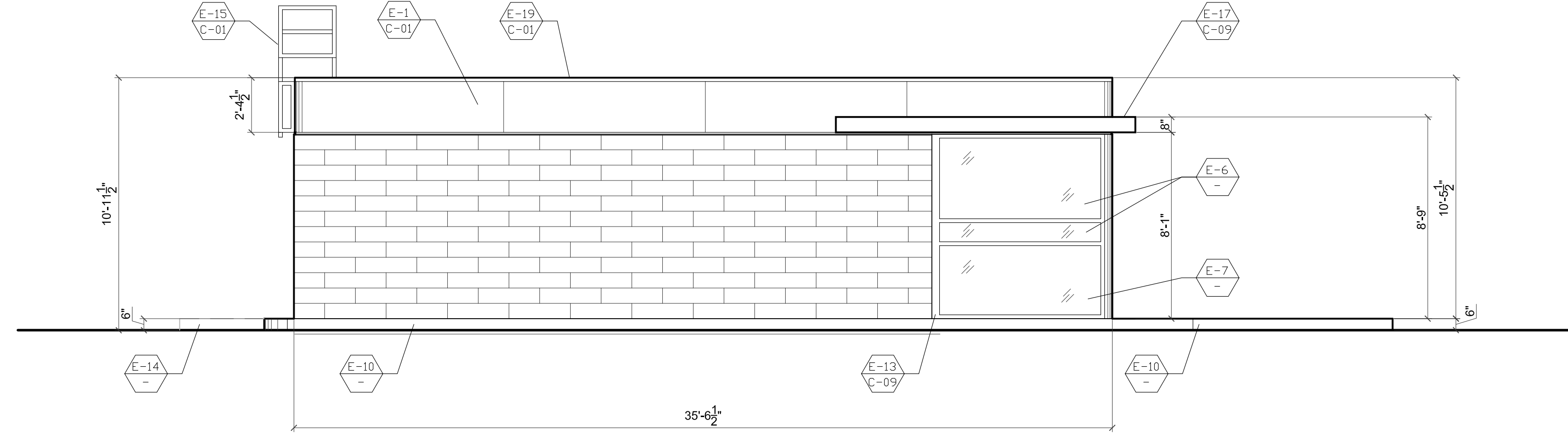
**1** EXISTING C-STORE REMODEL - EAST ELEVATION SCALE : 1/4" = 1'-0"



**2** EXISTING C-STORE REMODEL - NORTH ELEVATION SCALE : 1/4" = 1'-0"



**3** EXISTING C-STORE REMODEL - WEST ELEVATION SCALE : 1/4" = 1'-0"



**4** EXISTING C-STORE REMODEL - SOUTH ELEVATION SCALE : 1/4" = 1'-0"

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# LEGEND

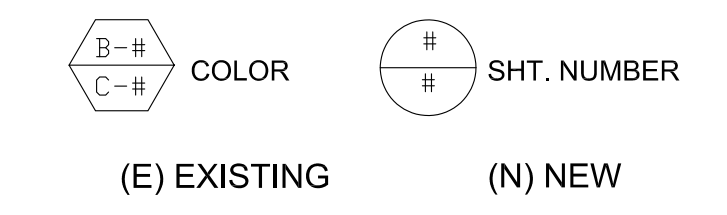
## MATERIAL

- B-01 7/8" STUCCO FINISH (SAND FINISH)
- B-02 3" MTL. FLASHING (METALLIC FINISH) -TYP.
- B-03 1" MTL. REVEALS PAINTED TO MATCH WALL - TYP.
- B-04 STONE VENEER (MOJAVE COUNTRY LEDGESTONE: CSV-20042) -LIGHT BEIGE COLOR
- B-05 EXTERIOR TILE - LIGHT WOOD GRAIN FINISH - ARIZONA TILE: SAV WOOD- MIELE (8 X 32)
- B-06 10' X 10' MTL. CARWASH BAY ROLL UP DOOR W/ 18" HIGH MTL. KICKPLATE - (GREY/METALLIC FINISH)
- B-07 TEMPERED GLASS OR PLEXIGLASS FOR ROLL UP DOOR
- B-08 MTL. 3' X 7' MTL. DOOR W/ 2" MTL. DOOR TRIM
- B-09 1/4" COMMERCIAL GLAZING - TEMP.
- B-10 NEW 6" CONC. CURB
- B-11 CARWASH WALL SIGNAGES - LITE CHANNEL LETTERS -DONE BY OTHERS (SIGN CO.) THE LETTERS FOR THE SIGNAGE IS FOR PLANNING PURPOSE AND REFERENCE ONLY.
- B-12 CARWASH ENTRANCE SIGN
- B-13 CARWASH EXIT SIGN
- B-14 CARWASH SIGNAGE BAND
- B-15 GENERAL CARWASH INFO. SIGNAGE - TYP.
- B-16 NEW DOWN LIGHTING LED WALL PACK - LITHONIA - BLACK
- B-17 EXIST. 6' HIGH BOUNDARY - 8" THK. CMU WALL
- B-18 NEW MTL. AWNING / WALL CANOPY (BLACK FINISH TO MATCH EXIST. C-STORE) W/ 4" DIA. ULTRA RT 4 LED W/ NICKEL TRIM FINISH - DOWN LIGHTING @ 20" O.C.
- B-19 NEW PHANTOM LSI WALL LED SCONCE - SOFT GLOW LIGHT W/ MITERED WHITE SILK SCREENED GLASS & BRONZE POWDER COATED ALUMINUM CONSTRUCTION
- B-20 MTL. MULLIONS/TRIM (BLACK FINISH TO MATCH EXIST. C-STORE)
- B-21 NEW MTL. FACIA CLADDING (GREY/METALLIC FINISH) - UNDER TOWER OVER HANG W/ 4" DIA. ULTRA RT 4 LED W/ NICKEL TRIM FINISH - DOWN LIGHTING @ 3'-0" O.C.
- B-22 NEW CARWASH SIGNAGE. MTL. FINISH BACK PANEL W/ CHANNEL LETTERS LITE FROM BEHIND.

## COLORS

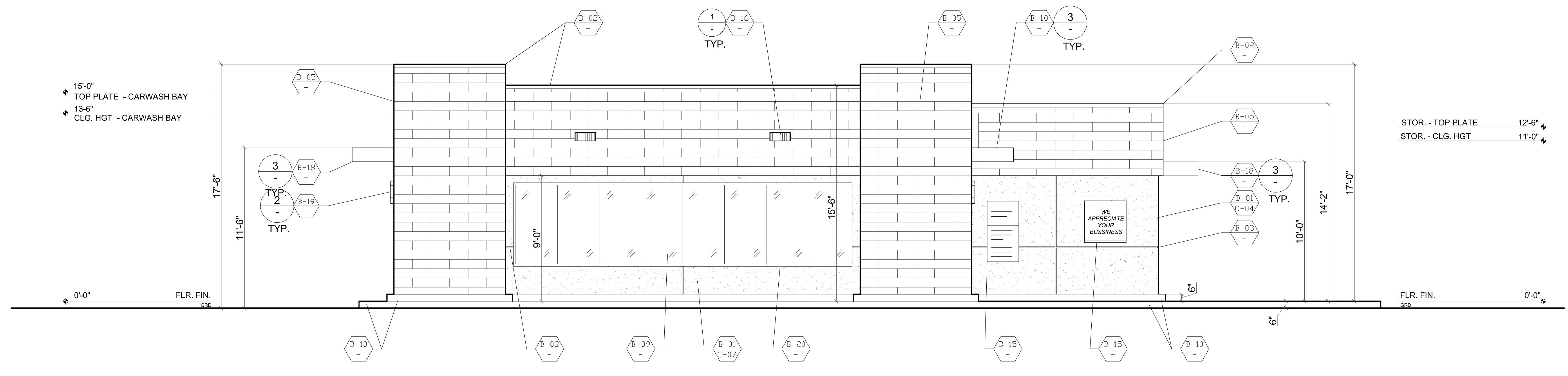
- C-01 (E) MOBIL WHITE ENAMEL- EGGSHELL WHITE (P8); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-02 (E) MOBIL BLUE (P23); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-03 (E) MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - SEMI-GLOSS PAINT
- C-04 (E) DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - SEMI-GLOSS PAINT
- C-05 (E) YELLOW ENAMEL
- C-06 (E) MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - ACRYLIC FLAT PAINT
- C-07 (E) DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - ACRYLIC FLAT PAINT
- C-08 (E) LIGHT BEIGE- DUNN EDWARDS: DRY DUNE (DE 6114) - ACRYLIC FLAT PAINT
- C-09 (E) BLACK ENAMEL
- C-10 (E) MOBIL GRAY (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-11 (E) MOBIL DARK GRAY- GUN METAL (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-12 (E) MOBIL RED; JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT

## SYMBOL



## NOTE:

- EXIST. PAINTED MTL. PANELS WILL BE REPAINTED AT THE EXISTING C-STORE/SERVICE BUILDING.
- EXIST. BRICK VENEER TO BE REPLACED WITH NEW EXTERIOR TILE- SAV WOOD - MATCHING THE NEW CARWASH.
- NEW COLORS, MATERIALS, AND TEXTURES OF PROPOSED CARWASH BUILDING WILL MATCH AND COORDINATE WITH THE EXISTING CONVENIENCE STORE / SERVICE BUILDING.
- THE FUEL VAPOR CANISTER WILL BE RELOCATED AND ALL PIPING WILL BE PAINTED TO MATCH EXTERIOR OF THE CONVENIENCE STORE BUILDING.



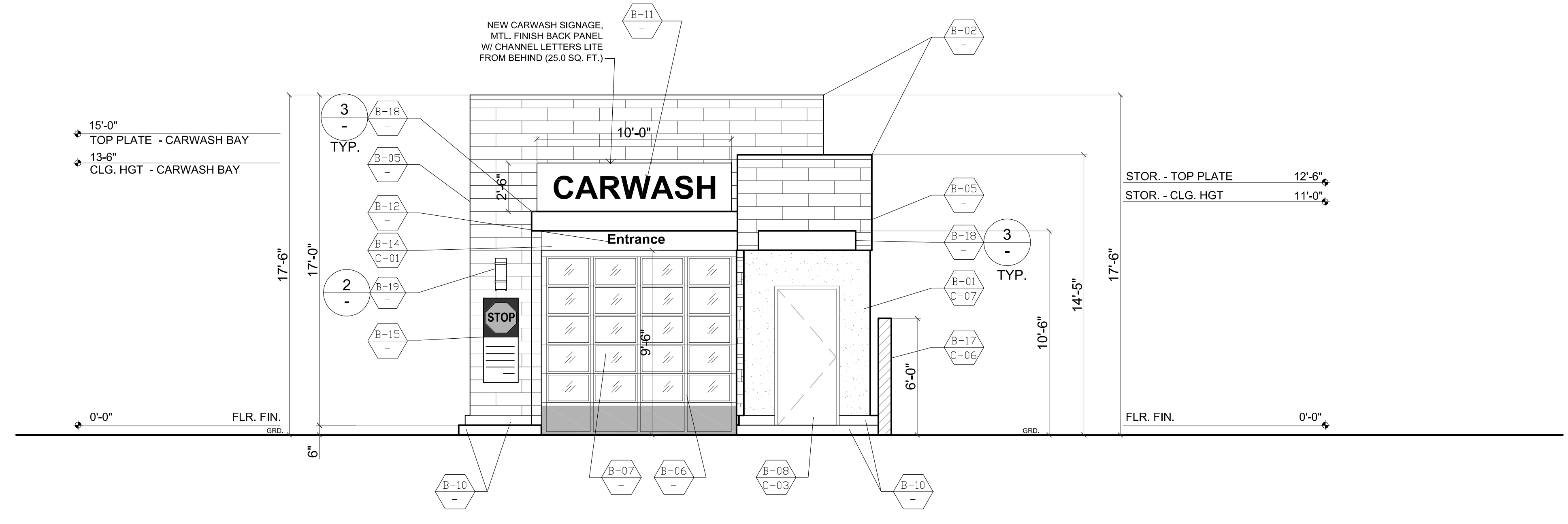
**5** PROPOSED CARWASH / EAST ELEVATION - VER. 2

SCALE : 1/4" = 1'-0"

EXHIBIT "A"

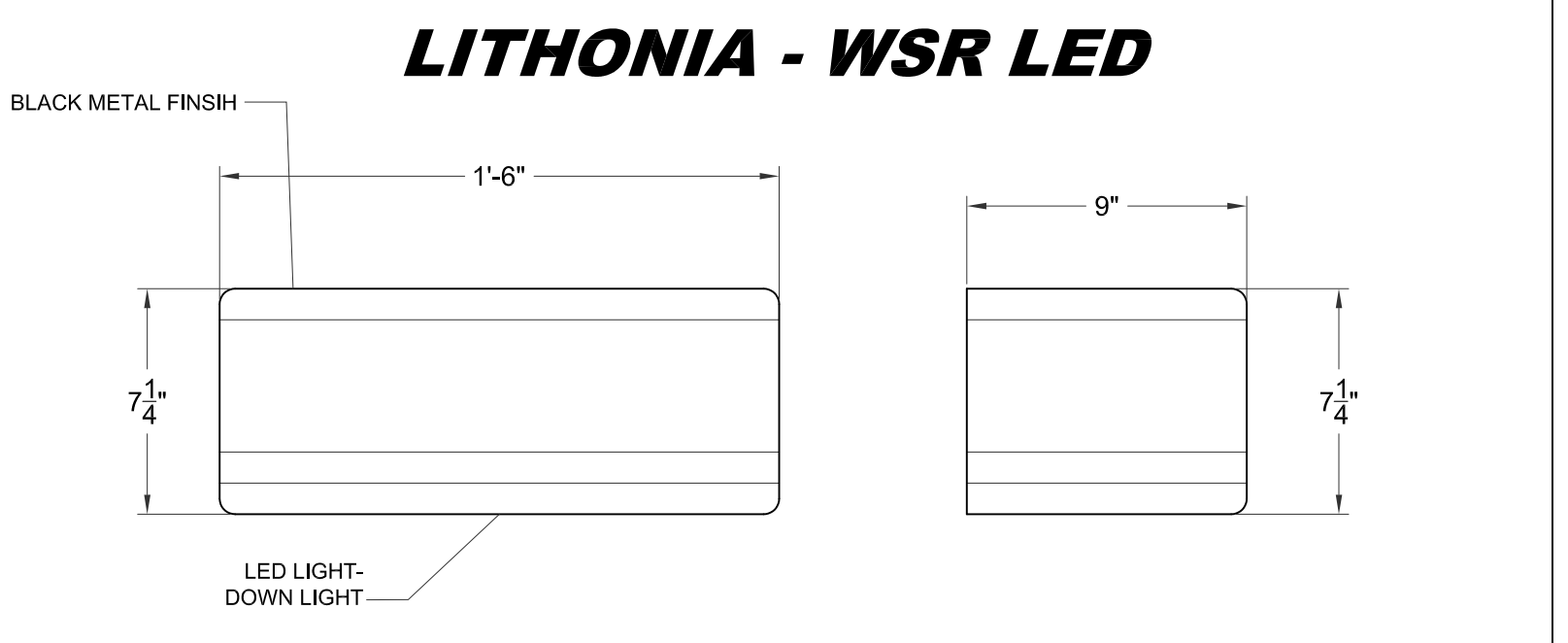
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Case No. APCSV-2019-1481-SPE-SPP-CU-ZV

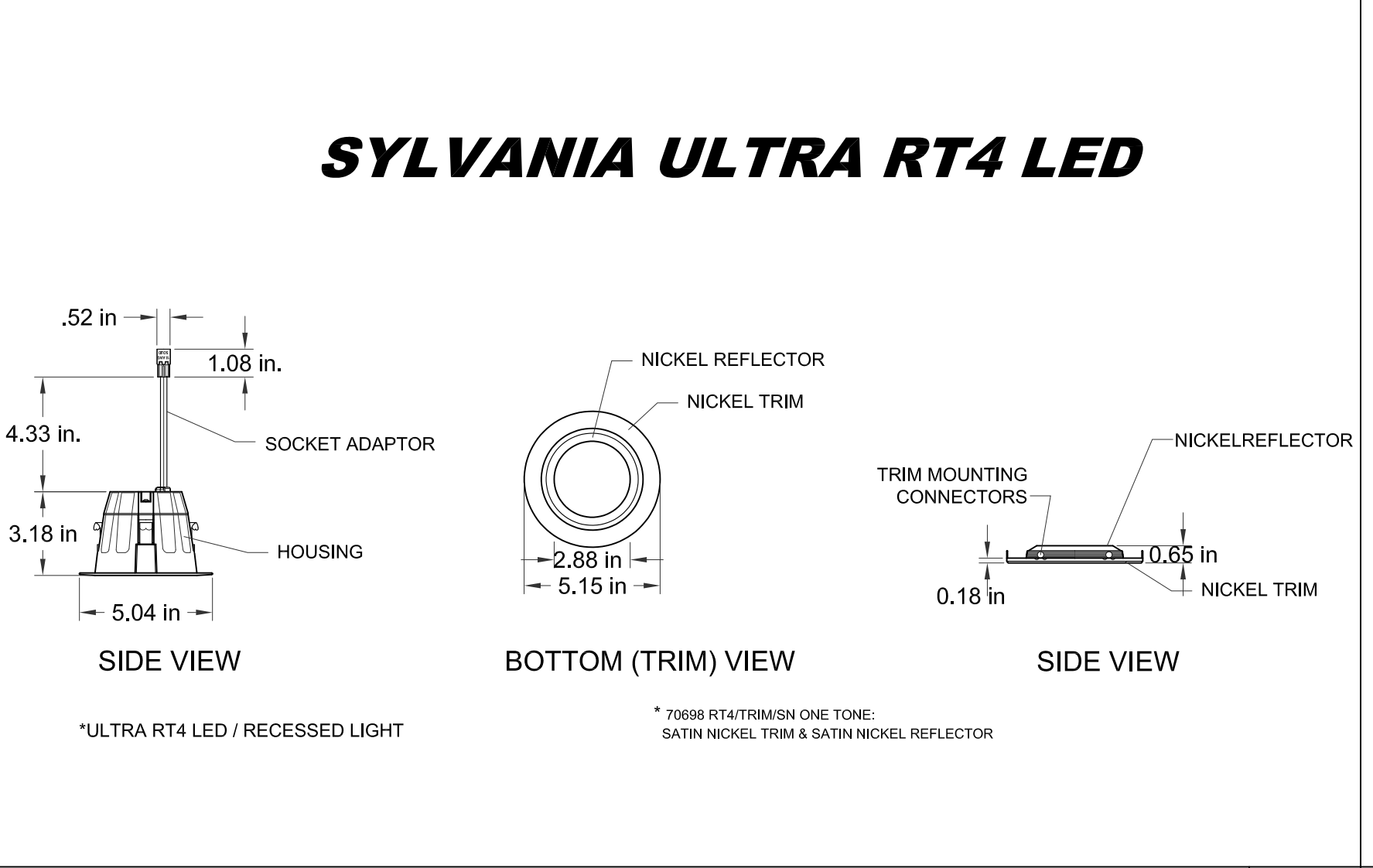


**6** PROPOSED CARWASH / NORTH ELEVATION - VER. 2

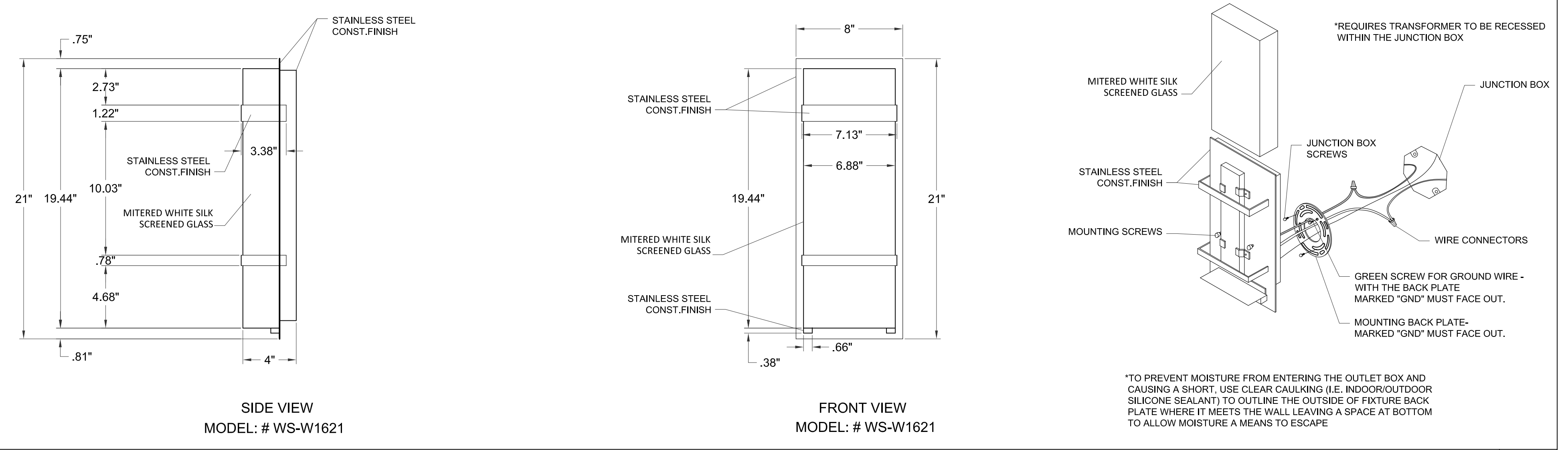
SCALE : 1/4" = 1'-0"



TYP. PIC. WALL PACK SCALE: NO SCALE **1**



EXTERIOR RECESSED LIGHT - TYP. SCALE: NO SCALE **3**



WALL SCONCE - PHANTOM LED - TYP. SCALE: NO SCALE **2**

# LEGEND

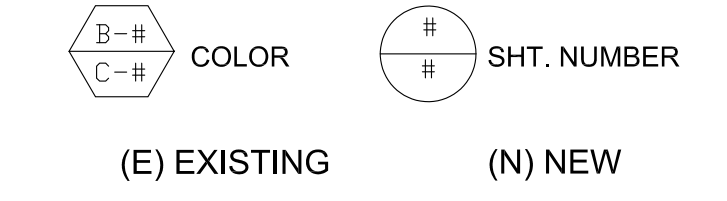
## MATERIAL

- B-01 7/8" STUCCO FINISH (SAND FINISH)
- B-02 3" MTL. FLASHING (METALLIC FINISH) -TYP.
- B-03 1" MTL. REVEALS PAINTED TO MATCH WALL - TYP.
- B-04 STONE VENEER (MOJAVE COUNTRY LEDGESTONE: CSV-20042) -LIGHT BEIGE COLOR
- B-05 EXTERIOR TILE - LIGHT WOOD GRAIN FINISH - ARIZONA TILE: SAV WOOD- MIELE (8 X 32)
- B-06 10' X 10' MTL. CARWASH BAY ROLL UP DOOR W/ 18" HIGH MTL. KICKPLATE - (GREY/METALLIC FINISH)
- B-07 TEMPERED GLASS OR PLEXIGLASS FOR ROLL UP DOOR
- B-08 MTL. 3' X 7' MTL. DOOR W/ 2" MTL. DOOR TRIM
- B-09 1/4" COMMERCIAL GLAZING - TEMP.
- B-10 NEW 6" CONC. CURB
- B-11 CARWASH WALL SIGNAGES - LITE CHANNEL LETTERS -DONE BY OTHERS (SIGN CO.) THE LETTERS FOR THE SIGNAGE IS FOR PLANNING PURPOSE AND REFERENCE ONLY.
- B-12 CARWASH ENTRANCE SIGN
- B-13 CARWASH EXIT SIGN
- B-14 CARWASH SIGNAGE BAND
- B-15 GENERAL CARWASH INFO. SIGNAGE - TYP.
- B-16 NEW DOWN LIGHTING LED WALL PACK - LITHONIA - BLACK
- B-17 EXIST. 6' HIGH BOUNDARY - 8" THK. CMU WALL
- B-18 NEW MTL. AWNING / WALL CANOPY (BLACK FINISH TO MATCH EXIST. C-STORE) W/ 4" DIA. ULTRA RT 4 LED W/ NICKEL TRIM FINISH - DOWN LIGHTING @ 20" O.C.
- B-19 NEW PHANTOM LSI WALL LED SCONCE - SOFT GLOW LIGHT W/ MITERED WHITE SILK SCREENED GLASS & BRONZE POWDER COATED ALUMINUM CONSTRUCTION
- B-20 MTL. MULLIONS/TRIM (BLACK FINISH TO MATCH EXIST. C-STORE)
- B-21 NEW MTL. FACIA CLADDING (GREY/METALLIC FINISH) - UNDER TOWER OVER HANG W/ 4" DIA. ULTRA RT 4 LED W/ NICKEL TRIM FINISH - DOWN LIGHTING @ 3'-0" O.C.
- B-22 NEW CARWASH SIGNAGE. MTL. FINISH BACK PANEL W/ CHANNEL LETTERS LITE FROM BEHIND.

## COLORS

- C-01 (E) MOBIL WHITE ENAMEL- EGGSHELL WHITE (P8); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-02 (E) MOBIL BLUE (P23); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-03 (E) MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - SEMI-GLOSS PAINT
- C-04 (E) DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - SEMI-GLOSS PAINT
- C-05 (E) YELLOW ENAMEL
- C-06 (E) MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - ACRYLIC FLAT PAINT
- C-07 (E) DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - ACRYLIC FLAT PAINT
- C-08 (E) LIGHT BEIGE- DUNN EDWARDS: DRY DUNE (DE 6114) - ACRYLIC FLAT PAINT
- C-09 (E) BLACK ENAMEL
- C-10 (E) MOBIL GRAY (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-11 (E) MOBIL DARK GRAY- GUN METAL (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-12 (E) MOBIL RED; JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT

## SYMBOL

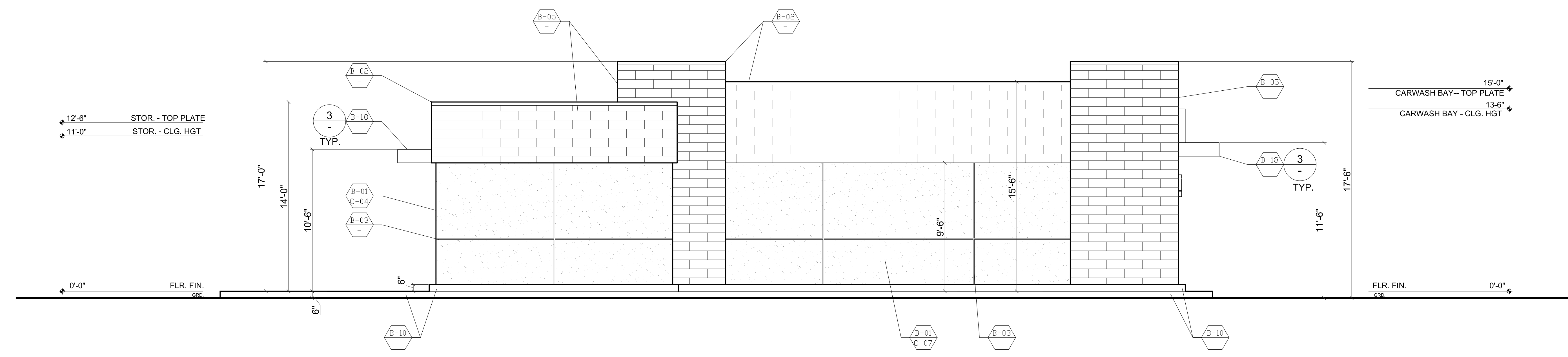


## NOTE:

- EXIST. PAINTED MTL. PANELS WILL BE REPAINTED AT THE EXISTING C-STORE/SERVICE BUILDING.
- EXIST. BRICK VENEER TO BE REPLACED WITH NEW EXTERIOR TILE- SAV WOOD - MATCHING THE NEW CARWASH.
- NEW COLORS, MATERIALS, AND TEXTURES OF PROPOSED CARWASH BUILDING WILL MATCH AND COORDINATE WITH THE EXISTING CONVENIENCE STORE / SERVICE BUILDING.
- THE FUEL VAPOR CANISTER WILL BE RELOCATED AND ALL PIPING WILL BE PAINTED TO MATCH EXTERIOR OF THE CONVENIENCE STORE BUILDING.

**NADER'S MOBIL GAS STATION REMODEL / CARWASH PROJECT**  
 4801 LAUREL CANYON BLVD., VALLEY VILLAGE, CA 91607

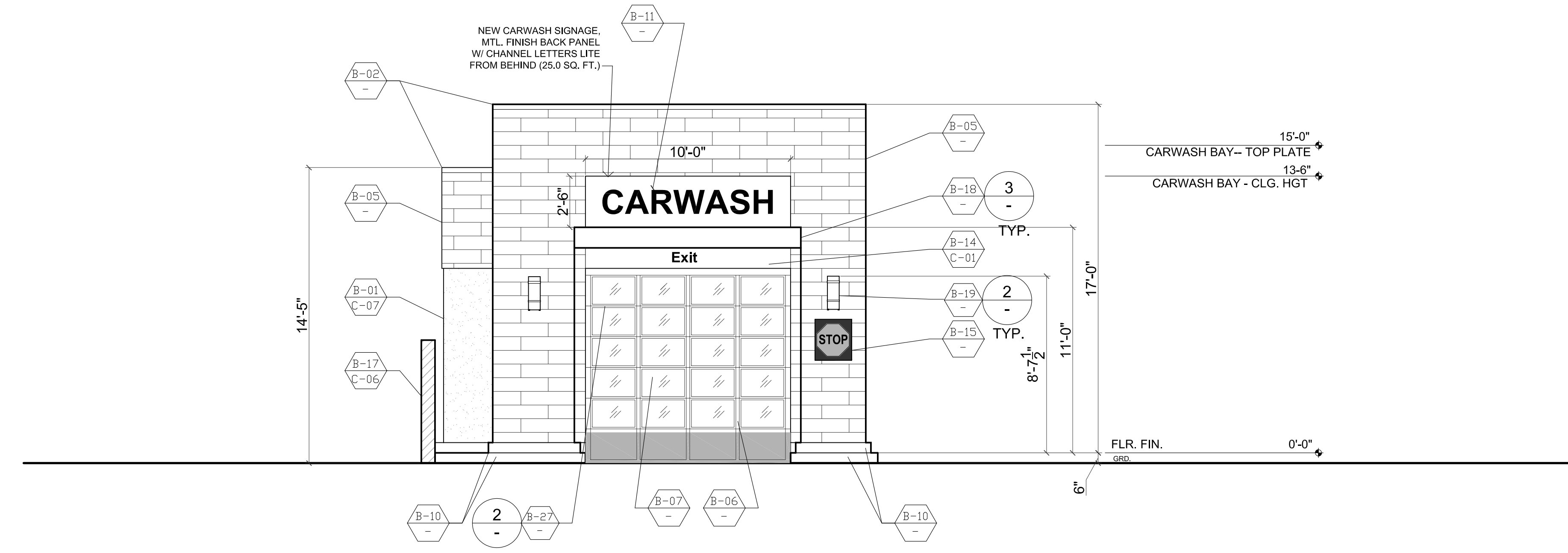
**MK Design**  
 535 W. ALLEN AVE., UNIT 23, SAN DIMAS, CA 91773



**7 PROPOSED CARWASH / WEST ELEVATION - VER. 2**

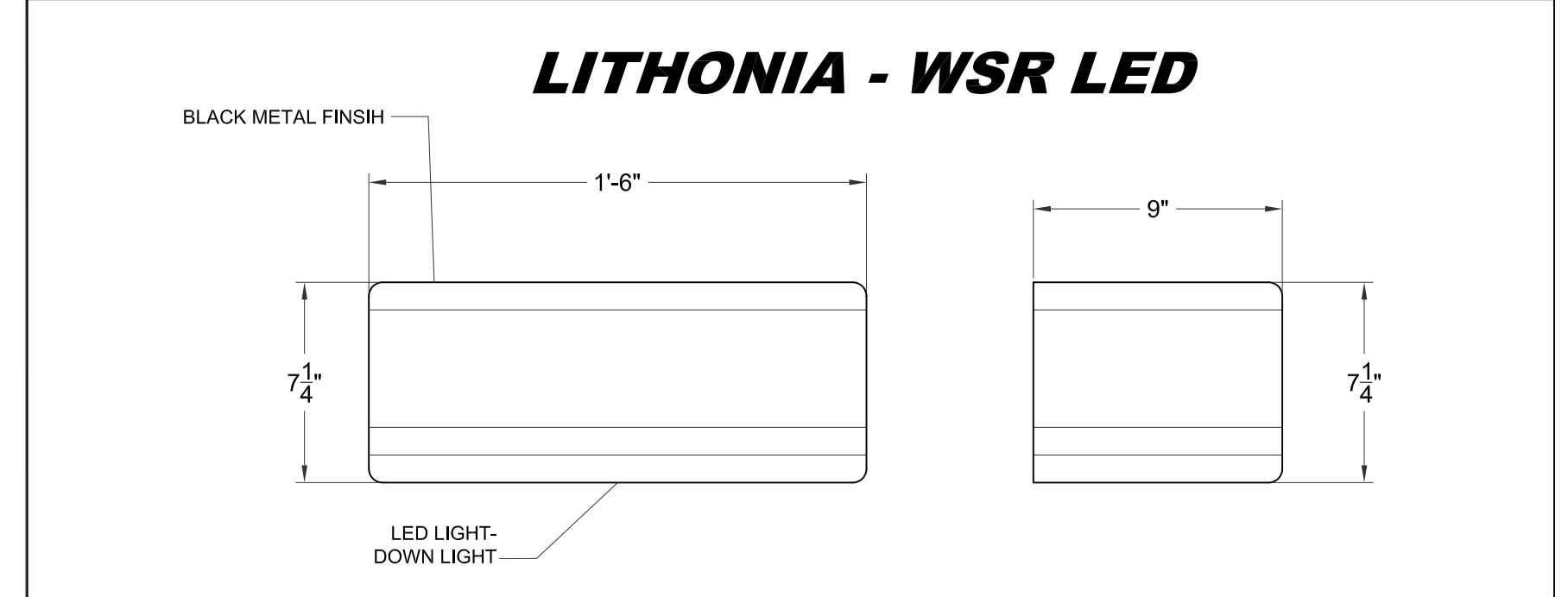
SCALE: 1/4" = 1'-0"

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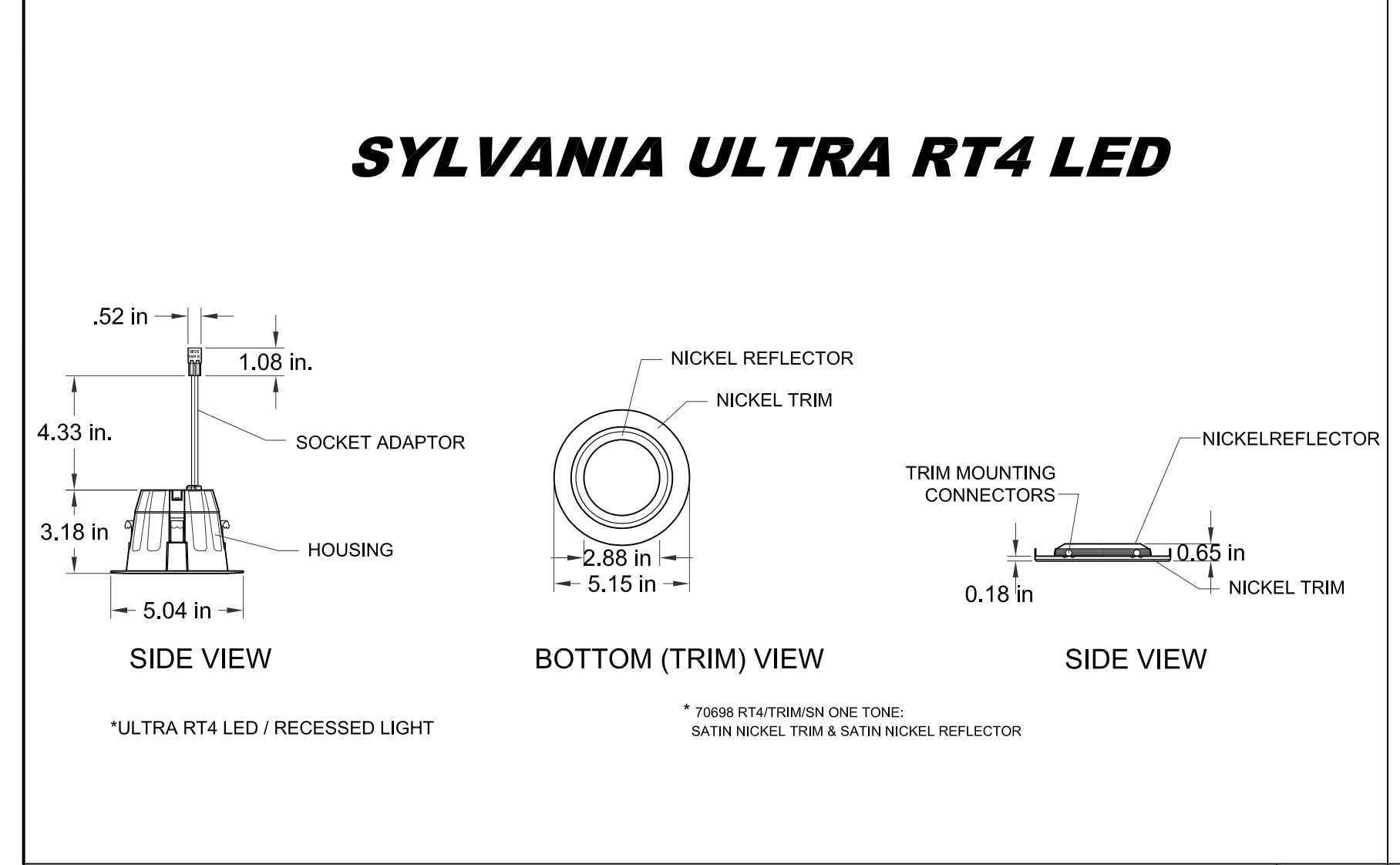


**8 PROPOSED CARWASH / SOUTH ELEVATION - VER. 2**

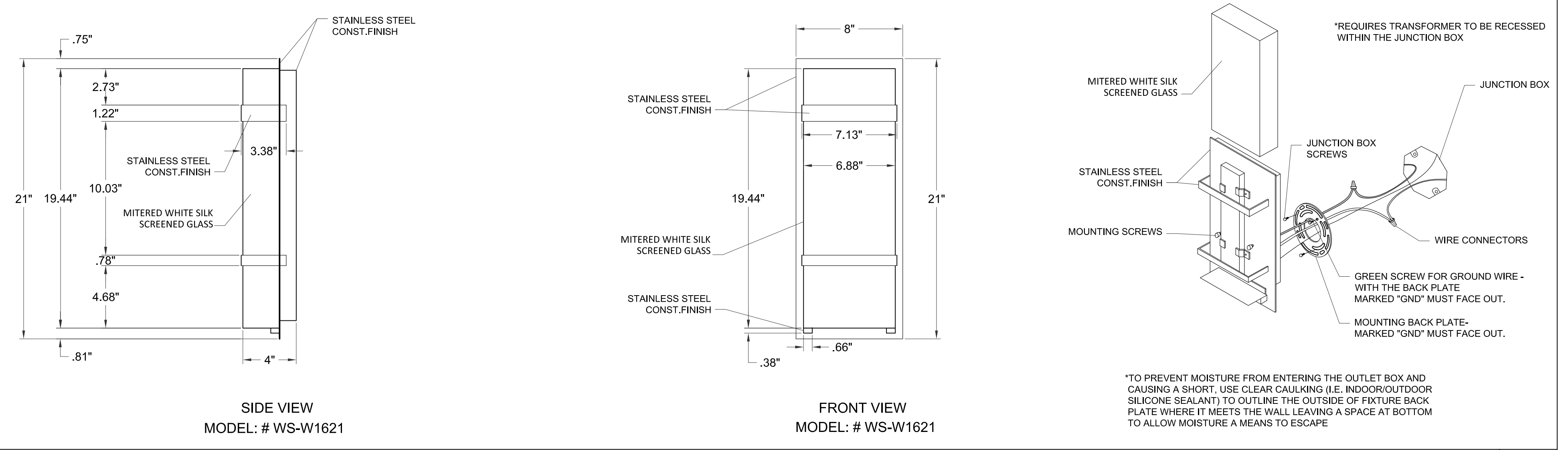
SCALE: 1/4" = 1'-0"



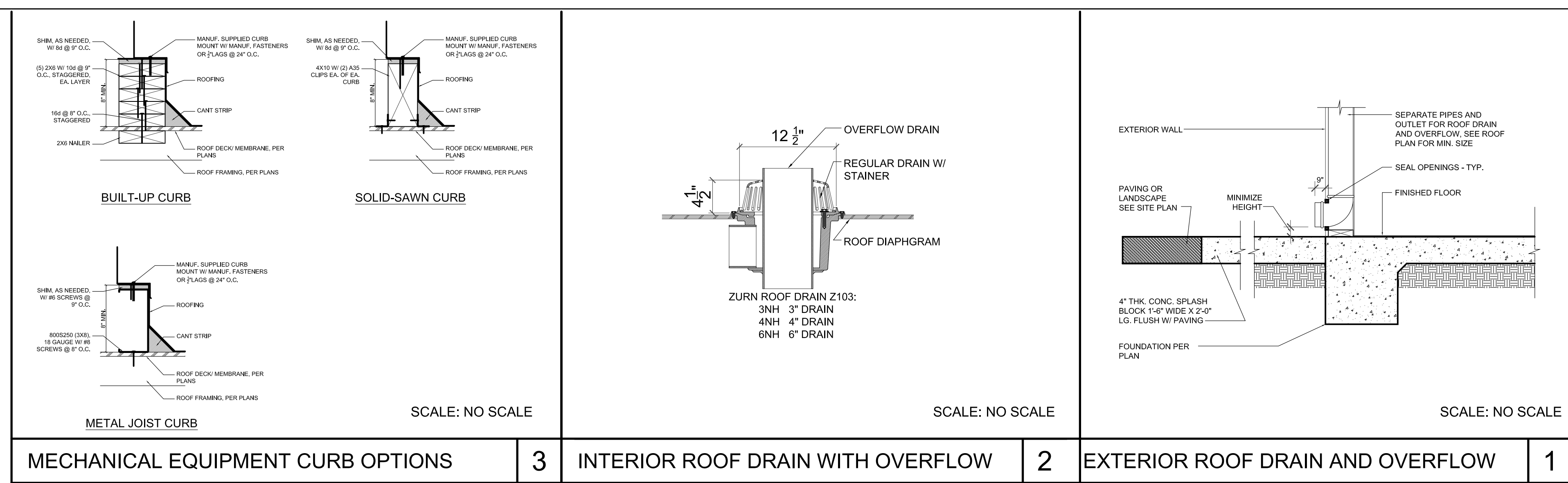
**TYP. PIC. WALL PACK** SCALE: NO SCALE **1**



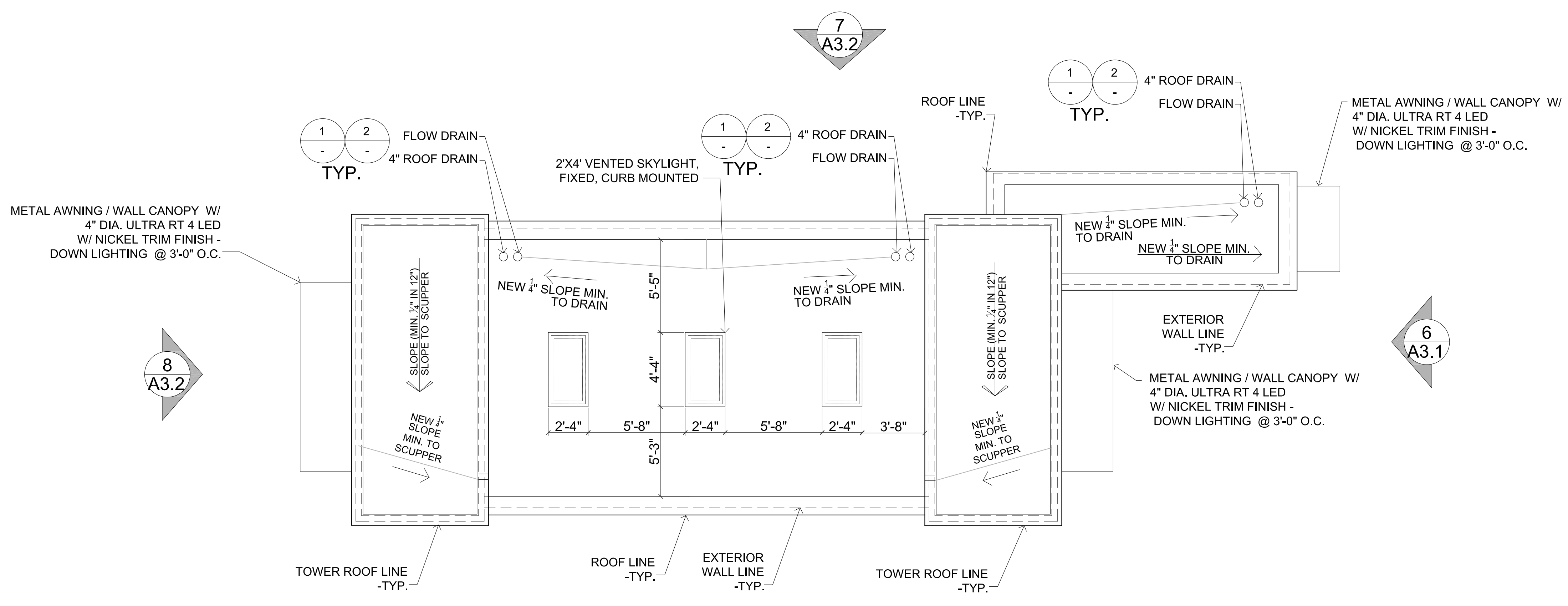
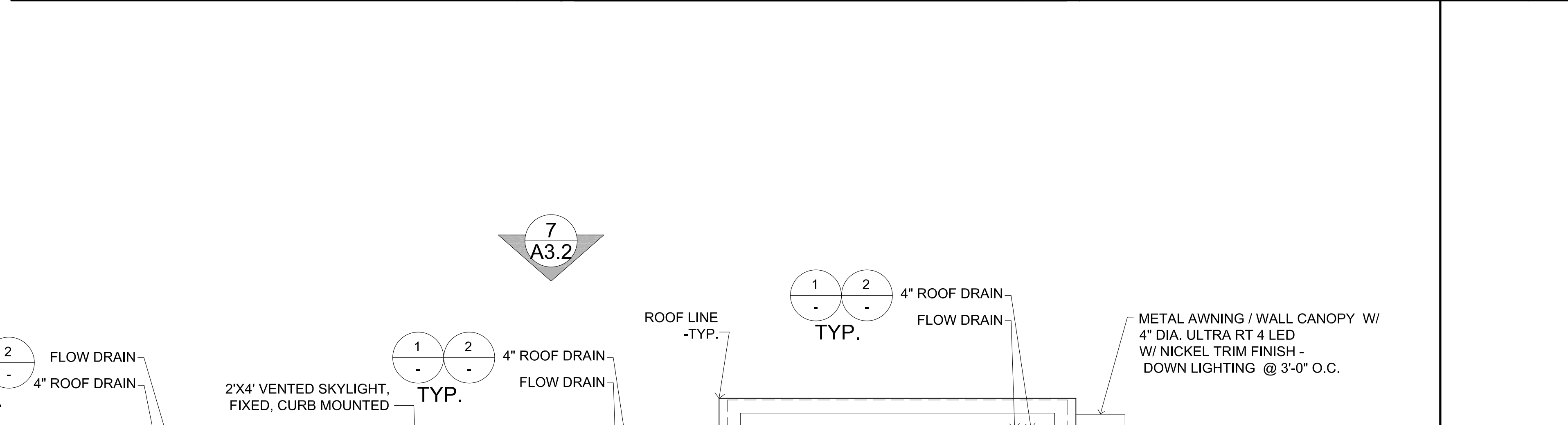
**EXTERIOR RECESSED LIGHT - TYP.** SCALE: NO SCALE **3**



**WALL SCONCE - PHANTOM LED - TYP.** SCALE: NO SCALE **2**

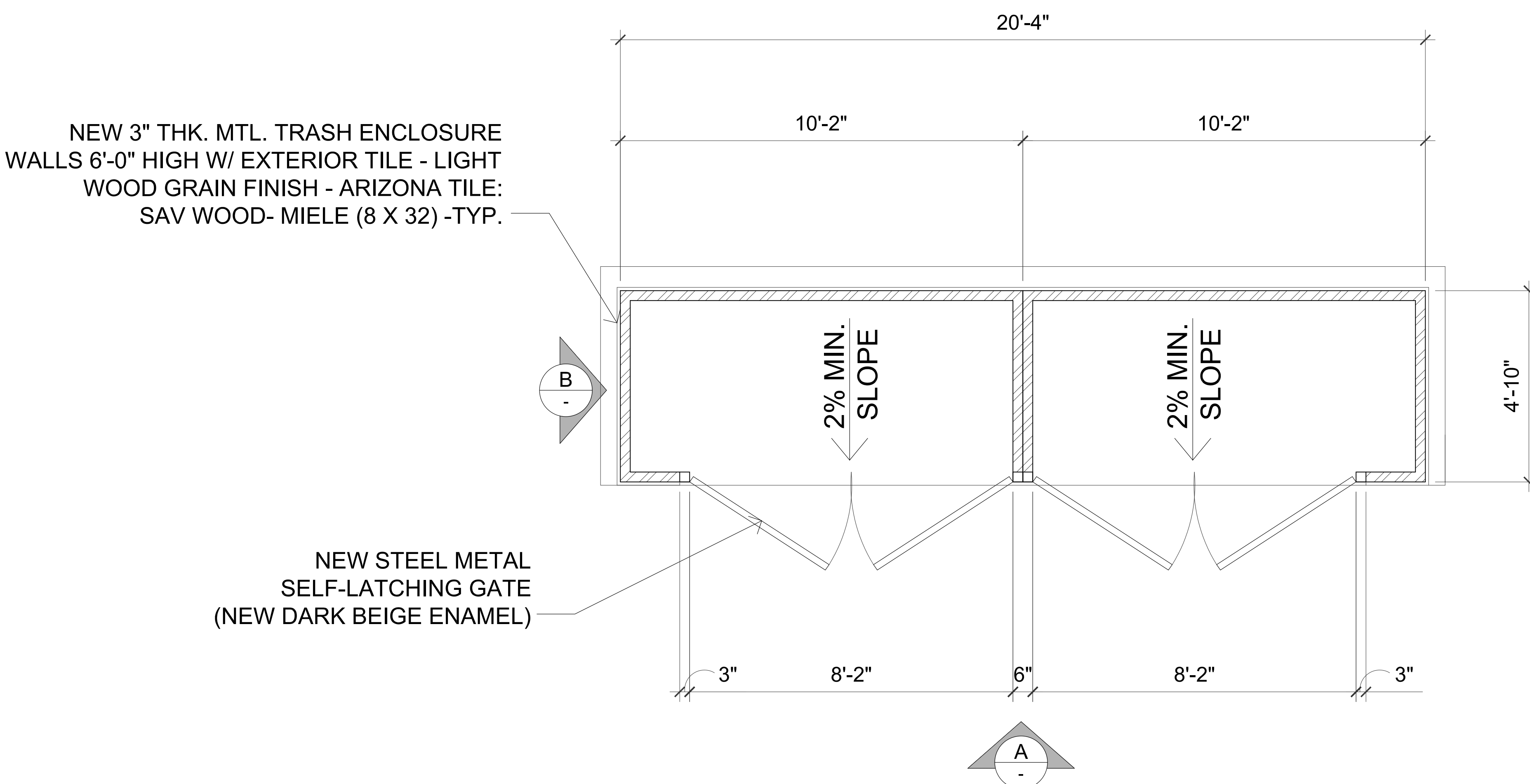


- ### ROOF PLAN GENERAL NOTES
- NEW ROOF (FLAT ROOF): 'CLASS B' MODIFIED BITUMEN MEMBRANE ROOF SYSTEM - ICC# ESR-1274  
SUBSTRATE (MIN): 1/2" PLYWD.  
BASE SHEET (MIN): TYPE G2 #75 BASE SHEET, HOT MOPPED IN PLACE OR MECHANICALLY FASTENED  
PLY SHEET (MIN): TYPE G1 4 PLY 15# MEMBRANE, HOT MOPPED IN PLACE OR ASPHALT ADHESIVE BETWEEN LAYERS  
MEMBRANE (MIN): MODIFIED BITUMEN SHEET  
SURFACING COATING: GRAVEL, 400 LB/SQ MAX LAID IN HOT ROOFING ASPHALT  
(MAY BE SUBSTITUTED BY ANY METHOD LISTED IN ESR-1274)
  - NEW FLASH ROOFING MEMBRANE PENETRATIONS MUST USE PRE-FABRICATED BOOTS, FITTED COVERINGS, AND/OR OTHER ACCESSORIES AS REQUIRED BY ROOFING SYSTEM MANUFACTURER.
  - ROOF DRAINAGE IS NOT PERMITTED TO FLOW OVER PUBLIC PROPERTY.
  - NEW CONDITIONS MUST DIVERT ALL WATER TO ROOF DRAINS.
  - VERIFY THAT ALL ROOF AREAS HAVE 1/4" FT. MIN. SLOPE, INCLUDING CRICKETS.
  - IF ROOF ACCESS IS THRU SUSPENDED CEILING AND MUST NOT BE IMPEDED.
  - ALL ROOF TOP EQUIPMENT SHALL BE SCREENED FROM VIEW
  - ROOF ACCESS MUST HAVE A LOCKING MECHANISM THAT IS OPERABLE FROM THE INSIDE WITHOUT THE NEED OF A KEY.
  - ROOFING SURFACE MUST EXTEND UP THE REAR FACE OF PARAPET WALLS OR BE PROVIDED WITH ROOF-WALL FLASHING AS SHOWN IN A6.4.
  - PROVIDE 3" MIN. CANT STRIPS AT ALL ROOF PARAPET/WALL TRANSITIONS UNLESS NOTED OTHERWISE BY ROOF FINISH MANUFACTURER.
  - DRAINAGE IS NOT PERMITTED TO FLOW OVER WALKWAYS.
  - SECONDARY ROOF DRAINAGE SYSTEM (OVERFLOW) SHALL BE A SEPARATE SYSTEM OF PIPING INDEPENDENT OF THE PRIMARY ROOF DRAINAGE SYSTEM.
  - ROOF DRAINS TO BE CONSTRUCTED OF CAST-IRON, COPPER OR COPPER-ALLOY, OR PLASTIC.
  - SURROUNDING TREES TO BE TRIMMED BACK AS TO NOT SHED LEAVES AND DEBRIS ONTO ROOF.



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**PROPOSED NEW CARWASH - ROOF PLAN**  
SCALE: 1/4" = 1'-0"



**NEW TRASH ENCLOSURE PLAN**  
SCALE: 1/2" = 1'-0"

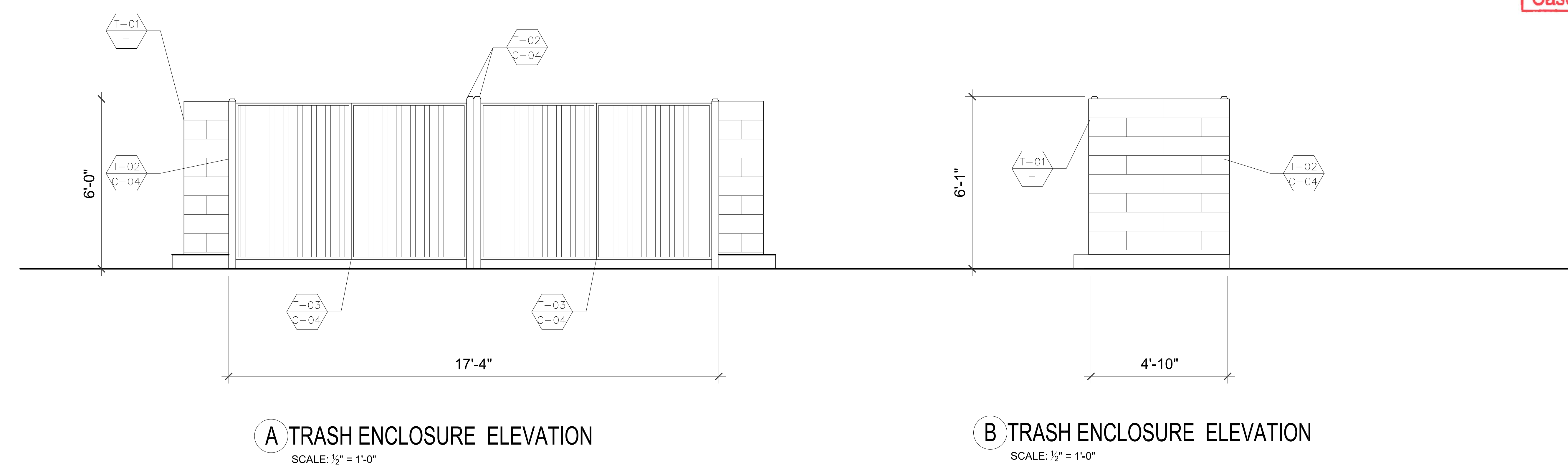
- MATERIAL**
- T-01 3" THK. MTL. TRASH ENCLOSURE WALL W/ EXTERIOR TILE - LIGHT WOOD GRAIN FINISH - ARIZONA TILE: SAV WOOD- MIELE (8 X 32) -TYP.
  - T-02 MTL. GATE POST - 3" X 3" POST. - TYP.
  - T-03 LOCKABLE SELF LATCHING STEEL CORRUGATED METAL GATE

- COLORS**
- C-04 DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - SEMI-GLOSS PAINT

- SYMBOL**
- B-#  
C-# COLOR    
 #  
# SHT. NUMBER  
 (E) EXISTING     (N) NEW

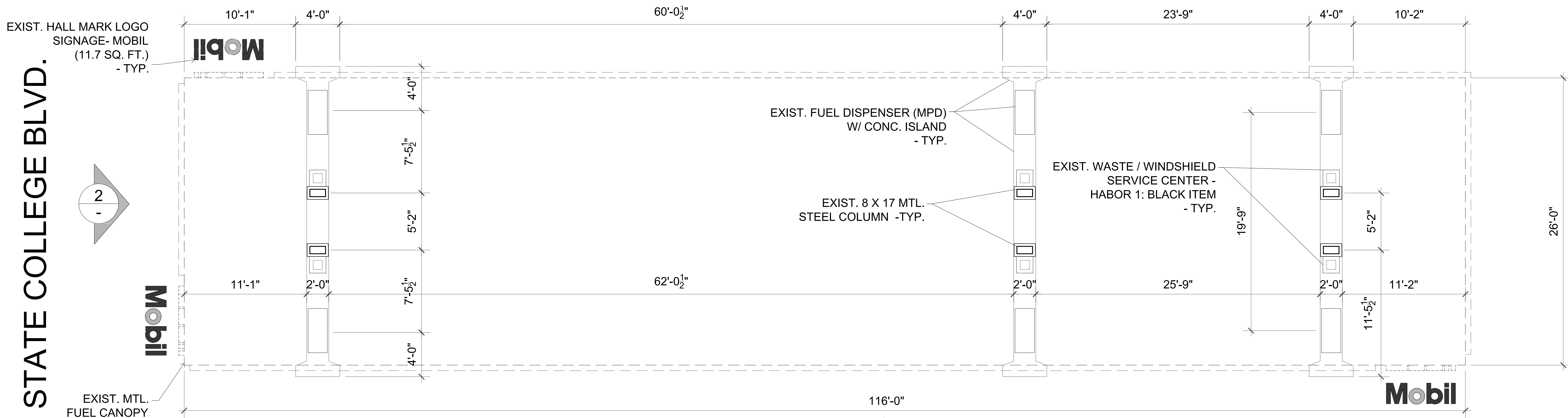
**NOTE:**  
PAINTED MTL. PANELS WILL BE PAINTED TO MATCH MOBIL / EXISTING C-STORE/SERVICE BUILDING.

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**A TRASH ENCLOSURE ELEVATION**  
SCALE: 1/2" = 1'-0"

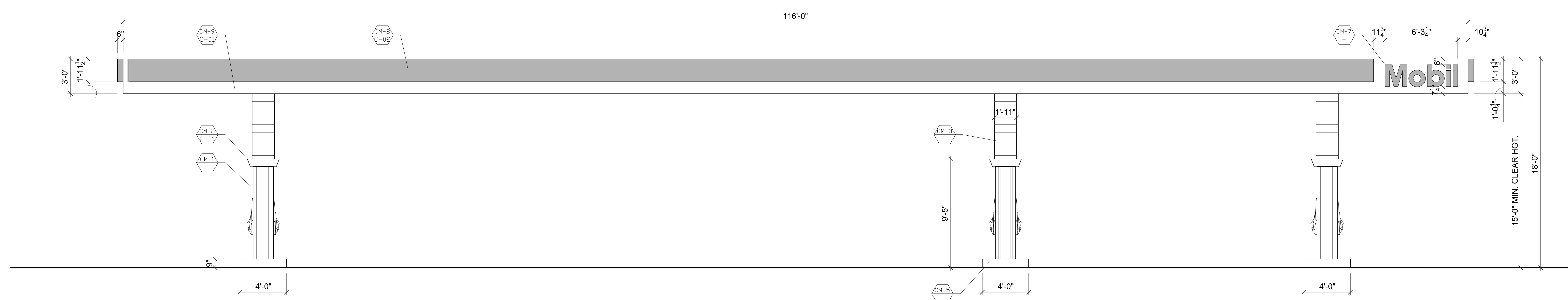
**B TRASH ENCLOSURE ELEVATION**  
SCALE: 1/2" = 1'-0"



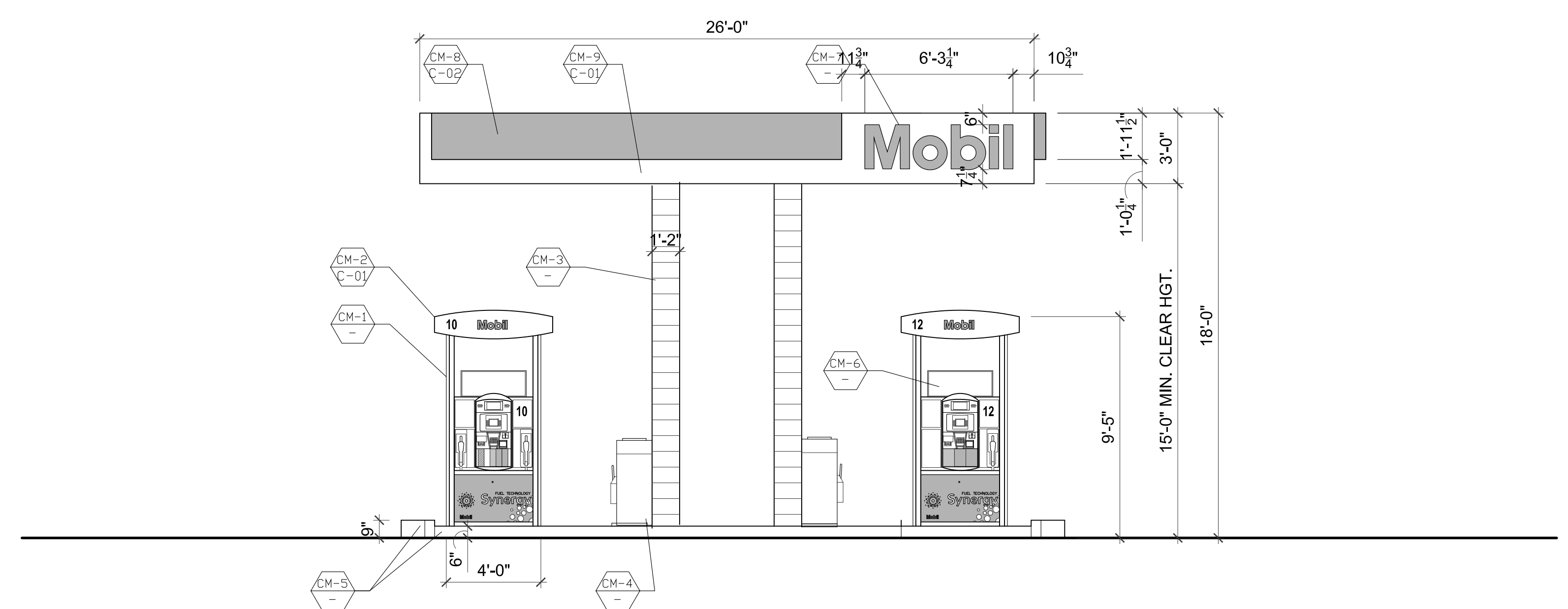
N. STATE COLLEGE BLVD.

E. CHAPMAN AVE.

**EXIST. CANOPY PLAN**  
SCALE: 1/4" = 1'-0"



**1 EXIST. CANOPY EAST ELEVATION**  
SCALE: 1/4" = 1'-0"



**2 EXIST. CANOPY SOUTH ELEVATION**  
SCALE: 1/4" = 1'-0"

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**LEGEND**

**MATERIAL**

- CM-1 EXIST. FUEL DISPENSER W/ MOBIL LOGO - TYP.
- CM-2 EXIST. MTL. VALANCE WITH BRANDED MOBIL- TYP.
- CM-3 EXIST. 8'X17' MTL. COLUMN W/ NEW EXTERIOR TILE - LIGHT WOOD GRAIN FINISH - ARIZONA TILE; SAV WOOD-MIELE (TO MATCH CARWASH AND EXIST. C-STORE - TYP.
- CM-4 EXIST. WASTE / WINDSHIELD SERVICE CENTER - HAVOR 1: BLACK ITEM - TYP.
- CM-5 EXIST. ISLAND CONC. CURB, TYP.
- CM-6 EXIST. DISPENSER P.O.S. - SIGNAGE
- CM-7 EXIST. MOBIL HALLMARK BRAND SIGNAGE - ILLUMINATED (11.7 SQ. FT.). (ACTUAL SIGNAGE PERMITTING BY SIGN CO.)
- CM-8 EXIST. 3MM ACM MTL. FACIA 3D
- CM-9 NEW 3MM ACM MTL. FACIA

**SYMBOL**

- MATERIAL
- COLOR
- DETAIL NUMBER
- SHT. NUMBER

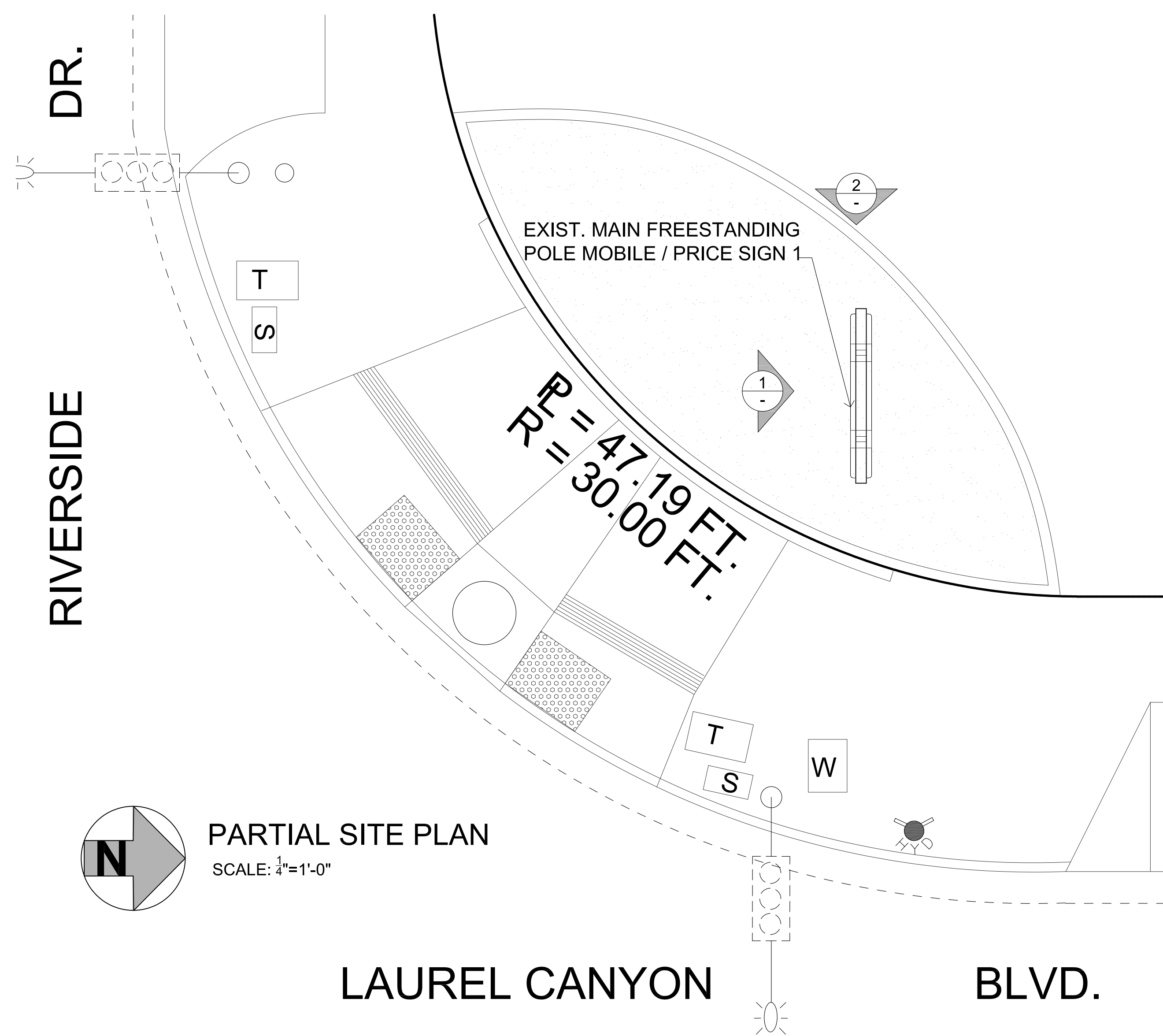
**COLORS**

- C-01 MOBIL WHITE ENAMEL- EGGSHELL WHITE (P8); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-02 MOBIL BLUE (P23); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-03 MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - EMAMEL
- C-04 DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - ENAMEL
- C-05 YELLOW ENAMEL
- C-06 MEDIUM BEIGE - DUNN EDWARDS: DOVER PLAINS (DE 6116) - ACRYLIC FLAT PAINT
- C-07 DARK BEIGE-DUNN EDWARDS: BURNS CAVE (DE 6098) - ACRYLIC FLAT PAINT
- C-08 LIGHT BEIGE- DUNN EDWARDS: DRY DUNE (DE 6114) - ACRYLIC FLAT PAINT
- C-09 BLACK ENAMEL
- C-10 MOBIL GRAY (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-11 MOBIL DARK GRAY- GUN METAL (P9); JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT
- C-12 MOBIL RED: JONES BLAIR-ACRYLITHANE SEMI-GLOSS PAINT

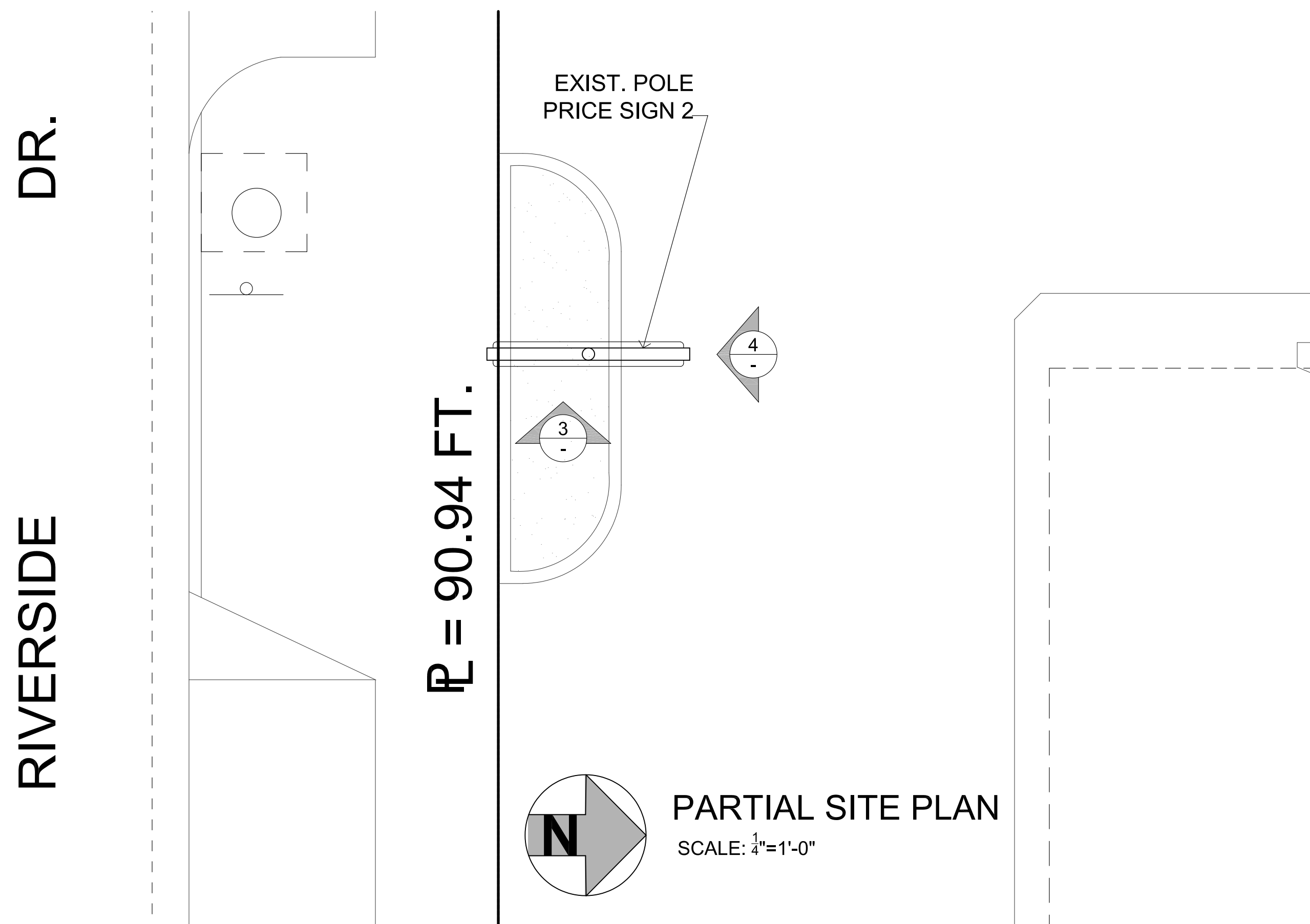

NADER'S MOBIL GAS STATION REMODEL / CARWASH PROJECT PROJ.  
4801 LAUREL CANYON BLVD., VALLEY VILLAGE, CA 91607

**MK Design**  
535 W. ALLEN AVE., UNIT 23, SAN DIMAS, CA 91773

Date: 2/23/2022  
Scale: 1/2" = 1'-0"  
Drawn: KK  
Job:  
Sheet: **C02**



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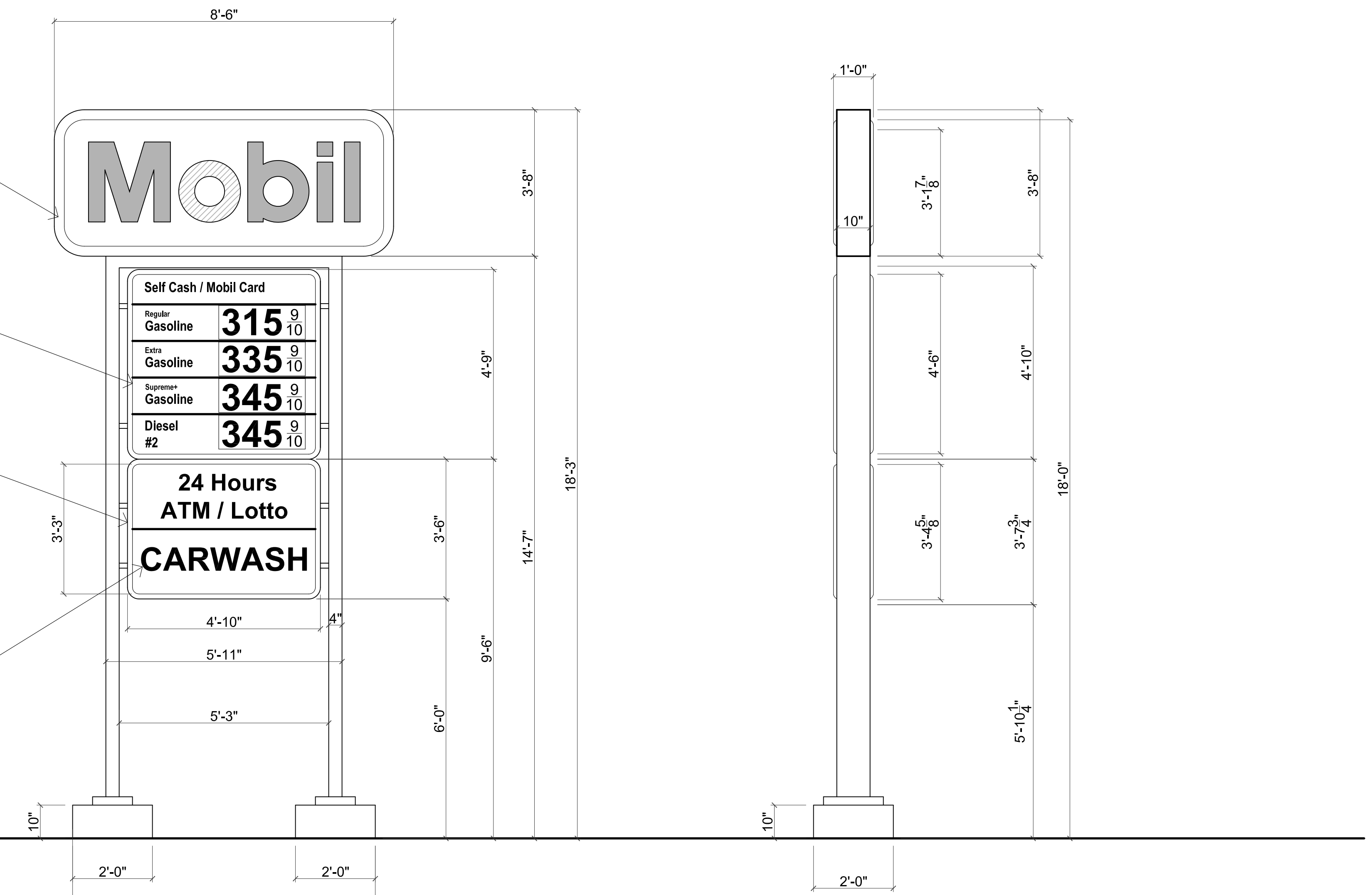


EXIST. MOBIL HALL MARK SIGNAGE (30 SQ. FT. EA. SIDE / 60 SQ. FT. TOTAL FOR BOTH SIDES)

EXIST. LED LITE PRICE SIGNAGE (22.8 SQ. FT. EA. SIDE / 35.6 SQ. FT. TOTAL FOR BOTH SIDES)

EXIST. AD SIGNAGE (16.8 SQ. FT. EA. SIDE / 33.6 SQ. FT. TOTAL FOR BOTH SIDES)

EXIST. AD SIGNAGE TO REPLACE AND PUT NEW CARWASH (NO NEW SQAURE FOOTAGE ADDED)

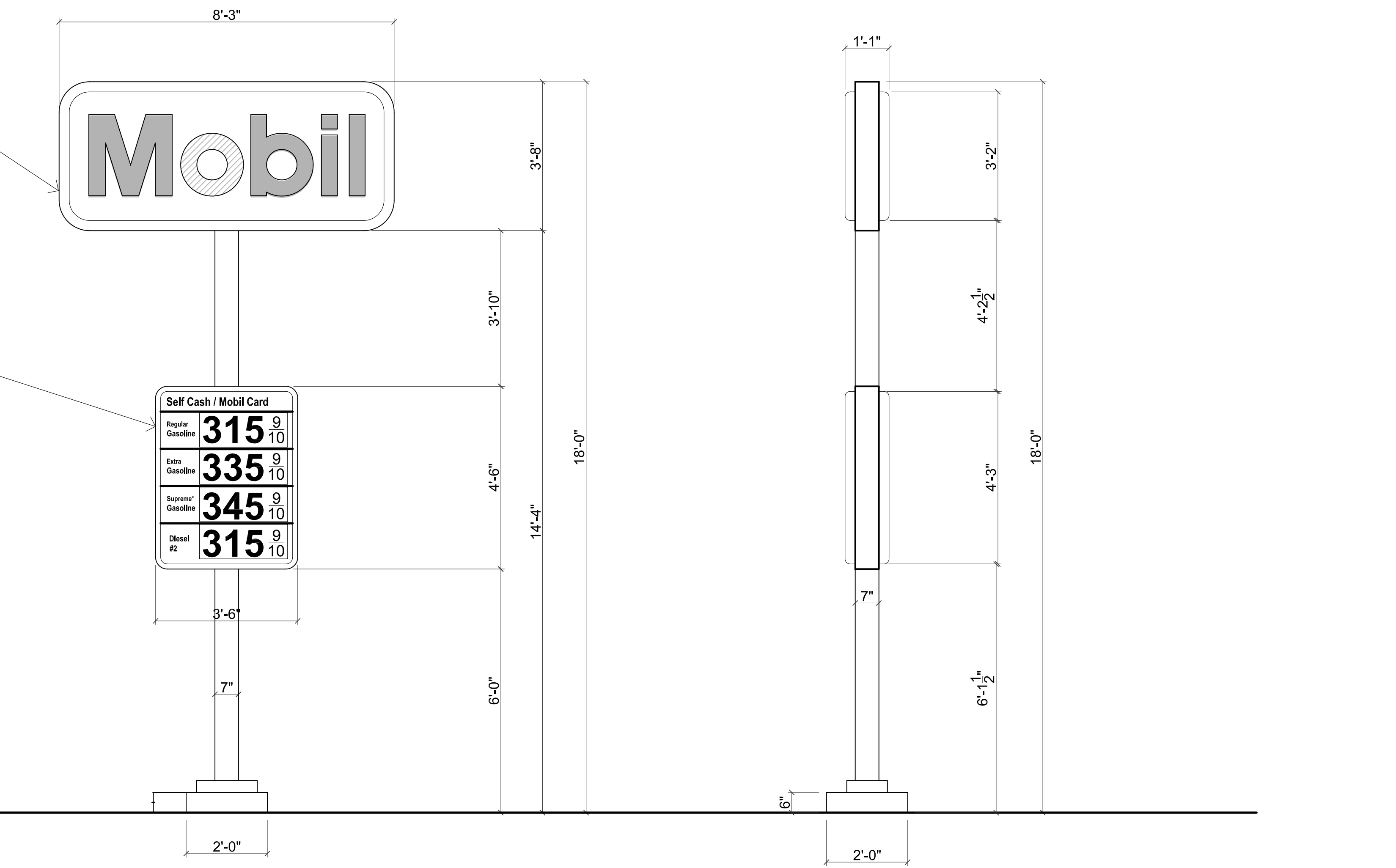


① EXIST. MAIN FREESTANDING POLE SIGN 1 W/ NEW SIGNAGE CHANGE  
 SCALE: 1/2"=1'-0"

② EXIST. MAIN FREESTANDING POLE SIGN 1 W/ NEW SIGNAGE CHANGE  
 SCALE: 1/2"=1'-0"

EXIST. MOBIL HALL MARK SIGNAGE (30 SQ. FT. EA. SIDE / 60 SQ. FT. TOTAL)

EXIST. LED LITE PRICE SIGNAGE (15.8 SQ. FT. EA. SIDE / 31.5 SQ. FT. TOTAL FOR BOTH SIDES)

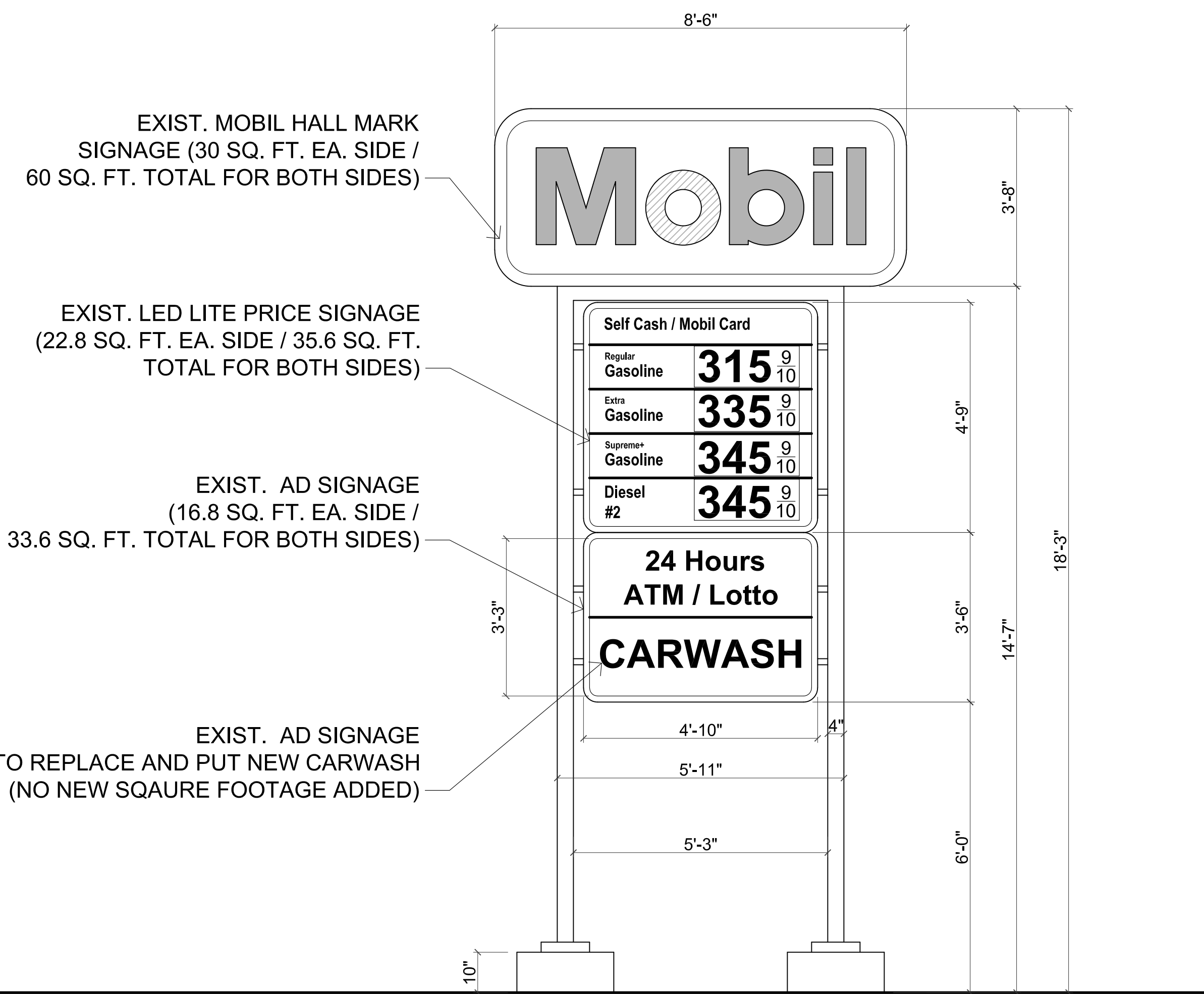


③ EXIST. POLE MOBIL/PRICE SIGN - 2  
 SCALE: 1/2"=1'-0"

④ EXIST. POLE MOBIL/PRICE SIGN - 2  
 SCALE: 1/2"=1'-0"

**FOR PLANNING ONLY / SIGNAGE PERMITTING DONE BY OTHERS**

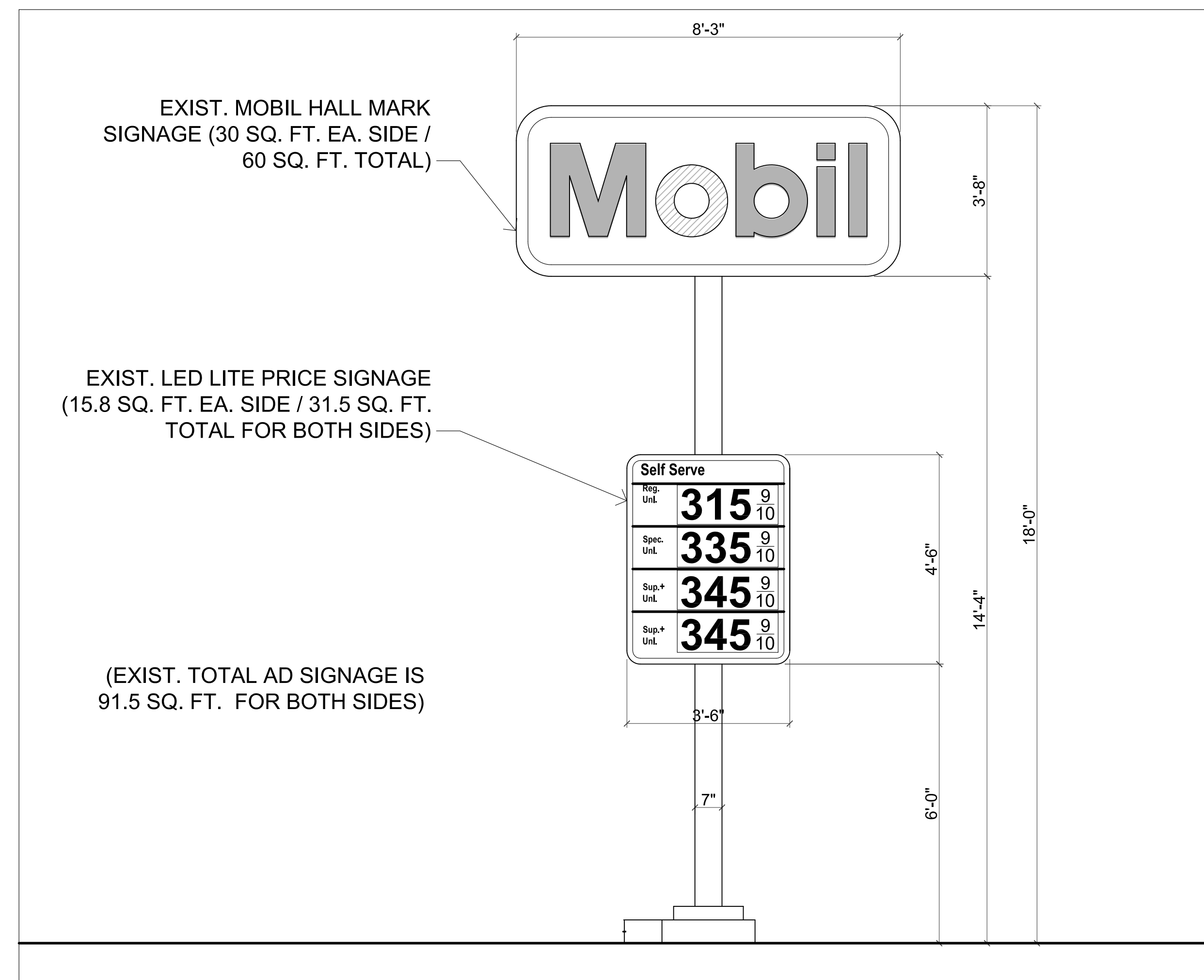




EXISTING MAIN PRICE POLE SIGNAGE LOCATED AT THE INTERSECTION FOR LAUREL CANYON BLVD. & RIVERSIDE DR.

**EXISTING MAIN PRICE POLE SIGNAGE**

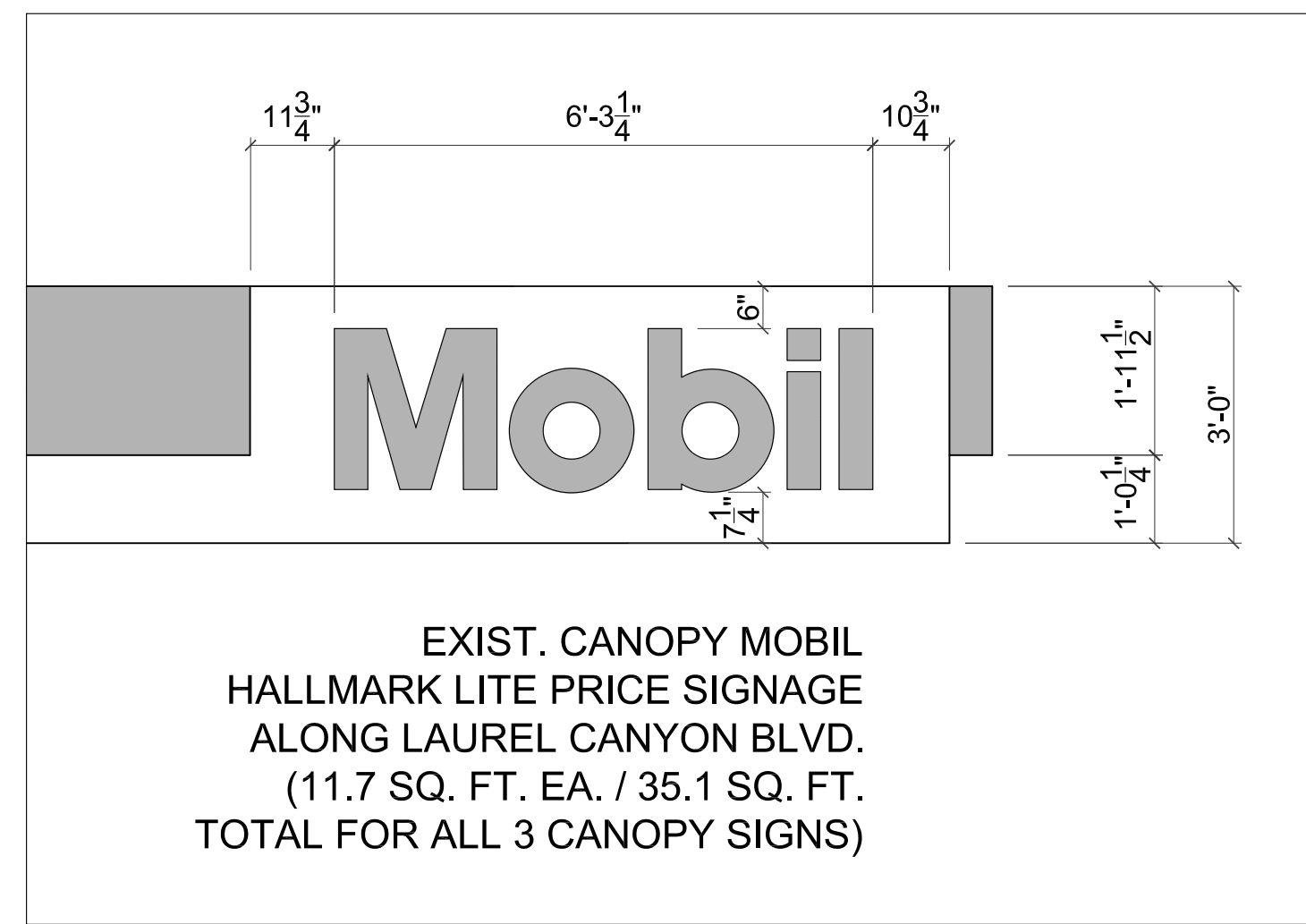
SCALE:  
1/2" = 1'-0"



EXISTING PRICE POLE SIGNAGE LOCATED AT THE SOUTH OF THE PROPERTY AT RIVERSIDE DR.

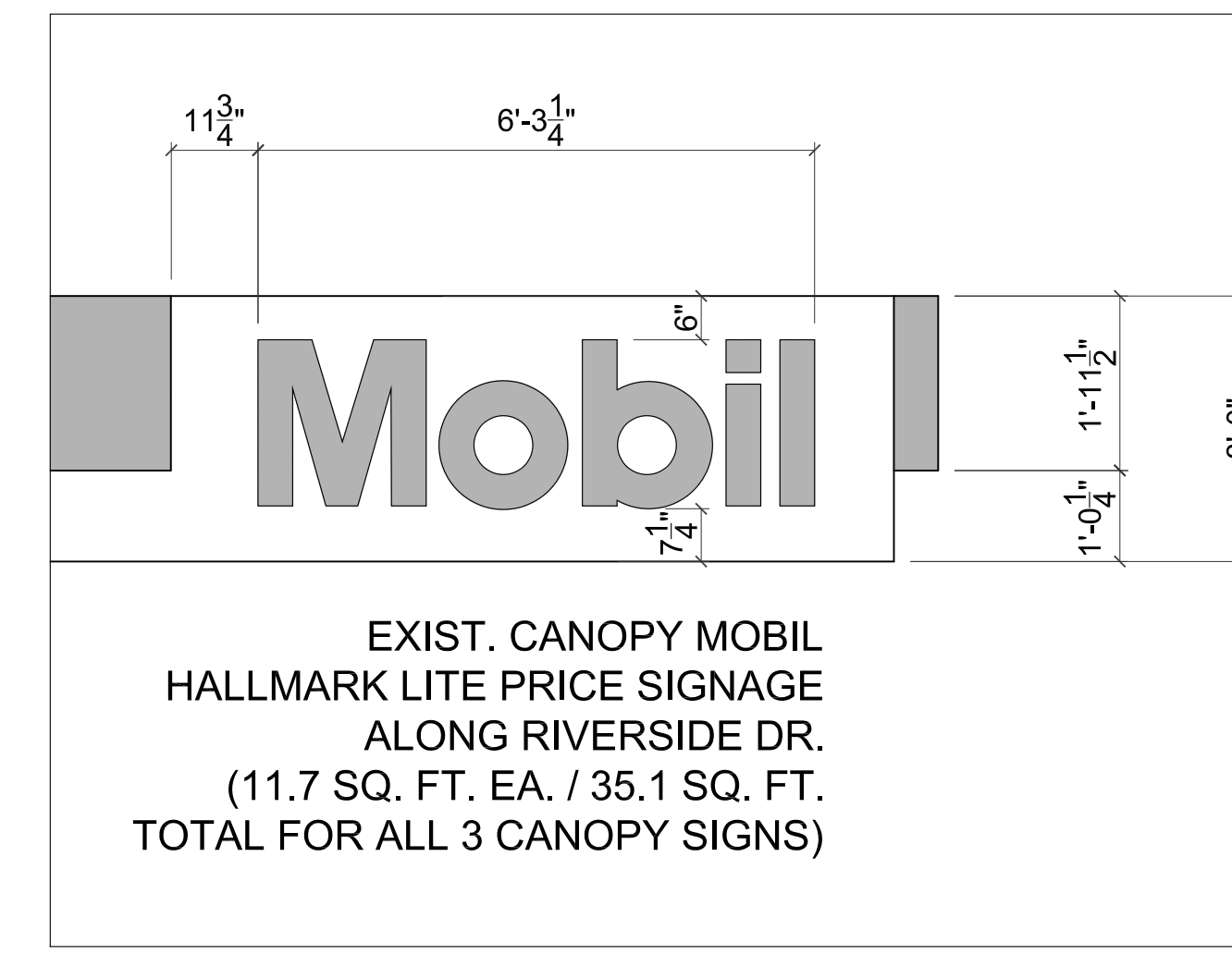
**EXISTING MAIN PRICE POLE SIGNAGE**

SCALE:  
1/2" = 1'-0"

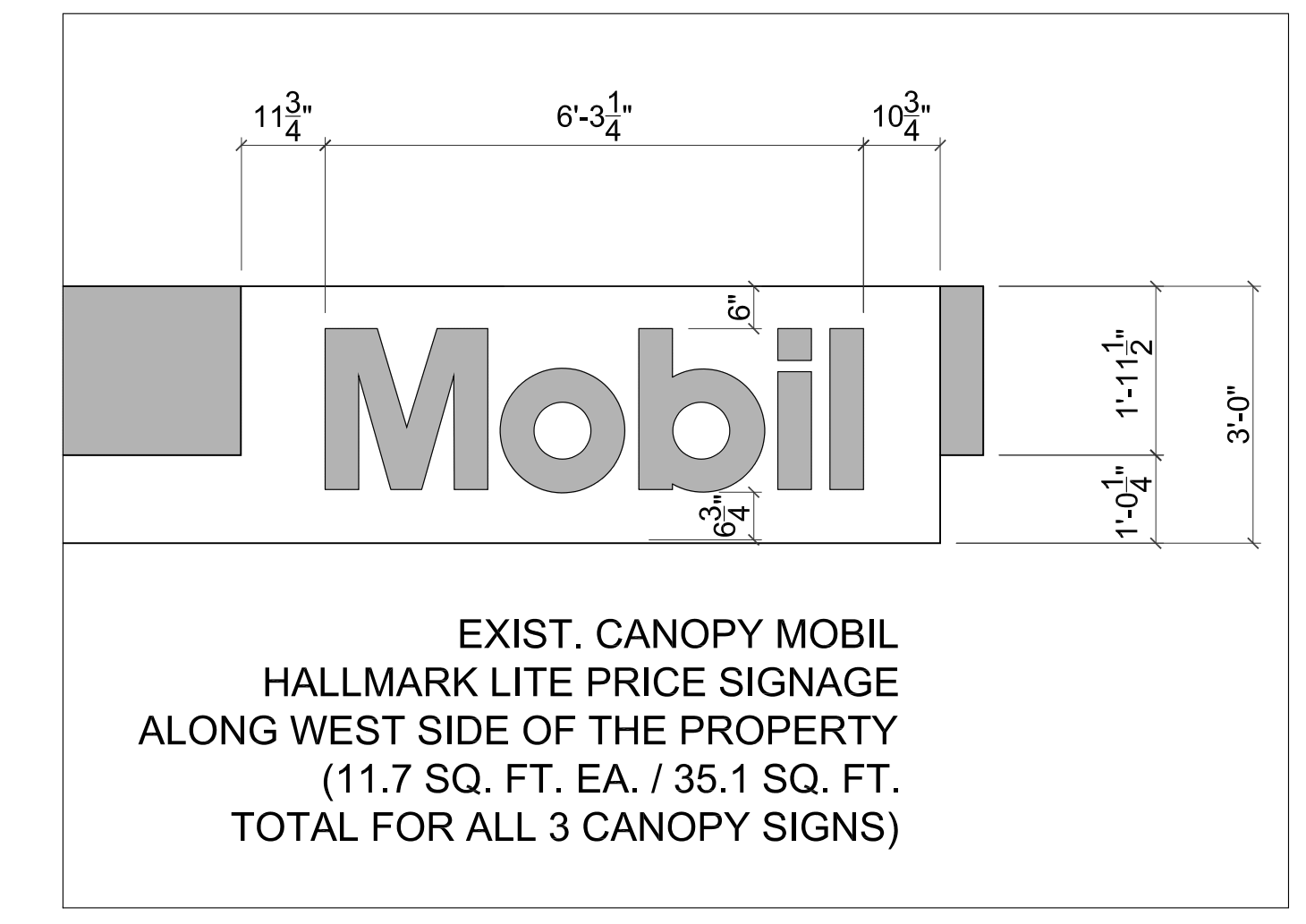


EXIST. CANOPY MOBIL HALLMARK LITE PRICE SIGNAGE ALONG LAUREL CANYON BLVD. (11.7 SQ. FT. EA. / 35.1 SQ. FT. TOTAL FOR ALL 3 CANOPY SIGNS)

EXISTING (3) CANOPY MOBIL HALLMARK SIGNAGE (35.1 SQ. F.T. TOTAL)



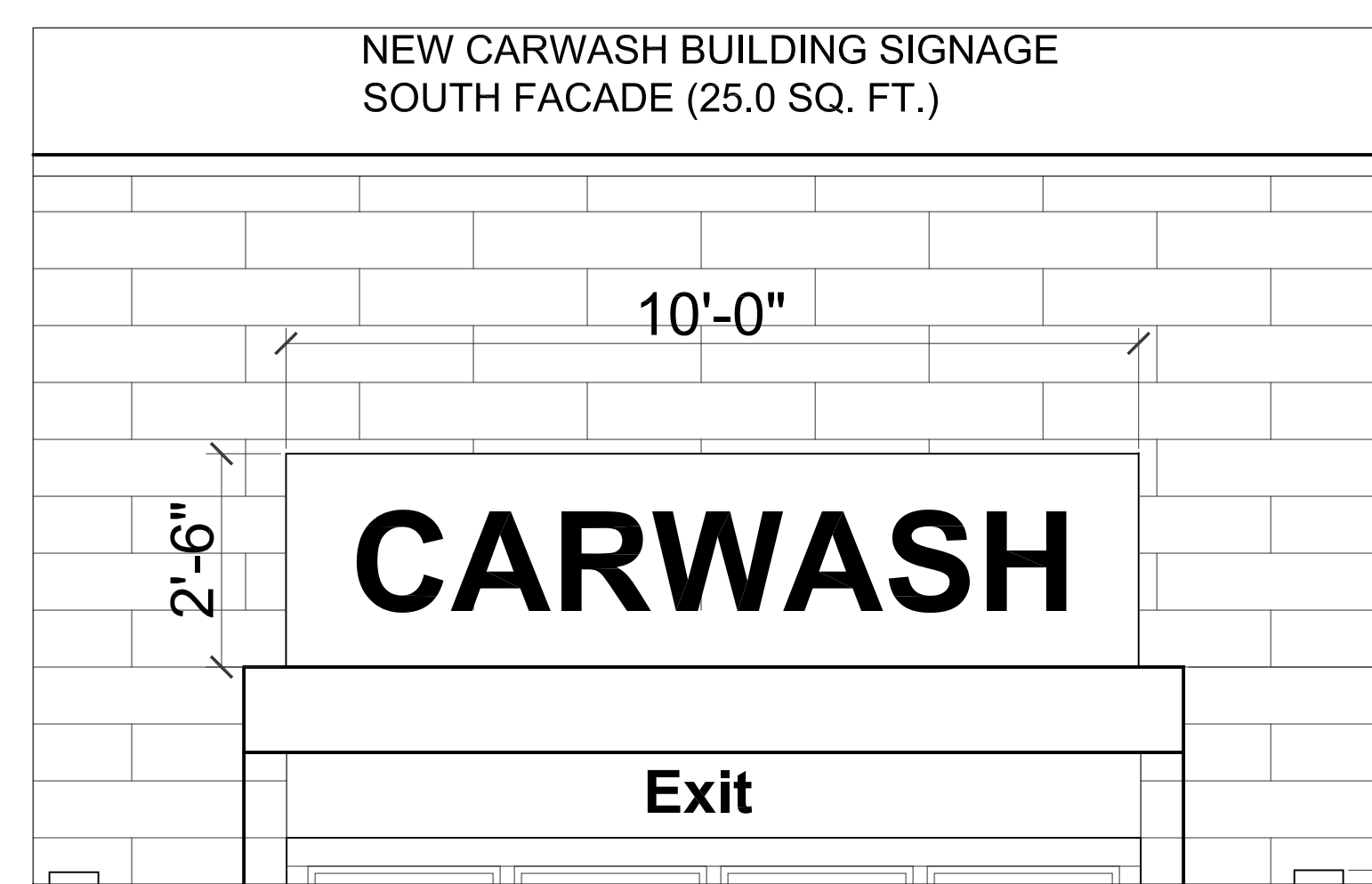
EXIST. CANOPY MOBIL HALLMARK LITE PRICE SIGNAGE ALONG RIVERSIDE DR. (11.7 SQ. FT. EA. / 35.1 SQ. FT. TOTAL FOR ALL 3 CANOPY SIGNS)



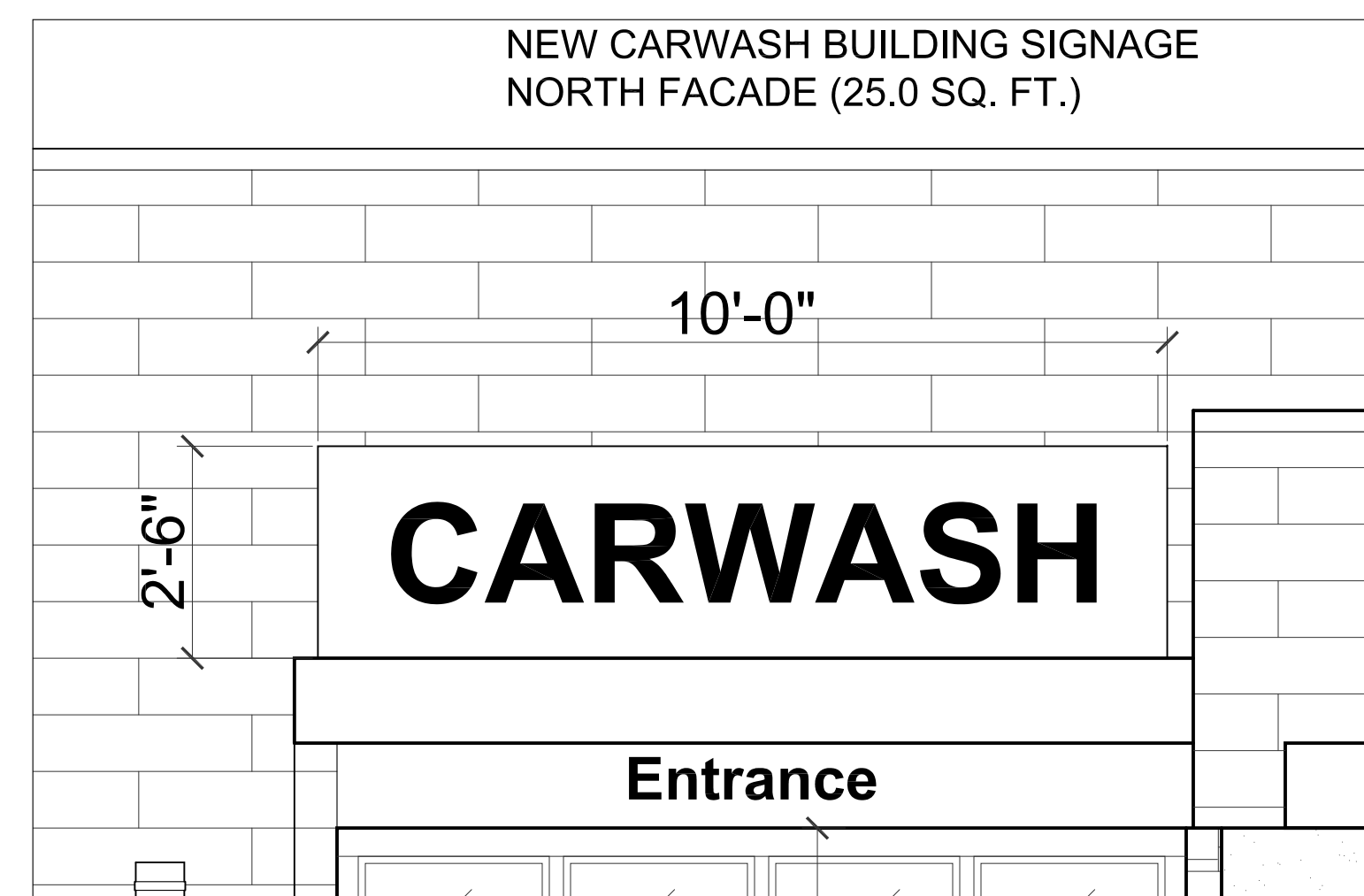
EXIST. CANOPY MOBIL HALLMARK LITE PRICE SIGNAGE ALONG WEST SIDE OF THE PROPERTY (11.7 SQ. FT. EA. / 35.1 SQ. FT. TOTAL FOR ALL 3 CANOPY SIGNS)

**EXISTING CANOPY SIGNAGE**

SCALE:  
1/2" = 1'-0"



NEW CARWASH BUILDING SIGNAGE SOUTH FACADE (25.0 SQ. FT.)

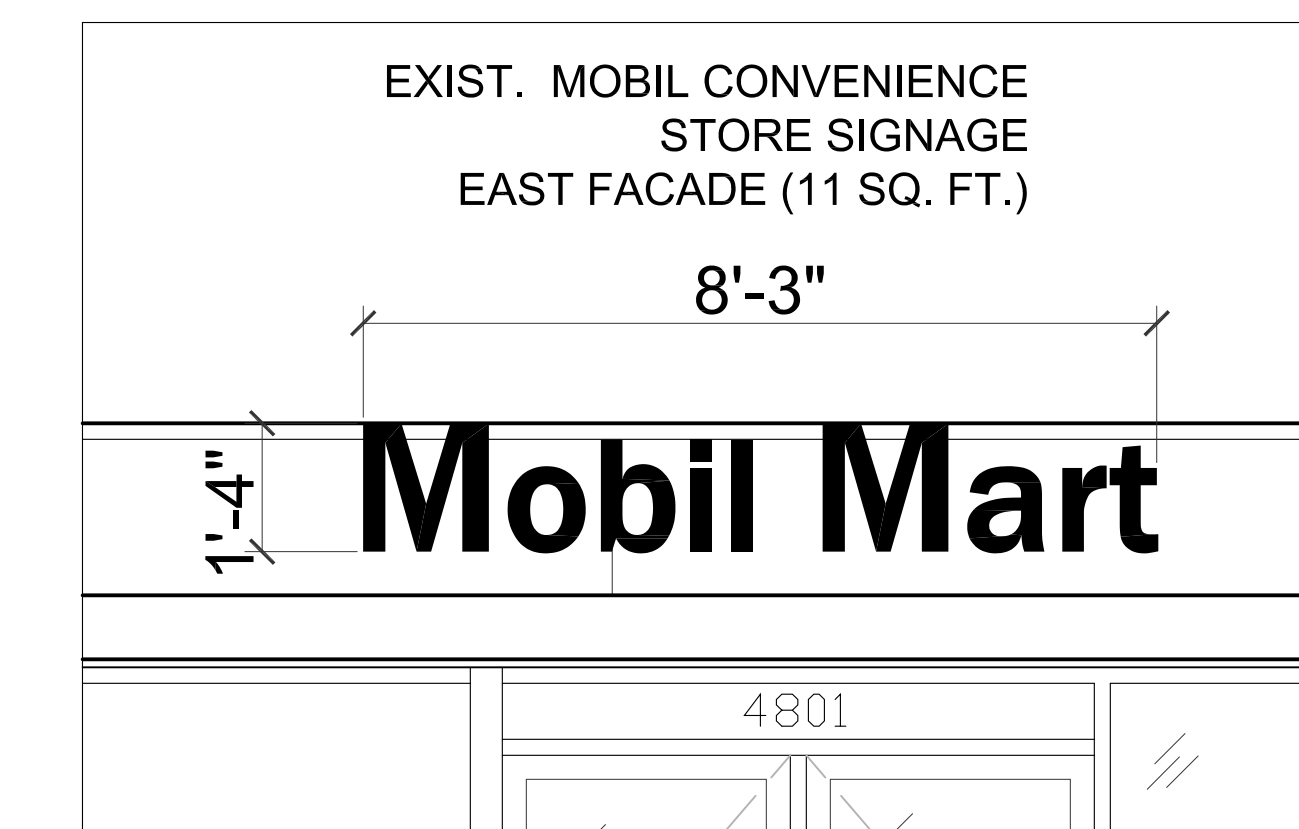


NEW CARWASH BUILDING SIGNAGE NORTH FACADE (25.0 SQ. FT.)

NEW (2) CARWASH SIGNAGE (50 SQ. FT. TOTAL)

**NEW CARWASH SIGNAGE**

SCALE:  
1/2" = 1'-0"



EXIST. MOBIL CONVENIENCE STORE SIGNAGE EAST FACADE (11 SQ. FT.)

EXISTING CONVENIENCE STORE MOBIL SIGNAGE (11 SQ. FT. TOTAL)

**EXISTING CONVENIENCE STORE SIGNAGE**

SCALE:  
1/2" = 1'-0"

**ESTIMATED TOTAL SIGNAGE:**

POLE SIGNAGE:  
EXIST. POLE SIGNAGE (1): 129.2 SQ. FT.  
EXIST. POLE SIGNAGE (2): 91.5 SQ. FT.  
TOTAL POLE SIGN SQ. FT. FOR BOTH SIDES: 220.7 SQ. FT.

EXIST. CANOPY MOBIL HALLMARK SIGNAGE:  
EAST SIGNAGE: 11.7 SQ. FT.  
WEST SIGNAGE: 11.7 SQ. FT.  
SOUTH SIGNAGE: 11.7 SQ. FT.  
TOTAL SQUARE FOOTAGE: 35.1 SQ. FT.

EXIST. CONVENIENCE STORE ENTRY SIGNAGE:  
MOBIL SIGNAGE (EAST SIDE OF BUILDING FACADE): 11.0 SQ. FT.

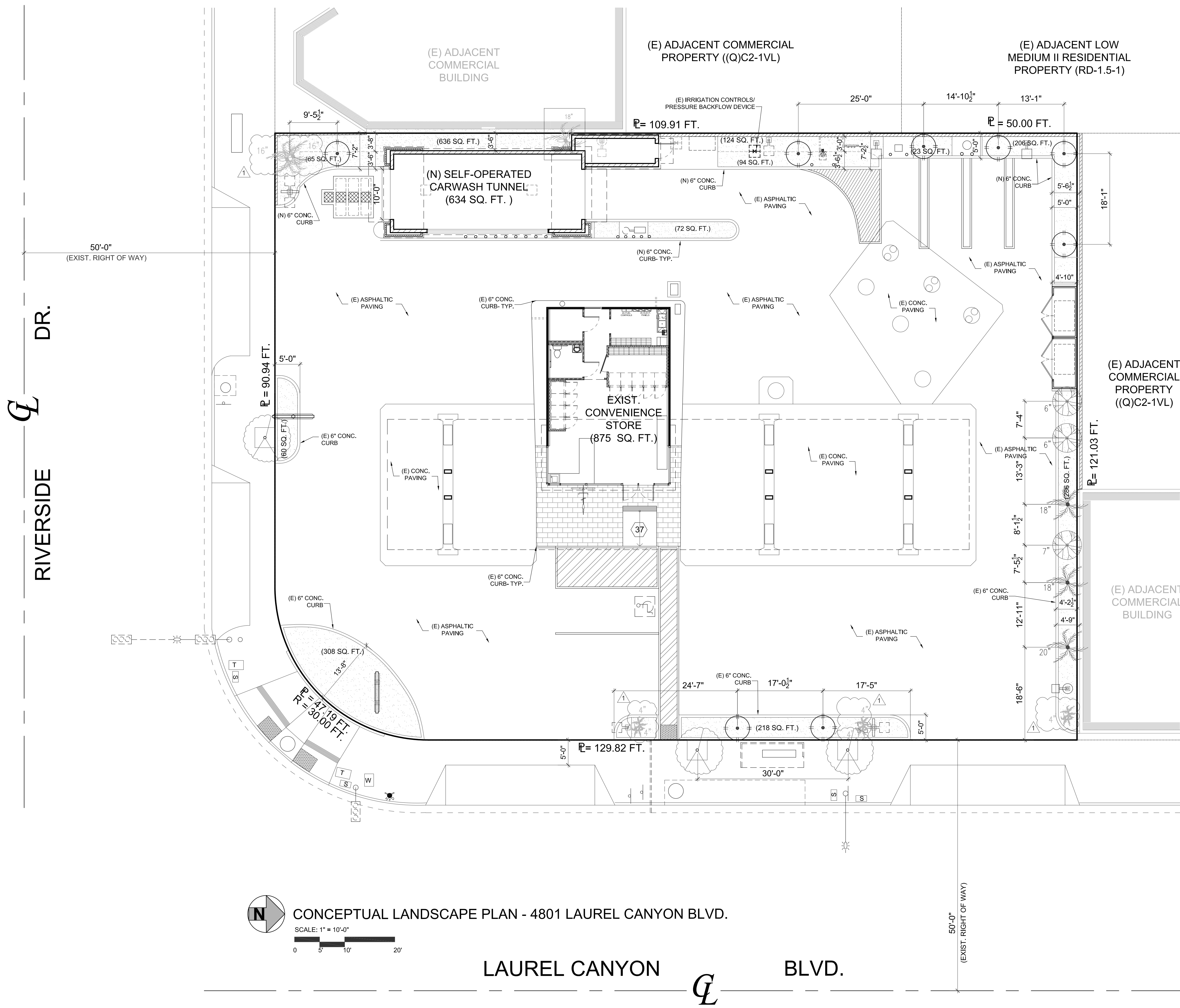
TOTAL EXISTING SIGNAGE AREA: 266.8 SQ. FT.

NEW SIGNAGE:  
SOUTH/CARWASH CHANNEL EXIT SIGNAGE: 25.0 SQ. FT.  
NORTH/CARWASH CHANNEL ENTRY SIGNAGE: 25.0 SQ. FT.  
TOTAL NEW SIGNAGE SQUARE FOOTAGE: 50.0 SQ. FT.

TOTAL SIGNAGE AREA: 266.8 + 50.0 = 316.8 SQ. FT.

**EXHIBIT "A"**  
Page No. 10 of 11  
Case No. APCSV-2019-1481-SPE-SPP-CU-ZV

**FOR PLANNING ONLY / SIGNAGE PERMITTING DONE BY OTHERS**



**CONCEPTUAL LANDSCAPE PLAN - 4801 LAUREL CANYON BLVD.**  
 SCALE: 1" = 10'-0"  
 0 5 10 20

LAUREL CANYON BLVD.

**LANDSCAPE LEGEND**

SYMBOL	QUANTITY	TYPE	SIZE	REMARKS
	8	CRAPE MYRTLE / LAGERSTROEMIA 'PECOS'	15 GAL / 24" BOX	NEW TREE GOOD FORM & COLOR
	8	EVERGREEN SHRUB/TREES (JUNIPERUS)	6"-7" DIA	EXIST. TREE
	3	EXIST. MATURE PALM TREES (MEXICAN PALMS)	16"-20" DIA	EXIST. TREE
	6	PIGMY DATE PALMS (PHOENIX ROEBELNI) TO BE REMOVED	4" DIA	EXIST. TREE
	3	PHOTINIA SERRULATA CHINESE PHOTINIA	10' TALL MIN.	NEW STREET TREE
	3	EXISTING MATURE PALM TREES TO BE REMOVED	4' - 8' TALL	
		NEW PLANTER AREA, NEW SHRUBS AND GROUND COVER TO BE DROUGHT TOLERANT AND MIXED VARIETY		
		EXISTING PLANTER AREA, NEW SHRUBS AND GROUND COVER TO BE DROUGHT TOLERANT AND MIXED VARIETY		

**IRRIGATION SYSTEM DESIGN STATEMENT**

A PERMANENT AUTOMATIC IRRIGATION SYSTEM SHALL BE DESIGNED AND INSTALLED TO IRRIGATE ALL PLANTING AREAS. THE DESIGN OF THE IRRIGATION SYSTEM SHALL EMPHASIZE WATER CONSERVATION AND PROVIDE EFFICIENT AND UNIFORM DISTRIBUTION OF IRRIGATION WATER. IN SMALL PLANTER AREAS WHERE APPROPRIATE, DRIP IRRIGATION SYSTEM MAY BE INSTALLED TO PROVIDE WATER DIRECTLY TO ROOT ZONE OF PLANTS. THE IRRIGATION SYSTEM SHALL UTILIZE CONVENTIONAL SPRAY HEADS IN LAWN AREAS AND LARGER PLANTING AREAS. THE AUTOMATIC IRRIGATION SYSTEM SHALL BE DESIGNED IN ACCORDANCE WITH THE CITY OF LOS ANGELES LANDSCAPE REQUIREMENTS.

**GENERAL NOTES:**

ALL LANDSCAPE SHALL COMPLY WITH THE LOS ANGELES LANDSCAPE ORDINANCE (ORDINANCE 170.978) AND VALLEY VILLAGE SPECIFIC PLAN (ORDINANCE 168.613) STANDARDS.  
 CURRENTLY, THERE ARE NO PROTECTED TREES AT THE PROJECT SITE AS DESCRIBED BY THE LOS ANGELES CITY LANDSCAPE ORDINANCE.  
 TREES, SHRUBS, AND GROUND COVER SHALL BE DROUGHT TOLERANT AND IN VARIETY.  
 ALL LANDSCAPED AREA WILL BE EQUIPPED WITH AN AUTOMATIC SPRINKLER AND DRIP IRRIGATION SYSTEM TO CONSERVE WATER.  
 ALL TREES TO BE MIN. OF 15 GAL / 24 INCH BOX AND AROUND THE BUFFER PLANTER AREAS, IT SHALL BE NO LESS THAN 25' ON CENTER. TREES SHALL BE PLANTED AROUND PARKING AREA, AT LEAST 4 STALLS, FOR SHADING.

**GENERAL INFORMATION**

**PROPERTY OWNER:**  
 N & D OIL CORP.  
 ATT: HATTAR, NADER  
 9431 BRANDON CT.  
 NORTHRIDGE, CA 9125  
 TEL: (818) 980-1366  
 EMAIL: NadersMobil@gmail.com

**PROJECT OWNER / PLANTING:**  
 N & D OIL CORP.  
 ATT: HATTAR, NADER  
 4801 N. LAUREL CANYON BLVD.  
 NORTH HOLLYWOOD, CA 91607  
 TEL: (818) 980-1366  
 EMAIL: NadersMobil@gmail.com

**REPRESENTATIVES / DESIGNERS:**  
 MK DESIGN  
 KANG & IAN MARR  
 535 W. ALLEN AVE., UNIT 23  
 SAN DIMAS, CA 91773  
 TEL: KEN: 626-374-3834  
 Email: kangdesign@hotmail.com  
 TEL: IAN: 619-913-2751  
 Email: iamarr@hotmail.com

**SITE-ASSESSOR'S PARCEL NUMBER:**  
 APN: 2356-008-021

**PROJECT SITE ADDRESS:**  
 4801 N. LAUREL CANYON BLVD.  
 VALLEY VILLAGE, CA 91607

**TOTAL LOT AREA:** 19,164.3 SQ. FT. / 0.44 ACRES

**LEGAL DESCRIPTION:**  
 LOTS 62, 63, AND 64 OF TRACT NO. 7808, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 115 PAGE 44 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 2356-008-021  
 TRACT: TR 7808

**ZONING:** (Q) C-2-1VL  
 (COMMERCIAL)

**LANDUSE:** NEIGHBORHOOD OFFICE COMMERCIAL

**SPECIFIC PLAN AREA:** VALLEY VILLAGE

**BUILDING CODE USED:** 2016 C.B.C.

**TYPE OF CONSTRUCTION:** Type V B

**OCCUPANCY TYPE:** M

**EXIST. LANDSCAPING:**  
 TOTAL EXIST. LANDSCAPE: 1,448.00 SQ. FT. / 19,164.30 SQ. FT.

**TOTAL EXISTING LANDSCAPE PERCENTAGE PROVIDED:** .075= 8%

**TOTAL NEW LANDSCAPE PERCENTAGE PROPOSED:** 434 SQ.FT. / 2.3%

**TOTAL LANDSCAPE:** 1,882 SQ. FT. / 19,164.30 SQ.FT. = 0.098 = 10%

**NEW LANDSCAPING AREA:**  
 • PROPOSED NEW AREA: 437.00 SQ.FT.  
 • EXISTING AREA: 1,655.00 SQ.FT.  
 TOTAL AREA: 2,092.00 SQ.FT. / 19,164.30 SQ. FT.

**TOTAL LANDSCAPE PERCENTAGE PROVIDED:** .109 = 11%  
 (TOTAL DOES NOT INCLUDE LANDSCAPING AT THE RIGHT OF WAY AREA)

**LEGEND**

	PEDESTRIAN WALKWAY	(N) NEW (E) EXISTING
--	--------------------	-------------------------

**EXHIBIT "A"**  
 Page No. 11 of 11  
 Case No. APCSV-2019-1481-SPE-SPP-CU-ZV

APCSV-2019-1481-SPE-SPP-CU-ZV

Exhibits

Exhibit B – Maps – Vicinity, Land Use, Zoning, Radius



R1-1

R3-1

IOJC2-1VL

IOJC2-1VL

R1-1

LA MAIDA ST

AGNES AVE

RD2-1

R1-1

RD1.5-1

LAUREL CANYON BLVD



IOJC2-1VL

HWY DED

HWY DED

VANTAGE AVE

HWY DED

RIVERSIDE DR

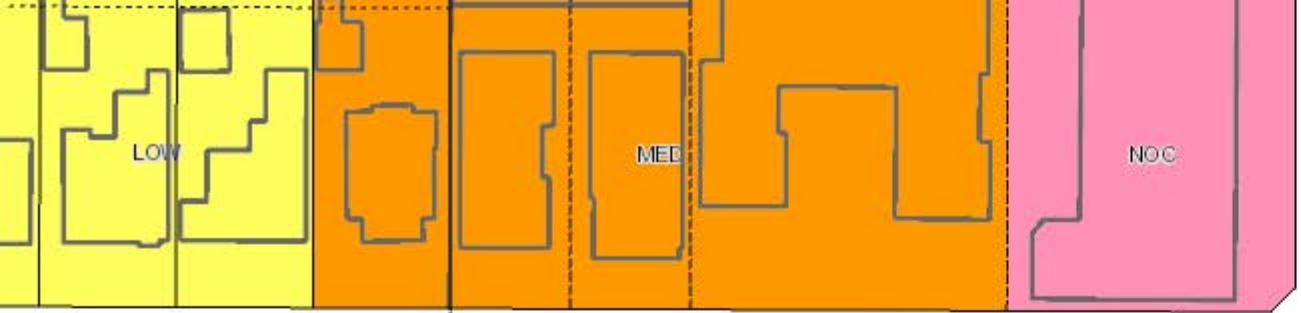
VL-R10

IOJC2-1VL

LARIAC5 2017 Color-Ortho

0.02 Miles

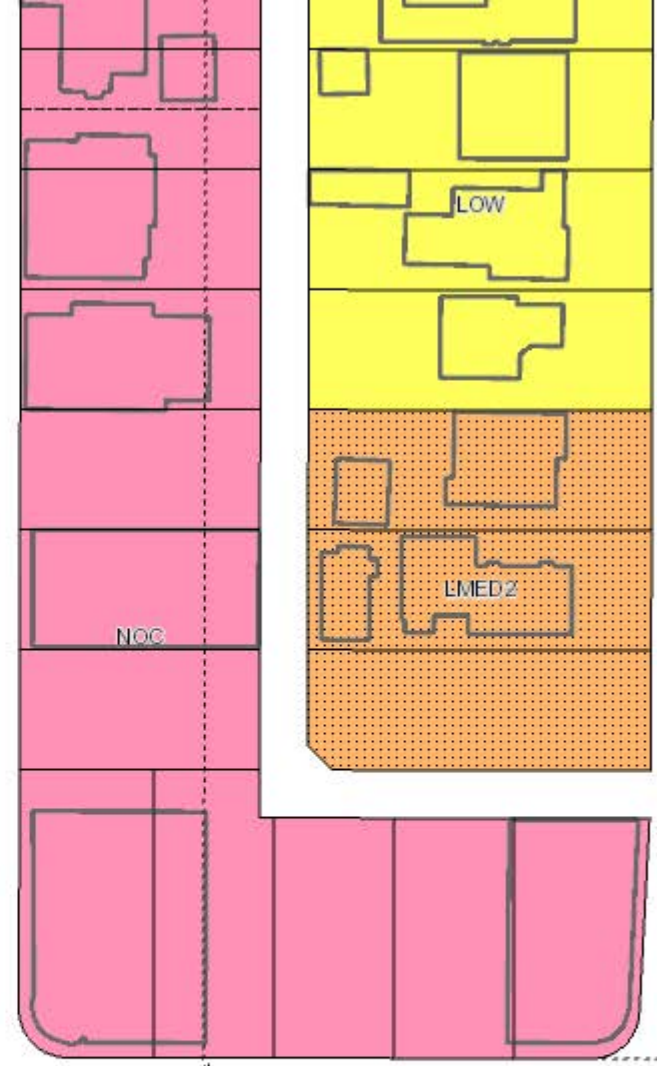
80 Feet



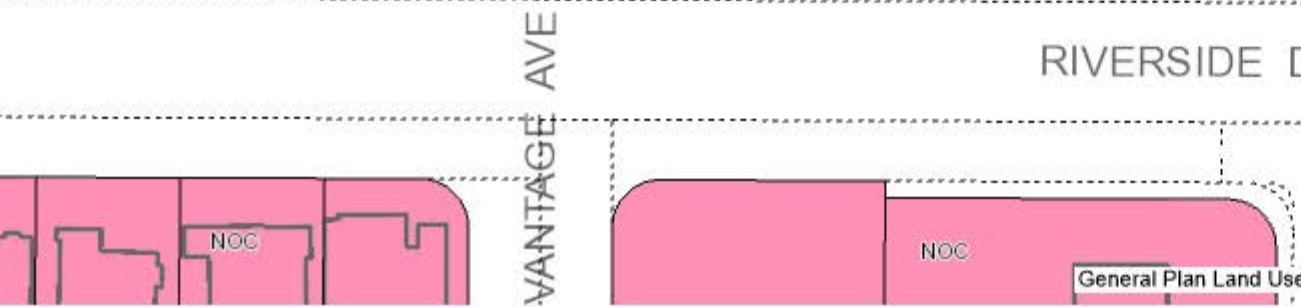
LA MAIDA ST



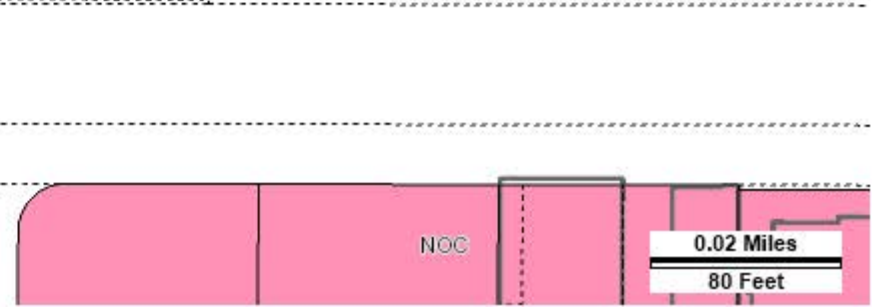
LAUREL CANYON BLVD



AGNES AVE



RIVERSIDE DR

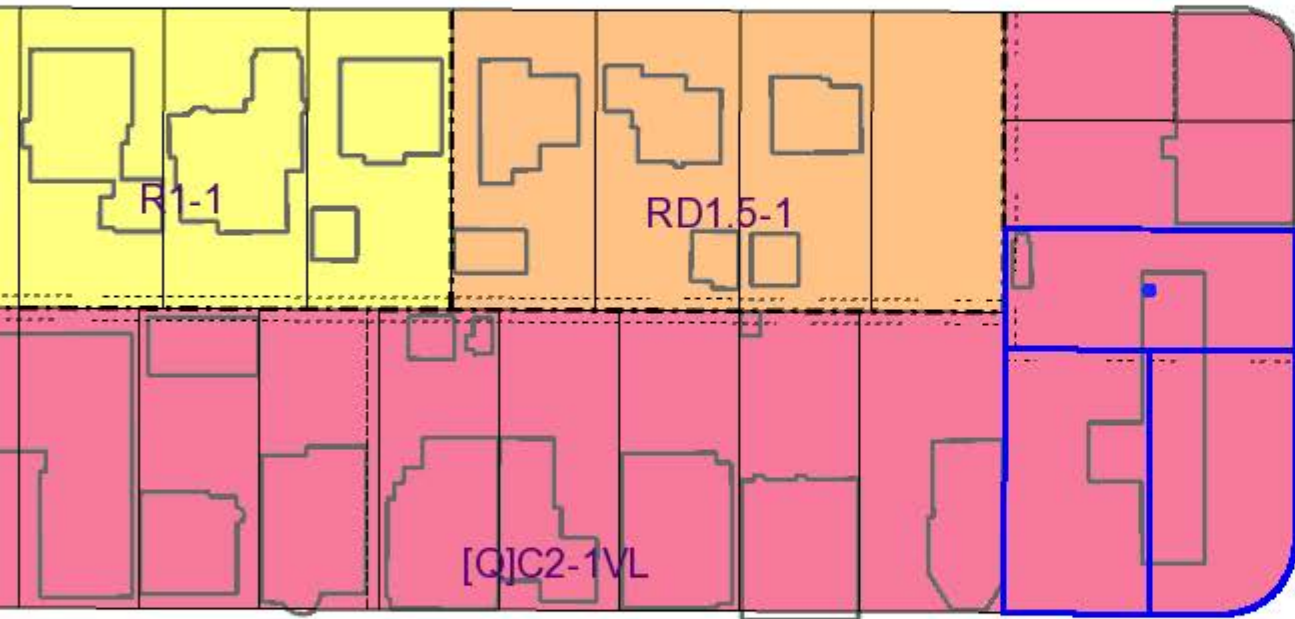


0.02 Miles  
80 Feet

General Plan Land Use



LA MAIDA ST



HWY DED

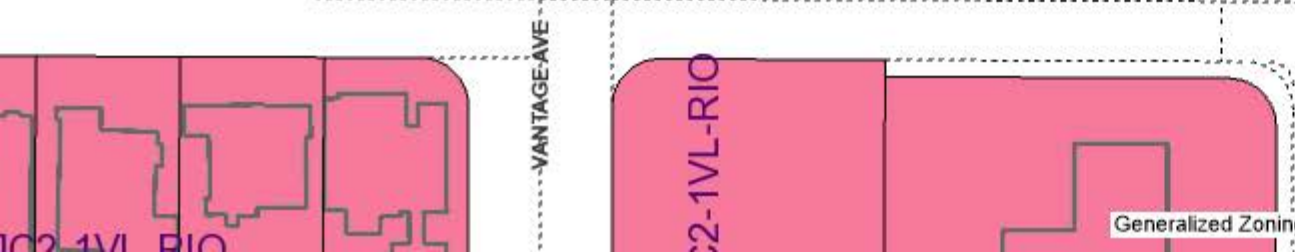
LAUREL CANYON BLVD



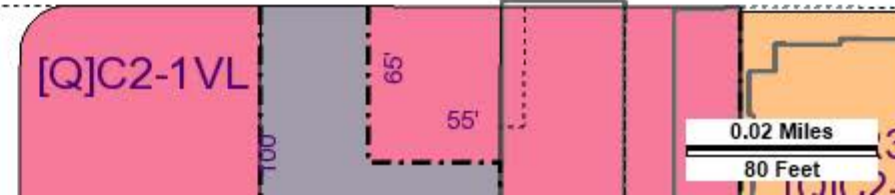
HWY DED

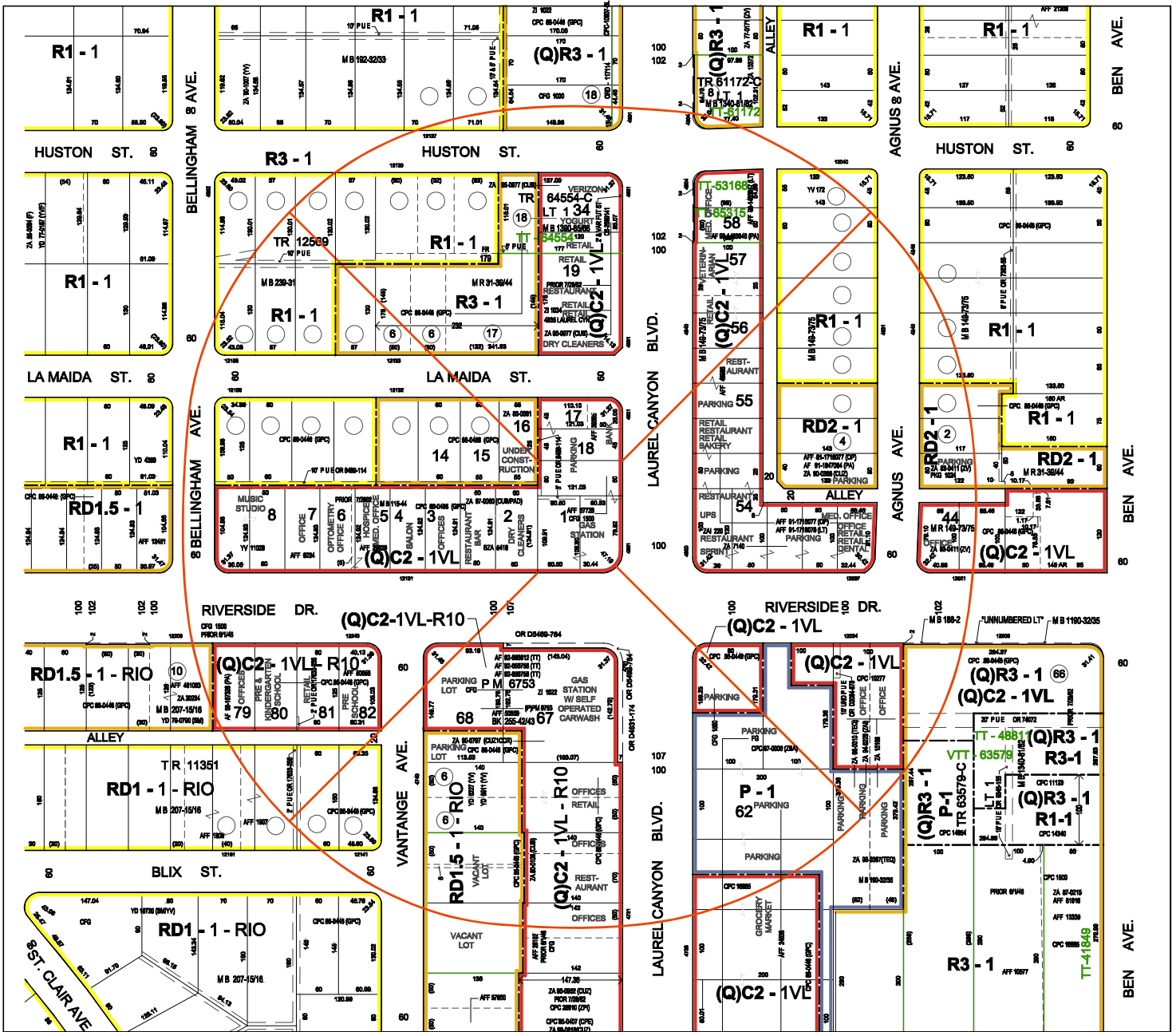
RIVERSIDE DR

HWY DED



Generalized Zoning





## CONDITIONAL USE MINI-SHOPPING CENTER - CARWASH/24 HRS (12.24-W,27)

**LEGAL DESCRIPTION:**

LOTS 62, 63, AND 64 OF TRACT NO. 7808, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 115 PAGE 44 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.  
APN: 2356-008-021

TRACT: TR 7808  
LOTS: (62, 63, & 64)

C.D.: 2  
C.T.: 1240.00  
P.A.: NORTH HOLLYWOOD - VALLEY VILLAGE  
D.M.: M B 115-44

THOMAS BROS.  
PAGE: 633  
GRID: G4

**ASSESSOR'S PARCEL NUMBER:**

2356-008-021

**LOTS:**

62, 63, & 64

**PROJECT ADDRESS:**

4801 LAUREL CANYON BLVD.,  
NORTH HOLLYWOOD, CA  
LOS ANGELES, CA 91607

**TOTAL LOT AREA:**

19,164.3 SQ. FT. /  
0.44 ACRES

**ZONING: (Q) C2-1VL**

GENERAL: COMMERCIAL

LAND USE: NEIGHBORHOOD OFFICE COMMERCIAL

**LAND USE SYMBOLS**

- - SINGLE-FAMILY DWELLING
- ② - DUPLEX
- NO - NUMBER OF APARTMENT UNITS ON ONE.
- VIC - TEMP. VACANT (UNDER CONSTRUCTION)
- VAC - VACANT LOT
- PKG - PARKING LOT
- ▲ - DIFFERENT PARCEL NUMBERS, SAME OWNER

CASE NO.:

DATE: 7/19/2018

### MK Design

535 W. ALLEN AVE., STE 23  
SAN DIMAS, CA. 91773

CONTACTS: KEN KANG CELL: 626-374-3834  
kangdesign@hotmail.com

IAN MARR CELL: 619-913-2751  
iamarr@hotmail.com

### ABUTTING OWNERS MAP 500' RADIUS MAP / LAND USE



SCALE: 1" = 100'-0"

Exhibit C – Environmental Clearance (ENV-2019-1482-CE) – Notice of Exemption,  
Narrative Attachment, Noise Studies



COUNTY CLERK'S USE

**CITY OF LOS ANGELES**  
OFFICE OF THE CITY CLERK  
200 NORTH SPRING STREET, ROOM 395  
LOS ANGELES, CALIFORNIA 90012  
**CALIFORNIA ENVIRONMENTAL QUALITY ACT**  
**NOTICE OF EXEMPTION**

(PRC Section 21152; CEQA Guidelines Section 15062)

Filing of this form is optional. If filed, the form shall be filed with the County Clerk, 12400 E. Imperial Highway, Norwalk, CA 90650, pursuant to Public Resources Code Section 21152(b) and CEQA Guidelines Section 15062. Pursuant to Public Resources Code Section 21167 (d), the posting of this notice starts a 35-day statute of limitations on court challenges to reliance on an exemption for the project. Failure to file this notice as provided above, results in the statute of limitations being extended to 180 days.

PARENT CASE NUMBER(S) / REQUESTED ENTITLEMENTS

APCSV-2019-1481-SPE-SPP-CU-ZV / Specific Plan Exception, Project Permit Compliance, Conditional Use, and Variance

LEAD CITY AGENCY

**City of Los Angeles (Department of City Planning)**

CASE NUMBER

ENV-2019-1482-CE

PROJECT TITLE

COUNCIL DISTRICT

2 – Krekorian

PROJECT LOCATION (Street Address and Cross Streets and/or Attached Map)

**4801 – 4815 N. Laurel Canyon Boulevard; 12107 – 12111 W. Riverside Drive**

Map attached.

PROJECT DESCRIPTION:

See attachment

Additional page(s) attached.

NAME OF APPLICANT / OWNER:

**Nader Hattar, N&D Corporation**

CONTACT PERSON (If different from Applicant/Owner above)

**Ken Kang, MK Design**

(AREA CODE) TELEPHONE NUMBER

**(626)374-3834**

EXT.

EXEMPT STATUS: (Check all boxes, and include all exemptions, that apply and provide relevant citations.)

STATE CEQA STATUTE & GUIDELINES

STATUTORY EXEMPTION(S)

Public Resources Code Section(s) \_\_\_\_\_

CATEGORICAL EXEMPTION(S) (State CEQA Guidelines Sec. 15301-15333 / Class 1-Class 33)

CEQA Guideline Section(s) / Class(es) **Section 15303, Class 3(a)**

OTHER BASIS FOR EXEMPTION (E.g., CEQA Guidelines Section 15061(b)(3) or (b)(4) or Section 15378(b) )

JUSTIFICATION FOR PROJECT EXEMPTION:

Additional page(s) attached

See attached

None of the exceptions in CEQA Guidelines Section 15300.2 to the categorical exemption(s) apply to the Project.

The project is identified in one or more of the list of activities in the City of Los Angeles CEQA Guidelines as cited in the justification.

IF FILED BY APPLICANT, ATTACH CERTIFIED DOCUMENT ISSUED BY THE CITY PLANNING DEPARTMENT STATING THAT THE DEPARTMENT HAS FOUND THE PROJECT TO BE EXEMPT.

If different from the applicant, the identity of the person undertaking the project.

**CITY STAFF USE ONLY:**

CITY STAFF NAME AND SIGNATURE

Andrew Jorgensen

STAFF TITLE

City Planner

ENTITLEMENTS APPROVED

Specific Plan Exception, Project Permit Compliance, Conditional Use Permit, Variance

FEE:  
\$5,774

RECEIPT NO.  
0202611515

REC'D. BY (DCP DSC STAFF NAME)  
Sheila Toni

DISTRIBUTION: County Clerk, Agency Record

## ENV-2019-1482-CE / Attachment "A" (Revised)

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Project Address: 4801 – 4815 N. Laurel Canyon Boulevard; 12107 – 12111 W. Riverside Drive

Project Description: The project is the construction of a 736 square foot, self-operated car wash tunnel (17 ½ feet maximum height) to be operated from 7 a.m. to 10 p.m. daily, in conjunction with an existing gas station with convenience store, on an approximate 19,164 square foot commercial corner site. A total of 5 parking spaces will be provided. The car wash tunnel is proposed at the southwest corner of the site, adjacent to a commercial use. The car wash tunnel will be approximately 38 feet long and 16 feet wide and will be located at the southwest corner of the site, with the entrance from the rear/north end of the tunnel and exit at the south end facing Riverside Drive. The project includes new landscaping around the site, raising existing concrete masonry unit (CMU) walls of six feet in height (west property line) and four feet in height (north property line) by two feet each, the addition of 8 new on-site trees and 3 new street trees, four additional parking spaces, new lighting, two new signs, a new trash enclosure area, new air/water location and the addition of two coin operation vacuum hoses with sound dampeners. Access to the site will remain as is, which includes four driveway apron (two from each street frontage). Access to the carwash will be from the rear, north side of the tunnel, and exiting south near the Riverside Drive southwest driveway. The project includes the demolition of a small storage building (160 square feet), the removal of 9 palm trees (3 mature, 6 small) and grading of less than 500 cubic yards.

### **Notice of Exemption**

The City of Los Angeles determined that based on the whole of the administrative record, that the Project is exempt from CEQA pursuant to CEQA Guidelines, Section 15303, Class 3 (*new construction of a small structure not exceeding 2,500 square feet*) and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.

There are six (6) Exceptions which must be considered in order to find a project exempt under Class 3: (a) Location, (b) Cumulative Impacts; (c) Significant Effect; (d) Scenic Highways; (e) Hazardous Waste Sites; and (f) Historical Resources.

- (a) **Location.** *Classes 3, 4, 5, 6, and 11 are qualified by consideration of where the project is to be located. A project that is ordinarily insignificant in its effect on the environment may in a particularly sensitive environment be significant. Therefore, these classes may not be utilized where the project may impact on an environmental resource of hazardous or critical concern where designated, precisely mapped, and officially adopted pursuant to law by federal, state, or local agencies.*

The project is the construction and operation of a 736 square foot self-service drive-through car wash tunnel on a commercial corner lot zoned [Q]C2-1VL, currently developed with a gas station and convenience store and gas station canopies. The remainder of the site is mostly covered with asphalt except for landscape planter areas along the perimeter of the site which include shrubs as well as 20 non-protected trees comprised of palm trees, junipers, and pygmy palms.

The site is located within a commercial area of Valley Village, at the intersection of Laurel Canyon Boulevard and Riverside Drive. Surrounding properties are similarly zoned [Q]C2-1VL and developed with one and two story commercial buildings. The west adjoining property is developed with a drive-thru dry cleaners; north adjoining property is developed with a one story bank and related surface parking lot; south abutting property is developed with a gas station, convenience store and drive-thru car

wash; east abutting property is developed with a multi-tenant commercial center which include sit down eateries and a coffee shop; and the southeast abutting corner is developed with a grocery store and large parking lot. The subject property is a level lot, located 3.24 kilometers from the Hollywood Fault and is within a Liquefaction Area. The site is located within an urban built up area and is not located on or near a designated sensitive environmental area. Thus exception (a) does not apply.

- (b) **Cumulative Impact.** *The exception applies when, although a particular project may not have a significant impact, the impact of successive projects, of the same type, in the same place, over time is significant.*

Based on a review of databases including the City of Los Angeles Department of City Planning Zone Information Map Access System (ZIMAS) for nearby case approvals (<http://zimas.lacity.org/>), there is an existing gas station with convenience store and a drive-through car wash located across the street, at southwest corner of Laurel Canyon Boulevard and Riverside Drive. No other similar uses were found within a 500 foot radius of the subject site. In addition, the project is entirely consistent with the existing General Plan designation and zoning (other than the requested deviations), which accounts for the impacts of developments which are within their parameters. Any successive projects of the same type and nature would reflect a development that is consistent with the underlying land use designation and the LAMC, and thus would be subject to the same regulations and requirements, including development standards and environmental impacts. As such, the proposed project will not result in a significant cumulative impact. Thus, exception (b) does not apply.

- (c) **Significant Effect Due To Unusual Circumstances.** *This exception applies when, although the project may otherwise be exempt, there is a reasonable possibility that the project will have a significant effect due to unusual circumstances.*

The site is located on a commercially zoned property that permits auto-related uses such as gas stations. The site is surrounded by properties zoned and developed with commercial uses. The closest residential properties are located on La Maida Street, approximately 80 feet northwest of the site. The project consists of commercial uses and operations that are compatible with the surrounding development and consistent with the underlying zone. The project site is in a long-established neighborhood, and the site has been developed as it currently exists since at least 1986 (according to Assessor's Records.) The site does not demonstrate any unusual circumstances, and the project will not generate significant impacts. There are no unusual circumstances that indicate this project would reasonably result in a significant effect on the environment.

**Water Quality.** The project is not adjacent to any water sources and construction of the project will not impact water quality. Construction activities would not involve any significant excavation near an identified water source. Wastewater generated by the proposed car wash will be treated before it is discharged to the City Bureau of Sanitation. An industrial waste permit will be issued, which will ensure compliance with all water quality regulations and requirements. Thus, the project is not expected to negatively impact water quality.

Furthermore, the project will comply with the City's stormwater management provisions per LAMC 64. 70. Best Management Practices would also be required during general operation of the project to ensure that stormwater runoff meets the established water quality standards and waste discharge requirements. Therefore, development of the

proposed project would not degrade the quality of stormwater runoff from the site and would not result in any significant effects relating to water quality.

**Noise.** Further, the project must comply with the City of Los Angeles Noise Ordinance No. 144,331 and 161,574 and any subsequent ordinances which prohibit the emission or creation of noise beyond certain levels, during both construction and ongoing operation. The Ordinances cover both operational noise levels (i.e. postconstruction), as well as any noise impact during construction. Section 41.40 of the LAMC regulates noise from demolition and construction activities and prohibits construction activity (including demolition) and repair work, where the use of any power tool, device, or equipment would disturb persons occupying sleeping quarters in any dwelling hotel, apartment, or other place of residence, between the hours of 9:00 p.m. and 7:00 a.m. Monday through Friday, and between 6:00 p.m. and 8:00 a.m. on Saturdays and holidays; all such activities are also prohibited on Sundays. Section 112.05 of the LAMC also specifies the maximum noise level of construction machinery that can be generated in any residential zone of the city or within 500 feet thereof. As the project is required to comply with the above ordinances and regulations, it will not result in any significant noise impacts.

Further, per the CNEL (Community Noise Equivalent Level) Sound Study prepared by MK Design, dated January 7, 2021, "by averaging the peak noise levels (with the Recommended Regulatory Compliance Measures in place) against the measured existing ambient noise levels our site meets the Minimum Ambient Noise Level (Sec 111.01(a))." This study was voluntarily supplemented by the applicant with a Noise Study to supplement and clarify prior studies prepared by Omny Acoustics, dated December 7, 2021, which consisted of a detailed on-site noise survey and propagation modeling. In studying the existing ambient noise levels, the study found that on-site ambient noise levels from adjacent sources exceeded the composite limits generated by Omny based on several sources, including the LAMC and LA County Codes. The report notes that permanent future increases in operational noise when modeled shows full compliance with LAMC noise ordinances. Further, the report states that the Project's CNEL levels are "below existing ambient CNEL levels at all property adjacencies ... [resulting] in a CNEL increase well below CEQA significance thresholds at all locations." In further response to public comments received, the applicant has volunteered revised project features, which would be conditioned through entitlement approvals, such as operating standards to include daytime-only operations and the use of a vacuum system with sound dampeners. The site contains existing concrete masonry unit (CMU) walls of six feet in height (west property line) and four feet in height (north property line), which the project applicant voluntarily proposes to raise by two feet each, and which would further limit noise impacts. Thus, the project will not result in any significant effects relating to noise.

Therefore, the Project will not have a significant effect due to unusual circumstances.

- (d) **Scenic Highways.** This exception applies when, although the project may otherwise be exempt, there may be damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway.

Based on a review of the California Scenic Highway Mapping System ([http://www.dot.ca.gov/hq/LandArch/16\\_livability/scenic\\_highways/](http://www.dot.ca.gov/hq/LandArch/16_livability/scenic_highways/)), subject site is not located along a State Scenic Highway, nor are there any designated State Scenic Highways located near the project site. Based on this, the proposed project will not result in damage to scenic resources including trees, historic buildings, rock outcroppings, or

similar resources, within a highway officially designated as a state scenic highway, and this exception does not apply.

- (e) **Hazardous Waste Sites.** *Projects located on a site or facility listed pursuant to California Government Code 65962.5.*

Based on a review of the California Department of Toxic Substances Control "Envirostor Database" (<http://www.envirostor.dtsc.ca.gov/public/>), no known hazardous waste sites are located on or proximate to the project site. In addition, there is no evidence of historic or current use, or disposal of hazardous or toxic materials at this location. Based on this, the project will not result in a significant effect due hazardous waste and this exception does not apply.

- (f) **Historical Resources.** *Projects that may cause a substantial adverse change in the significance of an historical resource.*

Neither the project site itself nor any of the existing structures on the project site have been identified as a historic resource by local or state agencies, and the project site has not been determined to be eligible for listing in the National Register of Historic Places, California Register of Historical Resources, the Los Angeles Historic-Cultural Monuments Register, and/or any local register. Further, the project site was not found to be a potential historic resource based on the City's HistoricPlacesLA website or SurveyLA, the citywide survey of Los Angeles. Neither the State nor the City choose to treat the site as a historic resource, therefore, the proposed project cannot cause a substantial adverse change in the significance of a historical resource and this exception does not apply.

In conclusion, since the project meets all of the requirements of the categorical exemption set forth at CEQA Guidelines, Section 15303 and none of the applicable exceptions to the use of the exemption apply to the project, it is appropriate to determine this project is categorically exempt from the requirements of CEQA.

07 DECEMBER 2021

TO: DEMA HATTAR, NADER'S MOBIL  
IAN MARR, MK DESIGN  
KEN KANG, MK DESIGN



Omny Acoustics (“Omny”) hereby submits this revised draft report to Nader's Mobil (“Client”) regarding our Environmental Noise Impact Study performed for the proposed Nader's Mobil Remodel & Car Wash project located at 4801 Laurel Canyon Boulevard, Valley Village, California (“Project”). This report is revised to incorporate revisions to the Project's design.

## A. EXECUTIVE SUMMARY

Omny Acoustics has prepared this Environmental Noise Impact Report to identify and address any potential exceedances of local codes as well as CEQA significance thresholds. The study detailed herein is based on a detailed on-site noise survey, and a complete sound propagation modeling of the Project site and its surroundings. A brief summary of our findings is as follows:

- Due to high ambient site noise levels, property-line noise limits are controlled by the current ambient conditions at all property-line adjacencies (rather than by code-defined “minimum ambient” levels).
- Sound modeling shows full compliance of all Project noise sources with the project-specific limits specified in the *Los Angeles Municipal Code* and *Los Angeles County Code of Ordinances*.
- The highest possible Project CNEL noise impacts are below existing ambient CNEL levels at all property adjacencies. This results in a CNEL increase well below CEQA significance thresholds at all locations.
- Omny determines all CEQA noise impacts to fall under either the “Less Than Significant Impact” or the “No Impact” category under the project's current design.
- No noise mitigation is required.

## B. PROJECT BACKGROUND

The Nader's Mobil gas station is currently operational on the Project site at 4801 Laurel Canyon Boulevard. The station owners wish to remodel, adding an automatic car wash tunnel and vacuum cleaners for customer use.

An initial sound study (*CNEL Sound Study: Final Report*, dated 01/07/2021) was issued by MK Design, and a subsequent peer review by MD Acoustics (*4801 Laurel Canyon Car Wash – Acoustical Peer Review – Los Angeles, CA*, dated 05/17/2021) identified issues with the initial study's methodology and conclusions. The goal of this study is to address these concerns; providing an updated, thorough, and complete Environmental Noise Impact Study satisfying all code requirements.

## C. RESPONSES TO PEER REVIEW COMMENTS

Comments from MD Acoustics' peer review (5/17/21) of the Project's original noise impact study (1/7/21) are reproduced below, with Omny's response following each. To the best of our knowledge, Omny has performed

this new impact study in a manner satisfying all comments below, and to an overall level satisfactory to MD Acoustics' peer review.

### 7.0 Methodology

#### *Pg. 10*

1. Noise measurements were taken at a height of 36"-48" above finish grade. Section 111.02(a) of the Los Angeles Municipal Code states that the microphone shall be located four to five feet above the ground. Subsequently, the noise model should be calculated four to five feet above the ground.

**Omny Comments:** All noise modeling in this updated study utilizes a receiver elevation of 1.5 meters (approximately 4'11"), complying with the 4-5 foot requirement. While long-term unmanned measurements necessitated higher meter elevations to prevent tampering, concurrent manned measurements were conducted at the 1.5m elevation to make the appropriate height adjustments. See Section H of this report.

2. The baseline measurements were based off 60 second samples. Section 111.01(a) of the Los Angeles Municipal Code states that ambient noise shall be averaged over a period of at least 15 minutes.

**Omny Comments:** Baseline ambient measurements were taken at one-second intervals, but were used to calculate hourly metrics (60-minute Leq's and percentiles) for the purposes of this study. See section H.1 for additional details.

3. Streets and roads were modelled as a "uniform sound wall" of 60 dBA or 70 dBA. It is unclear how the propagation of street noise was calculated. Line sources such as roads should have a 3 dB drop-off over a doubling of distance. There is no distance given for the 60 dBA and 70 dBA assumptions or reference for these levels. The modeling approach is unclear.

**Omny Comments:** Road noise in Omny's modeling was calculated via standard line sources (one per lane of traffic, with equal sound power magnitudes assumed between all lanes of the same street). Sound power magnitudes were specified in units of sound power (dB re:1pW RMS) per meter, and were determined from the results of our 72-hour noise survey at the Project site. See Section H.2 of this report for additional details.

#### *Pg. 11*

4. Peak is defined in the report as measurements of short duration. Peak sound pressure is a specific metric which is not generally analyzed for environmental noise. It appears that in this report, peak refers to the Leq levels measured during the loudest minute of the car wash cycle. The metrics should be more clearly defined to determine what each calculation and measurement is referring to.

**Omny Comments:** Omny has made efforts to increase clarity in this updated analysis. It is presented in terms of equivalent (Leq) and percentile (LN) metrics as defined in the codes and ordinances referenced.

5. Ambient Noise Level is defined in the report as levels averaged across 15 minutes. As stated in Section 111.01(a) of the Los Angeles Code, ambient noise is taken exclusive of the particular noise sources to be measured. The report includes the operational noise within the ambient noise levels when only the existing road noise should be included. The metrics should be more clearly defined to determine what each calculation and measurement is referring to.

**Omny Comments:** The *Los Angeles Municipal Code* defines Ambient Noise as being “averaged over a period of at least 15 minutes.” This analysis uses hourly metrics, which are compliant with this definition. Our updated analysis also removes current operational noise from baseline ambient sound levels. See section I.1 for details.

6. The equation presented appears to linearly average sound levels when they should be logarithmically averaged.

**Omny Comments:** All averages and sums of decibel values are performed logarithmically in this updated study.

## 8.2 Samples Tables

### **Pg.13**

7. It appears that a sound level meter was not used which satisfies the requirements for a Type S2A meter as defined by ANSI S1.4. This type of meter is required by Section 111.01(l) of the Los Angeles Municipal Code. It appears that a sound level meter app and internal microphone was used which is insufficient at performing baseline readings as there is no method to calibrate an internal phone mic. ANSI standards are clear and specific on the use of pistonphone calibrators and use on Type 2 (or higher) hardware.

**Omny Comments:** All sound level meters and calibrators used during this updated study are Type-2 / Class-2 instruments satisfying the equipment accuracy requirements of *LACCO* §12.08.340, *LAMC* §111.01(L), *LAMC* §111.01(H) [where applicable], and all ANSI standards referenced therein.

8. The metric of CNEL is mentioned in this section and throughout the report, but CNEL levels are never calculated or presented.

**Omny Comments:** As seen in Section I.2 and Table 8 of this report, CNEL levels are now calculated at all property line receptors for all Project sources.



### 9.1 Traffic Level Description

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9. There is no reference or distance given for the expected sound levels given based on traffic flow. It appears to be based on sampled data from Location 3. When comparing Location 3 traffic levels versus sound levels, this methodology is anywhere from 0.27 to 6.33 dB off from the measured data. In traffic noise, a difference of 6 dB corresponds to a quadrupling of cars. The standard Federal Highway Administration's Traffic Noise Model or a similar established standard should be used for predicting noise from traffic.

**Omny Comments:** This updated analysis uses site-specific traffic noise levels, measured directly during Omny's 72-hour site noise survey.

### 11.1 Traffic Level Description

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10. There is no source given for the blower and vacuum data. It is unknown if these levels refer to sound pressure or sound power levels. No distance is given in relation to these sources. It is not indicated if the reference level is for the vacuum nozzle or turbine. Blower noise levels can vary greatly depending on the manufacturer, so it is important to note which blowers are being modeled. It is also unknown how many blowers have been modeled and what height they are modeled at. These noise sources are the main noise producers on the project site and there is no way to determine if these noise sources were properly evaluated as no reference level has been provided.

**Omny Comments:** Omny has obtained manufacturer-provided sound data for the car wash blower system (four blowers in total, measured as a single composite unit), which has been used in our sound modeling. No data has been provided for the vacuums, so Omny has used a conservative value of 90 dBA at one meter, free-field, for combined turbine and nozzle noise (approx. 91 dBA PWL modeled as a point source). This is based on previous dBA readings taken at similar vacuum systems in the past. See Section I.2 for additional information on blower and vacuum modeling.

### 11.2 Noise Criteria

11. There is no mention of the Los Angeles Municipal Code, the Land Use Noise Compatibility Matrix, or CEQA in this section. These must be addressed as a part of a Los Angeles noise study. The CEQA assessment should include an analysis of both construction and operational noise. Construction noise is never mentioned in the report.

**Omny Comments:** All applicable codes and ordinances are referenced and discussed in this updated analysis. Construction noise is not a part of the current study's scope, although general information and guidelines for construction noise have been included in Section J of this report.

### 11.3 Proposed Site Plan – With implemented Regulatory Compliance Measures, Peak Noise Levels

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12. The proposed 6' CMU wall appears to have a noise reduction of 50 dB which is a significant over assumption. Assuming a height of 3 ft for the vacuum, this wall would have a maximum reduction of 14 dB (See Appendix A). It appears that the STC rating of the wall was used to calculate overall reduction, which does not take into account sound that travels above the wall. More information on how to determine noise exposure based on distance and barrier insertion loss can be found within Federal Transit Authority Manual (Table 4-28) and HUD calculator.

**Omny Comments:** This updated analysis utilizes NoiseTools.net sound modeling by MAS Environmental, which calculates barrier attenuation according to ISO-9613-2 methods.

### 12.0 Recommendations

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13. The report states that 75 dBA is normally considered the upper threshold of acceptable noise at commercial structures, but no source is given. The threshold for powered equipment within 500 ft of a residence is given in Section 112.04 of the Los Angeles Municipal Code. Machinery may not cause the noise level on the premises of any other occupied property to exceed the ambient noise level by more than 5 decibels. CEQA also requires that operational noise must not increase by 3 dBA to or within the normally or clearly unacceptable categories or 5 dBA or greater. The report does not calculate the overall increase from the existing condition or the existing and operational CNEL levels.

**Omny Comments:** A total level of no more than 5.0 dBA above ambient allows for an equipment level that is up to approximately 3.3 dBA higher than the ambient level, as  $10^*log_{10}(10^{L/10}+10^{(L+3.3)/10}) \approx L + 5$ , for any hypothetical level "L." By ensuring that a Project noise source operating no more than "N" percent of any one hour does not exceed the corresponding "LN" percentile ambient level, the above ambient-plus-five condition is met automatically for summed ambient and equipment noise.

14. The report does not clearly label the receptors at the commercial and residential sites or address all surrounding sites.

**Omny Comments:** Receptor locations under each individual sound model are taken as being the point on the property line (at 1.5m elevation) having the greatest Project noise impact. When said location is occupied by one or more barrier wall(s), the location 1.0m away from the wall(s) on the receiving property's side is used. Exact locations of maximum impact along each property line vary slightly between models, but the results compared in this report are always those at the most-impacted point under each modeled condition. See Section H.2 for additional details.

### 13.0 Conclusions

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- 15. It is unclear in this section how the report compares the operational noise levels to the existing condition. The report does not specify at which receptor the operational levels are averaged with the measured existing levels.**

**Omny Comments:** This report aims to clarify confusion on this matter, and to more clearly compare modeled Project-generated noise levels against existing ambient levels.

- 16. Most of the measured existing levels (8/10) already exceed the Minimum Ambient Noise Level given by Section 111.03 of the Municipal Code. It is unclear what regulation the noise levels comply with.**

**Omny Comments:** This report aims to clarify confusion on this matter, and to more clearly compare modeled Project noise levels against existing ambient levels.

- 17. The existing measurement in Location 5 was 65.8 dBA. The calculated noise levels at that location were 74 dBA. Adding that to the existing condition gives an operational maximum condition of 74.6 dBA. Assuming a condition of 65.8 dBA for 10 minutes and 74.6 dBA for 5 minutes as assumed in the report, the overall operational Leq level is 70.9 dBA, which is 5.1 dB over the existing condition. This calculation, however, cannot be verified as the assumptions used (which have been taken from the report) were not obtained by the required methods and standards.**

**Omny Comments:** This updated study compares the highest modeled Project noise impacts along each property line directly against baseline ambient levels at the same locations. Percentile “LN” levels are used to describe ambient conditions in this analysis, in order to suppress the effects of short-term high-level noises occurring regularly at the site, which could otherwise raise measured ambient levels unreasonably high under an “Leq” interpretation. In the case where a particular source will operate no more than “N” percent (cumulative) of any one-hour period, it is compared against the corresponding “LN” ambient level. By ensuring that a Project noise source operating no more than “N” percent of any one-hour period does not exceed the corresponding “LN” percentile ambient level of that same period, the ambient-plus-five (Leq) condition is met automatically.

## **D. SITE LOCATION, LAYOUT, AND ZONING**

The 4801 Laurel Canyon Boulevard site is located on the Northwest corner of Laurel Canyon Boulevard and Riverside Drive. According to the *North Hollywood – Valley Village General Plan Land Use Map (2009)*, the site is categorized as a Neighborhood Commercial zone. It is bordered by other similarly-zoned properties as well as one Low-Medium Density Residential property. Layout of the Project site (current “as-built”) is illustrated in Figure 1 and Figure 2, with a zoning map of the site and surroundings provided in Figure 3.

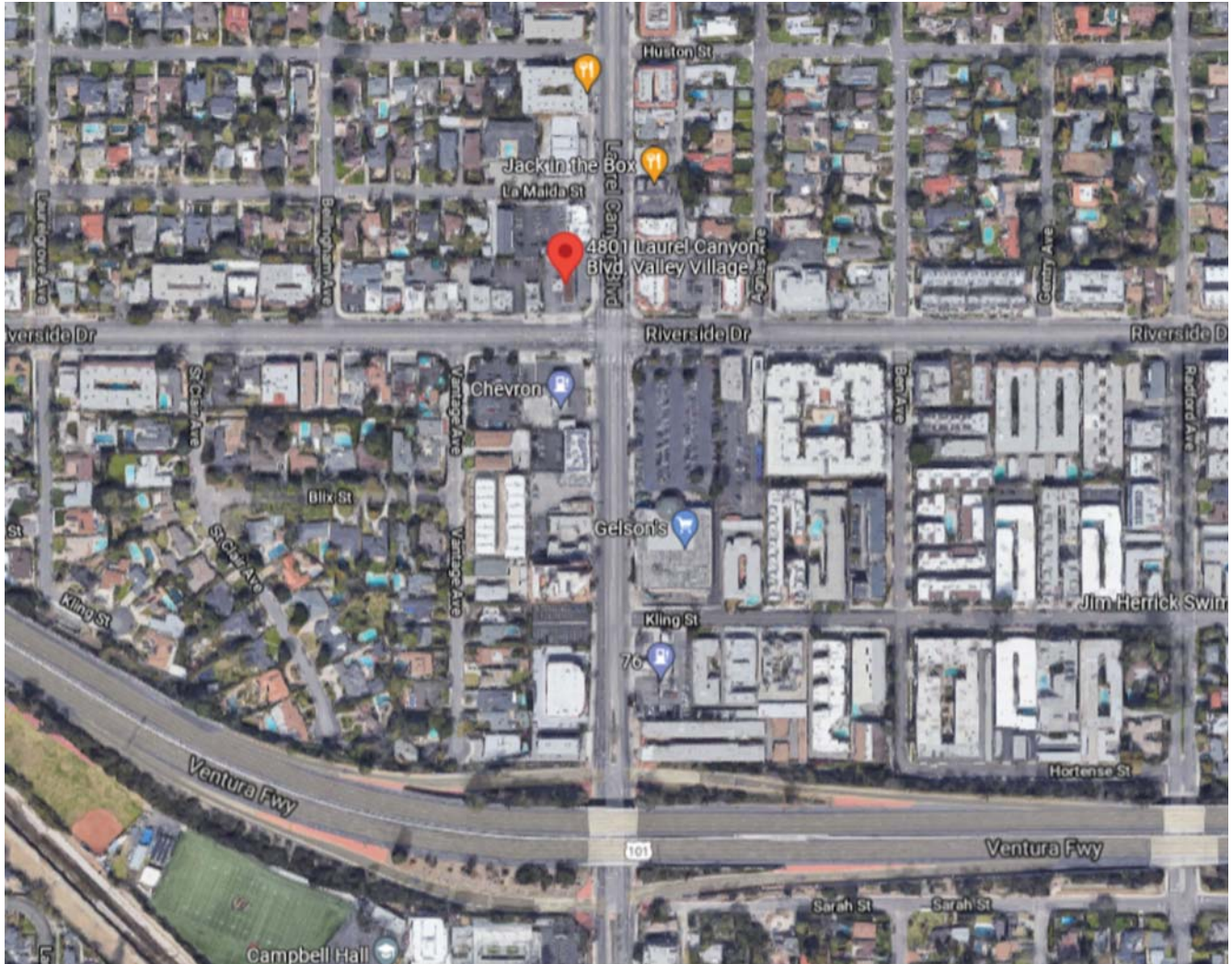


Figure 1: 4801 Laurel Canyon Blvd. Site (red marker) and general vicinity, with all major ambient noise sources included.

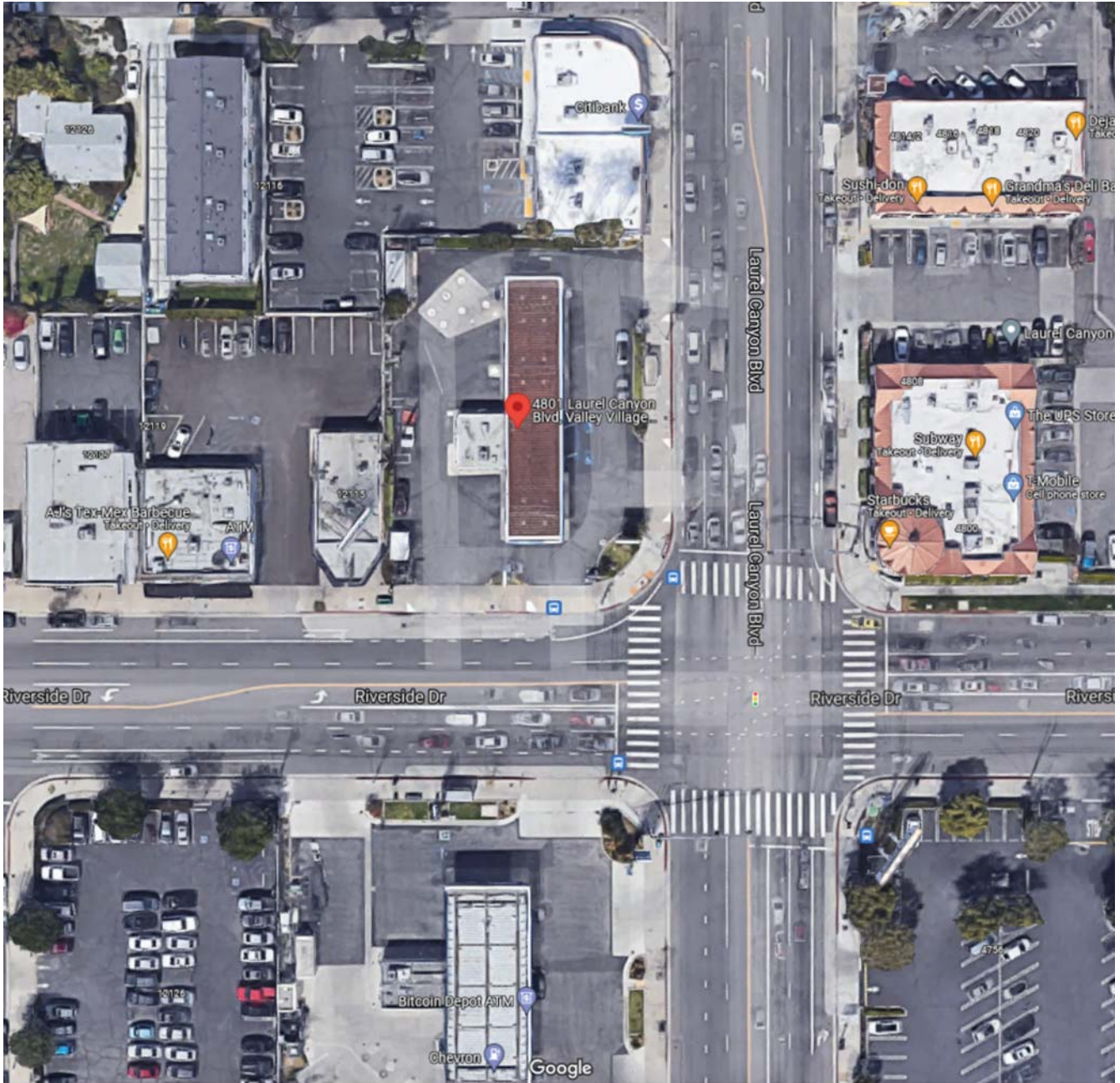


Figure 2: 4801 Laurel Canyon Blvd. site (red marker) and its direct adjacencies.

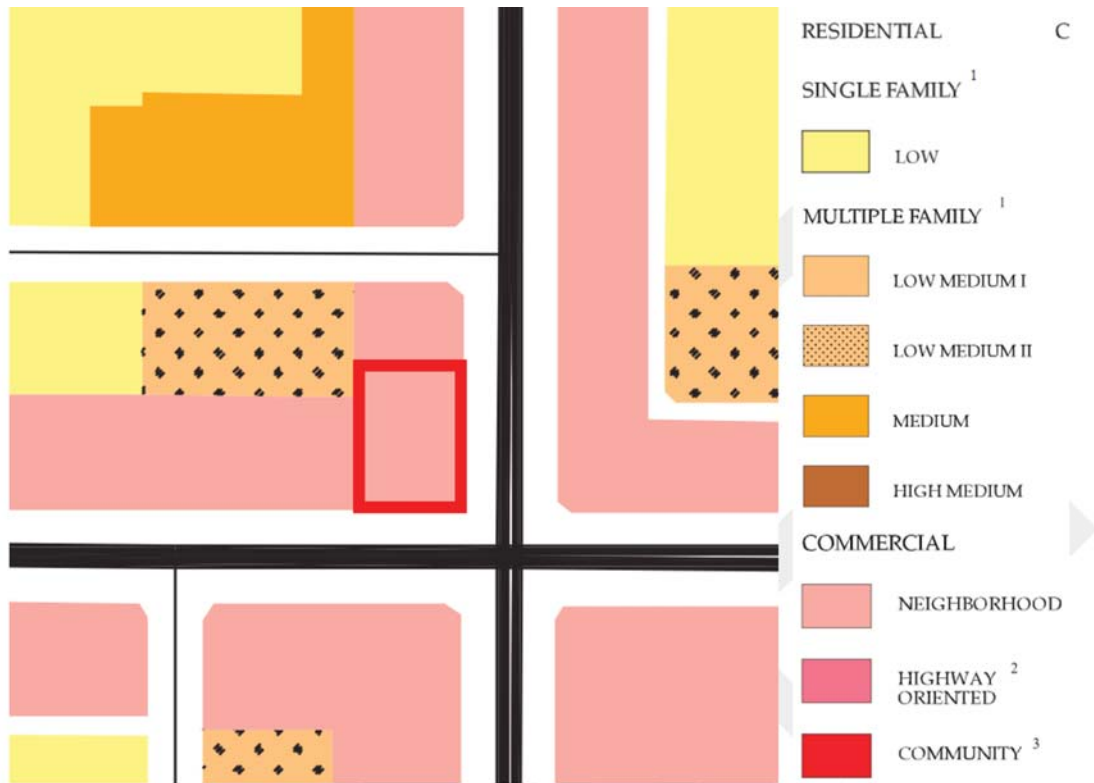


Figure 3: Land-use zoning map of 4801 Laurel Canyon Blvd. project site (red outline) and its immediate surroundings.

The relevant noise impacts generated by the Project will be those at the following neighboring properties. These property-line receiver locations are used as the primary subject of Omny’s acoustical analysis for the remainder of this report.

- Townhouses at 12120 La Maida Street – low/medium residential property located Northwest of Project site; townhouse parking lot shares short section of Project’s current West-facing 6-foot barrier wall.
- Citibank at 4821 Laurel Canyon Boulevard – neighborhood commercial property located North of Project site; bank building and parking lot share entire length of Project’s Northern property line and current North-facing 4-foot barrier wall.
- Summit Kleanerette at 12115 Riverside Drive – neighborhood commercial property located West of Project site; building and parking lot share majority of Project site’s Western property line and current West-facing 6-foot barrier wall.
- Chevron gas station at 4757 Laurel Canyon Boulevard – neighborhood commercial property located across Riverside Drive from Project and constituting Project’s nearest Southern adjacency.
- Shopping Center at 4800-4808 Laurel Canyon Boulevard – neighborhood commercial property located across Laurel Canyon Boulevard from Project and constituting Project’s nearest Eastern adjacency.

The Project site's entire North- and West-facing property lines – where such lines are not occupied by the facades of neighboring buildings – are covered by solid concrete masonry unit (CMU) barrier walls. The West-facing wall is an approximate height of 1.8m (6'0"), while the North-facing wall is an approximate height of 1.2m (4'0"). These walls currently serve as partial acoustical screens between receptor properties and Project-generated noise sources. Project drawings include plans to raise each of these walls by two feet (0.6m), raising the West-facing wall to a height of 2.4m (8'0") and the North-facing wall to a height of 1.8m (6'0"). This will provide additional acoustical screening beyond what exists at the current Project site.

## E. EXISTING AMBIENT NOISE SOURCES

The primary sources of existing ambient noise at the Project site are the vehicle traffic on its two major cross streets, which dominates the outdoor sound field at the site for the vast majority of the day. The Ventura Freeway (US-101, located approximately 350m to the South) is the site's secondary source, with near-constant distant traffic noise being audible from the freeway throughout the day and night.

Tertiary sound sources are stops on local bus lines and commuter airplane flyovers. The site includes two Metro bus stations – one on the East side of the property along Laurel Canyon Boulevard, and one on the South side along Riverside Drive – both of which provide significant short but periodically-repeating noise level contributions. Airplane flyovers are frequent and clearly audible, though they do not typically contribute noise levels comparable to street or freeway sources. The site is located in the vicinity of three small airports – Hollywood Burbank Airport approximately 5km away, Van Nuys Airport approximately 10km away, and Whiteman Airport approximately 11km away – but is not located within or near the 65dBA CNEL contours of any of the three Airports.

## F. REMODEL AND CAR WASH TUNNEL ADDITION

The Nader's Mobil gas station is proposing a remodel, the two primary acoustical concerns of which are the future noise generated by (1) new coin-operated car vacuum cleaners to be installed near the Project site's Northwest corner and (2) new hot-air dryers (system of four) to be installed just inside the exit door of a new automated tunnel car wash system. According to Project plans and product submittals, the car vacuum cleaners will be equipped with a sound dampener system, and the hot-air dryers have the potential to be equipped with full-cover surrounds and/or silencer cones.

This report examines the predicted sound fields generated by these individual and combined noise sources, comparing them against the existing ambient levels (excluding current gas station contributions) determined in Omny's site noise survey.

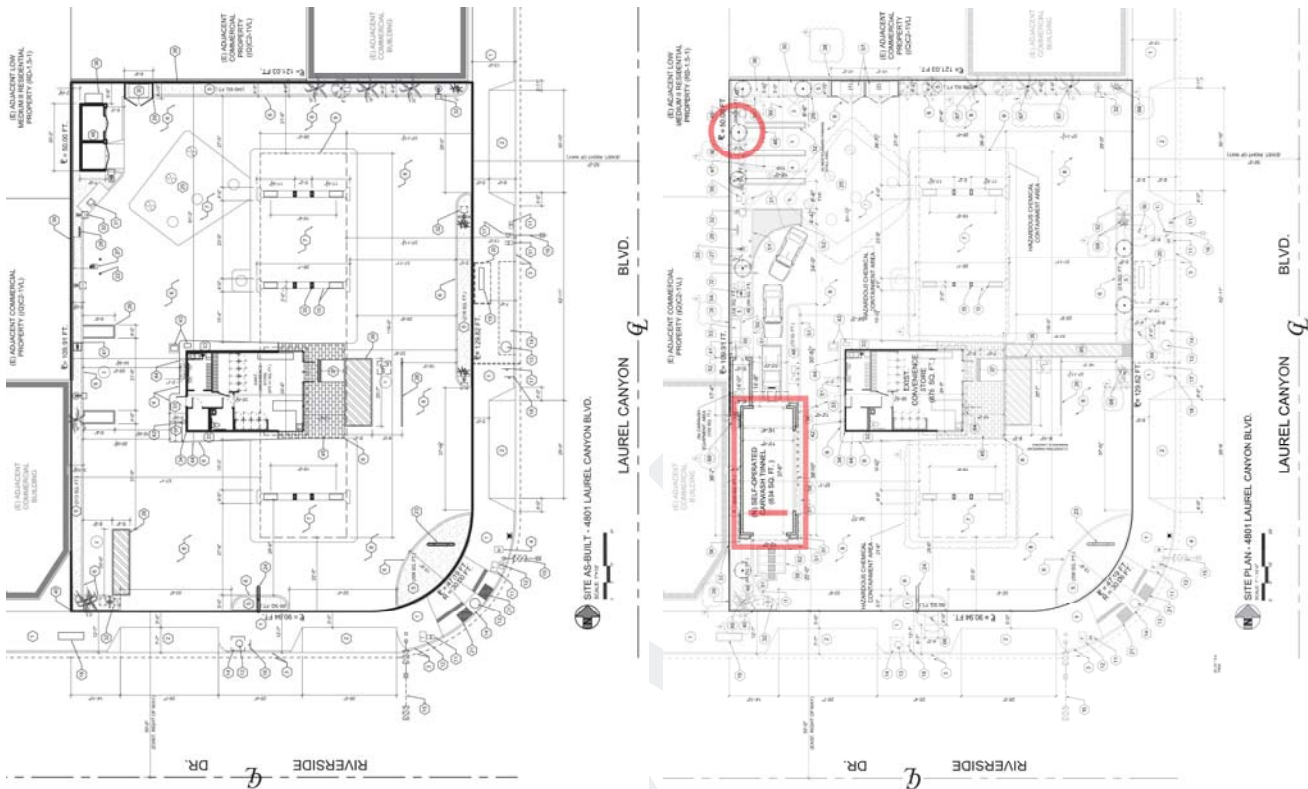


Figure 4: Project property as-built drawings (left, Drawing AB.1, 1/6/2021) vs. proposed site plan (right, Drawing A1.0, 11/9/2021) with new vacuum cleaner, car wash tunnel, and air-dryer locations indicated in red.

## G. CODES AND ORDINANCES

This section identifies and summarizes all known acoustical codes and ordinances which may be applicable to the Project. Relevant sections from each code document are summarized in the following subsections, and their composite requirements are detailed in Subsection G.5. Project-relevant text from each document is also directly quoted in Appendix A for reference.

### 1. LOS ANGELES COUNTY CODE OF ORDINANCES

Chapter 12.08 (“Noise Control”) of the *Los Angeles County Code of Ordinances (LACCO)* allows a project to produce noise levels up to the following at neighboring property lines. Allowed noise levels are dependent upon the type of receptor property, the time of day, the existing ambient noise levels, and the cumulative duration of the noise produced by the Project.



Receptor property type:	Time of day:	LACCO Ch. 12.08 project noise limit:				
		30-60 min/hr:	15-30 min/hr:	5-15 min/hr:	1-5 min/hr:	< 1 min/hr:
Noise-sensitive area	24 hours	45 dBA or ambient L50 <sup>1</sup>	50 dBA or ambient L25	55 dBA or ambient L8.3	60 dBA or ambient L1.7	65 dBA or ambient L0
Residential property	7:00 AM – 10:00 PM	50 dBA or ambient L50	55 dBA or ambient L25	60 dBA or ambient L8.3	65 dBA or ambient L1.7	70 dBA or ambient L0
	10:00 PM – 7:00 AM	45 dBA or ambient L50	50 dBA or ambient L25	55 dBA or ambient L8.3	60 dBA or ambient L1.7	65 dBA or ambient L0
Commercial property	7:00 AM – 10:00 PM	60 dBA or ambient L50	65 dBA or ambient L25	70 dBA or ambient L8.3	75 dBA or ambient L1.7	80 dBA or ambient L0
	10:00 PM – 7:00 AM	55 dBA or ambient L50	60 dBA or ambient L25	65 dBA or ambient L8.3	70 dBA or ambient L1.7	75 dBA or ambient L0
Industrial property	24 hours	70 dBA or ambient L50	75 dBA or ambient L25	80 dBA or ambient L8.3	85 dBA or ambient L1.7	90 dBA or ambient L0

**Table 1: Project noise limits summarized from Chapter 12.08 of the Los Angeles County Code of Ordinances.**

As can be seen in Table 1 above, the LACCO noise ordinance includes provisions for projects in areas having high ambient noise levels. In any case where the existing ambient noise levels (those when no Project noise sources are active) exceed a prescribed noise limit, the limit is replaced by the ambient level exceeded during the same cumulative portion of any one-hour period.

According to the *North Hollywood – Valley Village General Plan Land Use Map (2009)*, receptor properties in the vicinity of Nader’s Mobil include only residential and commercial properties.

## 2. LOS ANGELES MUNICIPAL CODE

Chapter XI (“Noise Regulation”) of the *Los Angeles Municipal Code (LAMC)* allows a project to produce noise levels up to the following at neighboring property lines, as detailed in Table 2. As with those of LACCO Ch. 12.08, LAMC Ch. XI noise limits are dependent upon receptor property type, time of day, and cumulative duration of project-generated noise.

<sup>1</sup> LN or L<sub>N</sub> (where N is a percentage value from 0 to 100) – The sound pressure level exceeded during N percent of a measurement period. For example, the L25 level of a 60-second acoustic measurement is the level which is exceeded for a total of 15 seconds (25%) of that measurement.

Receptor property type:	Time of day:	LAMC Ch. XI project noise limit:		
		15-60 min/hr:	5-15 min/hr:	< 5 min/hr:
A1, A2, RA, RE, RS, RD, RW1, RW2, R1, R2, R3, R4, and R5	7:00 AM – 10:00 PM	50 dBA or ambient L25	55 dBA or ambient L8.3	60 dBA or ambient L1.7
	10:00 PM – 7:00 AM	40 dBA or ambient L25	45 dBA or ambient L8.3	50 dBA or ambient L1.7
P, PB, CR, C1, C1.5, C2, C4, C5, and CM	7:00 AM – 10:00 PM	60 dBA or ambient L25	65 dBA or ambient L8.3	70 dBA or ambient L1.7
	10:00 PM – 7:00 AM	55 dBA or ambient L25	60 dBA or ambient L8.3	65 dBA or ambient L1.7
M1, MR1, and MR2	7:00 AM – 10:00 PM	60 dBA or ambient L25	65 dBA or ambient L8.3	70 dBA or ambient L1.7
	10:00 PM – 7:00 AM	55 dBA or ambient L25	60 dBA or ambient L8.3	65 dBA or ambient L1.7
M2 and M3	7:00 AM – 10:00 PM	65 dBA or ambient L25	70 dBA or ambient L8.3	75 dBA or ambient L1.7
	10:00 PM – 7:00 AM	65 dBA or ambient L25	70 dBA or ambient L8.3	75 dBA or ambient L1.7

**Table 2: Project noise limits summarized from Chapter XI of the Los Angeles Municipal Code.**

Unlike LACCO Ch. 12.08, LAMC Ch. XI does not explicitly specify nor define a sound pressure level (SPL) metric for use in determining baseline ambient noise levels, and thus requires some interpretation. Further, LAMC does not include provisions for cases with existing ambient levels above code-specified “minimum ambient” levels. Thus, Omny interprets LAMC ambient levels using the methods of LACCO, and considers it reasonable to adjust duration-dependent noise limits using the corresponding ambient percentile (LN) levels as done in Table 2.

According to the *North Hollywood – Valley Village General Plan Land Use Map (2009)*, receptor properties in the vicinity of Nader’s Mobil include low-density single-family residential (types RE9, RS, R1), low-medium-II multi-family residential (types RD2, RD1.5), medium-density multi-family residential (type R3), and neighborhood commercial (types CR, C1, C1.5, RAS3, RAS4.P).

### 3. LOS ANGELES GENERAL PLAN

Exhibit I (“Guidelines for Noise Compatible Land Use”) of the Noise Element of the *Los Angeles General Plan (LAGP)* gives the following guidelines for CNEL<sup>2</sup> noise levels deemed appropriate for various land uses. Although these levels are primarily used for determining suitability of sites for new construction, they can also be used to determine appropriate project CNEL noise limits. It should be a goal of the Project to prevent its noise impact from raising the CNEL at any point on a neighboring property to a level inappropriate for that property’s land use category.

#### Exhibit I: Guidelines for Noise Compatible Land Use

(Based on the Governor’s Office of Planning and Research, “General Plan Guidelines”, 1990. To help guide determination of appropriate land use and mitigation measures vis-a-vis existing or anticipated ambient noise levels)

Land Use Category	Day-Night Average Exterior Sound Level (CNEL dB)						
	50	55	60	65	70	75	80
Residential Single Family, Duplex, Mobile Home	A	C	C	C	N	U	U
Residential Multi-Family	A	A	C	C	N	U	U
Transient Lodging, Motel, Hotel	A	A	C	C	N	U	U
School, Library, Church, Hospital, Nursing Home	A	A	C	C	N	N	U
Auditorium, Concert Hall, Amphitheater	C	C	C	C/N	U	U	U
Sports Arena, Outdoor Spectator Sports	C	C	C	C	C/U	U	U
Playground, Neighborhood Park	A	A	A	A/N	N	N/U	U
Golf Course, Riding Stable, Water Recreation, Cemetery	A	A	A	A	N	A/N	U
Office Building, Business, Commercial, Professional	A	A	A	A/C	C	C/N	N
Agriculture, Industrial, Manufacturing, Utilities	A	A	A	A	A/C	C/N	N

A = Normally acceptable. Specified land use is satisfactory, based upon assumption buildings involved are conventional construction, without any special noise insulation.

C = Conditionally acceptable. New construction or development only after a detailed analysis of noise mitigation is made and needed noise insulation features are included in project design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning normally will suffice.

N = Normally unacceptable. New construction or development generally should be discouraged. A detailed analysis of noise reduction requirements must be made and noise insulation features included in the design of a project.

U = Clearly unacceptable. New construction or development generally should not be undertaken.

Table 3: CNEL land-use guidelines taken from Exhibit I of the Los Angeles General Plan Noise Element (1999).

**2 CNEL (Community Noise Equivalent Level)** – A 24-hour, A-weighted, time-weighted, equivalent (Leq) noise level. Levels occurring from 7:00 AM – 7:00 PM are unweighted; levels occurring from 7:00 PM – 10:00 PM are assigned a penalty of +5dBA; and levels occurring from 10:00 PM – 7:00 AM are assigned a penalty of +10dBA.

#### 4. CALIFORNIA ENVIRONMENTAL QUALITY ACT

Appendix G of the *California Environmental Quality Act (CEQA) Statute and Guidelines (2021 edition)* provides three checklist questions to be addressed when determining the environmental noise impact of a new building project or renovation. The impact study checklist questions pertaining to noise (Subsection XIII) are quoted as follows. (Note that the most recent version of the CEQA noise checklist questionnaire has been condensed, with redundant questions from previous versions having been removed.)

*XIII. NOISE. Would the project result in:*

- a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?*
- b) Generation of excessive groundborne vibration or groundborne noise levels?*
- c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?*

For each checklist question above, a determination of *Potentially Significant Impact, Less Than Significant [Impact] with Mitigation Incorporated, Less Than Significant Impact, or No Impact* may be made based on the results of an Environmental Noise Impact Study.

Chapter I.2 (“Operational Noise”), Section 2.A, of the *City of Los Angeles CEQA Thresholds Guide (2006)* provides the following significance threshold for a *Significant Impact* determination:

*A project would normally have a significant impact on noise levels from project operations if the project causes the ambient noise level measured at the property line of affected uses to increase by 3 dBA in CNEL to or within the “normally unacceptable” or “clearly unacceptable” category, or any 5 dBA [CNEL] or greater noise increase (see the chart below).*

The guide then provides the following CNEL land-use compatibility table (reproduced in Table 4), similar to that found in Exhibit I of the *Los Angeles General Plan Noise Element* (reproduced in Table 3).

Land Use	Community Noise Exposure CNEL, db			
	Normally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable
Single Family, Duplex, Mobile Homes	50 - 60	55 - 70	70 - 75	above 70
Multi-Family Homes	50 - 65	60 - 70	70 - 75	above 70
Schools, Libraries, Churches, Hospitals, Nursing Homes	50 - 70	60 - 70	70 - 80	above 80
Transient Lodging - Motels, Hotels	50 - 65	60 - 70	70 - 80	above 80
Auditoriums, Concert Halls, Amphitheaters	-	50 - 70	-	above 65
Sports Arena, Outdoor Spectator Sports	-	50 - 75	-	above 70
Playgrounds, Neighborhood Parks	50 - 70	-	67 - 75	above 72
Golf Courses, Riding Stables, Water Recreation, Cemeteries	50 - 75	-	70 - 80	above 80
Office Buildings, Business and Professional Commercial	50 - 70	67 - 77	above 75	-
Industrial, Manufacturing, Utilities, Agriculture	50 - 75	70 - 80	above 75	-

**Normally Acceptable:** Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction without any special noise insulation requirements.

**Conditionally Acceptable:** New construction or development should be undertaken only after a detailed analysis of the noise reduction requirements is made and needed noise insulation features included in the design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning will normally suffice.

**Normally Unacceptable:** New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of the noise reduction requirements must be made and needed noise insulation features included in the design.

**Clearly Unacceptable:** New construction or development should generally not be undertaken.

*Table 4: CNEL land-use compatibility table from Chapter I.2 of the City of Los Angeles CEQA Thresholds Guide.*

## 5. PROJECT-SPECIFIC NOISE LIMITS

Based on the code requirements summarized in Subsections G.1 through G.4, and based on the ambient noise survey conducted at the Project site (see Subsection I.1), Omny determines the following composite noise limits for the Nader's Mobil remodel and car wash addition, shown in Table 5 below.

Receptor property:	Time of day:	Composite noise limit (dBA re:20μPa RMS):				
		30-60 min/hr	15-30 min/hr	5-15 min/hr	1-5 min/hr	24-hour CNEL
Townhouses 12120 La Maida	Daytime (7am-10pm)	60	63	66	70	71
	Nighttime (10pm-7am)	55	58	61	66	
Citibank 4821 Laurel Canyon	Daytime (7am-10pm)	60	62	65	70	73
	Nighttime (10pm-7am)	56	57	60	65	
Summit Kleanerette 12115 Riverside	Daytime (7am-10pm)	60	63	66	70	71
	Nighttime (10pm-7am)	55	58	61	66	
Chevron 4757 Laurel Canyon	Daytime (7am-10pm)	68	70	72	77	86
	Nighttime (10pm-7am)	62	64	67	71	
Shopping Center 4800-4808 Laurel Canyon	Daytime (7am-10pm)	69	72	75	79	82
	Nighttime (10pm-7am)	62	66	70	74	

*Table 5: Composite limits for project-generated noise, categorized by receptor property. Composite limits are calculated based on LACCO, LAMC, LAGP, CEQA, and Omny Acoustics' site noise survey results.*

## H. METHODOLOGY

### 1. SITE NOISE SURVEY

To obtain accurate records of current ambient noise levels at the Project site, Omny performed a site noise survey from Tuesday, October 12<sup>th</sup>, 2021 through Friday, October 15<sup>th</sup>, 2021. Sound monitoring stations were installed at four locations on Project property lines, and these unmanned stations recorded sound pressure levels (SPLs) continuously for a period of just over 72 hours (three full days). Shorter-term manned measurements were also taken at these same locations, to measure the typical frequency content of ambient noise along each Project property line.

Monitoring locations are shown in Figure 5, with images of individual monitoring stations shown in Figure 6. Full measurement details for the survey are provided in Table 6.



Figure 5: 72-hour sound monitoring station locations along Project property lines.



Figure 6 (top-left to bottom-right): Ambient monitoring stations #1, #2, #3, and #4.



Location:	Measurement device:	Mounting description:	Duration:	Measurements taken:
<b>Station #1</b>	Reed SD-4023 Type-2 SLM (SN H342103)	Inside weatherproof case with windscreen, installed on Mobil sign near Riverside Drive along South property line, approx. 4.6m (15') above ground.	2021-10-12 14:19:07 – 2021-10-15 15:57:51	LAS every 1 second
	Casella CEL-63X Type-2 SLM (SN 0442238)	On tripod with windscreen, 1.5m (~4'11") above ground at bottom of Mobil sign.	2021-10-13 13:52:59 – 14:28:05	LAF, LAS, LAeq every 1 second; 1/3-octave LZeq every 1 minute
<b>Station #2</b>	Reed SD-4023 Type-2 SLM (SN H342115)	Inside weatherproof case with windscreen, installed on light pole near Laurel Canyon Boulevard along East property line, approx. 3.7m (12') above ground.	2021-10-12 15:03:21 – 2021-10-15 16:04:40	LAS every 1 second
	Casella CEL-63X Type-2 SLM (SN 0442238)	On tripod with windscreen, 1.5m (~4'11") above ground at bottom of light pole.	2021-10-13 14:33:58 – 15:06:08	LAF, LAS, LAeq every 1 second; 1/3-octave LZeq every 1 minute
<b>Station #3</b>	Casella CEL-63X Type-2 SLM (SN 0166060)	Inside weatherproof case with windscreen, installed on palm tree near Citibank building along North property line, approx. 5.5m (18') above ground.	2021-10-12 15:29:12 – 2021-10-15 16:11:31	LAS every 1 second
	Casella CEL-63X Type-2 SLM (SN 0442238)	On tripod with windscreen, 1.5m (~4'11") above ground at bottom of palm tree.	2021-10-13 15:10:58 – 15:45:03	LAF, LAS, LAeq every 1 second; 1/3-octave LZeq every 1 minute
<b>Station #4</b>	Reed SD-4023 Type-2 SLM (SN H339627)	Inside weatherproof case with windscreen, installed on light pole near Summit Kleanerette building along West property line, approx. 3.7m (12') above ground.	2021-10-12 16:00:34 – 2021-10-15 16:18:47	LAS every 1 second
	Casella CEL-63X Type-2 SLM (SN 0442238)	On tripod with windscreen, 1.5m (~4'11") above ground at bottom of light pole.	2021-10-13 15:49:59 – 16:22:03	LAF, LAS, LAeq every 1 second; 1/3-octave LZeq every 1 minute

Table 6: Summary of all on-site measurements performed for ambient noise survey at Project site.

All sound level meters used during the ambient noise survey are Type-2 / Class-2 meters satisfying the equipment accuracy requirements of LACCO §12.08.340, LAMC §111.01(L), LAMC §111.01(H) [where applicable], and the ANSI standards referenced therein. All one-second measurements were taken using a “slow response” measurement (exponential time weighting with one-second constant) and the “A” frequency weighting, matching the LACCO and LAMC definitions used for determining both ambient and intrusive noise levels.

All sound level meters were field-calibrated directly prior to measurement using either a Reed R8090 Type-2 calibrator (SN 190918598) at 94.0 dB for the Reed SD-4023 meters or a Casella CEL-120/2 Type-2 calibrator (SN 1012428) at 114.0 dB for the Casella CEL-63X meters. Installed 72-hour logging meters were checked and recalibrated once per day to account for any calibration drift; short-term measurement meters were recalibrated prior to measurement at each new location.

Hourly equivalent<sup>3</sup> A-weighted (LAeq) levels and hourly noise level percentiles (LN) were calculated from this collected one-second (LAS) data. Hourly data was then used to calibrate our sound propagation model (see next subsection), and was also used to compute ambient-dependent property-line noise limits for the Project. Since unattended logging meters needed to be installed at high elevations to prevent tampering (as the current gas station was fully operational during the study), short-term attended ground-level (1.5m) measurements were used to adjust all high-elevation measurements to the code-required elevation range.

## 2. SOUND MODELING

Using measured sound pressure levels collected as described in the previous subsection, Omny has created a sound propagation model of the Project site to both fully model ambient site conditions and to predict future post-remodel environmental noise impacts. Equivalent and statistical noise levels were computed for each one-hour period, and for each one-octave frequency band<sup>4</sup>, and were used to decompose the measured sound field into the following predominant noise sources: (1) Riverside Drive traffic, (2) Laurel Canyon Boulevard traffic, (3) Ventura Freeway and other distant area traffic, and (4) on-site gas station customer traffic. Propagation models and parameters were then adjusted to obtain the best possible fit between computer model and in-field measurements. Generated sound maps of both ambient and predicted post-remodel noise conditions are shown in Section I of this report.

Receptor locations under each sound model are taken as the point on each property line (at 1.5m elevation) having the greatest Project noise impact. When said location is occupied by one or more barrier walls, the location 1.0m away from the wall(s) on the receptor property side is used. Exact locations of maximum impact along each property line vary slightly between models, but the results compared in this report are always those at the most impacted point under each modeled condition.

---

**3 Equivalent Noise Level (Leq or LAeq)** – The decibel level of a constant noise containing the same total acoustic energy as a fluctuating noise measured over the same time period.

**4 One-Octave Band** – A range of frequencies defined by a center frequency, a lower frequency, and an upper frequency; such that the upper frequency is twice the lower frequency, and the center frequency is the geometric mean of the lower and upper frequencies. The range of human hearing is typically analyzed in ten (10) one-octave frequency bands covering the approximate range of 20 Hz to 20 kHz.

## I. IMPACT STUDY RESULTS

### 1. AMBIENT SOUND FIELD

Hourly statistical breakdowns of measured A-weighted sound pressure levels at each measurement station are shown in Figure 7 and Figure 8, with equivalent levels shown in Figure 9. All percentile levels are shown on the same time and decibel scale for ease of comparison.

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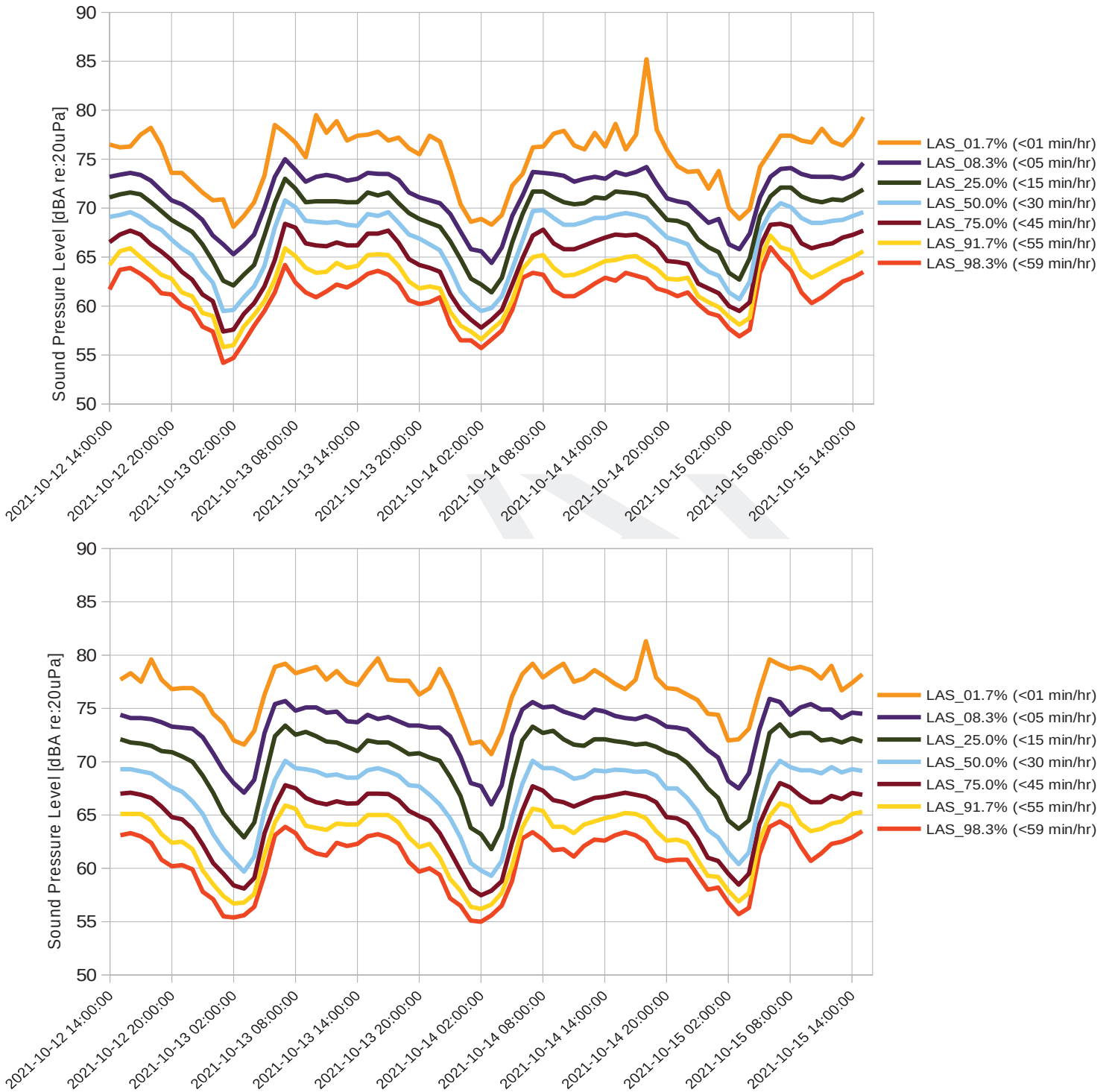


Figure 7: Hourly statistical (LN) A-weighted ambient noise levels measured at Station #1 (top) and Station #2 (bottom).

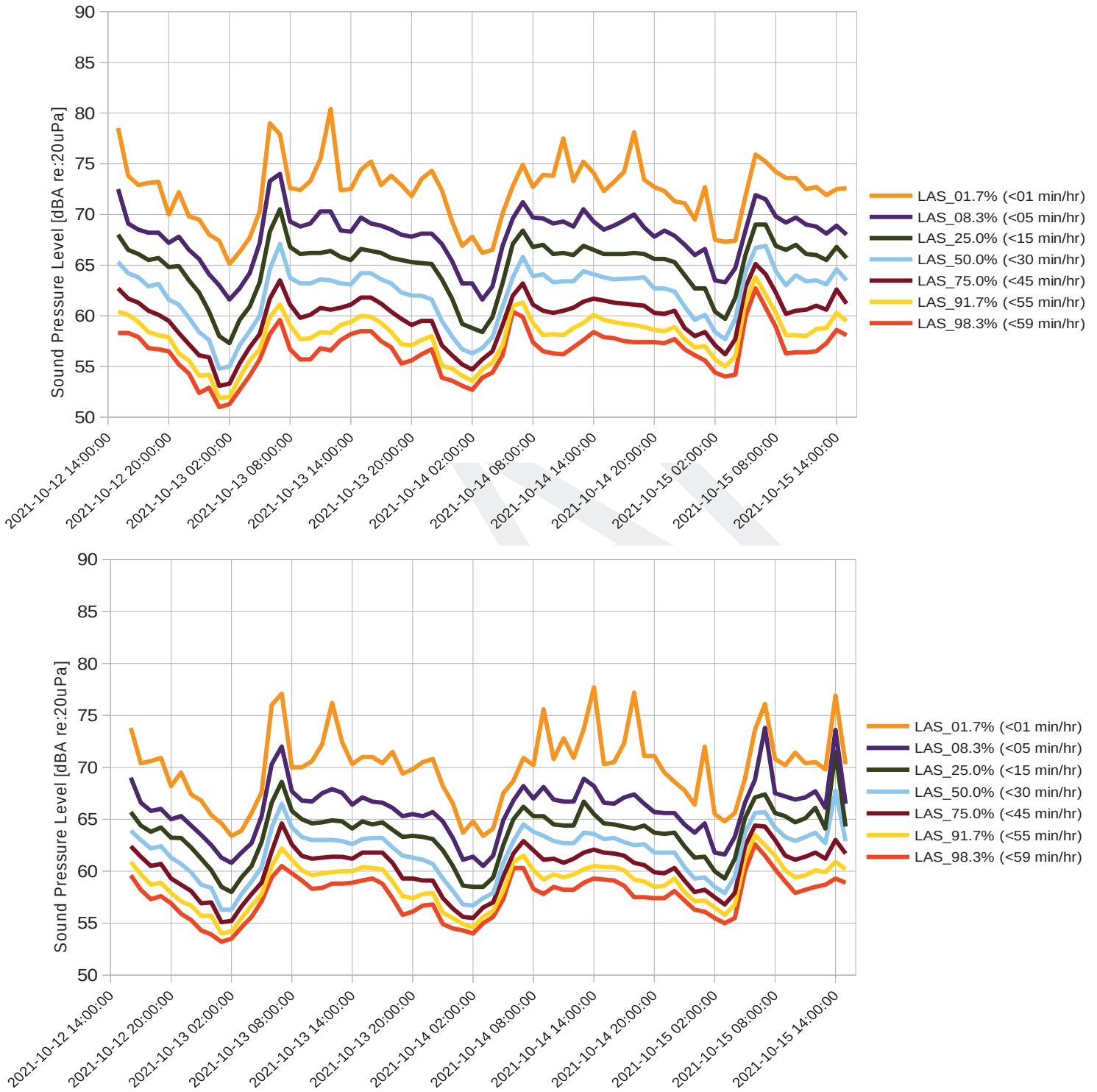


Figure 8: Hourly statistical (LN) A-weighted ambient noise levels measured at Station #3 (top) and Station #4 (bottom).

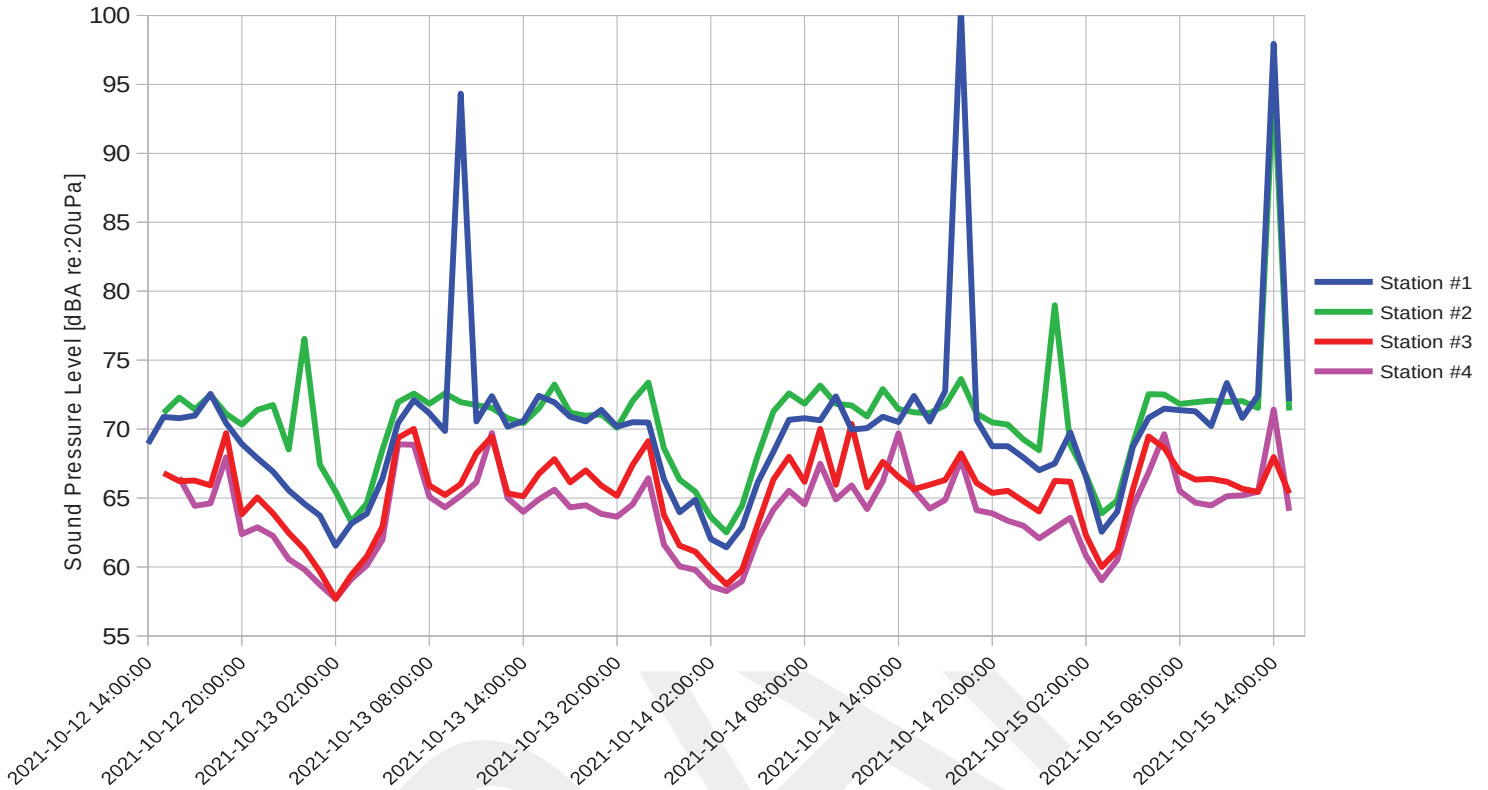


Figure 9: Hourly equivalent A-weighted (LAeq,1hr) ambient levels measured at 72-hour monitoring stations. High hourly equivalent levels at Station #1 are believed to be caused by loud braking of either Metro busses or garbage trucks, both of which were observed frequently on Riverside Drive.

Based on measured data shown in the previous plots, Omny calculates the following breakdown of typical ambient noise levels occurring at each property line (calculated/modeled at the points of greatest Project impact). Note that these levels are identical to those of Table 5 found in Section G.5 of this report.

Property line:	Time:	30-60 min/hr	15-30 min/hr	5-15 min/hr	1-5 min/hr	24-hour CNEL
Townhouses 12120 La Maida St.	Day	60	63	66	70	71
	Night	55	58	61	66	
Citibank 4821 Laurel Canyon Blvd.	Day	60	62	65	70	73
	Night	56	57	60	65	
Summit Kleanerette 12115 Riverside Dr.	Day	60	63	66	70	71
	Night	55	58	61	66	
Chevron 4757 Laurel Canyon Blvd.	Day	68	70	72	77	86
	Night	62	64	67	71	
Shopping Center 4800-4808 Laurel Cyn. Blvd.	Day	69	72	75	79	82
	Night	62	66	70	74	

Table 7: Baseline ambient sound pressure levels (dBA) at points of highest Project impact along each property line.

Due to levels varying greatly throughout any typical operation day, the levels summarized in Table 7 are calculated using the typical lowest daytime and nighttime hourly statistical (LN) sound levels. Since the time of day at which the lowest levels occurs varies somewhat from day to day, the 3-hour daytime period from 7:00 AM – 10:00 AM and the 3-hour nighttime period from 1:00 AM – 4:00 AM have been used to calculate these levels, allowing a conservative calculation of a typical on-site ambient level distribution.

Even conservative selection of time periods results in the existing on-site ambient noise levels controlling the Project-generated noise limits, as they are higher than the “minimum ambient” levels prescribed in the relevant noise codes during all hours of the day. Ambient noise maps of the Project site and surrounding area are shown in Figure 10 (nighttime L50, dBA) and Figure 11 (24-hr CNEL, dBA) respectively.

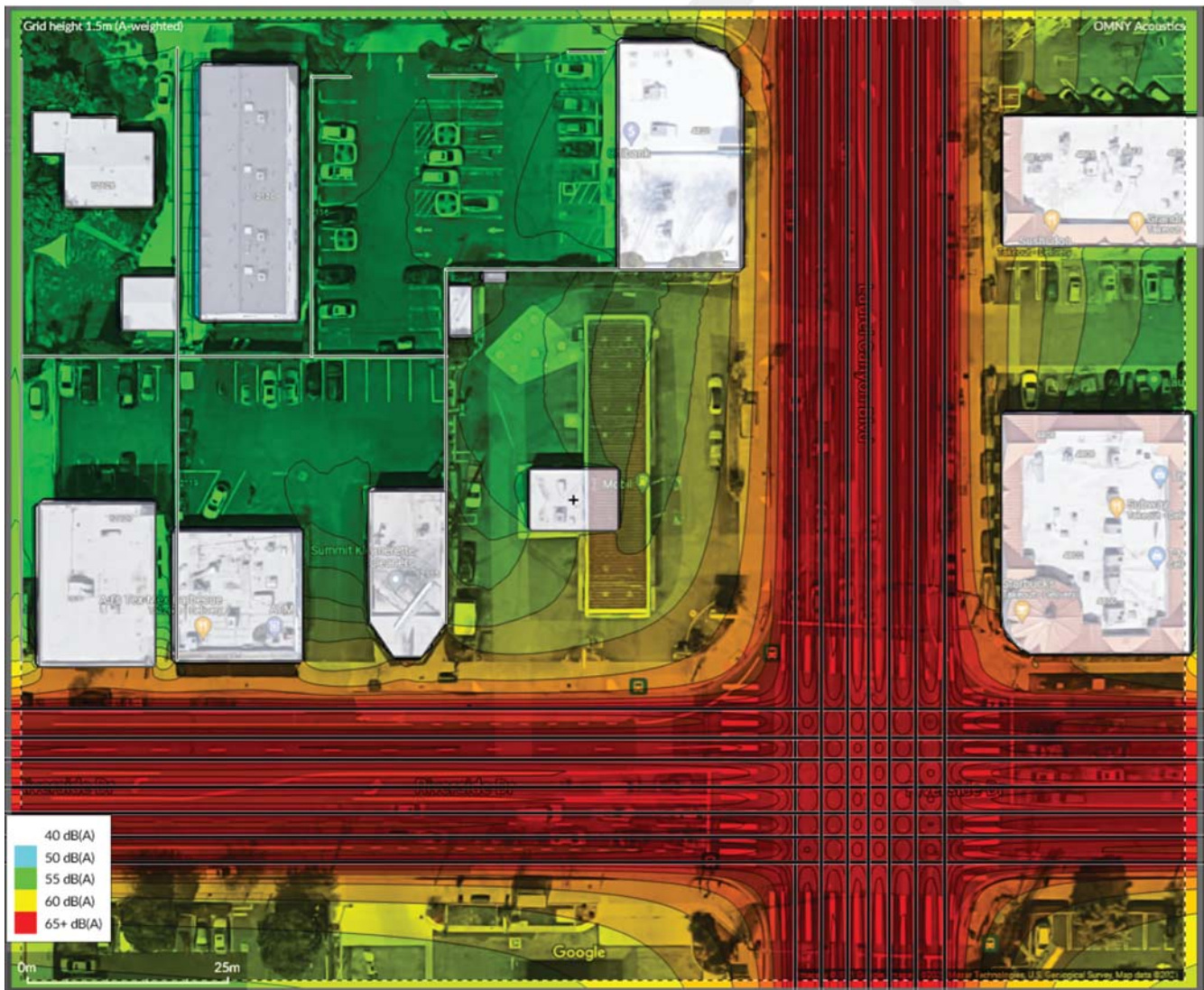


Figure 10: Contour map (1dBA contour lines) of typical nighttime L50 ambient noise levels.

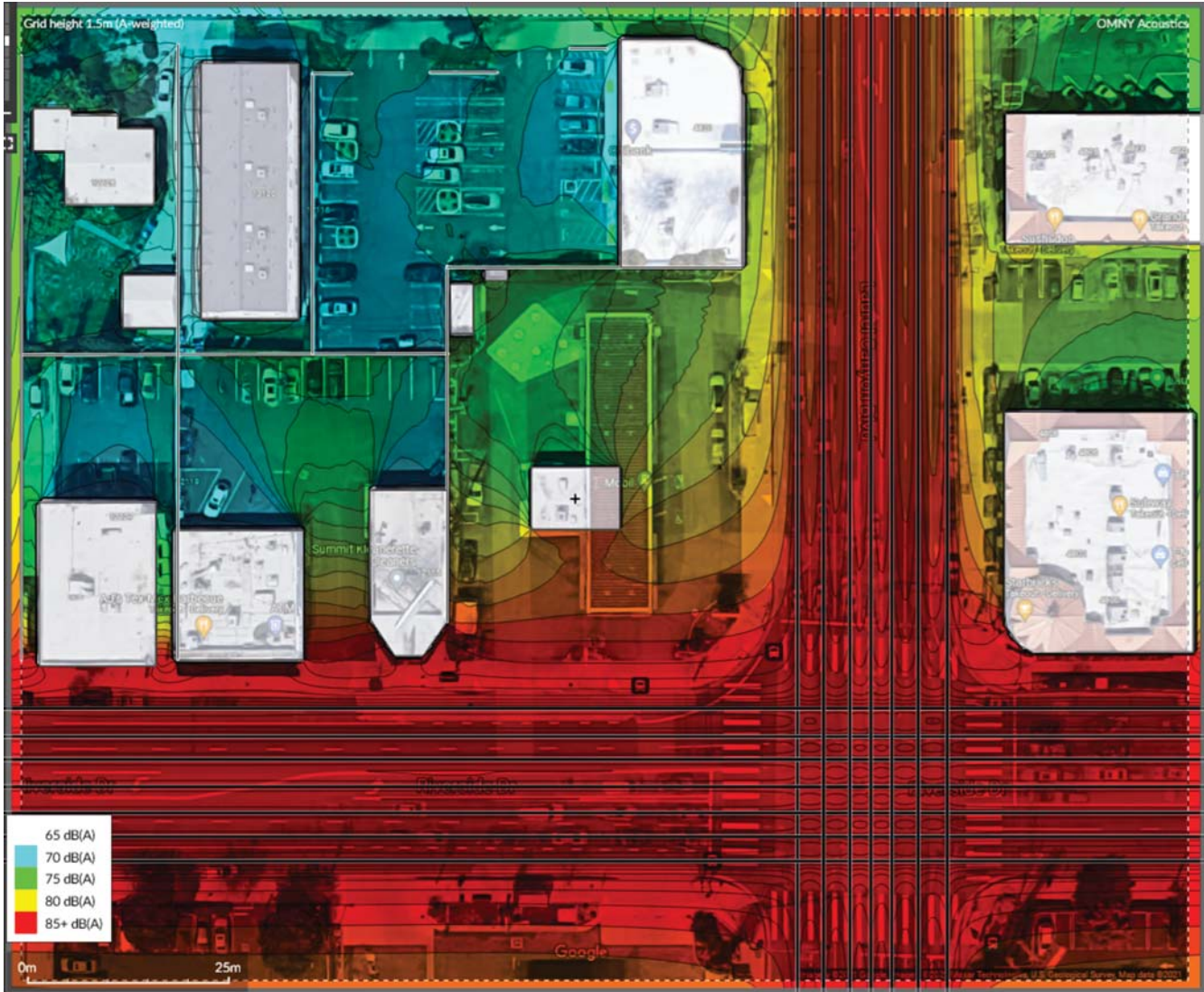


Figure 11: Contour map (1dBA contour lines) of typical ambient CNEL noise levels.

## 2. PROJECT-GENERATED NOISE IMPACTS

Project-generated noise impacts to the various receptor properties are modeled based on the following manufacturer data, site measurements, and assumptions.

- A-weighted sound pressure levels for the new 30HP Premier Touchless Drying System are provided in the product Owner's Manual and are reproduced in Figure 12 below. These are in-field readings taken from a similar tunnel-wash installation utilizing Premier plastic housings and no dryer-system surrounds or cone silencers. Equipment documentation claims a 4-5 dBA noise reduction resulting from



installation of these systems, but provides no further supporting data. As only single A-weighted values are provided, a “pink noise” spectrum of equal sound power per octave band is assumed for the purposes of frequency-dependent propagation and attenuation modeling. The dryer system is modeled as a single point source positioned approximately 1.4m (4’6”) inside the exit door of the car-wash tunnel, at an approximate elevation of 2.3m (7’6”), based on scaled Project drawings and equipment submittal diagrams.

DISTANCE FROM BAY	30HP EXIT DOOR CLOSED	30 HP EXIT DOOR OPEN
10 FT.	76	89
20 FT.	72	87
30 FT.	70	82
40 FT.	66	78
50 FT.	66	78
60 FT.	64	75

Figure 12: Noise level readings for 30HP Premier Touchless Dryer System at various distances from tunnel exit.

- In the absence of any manufacturer noise data for the new vacuum cleaner system, an A-weighted level of 90dBA SPL at one meter (neglecting reflections) is assumed, equating to an approximate sound power level of 91dBA PWL. This is a conservative assumption based on firm experience and past measurement of systems believed to be similar. Project plans indicate a “sound dampener system” to be installed on the vacuums, but no further details are provided. A “pink noise” spectrum is assumed for frequency-dependent propagation and attenuation modeling. The vacuum system is modeled as a point source with a one-meter elevation above ground level.
- Sound spectra of station traffic noise (caused by cars and customers on the Project property itself) have been measured directly at the site as part of Omny’s ambient site survey. Station traffic contributions to the overall sound field have been removed in ambient noise models (see Figures 10 and 11), but have been included in models of the Mobil station’s traffic noise impacts. Equivalent (Leq) traffic noise levels are conservatively assumed to remain constant 24 hours per day in Omny’s sound modeling. Station traffic noise is modeled as a series of equal-strength line sources oriented along the main driving paths at the gas station. Post-remodel traffic noise is assumed to increase from current levels, proportionally with respect to the added parking space count and total driving path-length added by the car wash tunnel and queue.

Omny's sound modeling of station traffic impacts is shown in Figure 13 and Figure 14, for current and estimated post-remodel conditions, respectively. Modeling results for car wash tunnel noise impacts – with tunnel doors closed and open – are illustrated in Figure 15 and Figure 16, respectively. Modeled noise impacts from the vacuum system are illustrated in Figure 17. Finally, a composite “worst case” scenario, including simultaneous open-door tunnel operation, vacuum system operation, and increased post-remodel traffic noise, is illustrated in Figure 18.

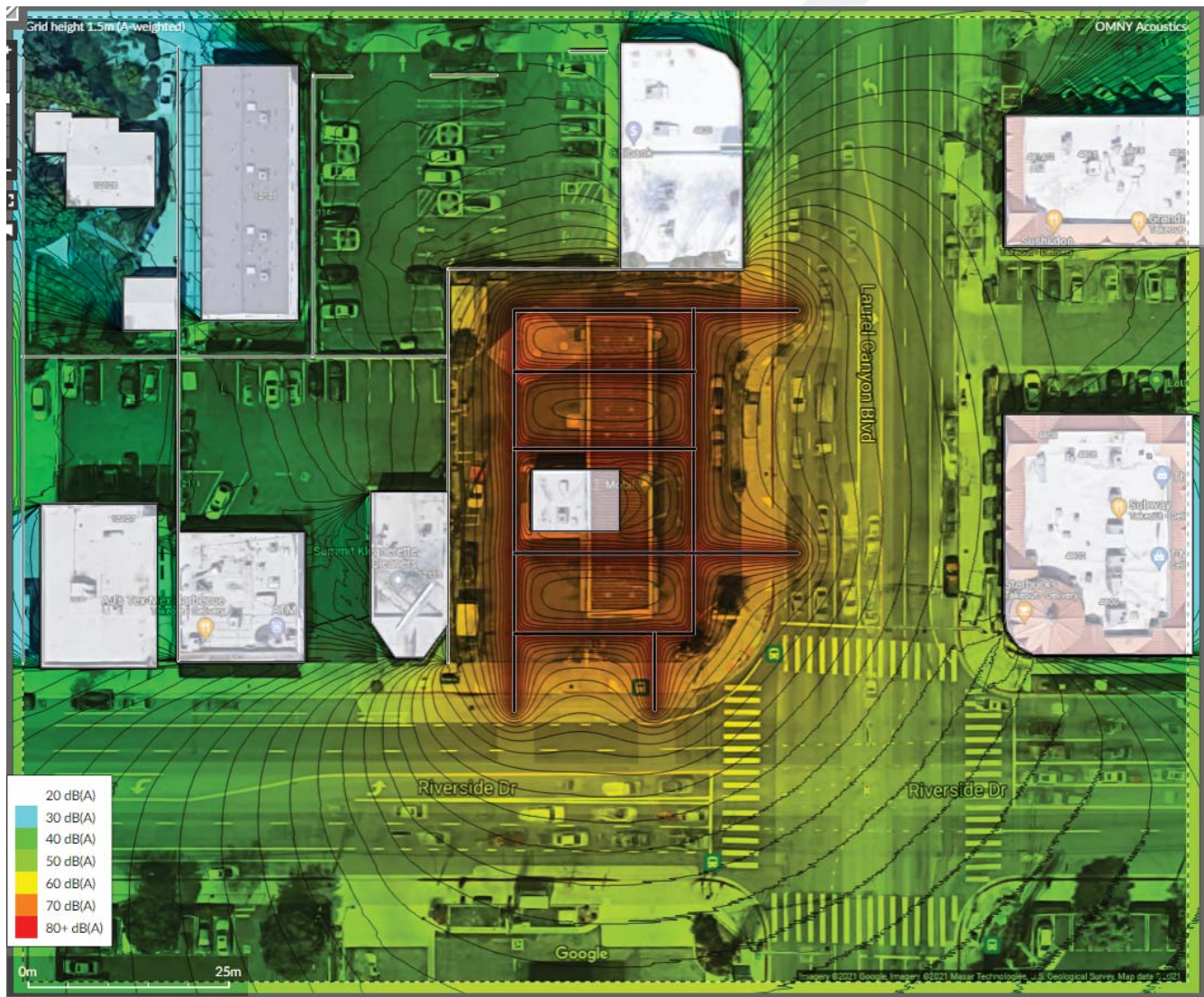


Figure 13: Contour map (1dBA contour lines) of current gas station traffic noise impact to surroundings.

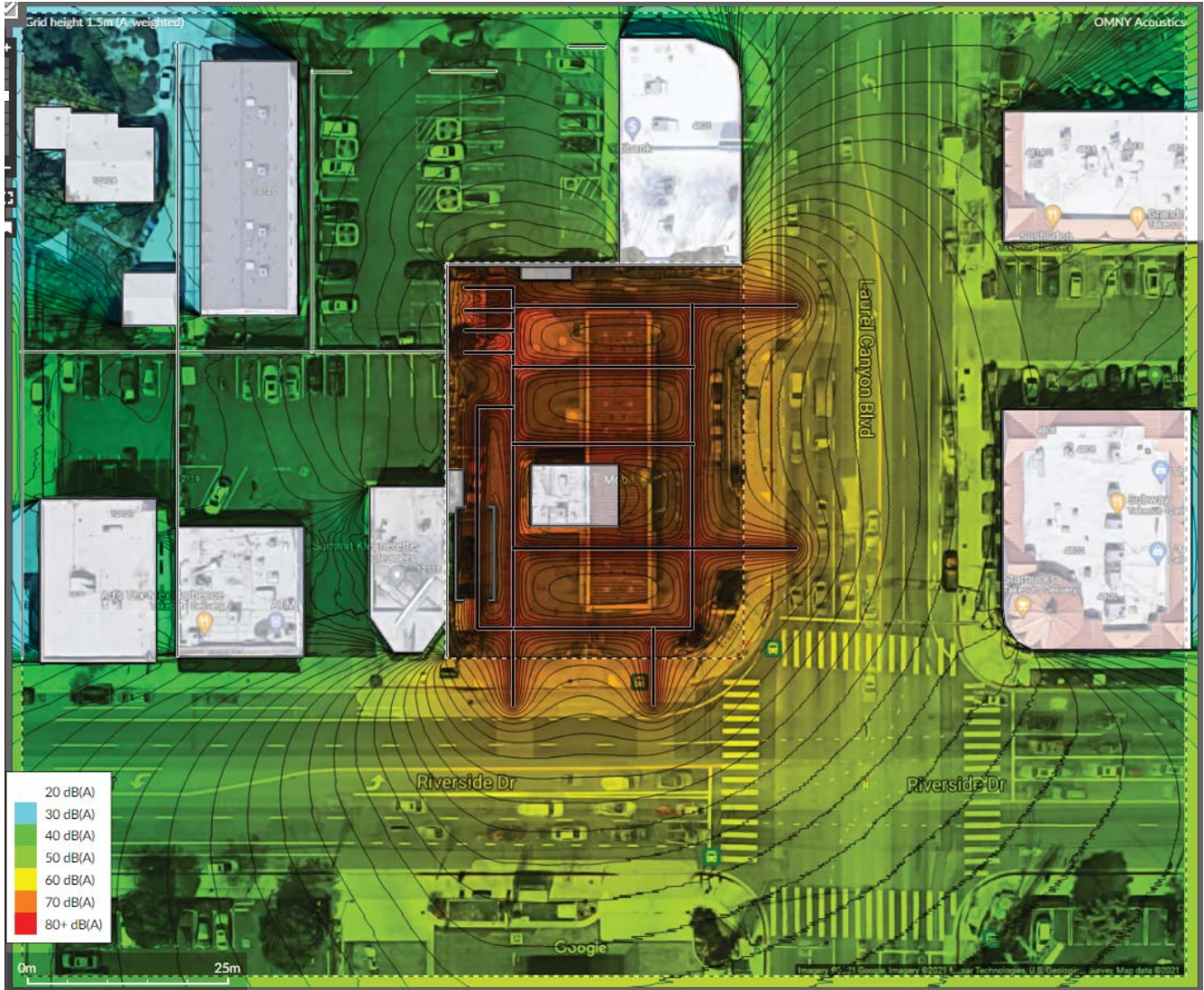


Figure 14: Contour map (1dBA contour lines) of predicted post-remodel gas station traffic noise impact to surroundings.

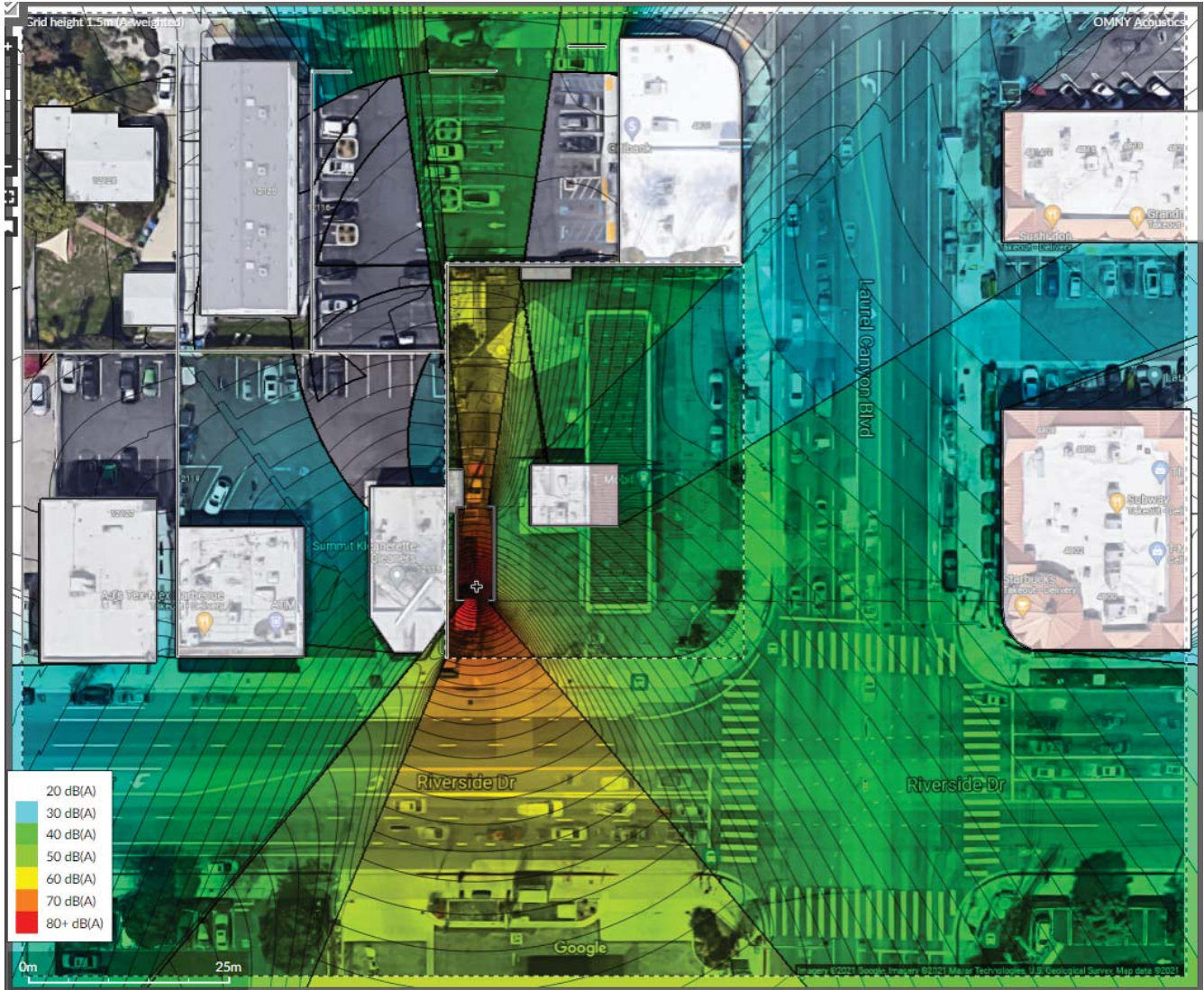


Figure 15: Contour map (1dBA contour lines) of predicted car wash noise impact to surroundings (with both tunnel doors closed).

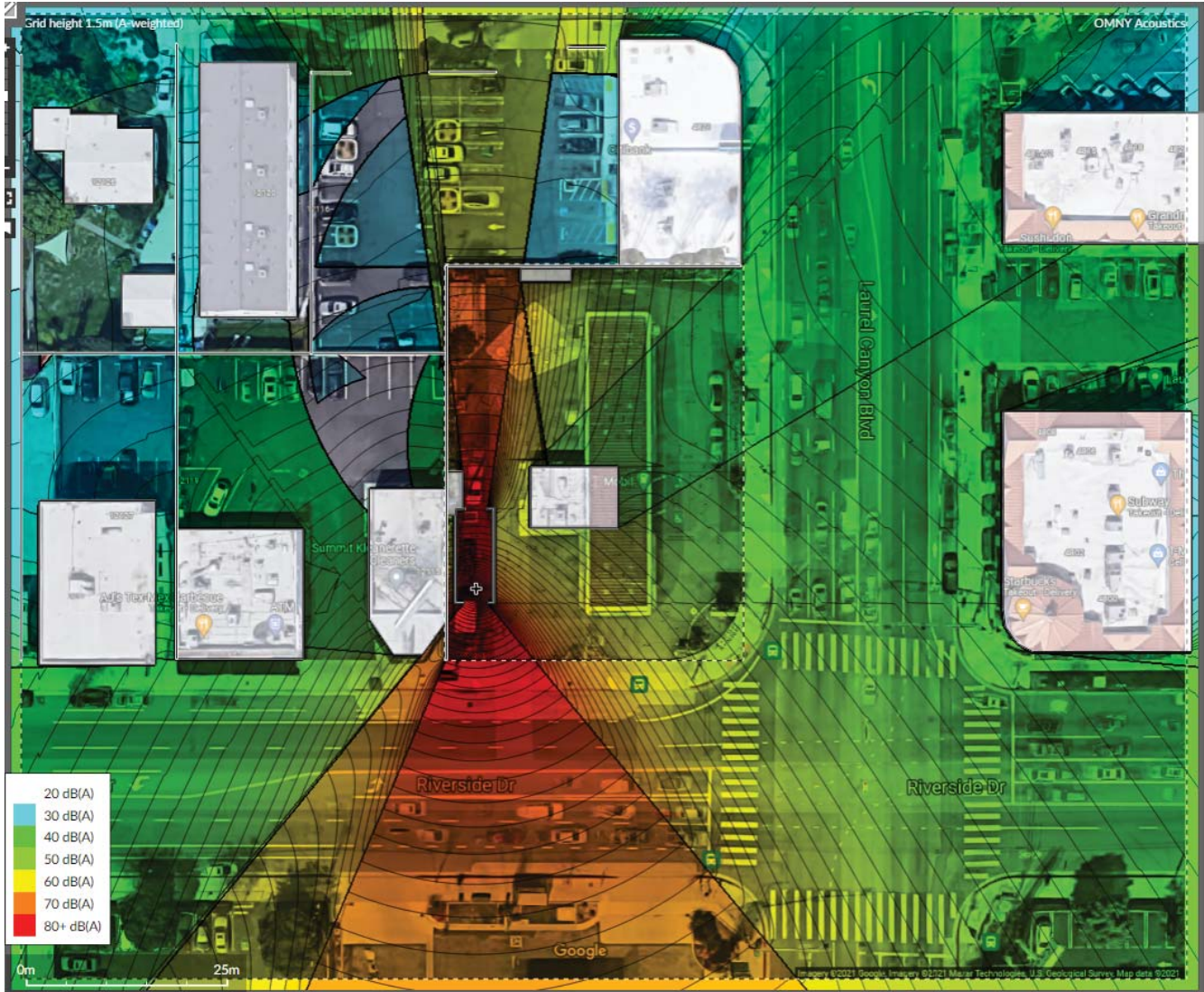


Figure 16: Contour map (1dBA contour lines) of predicted car wash noise impact to surroundings (with both tunnel doors open).

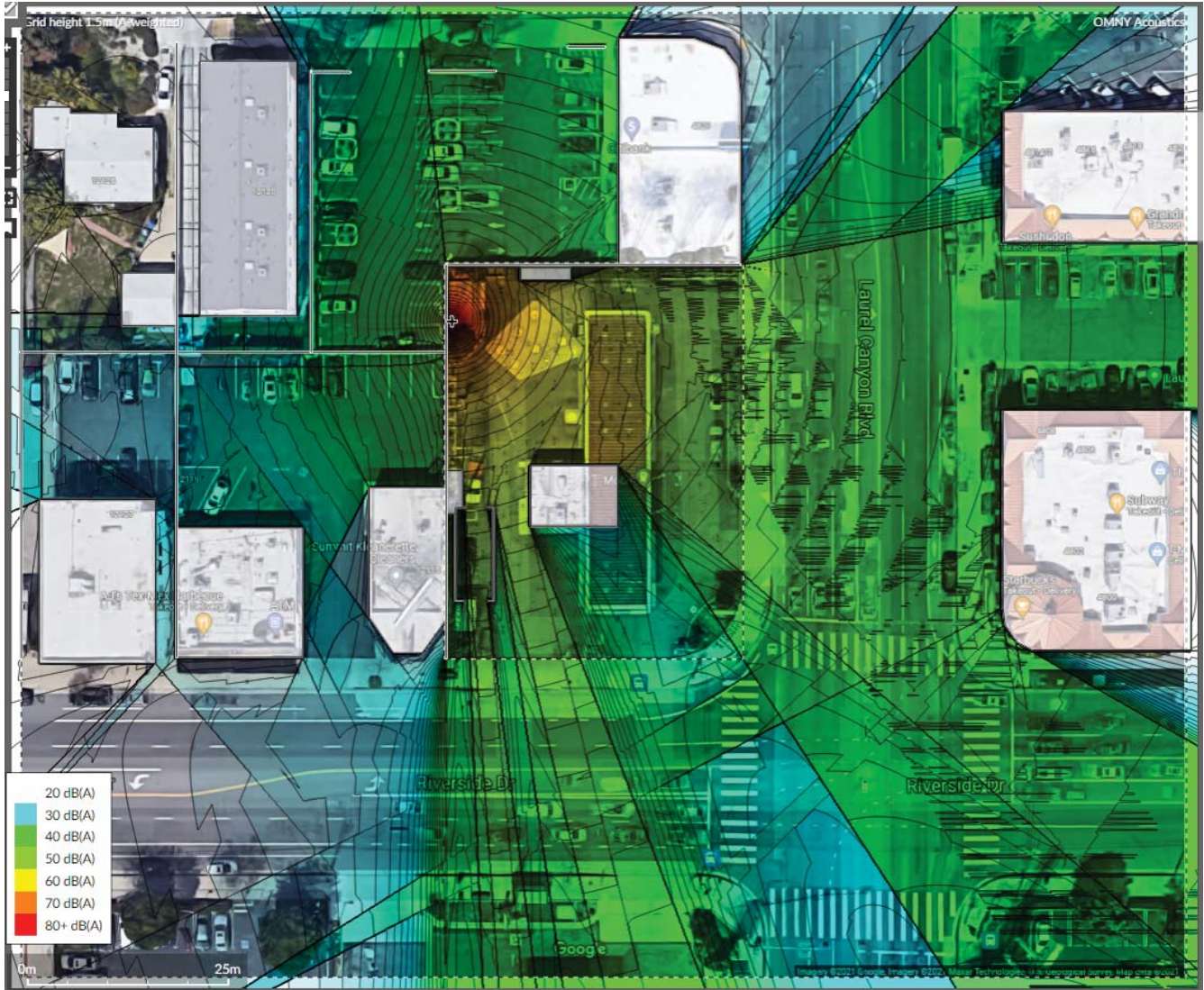
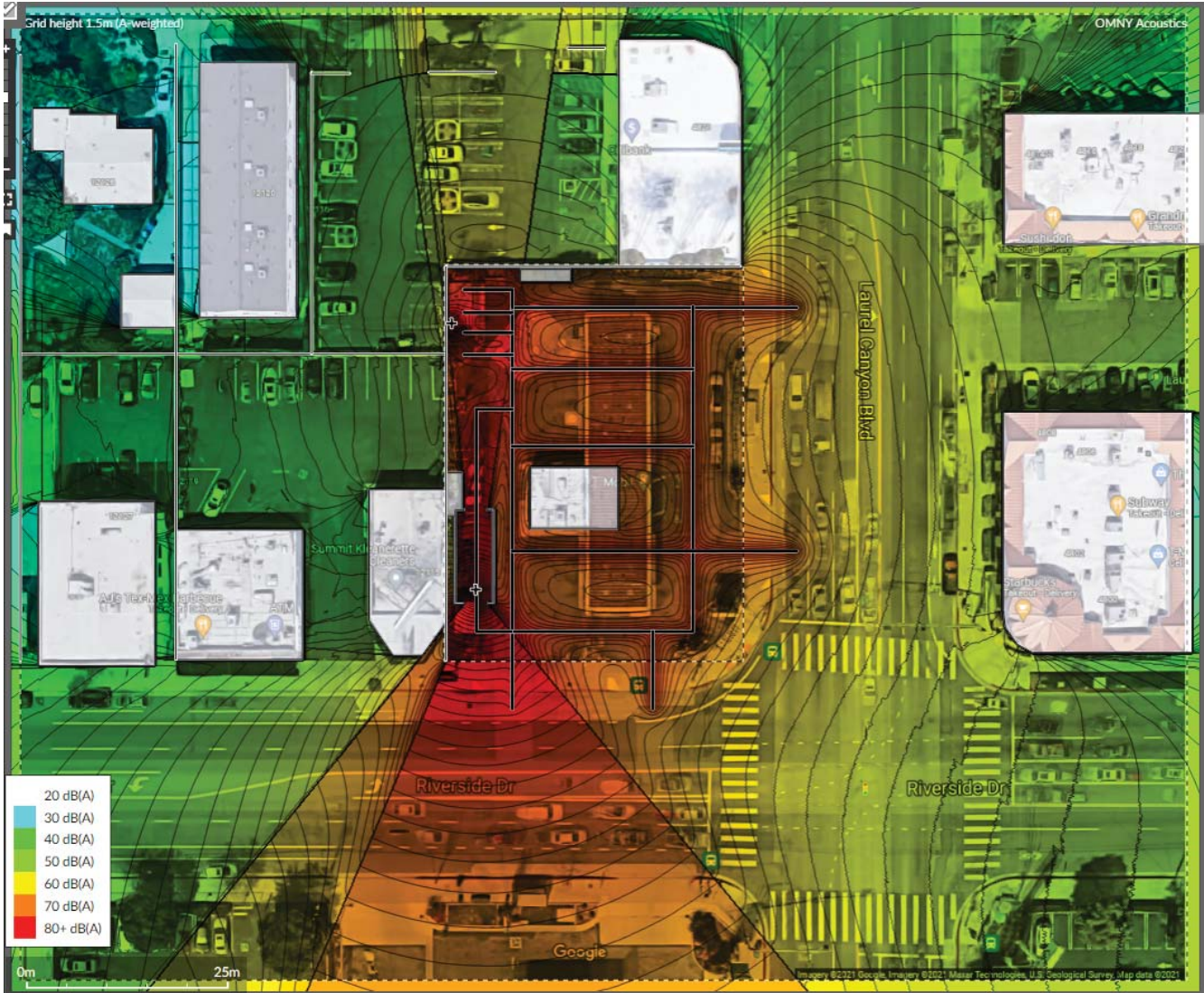


Figure 17: Contour map (1dBA contour lines) of predicted vacuum cleaner noise impacts to surroundings.



**Figure 18: Contour map (1dBA contour lines) of predicted worst-case composite noise impact to surroundings (including simultaneous increased traffic, vacuum system operation, and open-door car wash operation).**

A summary of the modeled gas station noise impacts at each property line and under various conditions can be seen in the following Table 8. These results can be compared against the composite property-line noise limits previously shown in Table 7. Note here that the tabulated CNEL impacts assume each source to be running continuously for 24 hours per day to determine its highest possible CNEL impact. As this is unlikely to occur, actual CNEL impacts will be lower than the maximum values shown here.

Project property line:	Modeled noise level (dBA Leq / dBA CNEL):									
	Station traffic		Vacuum cleaners		Car wash tunnel				Maximum Combined	
					Doors closed		Doors open			
	Leq	CNEL	Leq	CNEL	Leq	CNEL	Leq	CNEL	Leq	CNEL
Townhouses 12120 La Maida St.	52	59	55	62	43	49	55	61	59	66
Citibank 4821 Laurel Canyon Blvd.	56	63	51	58	46	52	58	64	60	67
Summit Kleanerette 12115 Riverside Dr.	52	59	50	56	23	30	35	42	54	61
Chevron 4757 Laurel Canyon Blvd.	57	63	38	45	57	64	69	76	69	76
Shopping Center 4800-4808 Laurel Canyon Blvd.	57	64	46	52	33	40	46	52	58	64

*Table 8: Summary of modeled Leq (dBA) and CNEL (dBA) noise level impacts. Green-shaded levels are fully compliant at any time of day or night under the current Project designs. Red-shaded levels are those potentially in excess of property-line noise limits during particular conditions or during particular times of day. Circumstances causing red-shaded levels are discussed in the following text.*

As seen in Table 8 above, there are a total of only four instances in which Leq noise levels generated by the Project have the potential to exceed the noise standards summarized previously in Table 7. These instances are explained in detail below. **All remaining conditions are fully compliant with all code limits during any time of day or night.**

**1. Impacts to Citibank Property from Car Wash.**

Predictions show that noise impacts from the car wash tunnel when entrance doors are open (58 dBA) may be slightly in exceedance (+2 dBA) of continuous-noise nighttime limits (56 dBA).

- a) **Nighttime:** Gas station owners have agreed to limit car wash availability to daytime hours only, therefore this condition will not occur during nighttime.
- b) **Daytime:** This condition is fully compliant with all daytime noise limits (60+ dBA) for any cumulative operation times per hour.
- c) **24-Hour:** CNEL noise contributions (max. 64 dBA) are within property-line limits (73 dBA) and are fully compliant.
- d) **Conclusion:** Due to the factors explained above, **Omny finds this condition fully compliant.**



## 2. Impacts to Citibank Property from Combined Sources.

Combined worst-case noise at this location is dominated by the car wash tunnel. The discussion and conclusions for this condition are the same as the previous condition. No exceedances occur due to any other sources operating simultaneously. **Omny finds this condition fully compliant.**

## 3. Impacts to Chevron Property from Car Wash

Modeling shows that noise impacts from the car wash tunnel when doors are open (69 dBA) may be in exceedance (+7 dBA) of continuous-noise nighttime limits (62 dBA), and in slight exceedance (+1 dBA) of continuous-noise daytime limits (68 dBA).

- a) **Nighttime:** Gas station owners have agreed to limit car wash availability to daytime hours only, therefore this condition will not occur during nighttime.
- b) **Daytime:** This condition would not occur for more than 50% of any cumulative daytime hour, subjecting it to the raised limit of 70 dBA for noise occurring for 15-30 cumulative minutes per hour, and resulting in daytime compliance.
- c) **24-Hour:** CNEL noise contributions (max. 76 dBA) are within property-line limits (86 dBA) and are fully compliant.
- d) **Conclusion:** Due to the factors explained above, **Omny finds this condition fully compliant.**

## 4. Impacts to Chevron Property from Combined Sources.

Combined worst-case noise at this location is dominated by the car wash tunnel. The discussion and conclusions for this condition are the same as the previous condition. No exceedances occur due to any other sources operating simultaneously. **Omny finds this condition fully compliant.**

## J. PROJECT CONSTRUCTION NOISE

Construction noise impact is outside the scope of Omny's current study. However, general information and guidelines regarding construction noise control are included in this section.

Noise due to construction work is regulated by Section 41.40 of the *Los Angeles Municipal Code*. Project-relevant code sections are quoted below.

### *SEC. 41.40. NOISE DUE TO CONSTRUCTION, EXCAVATION WORK – WHEN PROHIBITED.*

*(a) No person shall, between the hours of 9:00 P.M. and 7:00 A.M. of the following day, perform any construction or repair work of any kind upon, or any excavating for, any building or structure, where any of the foregoing entails the use of any power driven drill, riveting machine excavator or any other machine, tool, device or equipment which makes loud noises to the disturbance of persons occupying sleeping quarters in any dwelling hotel or apartment or other place of residence. In addition, the operation, repair or servicing of construction equipment and the job-site delivering of construction materials in such areas shall be prohibited during the hours herein specified. Any person who knowingly and wilfully violates the foregoing provision shall be*

*deemed guilty of a misdemeanor punishable as elsewhere provided in this Code. (Amended by Ord. No. 158,587, Eff. 1/29/84.)*

*(c) (Amended by Ord. No. 166,170, Eff. 9/29/90.) No person, other than an individual homeowner engaged in the repair or construction of his single-family dwelling shall perform any construction or repair work of any kind upon, or any earth grading for, any building or structure located on land developed with residential buildings under the provisions of Chapter I of this Code, or perform such work within 500 feet of land so occupied, before 8:00 a.m. or after 6:00 p.m. on any Saturday or national holiday nor at any time on any Sunday. In addition, the operation, repair or servicing of construction equipment and the job-site delivering of construction materials in such areas shall be prohibited on Saturdays and on Sundays during the hours herein specified. The provisions of this subsection shall not apply to persons engaged in the emergency repair of:*

- 1. Any building or structure.*
- 2. Earth supporting or endangering any building or structure.*
- 3. Any public utility.*
- 4. Any public way or adjacent earth.*

Noise impacts from construction are temporary, and are unavoidable. However, based on the above quoted information from LAMC, and our previous project experience, Omny recommends the following general guidelines for minimizing construction noise to the greatest extent feasible.

1. Limit construction activity to the hours of 7:00 AM to 9:00 PM on Monday through Friday, and 8:00 AM to 6:00 PM on Saturday, with no construction allowed on Sundays or holidays. This satisfies code-minimum scheduling requirements for Project construction. As an additional “good neighbor” policy, it may be advisable to further limit construction to the hours of 8:00 AM to 6:00 PM on Monday through Friday only, with no weekend or holiday construction.
2. Implement noise attenuation measures around the site perimeter to the extent feasible, which should include noise barriers and/or noise blankets. Particular attention should be paid to providing a noise barrier between construction activities and the residential property to the Northwest. Barrier material should have a minimum areal density of 2.0 pounds per square foot (2.0 lb/ft<sup>2</sup>). Acceptable materials include 3/4” plywood, sheet metal with minimum 18-gauge thickness, or any solid masonry wall. Barrier wall(s) should be gapless to the extent possible to minimize acoustical leaks.
3. Provide advance notification to surrounding land uses disclosing the construction schedule, including the various types of activities that would be occurring throughout the duration of the construction period.
4. Schedule high noise-producing activities during times when they would be least likely to interfere with any scheduled noise-sensitive activities of the neighboring land uses.
5. Ensure that construction equipment is properly muffled according to industry standards.

6. Place noise-generating construction equipment and staging areas away from sensitive uses where feasible. Locate stationary noise-generating equipment as far away as is practical from sensitive receptors.
7. Discourage any unnecessary idling of internal combustion engines.
8. To the extent feasible, route all construction-related traffic along major roadways and away from sensitive noise receptors.

The construction for the Project is planned to last no longer than three months at maximum, with the potential to be completed in as little as six weeks. Omny expects no significant issues related to construction noise, as all noise impacts will be temporary and can be mitigated.

## K. CEQA SIGNIFICANCE DETERMINATIONS

Based on the study results and accompanying recommendations outlined in previous sections of this report, Omny makes the following determinations for the CEQA noise questionnaire.

- A. Question G.XIII.(a):** Would the project result in: Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

**Omny Determination: Less Than Significant Impact.** Project operational noise is fully compliant with all relevant code limits under its current design and operation schedule, with no mitigation required. Construction noise is unavoidable, but will be short-term, and can be mitigated.

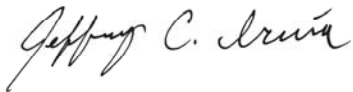
- B. Question G.XIII.(b):** Would the project result in: Generation of excessive groundborne vibration or groundborne noise levels?

**Omny Determination: No Impact.** The Project includes no significant sources of groundborne noise nor groundborne vibration during its typical operation. This type of project is not expected to include high-vibration-producing construction activities (e.g. pile-driving), and no vibration-sensitive operations are located in the Project vicinity.

- C. Question G.XIII.(c):** Would the project result in: For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

**Omny Determination: No Impact.** This condition does not apply to the Project location.

This concludes our Environmental Noise Impact Report. Feel free to contact Omny Acoustics with any questions or concerns.

A handwritten signature in black ink that reads "Jeffrey C. Irwin".

Jeffrey C. Irwin  
Principal Engineer  
Omny Acoustics  
07 December 2021

DRAFT

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## APPENDIX A: CODES AND ORDINANCES

### 1. LOS ANGELES COUNTY CODE OF ORDINANCES (LAGCO)

#### Chapter 12.08 - NOISE CONTROL

##### 12.08.030 - Terminology—Conformity with ANSI standards.

All terminology used in this chapter, not defined in this Part 2, shall be in conformance with applicable publications of the American National Standards Institute (ANSI) or its successor body.

##### 12.08.040 - Definitions applicable.

The following words, phrases and terms as used in this chapter shall have the meanings as indicated in this Part 2.

##### 12.08.060 - Ambient noise histogram.

"Ambient noise histogram" means the composite of all noise from sources near and far, excluding the alleged intrusive noise source. In this context, the ambient noise histogram shall constitute the normal or existing level of environmental noise at a given location.

##### 12.08.070 - A-weighted sound level.

"A-weighted sound level" means the sound level in decibels as measured on a soundlevel meter using the A-weighting network. The level so read is designated dB(A) or dBA.

##### 12.08.080 - Commercial property.

"Commercial property" means a parcel of real property which is developed and used either in part or in whole for commercial purposes. In cases of multiple land uses of any property, the county zoning classification of such property pursuant to county Ordinance 1494, as amended, shall be applicable. (See Title 22 of this code.)

##### 12.08.090 - Construction.

"Construction" means any site preparation, assembly, erection, substantial repair, alteration, or similar action, for or of public or private rights-of-way, structures, utilities, or similar property.

##### 12.08.110 - Decibel.

"Decibel" means a unit for measuring the amplitude of a sound, equal to 20 times the logarithm to the base of 10 of the ratio of the pressure of the sound measured to the reference pressure, which is 20 micropascals.

##### 12.08.120 - Dwelling unit.

"Dwelling unit" means a single unit providing complete independent living facilities for one or more persons, including permanent provisions for living, sleeping, eating, cooking and sanitation.

12.08.150 - Fixed noise source.

"Fixed noise source" means a stationary device which creates sounds while fixed or motionless, including but not limited to residential, agricultural, industrial and commercial machinery and equipment, pumps, fans, compressors, air conditioners and refrigeration equipment.

12.08.190 - Impulsive noise.

"Impulsive noise" means a sound of short duration, usually less than one second and of high intensity, with an abrupt onset and rapid decay.

12.08.210 - Intrusive noise.

"Intrusive noise" means that alleged offensive noise which intrudes over and above the existing ambient noise at the receptor property.

12.08.230 - Noise disturbance.

"Noise disturbance" means an alleged intrusive noise which violates an applicable noise standard as set forth in this chapter.

12.08.240 - Noise histogram.

"Noise histogram" means a graphical representation of the distribution of frequency of occurrence of all noise levels near and far measured over a given period of time.

12.08.250 - Noise level (L).

"Noise level (LN )" means that noise level expressed in decibels which exceeds the specified (LN) value as a percentage of total time measured. For instance, an L25 noise level means that noise level which is exceeded 25 percent of the time measured.

12.08.320 - Real property boundary.

"Real property boundary" means an imaginary line along the ground surface, and its vertical extension, which separates the real property owned by one person from that owned by another person, but not including intra-building real property divisions.

12.08.330 - Residential property.

"Residential property" means a parcel of real property which is developed and used either in part or in whole for residential purposes, other than transient uses such as hotels and motels. In cases of multiple land uses of any property, the county zoning classification of such property pursuant to county Ordinance 1494, as amended, shall be applicable.

12.08.340 - Sound level meter.

"Sound level meter" means an instrument, including a microphone, an amplifier, an output meter and frequency weighting network, for the measurement of sound levels, which satisfies the requirements pertinent

for Type S2A meters in American National Standards Institute specifications for sound level meters, S1.4-1971, or the most recent revision thereof.

Part 3 - COMMUNITY NOISE CRITERIA

12.08.370 - Decibel measurement — Basis.

Any decibel measurement made pursuant to the provisions of this chapter shall be based on a reference sound-pressure of 20 micropascals, as measured with a sound level meter using the A-weighted network (scale) at slow response, or at the fast response when measuring impulsive sound levels and vibrations.

12.08.380 - Noise zones designated.

Receptor properties described hereinafter in this chapter are hereby assigned to the following noise zones:

Noise Zone I — Noise-sensitive area; Noise Zone II — Residential properties; Noise Zone III — Commercial properties; Noise Zone IV — Industrial properties.

12.08.390 - Exterior noise standards — Citations for violations authorized when.

A. Unless otherwise herein provided, the following exterior noise levels shall apply to all receptor properties within a designated noise zone:

Noise Zone	Designated Noise Zone Land Use	Time Interval	Exterior Noise Level (dB)
I	Noise-sensitive area	Anytime	45
II	Residential properties	10:00 pm to 7:00 am (nighttime)	45
		7:00 am to 10:00 pm (daytime)	50
III	Commercial properties	10:00 pm to 7:00 am (nighttime)	55
		7:00 am to 10:00 pm (daytime)	60
IV	Industrial properties	Anytime	70

B. Unless otherwise herein provided, no person shall operate or cause to be operated, any source of sound at any location within the unincorporated county, or allow the creation of any noise on property owned, leased, occupied or otherwise controlled by such person which causes the noise level, when measured on any other property either incorporated or unincorporated, to exceed any of the following exterior noise standards:

Standard No. 1 shall be the exterior noise level which may not be exceeded for a cumulative period of more than 30 minutes in any hour. Standard No. 1 shall be the applicable noise level from subsection A of this section; or, if the ambient L50 exceeds the foregoing level, then the ambient L50 becomes the exterior noise level for Standard No. 1.

Standard No. 2 shall be the exterior noise level which may not be exceeded for a cumulative period of more than 15 minutes in any hour. Standard No. 2 shall be the applicable noise level from subsection A of this section plus 5dB; or, if the ambient L25 exceeds the foregoing level, then the ambient L25 becomes the exterior noise level for Standard No. 2.

Standard No. 3 shall be the exterior noise level which may not be exceeded for a cumulative period of more than five minutes in any hour. Standard No. 3 shall be the applicable noise level from subsection A of this section plus 20dB **[Note: this is believed to be a typo and should read 10dB, not 20dB. The more conservative 10dB interpretation is used in this study.];** or, if the ambient L8.3 exceeds the foregoing level, then the ambient L8.3 becomes exterior noise level for Standard No. 3.

Standard No. 4 shall be the exterior noise level which may not be exceeded for a cumulative period of more than one minute in any hour. Standard No. 4 shall be the applicable noise level from subsection A of this section plus 15dB; or, if the ambient L1.7 exceeds the foregoing level, then the ambient L1.7 becomes the exterior noise level for Standard No. 4.

Standard No. 5 shall be the exterior noise level which may not be exceeded for any period of time. Standard No. 5 shall be the applicable noise level from subsection A of this section plus 20dB; or, if the ambient L0 exceeds the foregoing level then the ambient L0 becomes the exterior noise level for Standard No. 5.

C. If the measurement location is on a boundary property between two different zones, the exterior noise level utilized in subsection B of this section to determine the exterior standard shall be the arithmetic mean of the exterior noise levels in subsection A of the subject zones. Except as provided for above in this subsection C, when an intruding noise source originates on an industrial property and is impacting another noise zone, the applicable exterior noise level as designated in subsection A shall be the daytime exterior noise level for the subject receptor property.

D. The ambient noise histogram shall be measured at the same location along the property line utilized in subsection B of this section, with the alleged intruding noise source inoperative. If for any reason the alleged intruding noise source cannot be turned off, the ambient noise histogram will be estimated by performing a measurement in the same general area of the alleged intruding noise source but at a sufficient distance such that the noise from the alleged intruding noise source is at least 10dB below the ambient noise histogram in order that only the actual ambient noise histogram be measured. If the difference between the ambient noise histogram and the alleged intruding noise source is 5 to 10dB, then the level of the ambient noise histogram itself can be reasonably determined by subtracting a one-decibel correction to account for the contribution of the alleged intruding noise source.

#### 12.08.400 - Interior noise standards.

A. No person shall operate or cause to be operated within a dwelling unit, any source of sound, or allow the creation of any noise, which causes the noise level when measured inside a neighboring receiving dwelling unit to exceed the following standards:

Standard No. 1 The applicable interior noise level for cumulative period of more than five minutes in any hour;  
or



Standard No. 2 The applicable interior noise level plus 5dB for a cumulative period of more than one minute in any hour; or

Standard No. 3 The applicable interior noise level plus 10dB or the maximum measured ambient noise level for any period of time.

B. The following interior noise levels for multifamily residential dwellings shall apply, unless otherwise specifically indicated, within all such dwellings with windows in their normal seasonal configuration.

Noise Zone	Designated Land Use	Time Interval	Allowable Interior Noise Level (dB)
All	Multifamily	10 pm—7 am	40
	Residential	7 am—10 pm	45

C. If the measured ambient noise level reflected by the L50 exceeds that permissible within any of the interior noise standards in subsection A of Section 12.08.390, the allowable interior noise level shall be increased in 5dB increments in each standard as appropriate to reflect said ambient noise level (L50).

#### 12.08.420 - Measurement methods.

A. Utilizing the A-weighting scale of the sound-level meter and the "slow" meter response (use "fast" response for impulsive type sounds), the noise level shall be measured at a position or positions at any point on the receiver's property.

B. In general, the microphone shall be located four to five feet above the ground; 10 feet or more from the nearest reflective surface, where possible. However, in those cases where another elevation is deemed appropriate, the latter shall be utilized.

## 2. LOS ANGELES MUNICIPAL CODE (LAMC)

### CHAPTER XI

### NOISE REGULATION

### ARTICLE 1

### GENERAL PROVISIONS

#### SEC. 111.01. DEFINITIONS.

Unless the context otherwise clearly indicates, the words and phrases used in this chapter are defined as follows:

(a) "Ambient Noise" is the composite of noise from all sources near and far in a given environment, exclusive of occasional and transient intrusive noise sources and of the particular noise source or sources to be measured. Ambient noise shall be averaged over a period of at least 15 minutes at a location and time of day comparable to that during which the measurement is taken of the particular noise source being measured.

(c) "Decibel" (dB) is a unit of level which denotes the ratio between two (2) quantities which are proportional to power; the number of decibels corresponding to the ratio of two (2) amounts of power is ten (10) times the logarithm to the base (10) of this ratio.

(h) "Octave Band Noise Analyzer" is an instrument for measurement of sound levels in octave frequency bands which satisfies the pertinent requirements for Class II octave band analyzers of the American National Standard Specifications for Octave, Half-Octave, and Third-Octave Band Filters, S1.11-1966 or the most recent revision thereof.

(l) "Sound Level Meter" is an instrument including a microphone, an amplifier, an output meter, and "A" frequency weighting network for the measurement of sound levels which satisfies the pertinent requirements for Type S2A meters in American Standard Specifications for sound level meters in S1.4-1971 or the most recent revision thereof.

#### SEC. 111.02. SOUND LEVEL MEASUREMENT PROCEDURE AND CRITERIA.

(a) Any sound level measurement made pursuant to the provisions of this chapter shall be measured with a sound level meter using the "A" weighting and response as indicated in Section 111.01(k) of this article.

Except when impractical, the microphone shall be located four to five feet above the ground and ten feet or more from the nearest reflective surface. However, in those cases where another elevation is deemed appropriated, the latter shall be utilized.

Calibration of the sound level meter, utilizing an acoustic calibrator shall be performed immediately prior to recording any sound level data. The ambient noise level and the level of a particular noise being measured shall be the numerical average of noise measurements taken at a given location during a given time period.

(b) Where the sound alleged to be offending is of a type or character set forth below, the following values shall be added to the sound level measurement of the offending noise:

1. Except for noise emanating from any electrical transformer or gas metering and pressure control equipment existing and installed prior to the effective date of the ordinance enacting this chapter, any steady tone with audible fundamental frequency or overtones have 200 Hz..... +5

2. Repeated impulsive noise..... +5

3. Noise occurring more than 5 but less than 15 minutes in any period of 60 consecutive minutes between the hours of 7:00 a.m. and 10:00 p.m. of any day..... -5

4. Noise occurring five minutes or less in any period of 60 consecutive minutes, between the hours of 7:00 a.m. and 10:00 p.m. of any day..... -5 **[Note: this is believed to be a typo and should read -10dB rather than -5dB. The more conservative -10dB interpretation is used in this study.]**

SEC. 111.03. MINIMUM AMBIENT NOISE LEVEL.

Where the ambient noise level is less than the presumed ambient noise level designated in this section, the presumed ambient noise level in this section shall be deemed to be the minimum ambient noise level for purposes of this chapter.

TABLE II

SOUND LEVEL "A" DECIBELS

(In this chart, daytime levels are to be used from 7:00 a.m. to 10:00 p.m. and nighttime levels from 10:00 p.m. to 7:00 a.m.)

PRESUMED AMBIENT NOISE LEVEL (dB(A))		
ZONE	DAY	NIGHT
A1, A2, RA, RE, RS, RD, RW1, RW2, R1, R2, R3, R4, and R5	50	40
P, PB, CR, C1, C1.5, C2, C4, C5, and CM	60	55
M1, MR1, and MR2	60	55
M2 and M3	65	65

At the boundary line between two zones, the presumed ambient noise level of the quieter zone shall be used.

SEC. 112.03. CONSTRUCTION NOISE.

Noise due to construction or repair work shall be regulated as provided by Section 41.40 of this Code. (Amended by Ord. No. 161,574, Eff. 9/8/86.)

SEC. 112.04. POWERED EQUIPMENT INTENDED FOR REPETITIVE USE IN RESIDENTIAL AREAS AND OTHER MACHINERY, EQUIPMENT, AND DEVICES.

(Title and Section Amended by Ord. No. 161,574, Eff 9/8/86.)

(a) Between the hours of 10:00 p.m and. 7:00 a.m. of the following day, no person shall operate any lawn mower, backpack blower, lawn edger, riding tractor, or any other machinery, equipment, or other mechanical or electrical device, or any hand tool which creates a loud, raucous or impulsive sound, within any residential zone or within 500 feet of a residence.

(b) Except as to the equipment and operations specifically mentioned and related elsewhere in this Chapter or for emergency work as that term is defined in Section 111.01(d), and except as to aircraft, tow tractors, aircraft auxiliary power units, trains and motor vehicles in their respective operations governed by State or federal regulations, no person shall operate or cause to be operated any machinery, equipment, tools, or other mechanical or electrical device, or engage in any other activity in such manner as to create any noise which would cause the noise level on the premises of any other occupied property, or, if a condominium, apartment house, duplex, or attached business, within any adjoining unit, to exceed the ambient noise level by more than five (5) decibels.

(c) Notwithstanding the provisions of Subsection (a) above, no gas powered blower shall be used within 500 feet of a residence at anytime. Both the user of such a blower as well as the individual who contracted for the services of the user, if any, shall be subject to the requirements of and penalty provisions for this ordinance. Violation of the provisions of this subsection shall be punishable as an infraction in an amount not to exceed One Hundred Dollars (\$100.00), notwithstanding the graduated fines set forth in LAMC § 11.00(m). (Amended by Ord. No. 171,890, Eff. 2/13/98.)

DRAFT

### 3. LOS ANGELES GENERAL PLAN (LAGP)

From the Noise Element of the *Los Angeles General Plan (1999)*:

## Exhibit I: Guidelines for Noise Compatible Land Use

(Based on the Governor's Office of Planning and Research, "General Plan Guidelines", 1990. To help guide determination of appropriate land use and mitigation measures vis-a-vis existing or anticipated ambient noise levels)

Land Use Category	Day-Night Average Exterior Sound Level (CNEL dB)						
	50	55	60	65	70	75	80
Residential Single Family, Duplex, Mobile Home	A	C	C	C	N	U	U
Residential Multi-Family	A	A	C	C	N	U	U
Transient Lodging, Motel, Hotel	A	A	C	C	N	U	U
School, Library, Church, Hospital, Nursing Home	A	A	C	C	N	N	U
Auditorium, Concert Hall, Ampitheater	C	C	C	C/N	U	U	U
Sports Arena, Outdoor Spectator Sports	C	C	C	C	C/U	U	U
Playground, Neighborhood Park	A	A	A	A/N	N	N/U	U
Golf Course, Riding Stable, Water Recreation, Cemetery	A	A	A	A	N	A/N	U
Office Building, Business, Commercial, Professional	A	A	A	A/C	C	C/N	N
Agriculture, Industrial, Manufacturing, Utilities	A	A	A	A	A/C	C/N	N

A = Normally acceptable. Specified land use is satisfactory, based upon assumption buildings involved are conventional construction, without any special noise insulation.

C = Conditionally acceptable. New construction or development only after a detailed analysis of noise mitigation is made and needed noise insulation features are included in project design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning normally will suffice.

N = Normally unacceptable. New construction or development generally should be discouraged. A detailed analysis of noise reduction requirements must be made and noise insulation features included in the design of a project.

U = Clearly unacceptable. New construction or development generally should not be undertaken.

#### 4. CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

From Appendix G of the *California Environmental Quality Act (CEQA) Statute and Guidelines (2021)*:

	<b>Potentially Significant Impact</b>	<b>Less Than Significant with Mitigation Incorporated</b>	<b>Less Than Significant Impact</b>	<b>No Impact</b>
<b>XIII. NOISE.</b> Would the project result in:				
a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

From Section 2 of *Los Angeles CEQA Thresholds Guide (2005)*:

#### 2. DETERMINATION OF SIGNIFICANCE

##### A. Significance Threshold

A project would normally have a significant impact on noise levels from project operations if the project causes the ambient noise level measured at the property line of affected uses to increase by 3 dBA in CNEL to or within the "normally unacceptable" or "clearly unacceptable" category, or any 5 dBA or greater noise increase (see the chart below).

Land Use	Community Noise Exposure CNEL, db			
	Normally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable
Single Family, Duplex, Mobile Homes	50 - 60	55 - 70	70 - 75	above 70
Multi-Family Homes	50 - 65	60 - 70	70 - 75	above 70
Schools, Libraries, Churches, Hospitals, Nursing Homes	50 - 70	60 - 70	70 - 80	above 80
Transient Lodging - Motels, Hotels	50 - 65	60 - 70	70 - 80	above 80
Auditoriums, Concert Halls, Amphitheaters	-	50 - 70	-	above 65
Sports Arena, Outdoor Spectator Sports	-	50 - 75	-	above 70
Playgrounds, Neighborhood Parks	50 - 70	-	67 - 75	above 72
Golf Courses, Riding Stables, Water Recreation, Cemeteries	50 - 75	-	70 - 80	above 80
Office Buildings, Business and Professional Commercial	50 - 70	67 - 77	above 75	-
Industrial, Manufacturing, Utilities, Agriculture	50 - 75	70 - 80	above 75	-

**Normally Acceptable:** Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction without any special noise insulation requirements.

**Conditionally Acceptable:** New construction or development should be undertaken only after a detailed analysis of the noise reduction requirements is made and needed noise insulation features included in the design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning will normally suffice.

**Normally Unacceptable:** New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of the noise reduction requirements must be made and needed noise insulation features included in the design.

**Clearly Unacceptable:** New construction or development should generally not be undertaken.

Source: California Department of Health Services (DHS).



# CNEL Sound Study

Community Noise Equivalent Level (CNEL)

N & D Oil Corp.

4801 Laurel Canyon Blvd.  
Los Angeles, CA 91607

## Final Report

Jan 7, 2021



# 1.0 Introduction

N & D Oil Corp.  
4801 Laurel Canyon Blvd.  
Los Angeles, CA 91607

Sept 2, 2019

Attn: Nader Hattar

Subject: CNEL sound study at 4801 Laurel Canyon Blvd., Los Angeles

In accordance with your request, MK Design has prepared this CNEL sound study report for the proposed development at the subject site. The purpose of this report was to evaluate sound levels, construction and mitigation measures for the proposed construction.

Based upon the findings and observations during our investigation, we believe that sufficient information has been disclosed to allow for city staff to make their decisions.

This opportunity to be of service is sincerely appreciated. If you have any questions pertaining to this report, please call the under signed.

Respectfully submitted,

MK Design

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Appendix A List of Acronyms and Definitions

Appendix B Background Information

## 3.0 Project Description

The proposed project is a gas station convenience store with a new carwash building located within the city of Los Angeles, CA. (APN: 2356-008-021). A new carwash tunnel is proposed.

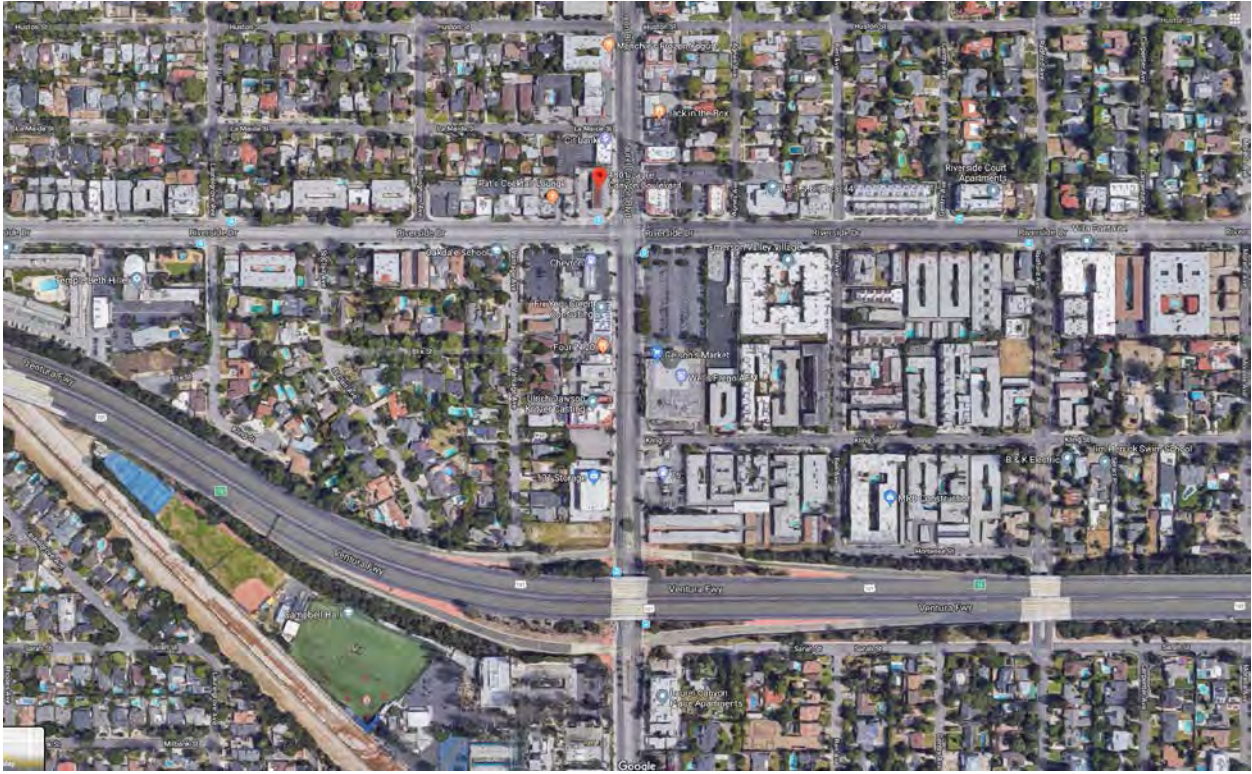
The general area is subject to a sound study per criteria required by planning department staff. As such a composite dB(A) sound level study will be provided by this report. No baseline was provided by the city of Los Angeles so site measurements and standard road sound wall will be used as the baseline.

As the project is not noted as located within the established sound bands provided by the city no loss or attenuation calculations are being provided. Additionally, given the city provided no parameters no reductions have been calculated for elevation changes, obstacles or absorption so that all sound levels within the provided bands are calculated as uniformly that sound level along the road sound wall. Reflective sound is considered to provide less than +3dB, and thusly was ignored.

The project details are as follows:

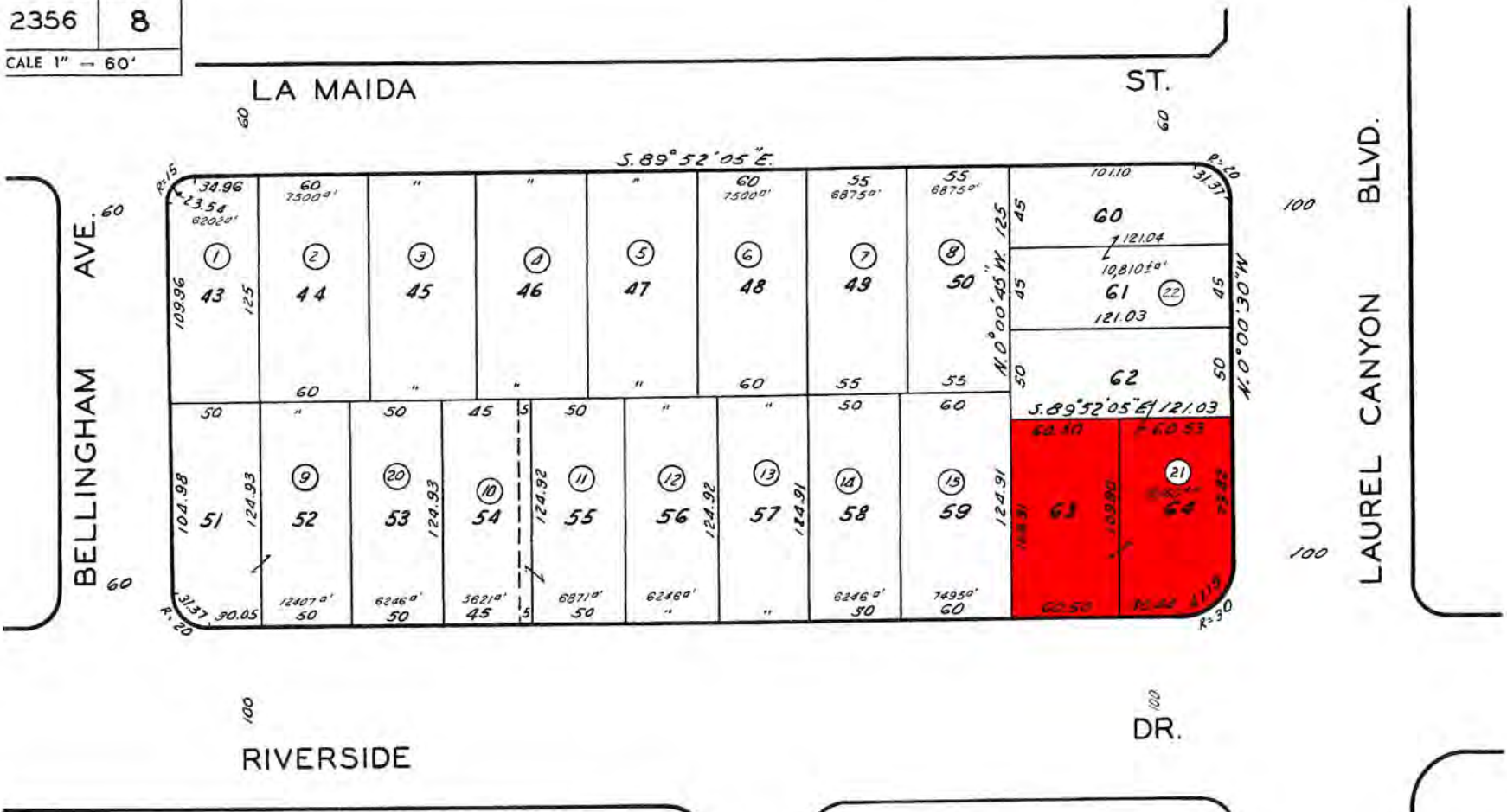
Site area:	19,164.3 sqft (0.44 acres)
Building area:	1,709 sqft
Stories:	1 story (max height 21'-11")
Project address:	4801 Laurel Canyon Blvd. Los Angeles, CA 91607
APN:	2356-008-021
Parking provided:	5 spaces
Zone:	(Q) C-2 -1VL (Commercial)
Construction Type:	V-B
Occupancy Type:	M

## 4.0 Vicinity Map



2356 8  
 SCALE 1" = 60'

2-6-65  
 721121819  
 750527  
 760913609



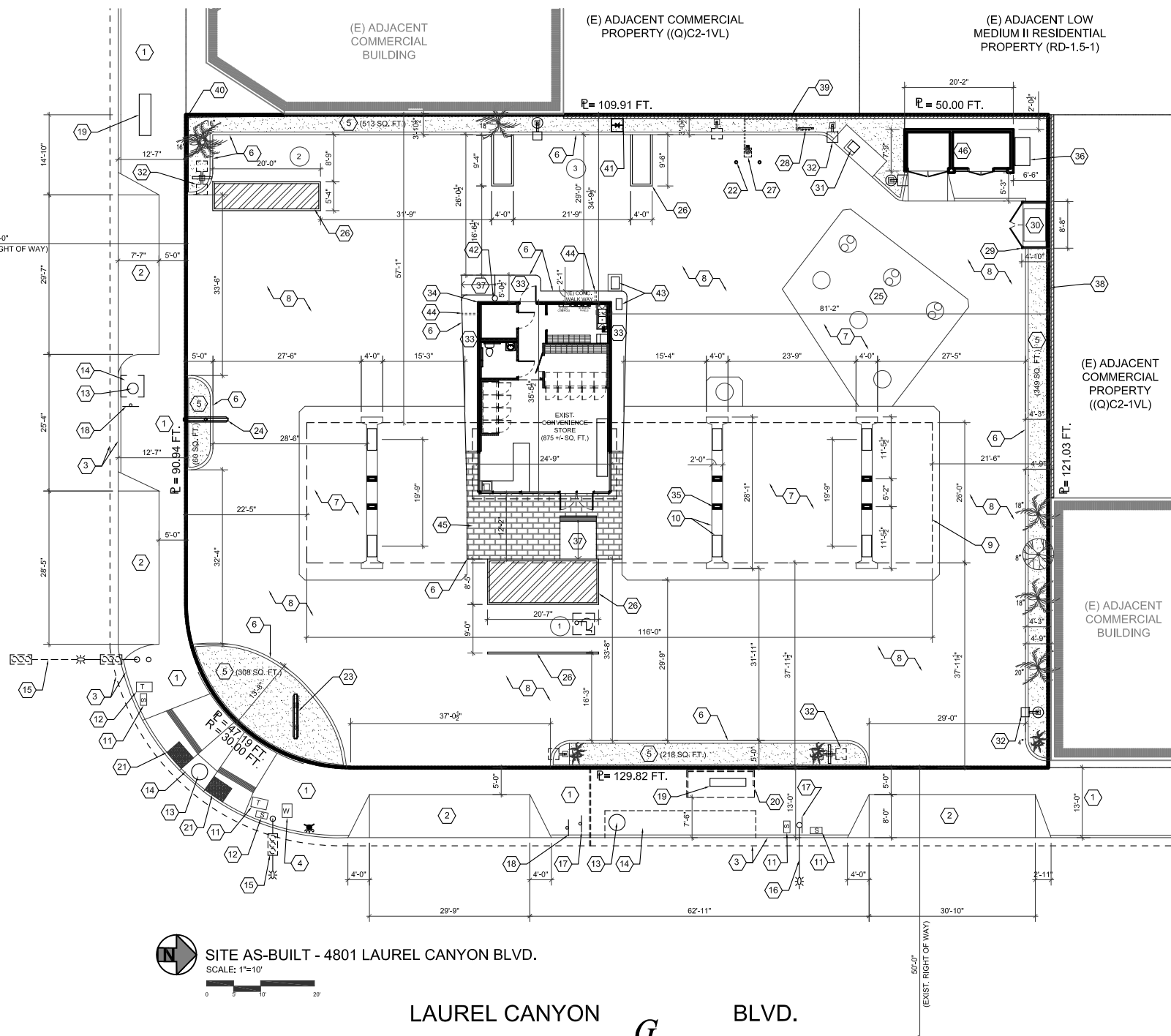
TRACT NO. 7808  
 M. B. 115 - 44

CODE  
 13

FOR PREV. ASSMT. SEE: 1733-8

ASSESSOR'S MAP  
 COUNTY OF LOS ANGELES, CALIF.

RIVERSIDE DR.



**SITE AS-BUILT - 4801 LAUREL CANYON BLVD.**  
SCALE: 1"=10'

LAUREL CANYON BLVD.

**LEGEND**

- # NOTES
- # PARKING SPACE NUMBER

**SITE NOTES**

- |  |  |  |
|--|--|--|
| <ul style="list-style-type: none"> <li>1 EXIST. CONC. SIDE WALK</li> <li>2 EXIST. CONC. DRIVEWAY</li> <li>3 EXIST. CONC. CURB &amp; GUTTER</li> <li>4 EXIST. WATER METER</li> <li>5 EXIST. LANDSCAPE PLANTER</li> <li>6 EXIST. 6" CONC. CURB - TYP.</li> <li>7 EXIST. CONC. PAVING</li> <li>8 EXIST. ASPHALTIC CONC. PAVING</li> <li>9 EXIST. MTL. FUELING CANOPY (26' X 116')</li> <li>10 EXIST. FUELING DISPENSER W/ 6" HIGH CONC. ISLAND - TYP.</li> <li>11 EXIST. TRAFFIC SIGNAL PULL VAULT</li> <li>12 EXIST. STREET LIGHT PULL VAULT</li> <li>13 EXIST. EXIST. MANHOLE</li> <li>14 EXIST. CATCH BASIN</li> <li>15 EXIST. TRAFFIC SIGNAL &amp; STREET LIGHT POLE</li> <li>16 EXIST. STREET LIGHT POLE</li> <li>17 EXIST. STREET -TRAFFIC SIGNAGE</li> <li>18 EXIST. STREET BUS SIGNAGE</li> </ul> | <ul style="list-style-type: none"> <li>19 EXIST. EXIST. BUS STOP BENCH</li> <li>20 EXIST. EXIST. BUS STOP STRUCTURE</li> <li>21 EXIST. STREET INTERSECTION ADA HANDICAP RAMP</li> <li>22 EXIST. 6" DIA. MTL. BOLLARD - TYP.</li> <li>23 EXIST. POLE MAIN-PRICE SIGN 1 - REFER. TO SHT. AB4</li> <li>24 EXIST. POLE PRICE SIGN 2 - REFER. TO SHT. AB4</li> <li>25 EXIST. UNDERGROUND STORAGE FUEL TANKS W/ CONC. PAD</li> <li>26 EXIST. PARKING PAINT STRIPING (4" WIDE) - TYP.</li> <li>27 EXIST. FUEL VAPOR UNIT CARBON CANISTER W/ MTL. ENCLOSURE</li> <li>28 EXIST. MTL. FUEL VENT - TYP.</li> <li>29 EXIST. MTL. TRASH ENCLOSURE AREA (4'-10" X 8'-8")</li> <li>30 EXIST. MTL. TRASH DUMPSTER</li> <li>31 EXIST. EXIST. AIR / WATER</li> <li>32 EXIST. L.E.D. ON SITE PARKING LIGHTING-TYP</li> <li>33 EXIST. CONC. WALK WAY</li> <li>34 EXIST. BUILDING (24'-9" X 35'-5/2')</li> <li>35 EXIST. (8' X 17') CANOPY MTL. STEEL COLUMN - TYP.</li> <li>36 EXIST. MAIN SWITCH ELEC. CABINET</li> </ul> | <ul style="list-style-type: none"> <li>37 EXIST. HANDICAP ACCESS RAMP</li> <li>38 EXIST. 6" CMU BOUNDARY WALL - 6" HIGH AVG. AT ADJACENT COMMERCIAL SIDE OF PROPERTY AND 4" HIGH ON OUR NORTH SIDE OF PROPERTY -TYP.</li> <li>39 EXIST. 6" HIGH - 8" CMU BOUNDARY WALL - TYP.</li> <li>40 EXIST. 4" HIGH - 6" CMU BOUNDARY WALL - TYP.</li> <li>41 EXIST. IRRIGATION BACK FLOW DEVICE W/ MTL. CAGE ENCLOSURE</li> <li>42 EXIST. SEWER VALVE</li> <li>43 EXIST. WATER VALVE/CONTROLS</li> <li>44 EXIST. 3" DRAIN</li> <li>45 EXIST. 12" X 24" TILE PAVING</li> <li>46 EXIST. STORAGE STRUCTURE</li> </ul> |
|--|--|--|

**GENERAL NOTES**

1. ASPHALT PAVING IN PARKING LOT OVER 95% COMPACT SUBGRADE PER ASTM D-1557-78. PROVIDE A.C. PAVING OVER BASE UNLESS NOTED OTHERWISE.
2. 9' X 18' HANDICAP STALL TO HAVE REQUIRED STRIPPING AND SIGNAGE WITH 1/2" PER FOOT MAXIMUM SLOPE IN ANY DIRECTION.
3. EXISTING DRIVEWAYS COMPLY WITH CITY OF LOS ANGELES.

**GENERAL INFORMATION**

**PROPERTY OWNER:**  
4801 LAUREL CANYON LLC  
ATT: HATTAR, NADER  
4801 LAUREL CANYON BLVD.  
LOS ANGELES, CA 91607  
TEL: (818) 980-1366  
EMAIL: NadersMobil@gmail.com

**PROJECT OWNER / APPLICANT:**  
4801 LAUREL CANYON LLC  
ATT: HATTAR, NADER  
4801 LAUREL CANYON BLVD.  
LOS ANGELES, CA 91607  
TEL: (818) 980-1366  
EMAIL: NadersMobil@gmail.com

**REPRESENTATIVES / DESIGNERS:**  
MK DESIGN  
KANG & IAN MARR  
535 W. ALLEN AVE., UNIT 23  
SAN DIMAS, CA 91773  
TEL: KEN: 626-374-3834  
Email:kangdesign@hotmail.com  
TEL: IAN:619-913-2751  
Email: iamarr@hotmail.com

**SITE-ASSESSOR'S PARCEL NUMBER:**  
APN: 2356-008-021

**PROJECT SITE ADDRESS:**  
4801 LAUREL CANYON BLVD.  
LOS ANGELES, CA 91607

**TOTAL LOT AREA:**  
LOT 62: 6,051.4 SQ. FT.  
LOT 63: 6,651.9 SQ. FT.  
LOT 64: 6,461.0 SQ. FT.  
  
LOT 62(6,051.4 SQ. FT.)+63(6,651.9 SQ. FT.)+64(6,461.0 SQ. FT.) = 19,164.3 SQ. FT.

**TOTAL LOT AREA:** 19,164.3 SQ. FT. / 0.44 ACRES

**LEGAL DESCRIPTION:**  
TRACT NO 7808 LOTS 62, 63 AND LOT 64

APN: 2356-008-021  
TRACT: TR 7808

**ZONING:** (Q) C-2-1VL  
(COMMERCIAL)

**LANDUSE:** NEIGHBORHOOD OFFICE COMMERCIAL

**SPECIFIC PLAN AREA:** VALLEY VILLAGE

**BUILDING CODE USED:** 2016 C.B.C.

**TYPE OF CONSTRUCTION:** Type V B

**OCCUPANCY TYPE:** M

**EXISTING NUMBER OF STORIES:** 1  
EXISTING BUILDING MAX. HEIGHT: 10'-11" +/-  
EXISTING BUILDING AVG. HEIGHT: 10'-11" +/-

SPRINKLERED  
NO KNOW EASEMENTS

**EXIST. LANDSCAPING:**  
TOTAL EXIST. LANDSCAPE: 1,448.00 SQ. FT./  
19,164.30 SQ. FT.  
TOTAL LANDSCAPE PERCENTAGE PROVIDED: .075= 8%

**PARKING REQUIRED:**  
EXIST. CONVENIENCE STORE (875 SQ. FT.)

EXIST. C-STORE PARKING REQUIRED 2 BY SAME AS BEFORE 1985 C-STORE PARKING AGREEMENT-2  
TOTAL STALLS REQUIRED: 2 STALLS

EXIST. STANDARD PARKING: 2  
EXIST. HANDICAP STALL: 1

TOTAL EXIST. PARKING STALLS PROVIDED: 3

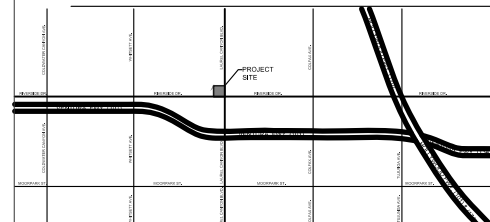
**EXISTING SIGNAGE:**  
POLE SIGN (1): 129.2 SQ. FT. TOTAL  
POLE SIGN (2): 91.5 SQ. FT. TOTAL

2 BUILDING HALLMARK SIGNAGE (MOBIL LOGO): 35.1 SQ. FT. TOTAL  
BUILDING MOBIL ENTRY SIGNAGE: 11 SQ. FT.

FUEL CANOPY HALLMARK SIGNAGE (3-WEST, EAST, & SOUTH FACADES): 35.1 SQ. FT. TOTAL

TOTAL EXISTING SIGNAGE SQ. FT.: 266.8 S.Q. FT.

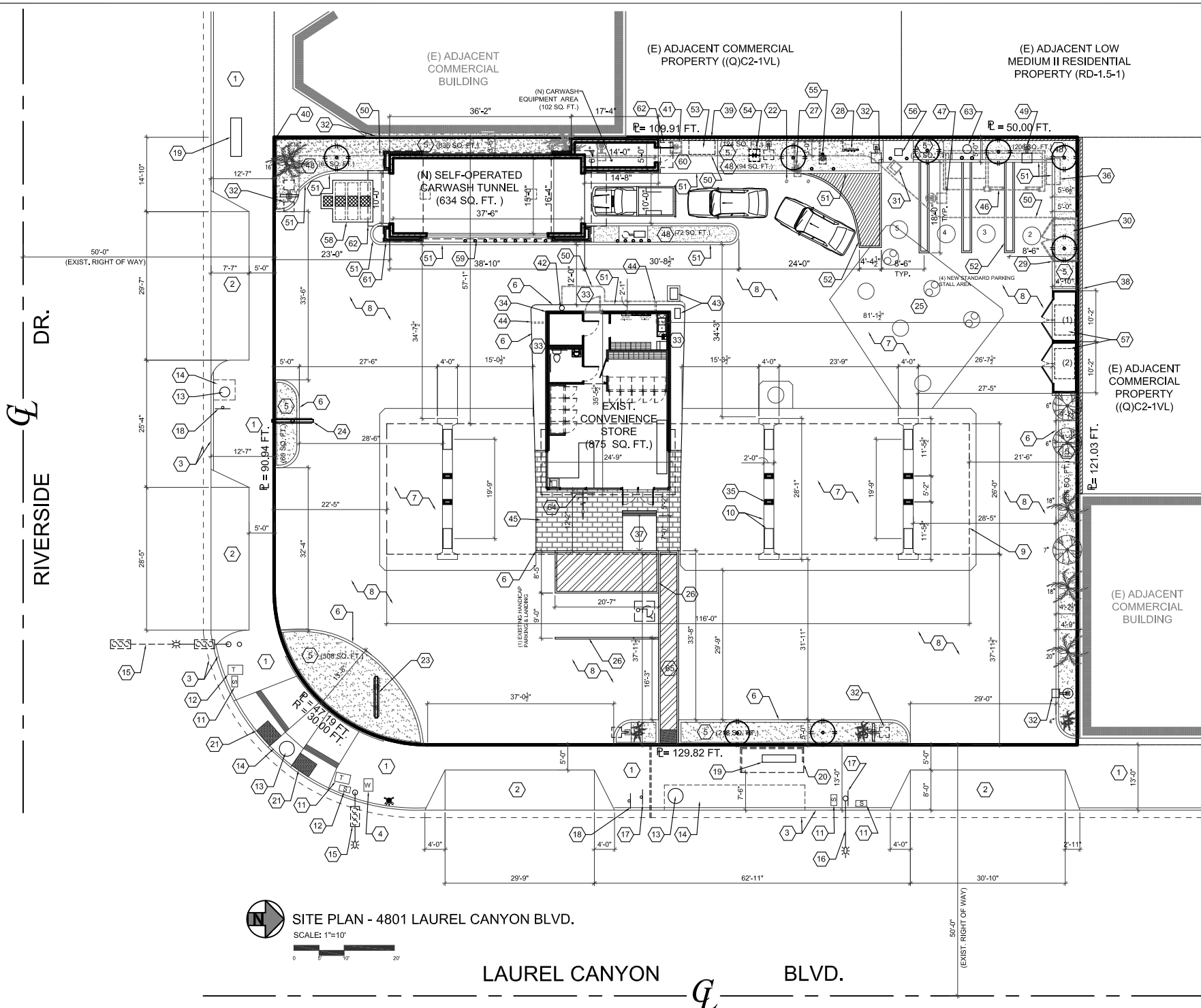
**VICINITY MAP**  
NOT TO SCALE



NADER'S MOBIL GAS STATION REMODEL / CARWASH PROJECT PROJ.  
4801 LAUREL CANYON BLVD., LOS ANGELES, CA 91607

**MK Design**  
535 W. ALLEN AVE., UNIT 23 SAN DIMAS, CA 91773

Date: 1/06/2021  
Scale: 1" = 10'-0"  
Drawn: KK  
Job:  
Sheet: 8  
**AB1**



**SITE PLAN - 4801 LAUREL CANYON BLVD.**  
SCALE: 1"=10'

LAUREL CANYON BLVD.

**GENERAL NOTES**

- SITE PAVING PER PLANS.
- EXIST. ASPHALTIC CONCRETE, NOT SHOWN TO BE ALTERED, REQUIRES PERMISSION FROM PUBLIC WORKS PRIOR TO ANY RESURFACING ACTIVITIES.
- 9" X 18" HANDICAP STALL TO HAVE REQUIRED STRIPPING AND SIGNAGE WITH 2" PER FOOT MAXIMUM SLOPE IN ANY DIRECTION.
- EXISTING DRIVEWAYS COMPLY WITH CITY OF LOS ANGELES.
- EXISTING PERIMETER MASONRY WALLS SHALL BE REPAIRED AS NECESSARY.
- ALL NEW UTILITY SERVICES SHALL BE UNDERGROUND.
- THESE PLANS ARE COMPLIANT W/ ADA & CBC ACCESSIBILITY STANDARDS.
- MINIMUM VERTICAL CLEARANCE OF 25' AT THE STAGING AREA FOR BIN SERVICE CLEARANCE. MINIMUM 13' VERTICAL CLEARANCE FOR SCOOP TRUCK.
- ALL DRIVEWAY AND STAGING AREAS MUST BE ABLE TO SUSTAIN A MINIMUM GROSS WEIGHT OF 60,000 LBS. PER VEHICLE.
- ALL STAGING AREAS ARE TO BE ON SITE, NO STREET STAGING IS PERMITTED.
- VISUAL CLEARANCE TO HAVE 7.5' TRIANGULAR CLEARANCE ON BOTH END OF DRIVEWAYS & AT INTERSECTIONS, IT IS 15' TRIANGULAR CLEARANCE WITH VERTICAL CLEARANCE OF OBSTRUCTIONS NO HIGHER THAN 3 FEET.
- ALL WORK WITHIN THE PUBLIC RIGHT OF WAY REQUIRES A SEPARATE PUBLICWORKS/ENGINEERING DEPARTMENT ENCROACHMENT PERMIT.
- GENERAL CONTRACTOR TO BE RESPONSIBLE FOR MAINTAINING AND ENFORCING SAFETY STANDARDS, CONDITIONS AND EQUIPMENT AS REQUIRED BY OSHA.

**GENERAL INFORMATION**

**PROPERTY OWNER:**  
N & D OIL CORP.  
ATT: HATTAR, NADER  
9431 BRANDON CT.  
NORTH RIDGE, CA 9125  
TEL: (818) 980-1366  
EMAIL: NadersMobil@gmail.com

**PROJECT OWNER / APPLICANT:**  
N & D OIL CORP.  
ATT: HATTAR, NADER  
4801 N. LAUREL CANYON BLVD.  
LOS ANGELES, CA 91607  
TEL: (818) 980-1366  
EMAIL: NadersMobil@gmail.com

**REPRESENTATIVES / DESIGNERS:**  
MK DESIGN  
KANG & IAN MARR  
535 W. ALLEN AVE., UNIT 23  
SAN DIMAS, CA 91773  
TEL: KEN: 626-374-3834  
Email: kangdesign@hotmail.com  
TEL: IAN: 619-913-2751  
Email: iamarr@hotmail.com

**SITE-ASSESSOR'S PARCEL NUMBER:**  
APN: 2356-008-021

**PROJECT SITE ADDRESS:**  
4801 N. LAUREL CANYON BLVD.  
LOS ANGELES, CA 91607

TOTAL LOT AREA: 19,164.3 SQ. FT. / 0.44 ACRES

**LEGAL DESCRIPTION:**  
LOTS 62, 63, AND 64 OF TRACT NO. 7808, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 115 PAGE 44 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 2356-008-021  
TRACT: TR 7808

**ZONING:** (Q) C-2-1VL  
(COMMERCIAL)

**LANDUSE:** NEIGHBORHOOD OFFICE COMMERCIAL

**SPECIFIC PLAN AREA:** VALLEY VILLAGE

**BUILDING CODE USED:** 2016 C.B.C.

**TYPE OF CONSTRUCTION:** Type V B

**OCCUPANCY TYPE:** M

**EXISTING NUMBER OF STORIES:** 1  
(EXISTING BUILDING MAX. HEIGHT: 21'-11" +/-)

**EXISTING NUMBER OF STORIES:** 1  
EXISTING BUILDING MAX. HEIGHT: 10'-11" +/-  
EXISTING BUILDING AVG. HEIGHT: 10'-11" +/-

**EXISTING STRUCTURES SITE COVERAGE:**  
CONVENIENCE STORE: 875 +/- SQ. FT.  
FUEL CANOPY 1 (26'-0" X 116'-0"): 3,016 +/- SQ. FT.  
TRASH ENCLOSURE (4'-10" X 8'-8"): 42 +/- SQ. FT.  
EXISTING STRUCTURES TOTAL: 3,933 +/- SQ. FT.

**EXIST. LANDSCAPING:**  
TOTAL EXIST. LANDSCAPE: 1,448.00 SQ. FT.,  
TOTAL EXIST. LANDSCAPE PERCENTAGE PROVIDED: .075% = 8%

TOTAL NEW LANDSCAPE PERCENTAGE PROPOSED: 434 SQ. FT. / 2.3%

TOTAL LANDSCAPE: 1,882 SQ. FT. / 19,164.30 SQ. FT. = 0.098 = 10%

**PARKING REQUIRED:**  
EXIST. CONVENIENCE STORE (875 SQ. FT.)  
TOTAL STALLS REQUIRED: 2 STALLS

EXIST. STANDARD PARKING: 2  
EXIST. HANDICAP STALL: 1  
TOTAL EXIST. PARKING STALLS PROVIDED: 3  
NEW PARKING REQUIRED: 736 SQ. FT./250 = 2.94 = 3

NEW PARKING REQUIRED; EXIST. REQUIRED FOR C-STORE AND SERVICE STATION (1985 C-STORE PARKING AGREEMENT); 2  
NEW SMALL CARWASH TUNNEL AND EQUIPMENT STORAGE  
PARKING REQUIRED: 3

NEW TOTAL PARKING REQUIRED: 5  
NEW TOTAL PARKING PROVIDED: 5

**PROPOSED PROJECT:**  
A NEW SMALL SELF OPERATED EXPRESS CARWASH (634 SQ. FT.) + CARWASH STORAGE (102) = 736 SQ. FT. MINOR SITE MODIFICATIONS (MODIFICATION TO EXISTING LANDSCAPE AND PARKING). IN ADDITION, NEW EXTERIOR TILE FINISH AT THE CURRENT BRICK VENEER FINISH AREA, ONLY, TO THE EXISTING C-STORE (875 SQ. FT.) TO MATCH THE PROPOSED CARWASH FINISH. IN ADDITION, NEW CARWASH SIGNAGE AT THE ENTRY AND EXIT OF THE TUNNEL (50 SQ. FT. TOTAL), AND THE EXISTING SIGNAGE TOTAL IS 253.4 SQ. FT.; THIS TOTAL IS 303.4 SQ. FT. AND NEW TRASH/RECYCLING ENCLOSURE (98 SQ. FT.), AND ITS EXTERIOR FINISH TO MATCH BUILDING (LIGHT WOOD GRAIN TILE). IN ADDITION, EXISTING FUEL CANOPY TO HAVE NEW FINISH (LIGHT WOOD GRAIN TILE) AT THE EXISTING COLUMNS TO MATCH BUILDINGS. THE PROJECT REQUIRES A PROJECT PERMIT COMPLIANCE (VALLEY VILLAGE SPECIFIC PLAN), CONDITIONAL USE PERMIT FOR CARWASH, AND A COMMERCIAL CORNER/ MINI SHOPPING CENTER CONDITIONAL USE PERMIT.

**NEW STRUCTURES:**  
\* SELF OPERATED CARWASH (634 SQ. FT.)  
\* CARWASH STOR. (102 SQ. FT.): 736 SQ. FT.  
\* NEW TRASH ENCLOSURE: 98 SQ. FT.  
TOTAL: 834 SQ. FT.

**NEW LANDSCAPING AREA:**  
\* PROPOSED NEW AREA: 437.00 SQ. FT.  
\* EXISTING AREA: 1,655.00 SQ. FT.  
TOTAL AREA: 2,092.00 SQ. FT./  
19,164.30 SQ. FT.  
TOTAL LANDSCAPE PERCENTAGE PROVIDED: .109 = 11%  
(TOTAL DOES NOT INCLUDE LANDSCAPING AT THE RIGHT OF WAY AREA)

**NEW PARKING REQUIRED:**  
\* EXIST. C-STORE PARKING REQUIRED 2 BY SAME AS BEFORE 1985 C-STORE PARKING AGREEMENT; 2 STALLS REQUIRED

\* NEW CARWASH BAY (634 SQ. FT.) + CARWASH STORAGE (102 SQ. FT.):  
736/250 = 2.94 = 3 STALLS

TOTAL STALLS REQUIRED: 5 STALLS

NEW PARKING: 4 STANDARD STALLS  
EXIST. HANDICAP STALL: 1  
TOTAL: 5 PARKING STALLS PROVIDED

**LOT COVERAGE:**  
\* NEW AND EXISTING STRUCTURES: 3,988.00 SQ. FT./  
\* LOT SIZE: 19,164.30 SQ. FT.  
TOTAL LOT COVERAGE: .21 = 21%

**ESTIMATED TOTAL SIGNAGE:**

**POLE SIGNAGE:**  
EXIST. POLE SIGNAGE (1): 129.2 SQ. FT.  
EXIST. POLE SIGNAGE (2): 91.5 SQ. FT.  
TOTAL POLE SIGN SQ. FT. FOR BOTH SIDES: 220.7 SQ. FT.

**EXIST. CANOPY MOBIL HALLMARK SIGNAGE:**  
EAST SIGNAGE: 11.7 SQ. FT.  
WEST SIGNAGE: 11.7 SQ. FT.  
SOUTH SIGNAGE: 11.7 SQ. FT.  
TOTAL SQUARE FOOTAGE: 35.1 SQ. FT.

**EXIST. CONVENIENCE STORE ENTRY SIGNAGE:**  
MOBIL SIGNAGE (EAST SIDE OF BUILDING FACADE): 11.0 SQ. FT.

TOTAL EXISTING SIGNAGE AREA: 266.8 SQ. FT.

**NEW SIGNAGE:**  
SOUTH/CARWASH CHANNEL EXIT SIGNAGE: 25.0 SQ. FT.  
NORTH/CARWASH CHANNEL ENTRY SIGNAGE: 25.0 SQ. FT.  
TOTAL NEW SIGNAGE SQUARE FOOTAGE: 50.0 SQ. FT.

TOTAL SIGNAGE AREA: 266.8 + 50.0 = 316.8 SQ. FT.

FIRE SPRINKLERED  
NO KNOWN EASEMENTS

**LEGEND**

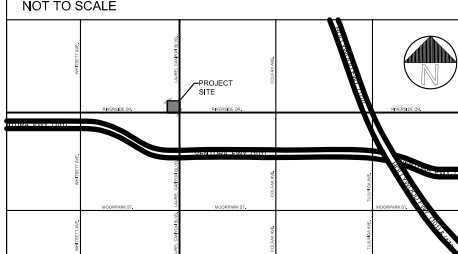
- NOTES
  - PARKING SPACE NUMBER
- SITE NOTES**
- EXIST. CONC. SIDE WALK
  - EXIST. CONC. DRIVEWAY
  - EXIST. CONC. CURB & GUTTER
  - EXIST. WATER METER
  - EXIST. LANDSCAPE PLANTER
  - EXIST. 6" CONC. CURB - TYP.
  - EXIST. CONC. PAVING
  - EXIST. ASPHALTIC CONC. PAVING
  - EXIST. MTL. FUELING CANOPY (26' X 116')
  - EXIST. FUELING DISPENSER W/ 6" HIGH CONC. ISLAND - TYP.
  - EXIST. TRAFFIC SIGNAL PULL VAULT
  - EXIST. STREET LIGHT PULL VAULT
  - EXIST. EXIST. MANHOLE
  - EXIST. CATCH BASIN
  - EXIST. TRAFFIC SIGNAL & STREET LIGHT POLE

- EXIST. STREET LIGHT POLE
- EXIST. STREET - TRAFFIC SIGNAGE
- EXIST. STREET BUS SIGNAGE
- EXIST. EXIST. BUS STOP BENCH
- EXIST. EXIST. BUS STOP STRUCTURE
- EXIST. STREET INTERSECTION ADA HANDICAP RAMP
- EXIST. 6" DIA. MTL. BOLLARD - TYP.
- EXIST. MAIN FREESTANDING POLE MOBILE / PRICE SIGN 1- REFER TO SHT. C03
- EXIST. POLE PRICE SIGN 2- REFER TO SHT. C03
- EXIST. UNDERGROUND STORAGE FUEL TANKS W/ CONC. PAD
- EXIST. PARKING PAINT STRIPING (4" WIDE) - TYP.
- EXIST. FUEL VAPOR CARBON CANISTER W/ MTL. ENCLOSURE
- EXIST. MTL. FUEL VENT - TYP.
- EXIST. MTL. TRASH ENCLOSURE AREA (4'-10" X 8'-8")
- EXIST. MTL. TRASH DUMPSTER
- EXIST. AIR / WATER TO BE RELOCATED
- EXIST. L.E.D. ON SITE PARKING LIGHTING-TYP
- EXIST. CONC. WALK WAY
- EXIST. BUILDING (24'-9" X 35'-5 1/2")
- EXIST. (8" X 17") CANOPY MTL. STEEL COLUMN- TYP.
- EXIST. MAIN SWITCH ELEC. CABINET TO BE RELOCATED

- EXIST. HANDICAP ACCESS RAMP
- EXIST. 6" CMU BOUNDARY WALL - 6" HIGH AVG. AT ADJACENT COMMERCIAL SIDE OF PROPERTY AND 4" HIGH ON OUR NORTH SIDE OF PROPERTY - TYP.
- EXIST. 6" HIGH - 8" CMU BOUNDARY WALL - TYP.
- EXIST. 4" HIGH - 6" CMU BOUNDARY WALL - TYP.
- EXIST. IRRIGATION BACK FLOW DEVICE W/ MTL. CAGE ENCLOSURE TO BE RELOCATED
- EXIST. SEWER VALVE
- EXIST. WATER VALVE/CONTROLS
- EXIST. 3" DRAIN
- EXIST. 12" X 24" TILE PAVING
- EXIST. STORAGE STRUCTURE TO BE REMOVED
- EXIST. L.E.D. ON SITE PARKING LIGHTING TO BE REMOVED
- NEW LANDSCAPE PLANTER
- NEW L.E.D. ON SITE PARKING LIGHTING
- EXIST. 6" CONC. CURB TO BE REMOVED - TYP.
- NEW 6" CONC. CURB - TYP.
- NEW PARKING AND SITE PAINT STRIPING - TYP.
- NEW MAIN SWITCH ELEC. CABINET LOCATION
- NEW IRRIGATION BACK FLOW DEVICE W/ MTL. CAGE ENCLOSURE LOCATION

- NEW LOCATION FOR THE EXIST. FUEL VAPOR CARBON CANISTER W/ MTL. ENCLOSURE
  - NEW AIR / WATER LOCATION
  - NEW MTL. TRASH ENCLOSURE W/ EXTERIOR TILE - LIGHT WOOD GRAIN FINISH - ARIZONA TILE; SAV WOOD- 2 X (4'-10" X 10'-2") W/ TRASH (1) & RECYCLE (2) DUMPSTER-REFER TO SHT. C01
  - NEW 1,500 GAL. CARWASH CLARIFIER
  - NEW 6" DIA. MTL. BOLLARDS - TYP.
  - NEW CONC. WALK WAY
  - NEW ROOF LINE
  - NEW MTL. AWNING / WALL CANOPY (GREY/METALLIC FINISH) W/ 4" DIA. ULTRA RT 4 LED W/ NICKEL TRIM FINISH - DOWN LIGHTING @ 20" O.C. - TYP.
  - NEW COIN OPERATED - 2 HOSE VACUUM
  - NEW (3) BIKE TUBE STEEL SERPINE RACK, PAINTED GREEN
  - NEW NEW PEDESTRIAN WALK WAY
- LEGEND**
- PEDESTRIAN WALKWAY    EXISTING LANDSCAPE    NEW LANDSCAPE

**VICINITY MAP**  
NOT TO SCALE



NADER'S MOBIL GAS STATION REMODEL / CARWASH PROJECT PROJ.  
 4801 N. LAUREL CANYON BLVD., LOS ANGELES, CA 91607  
 MK Design  
 535 W. ALLEN AVE., UNIT 23, SAN DIMAS, CA 91773  
 Date: 1/06/2021  
 Scale: 1"=10'-0"  
 Drawn: KK  
 Job:  
 Sheet: A1.0



## 7.0 Methodology

### **Sampling:**

Initial sampling is conducted on-site with a technician familiar with the measuring devices. All equipment has been factory calibrated within a 3 year time span and is field calibrated before each sampling session. Calibration samples at three different decibel levels will be selected by the technician. Provided all samples register within 1.5 decibels the sampling device is considered calibrated. All samples are taken with an omni-directional microphone for 60 seconds at a height of 36"-48" above finish grade. Samples are taken at an "A" weighted decibel scale. Measurements are provided for minimums, maximums and averaged values. Given the nature of the environment no weighting or noting is given to temperature, humidity or elevation.

Measurements were taken 9/3/2019 starting at 10am. At each location a total of three 60 second measurements were taken. The middle was selected each time based off of the Max dbA recorded. These measurements were used to establish the sound map and profile for the site.

An analytics module was secured at location 3. The module was set to trigger every 15 minutes for 30 seconds on recording with A-weighted Max, Min and Avg levels logged. This was the data used in addition to Google's Traffic API to establish the traffic level chart and general 24 hour levels. The module logged from 9/3 3:15pm to 9/13 11am.

### **General sound mapping:**

Sampling points or proposed points are to be located per field data. Streets and roads are to use a uniform sound wall of 60 dbA unless a high or greater traffic level is established which will use a 70 dbA sound wall.

Sound level propagation is calculated along a flat plane unless the average site slope exceeds 25% for more than 200 sqft along an area of significance to the study. This plane is assumed to occur roughly between 36"-48" above the finish grade relative to the local position of the measurement.

Window and door openings are calculated as if they are composed of the adjacent material. Openings are only calculated when they are at least 12" wide. All corners, despite radius, are treated as an incidence of occurrence. Unless specifically noted all walls are expected to have an additive reflectance of less than 3 decibels.

All instances of occurrence that are 6 decibels or less than the calculated sound pressure are ignored as an insignificant additive source.

Interior sound levels caused by exterior sound levels are labeled at each contour.

Hot Spot maps vs Persistence map. These are two different methods of representing the sound measurements across a site. A "Hot Spot" map acts as if the measurement was loudest point of the sound with the map interpreted from there. They tend to show sound levels lower than actual. A "Persistence" map takes sound measurements as points on a landscape similar to how surveyors take

points and show a topographic map. They tend to show levels higher than actual. As such, both models were shown for reference.

It should be noted that sound maps represent the worst-case scenario at the time when all sound generating sources are in operation. When these sound generators are not in operation the noise levels will be comparable to existing levels.

**Ambient Noise Levels:**

Sound measurements, unless otherwise noted, are assumed to be peak or measurements of short duration. For purposes of this report Ambient Noise Levels are those that have been averaged across a fifteen (15) minute time span in one (1) minute increments.

For this model it is assumed that the greatest Ambient Noise Level occurs during the last minute of each activity cycle hence the shortest activity cycle is three (3) minutes long. This gives a increment ratio of 5 peak increments to 10 non-peak increments. As it was found that non-peak increments were at or below the existing Ambient Noise Levels recorded, those will used in place of non-peak increments.

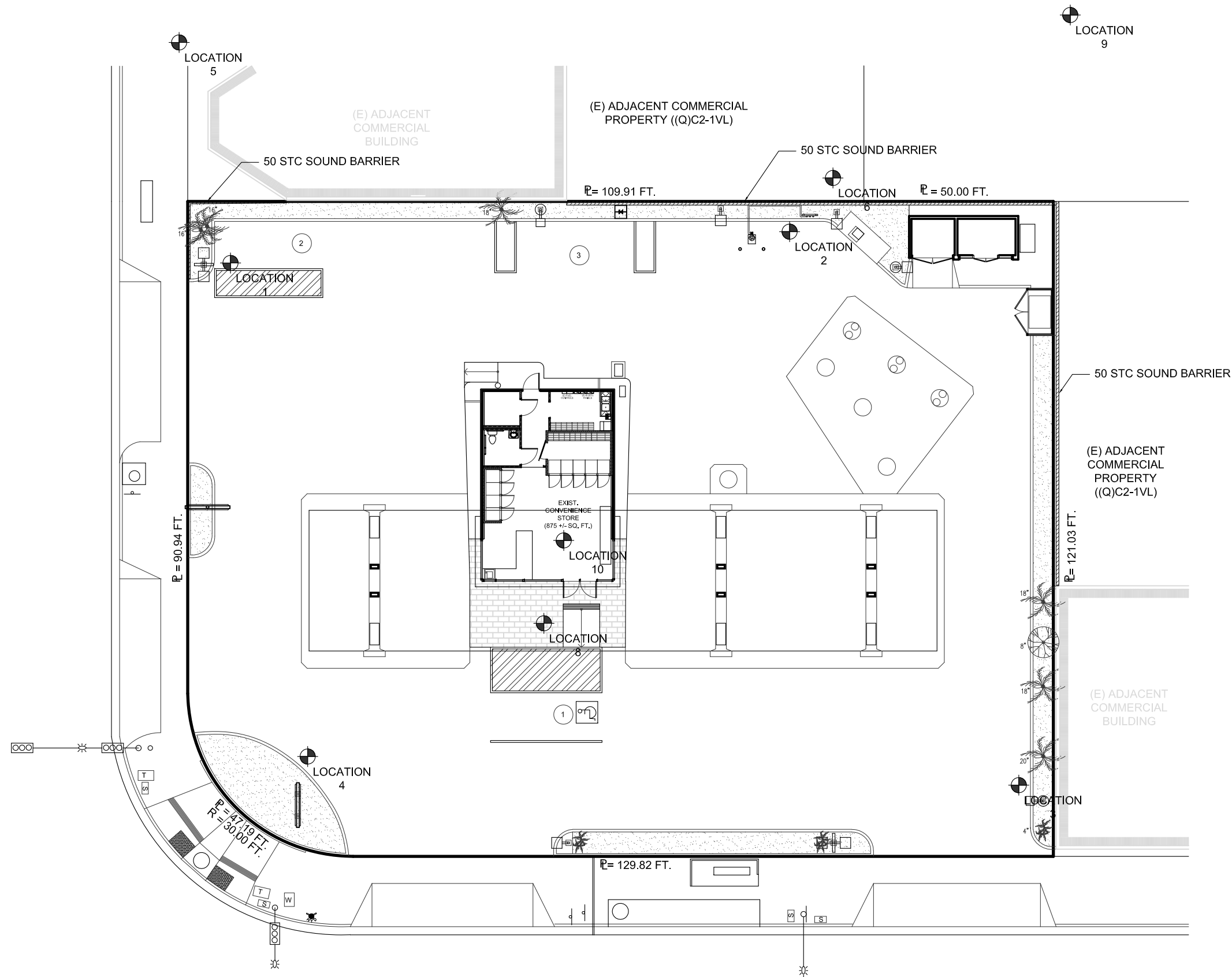
Therefor for purposes of this model Ambient Noise Levels will use the following equation for Ambient Noise Levels:

$$\frac{\sum_P(P > 0) + \sum_{Pn}(Pn > 0)}{\sum_n} = \textit{Ambient Noise Level}$$

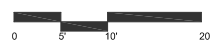
**Sound barriers:**

When are recommended or provided measurements will be provided as both a line of site, sound penetration, calculation and as a point of incidence. Lines of site provide sound levels at the sound walls opposite side where as a point of incidence calculation will provide the shortest line of travel around the sound wall to first object of significant incidence unless otherwise noted.

RIVERSIDE DR.



 SAMPLE LOCATIONS



LAUREL CANYON



BLVD.

## 8.2 Samples Tables

Samples were taking on site using a Samsung thin-film non-directional microphone ((2) 1.2 mm aperture). Recordings were processed with Sound Analyzer SLM – Spectrum Analyzer. Per CNEL standards all readings were A-weighted (ANSI S1.4) for human hearing reproduction.

**Temperature** 87F  
**Humidity** 25%  
**Barometer** 29.83 in

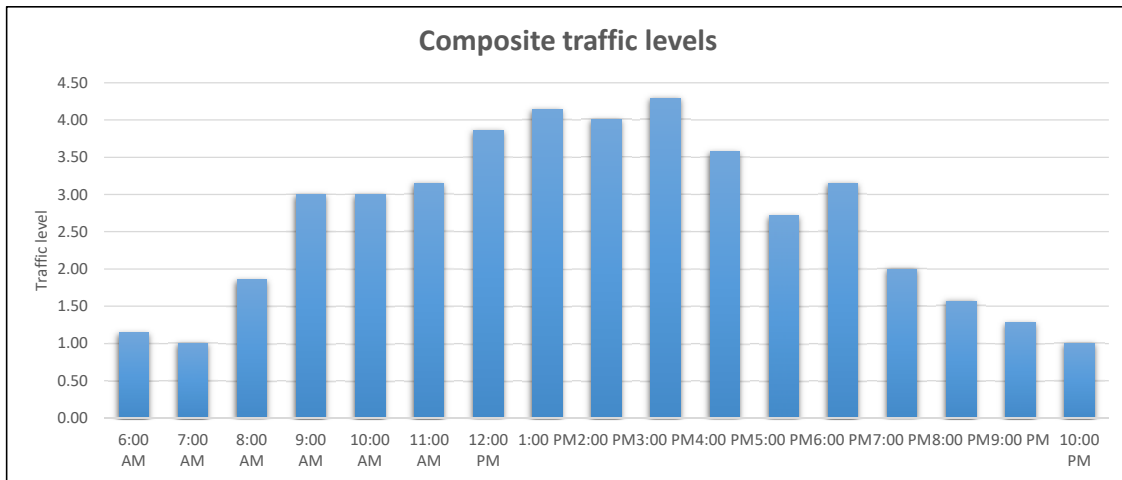
### Primary sampling

Location	Max dbA	Min dbA	LAF dbA	LAeq dbA
1	74.7	62.9	63.8	67.8
2	78.1	56.0	61.6	65.6
3	75.2	59.2	66.4	67.5
4	85.4	61.2	63.2	71.7
5	72.7	57.9	60.7	65.8
6	65.5	55.1	60.5	58.5
7	82.6	60.1	72.3	71.0
8	79.8	62.9	69.5	71.1
9	64.3	50.1	53.8	55.1
10	77.7	58.7	60.7	64.5

# 9.0 Traffic Levels Analysis

	Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday		avg	med
6:00 AM	1	1	1	2	1	1	1	8	1.14	1
7:00 AM	1	1	1	1	1	1	1	7	1.00	1
8:00 AM	1	2	2	3	2	2	1	13	1.86	2
9:00 AM	1	4	4	4	4	2	2	21	3.00	4
10:00 AM	1	4	3	4	4	3	2	21	3.00	3
11:00 AM	2	3	4	3	3	4	3	22	3.14	3
12:00 PM	2	3	4	5	5	5	3	27	3.86	4
1:00 PM	1	4	5	5	5	5	4	29	4.14	5
2:00 PM	2	4	5	4	5	5	3	28	4.00	4
3:00 PM	3	3	5	5	5	5	4	30	4.29	5
4:00 PM	1	4	3	5	5	5	2	25	3.57	4
5:00 PM	1	3	3	4	3	3	2	19	2.71	3
6:00 PM	1	3	5	4	4	4	1	22	3.14	4
7:00 PM	1	2	2	3	2	3	1	14	2.00	2
8:00 PM	1	2	3	2	1	1	1	11	1.57	1
9:00 PM	1	2	2	1	1	1	1	9	1.29	1
10:00 PM	1	1	1	1	1	1	1	7	1.00	1
	<b>22</b>	<b>46</b>	<b>53</b>	<b>56</b>	<b>52</b>	<b>51</b>	<b>33</b>	<b>313</b>		
avg	1.29	2.71	3.12	3.29	3.06	3.00	1.94			
med	1	3	3	4	3	3	2			

Highest traffic day: Wednesday  
 Lowest traffic day: Sunday  
 Highest traffic hour: 2pm  
 Lowest traffic hour: 7am, 10pm  
 Average traffic level: 2.63 low  
 Median traffic level: 2 low



Traffic level scale from 1 (low) to 13 (very high).  
 Google Traffic Services API map data 2019  
 Data module point 5J53+57 Los Angeles, California

## 9.1 Traffic Level Description

Traffic levels are used to calculate the street sound wall used in proposed calculations. Depending on the type of analysis the sound wall can be used as a termination point or as a generator.

Traffic levels are a composite description of traffic per direction or the combined tally of traffic directions along the perimeter of the test site. Traffic is graded between 1 (low) to 4 (very high). All data is pulled from Google's Traffic Estimator API in increments as described in the testing data. In cumulative analysis site with one side facing traffic will have two directions, sites with two sides will have four as so on.

Levels are weighted for signal lighting, and are described and awarded values as follows:

**1 (low)** – Little to freely moving traffic with three or more car lengths typically between vehicles. Vehicles are moving at the posted speed. Expected sound level: 60 dbA.

**2 (medium)** – Dense traffic moving at the posted speed with one or less car lengths between vehicles. Expected sound level: 70 dbA.

**3 (high)** – Dense traffic requiring vehicles to periodically come to a full stop. Expected sound level: 65 dbA.

**4 (very high)** – Dense traffic requiring vehicles to frequently stop or remain stopped for periods of 30 seconds or more. Expected sound level: 70 dbA.

Contour (dbA)	Area (sqft)
72.00	2,025.51
71.00	2,755.99
70.00	2,432.93
68.00	2,554.96
67.00	4,743.72
66.00	3,412.79
65.00	135.02
Average	68.43 dbA
weighted average	68.51 dbA

Table limited to property boundaries.



Existing Site Plan - Persistence map

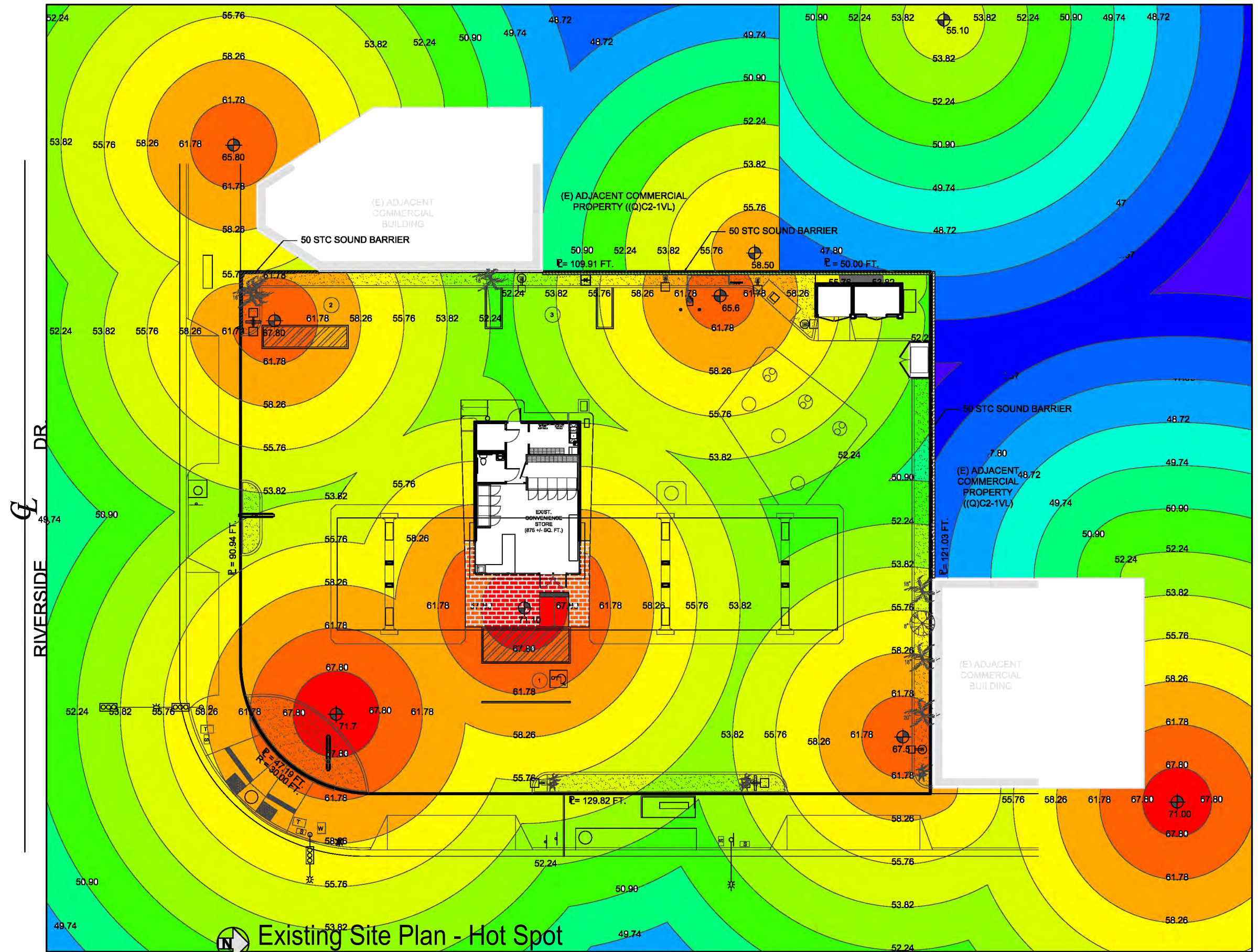


LAUREL CANYON BLVD.

Contour (dbA)	Area (sqft)
67.80	608.95
61.78	2,241.87
58.26	3,397.57
55.76	2,622.30
53.82	3,864.85
52.24	4,893.57
50.90	973.31
49.74	50.14

Average 56.29 dbA  
 Weighted average 55.74 dbA

Table limited to property boundaries.



Existing Site Plan - Hot Spot

LAUREL CANYON BLVD.



# Existing Sound Levels (dbA)

	Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday		avg	med
6:00 AM	62.47	63.18	63.73	63.67	62.59	63.92	63.67	<b>443.23</b>	63.32	63.67
7:00 AM	63.71	62.63	62.61	63.69	62.44	62.47	62.46	<b>440.01</b>	62.86	62.61
8:00 AM	63.99	65.38	64.11	66.95	65.17	65.27	62.48	<b>453.35</b>	64.76	65.17
9:00 AM	62.46	69.67	68.27	68.36	68.46	64.91	64.61	<b>466.74</b>	66.68	68.27
10:00 AM	62.41	67.17	66.34	67.49	68.39	67.18	64.85	<b>463.83</b>	66.26	67.17
11:00 AM	65.37	65.27	68.74	67.32	66.55	68.48	67.73	<b>469.46</b>	67.07	67.32
12:00 PM	66.01	66.38	68.47	70.19	72.45	70.43	68.79	<b>482.72</b>	68.96	68.79
1:00 PM	64.59	68.36	75.12	76.43	73.32	72.42	69.1	<b>499.34</b>	71.33	72.42
2:00 PM	64.78	68.94	74.56	69.47	71.28	73.69	66.34	<b>489.06</b>	69.87	69.47
3:00 PM	66.85	67.39	73.4	71.24	74.55	72.41	67.81	<b>493.65</b>	70.52	71.24
4:00 PM	62.5	68.29	66.96	70.97	72.39	73.47	65.83	<b>480.41</b>	68.63	68.29
5:00 PM	63.26	66.91	66.88	68.82	68.43	66.57	64.61	<b>465.48</b>	66.50	66.88
6:00 PM	62.91	67.7	76.13	68.48	67.78	65.96	63.68	<b>472.64</b>	67.52	67.7
7:00 PM	63.68	65.71	65.75	67.84	65.04	67.04	62.41	<b>457.47</b>	65.35	65.71
8:00 PM	63.27	66.8	67.38	63.79	63.94	63.88	62.37	<b>451.43</b>	64.49	63.88
9:00 PM	63.55	65.29	63.89	62.84	62.78	62.98	62.44	<b>443.77</b>	63.40	62.98
10:00 PM	63.69	62.72	62.41	63.95	62.52	63.21	62.49	<b>440.99</b>	63.00	62.72
11:00 PM	60.46	62.53	64.51	65.38	63.49	63.52	61.73	<b>441.62</b>	63.09	63.49
12:00 AM	60.35	60.31	60.82	62.72	64.38	63.72	62.94	<b>435.24</b>	62.18	62.72
1:00 AM	59.28	59.57	60.47	60.73	60.16	58.49	58.42	<b>417.12</b>	59.59	59.57
2:00 AM	60.62	57.32	58.93	57.29	60.37	58.23	57.79	<b>410.55</b>	58.65	58.23
3:00 AM	52.86	56.83	58.35	57.26	56.72	57.47	55.02	<b>394.51</b>	56.36	56.83
4:00 AM	57.29	60.31	59.27	61.36	59.38	58.72	56.19	<b>412.52</b>	58.93	59.27
5:00 AM	63.26	62.69	63.84	63.73	63.59	62.89	60.37	<b>440.37</b>	62.91	63.26
	<b>1499.62</b>	<b>1547.35</b>	<b>1580.94</b>	<b>1579.97</b>	<b>1576.17</b>	<b>1567.33</b>	<b>1514.13</b>	<b>10865.51</b>		
Daily										
avg	62.48	64.47	65.87	65.83	65.67	65.31	63.09	<b>64.68</b>		
med	63.68	66.8	66.96	67.84	67.78	66.57	64.61	<b>66.32</b>		
Average										
Day	63.94	66.54	68.19	67.99	67.84	67.52	64.88	<b>66.70</b>		
Night	59.57	60.34	61.24	61.52	61.34	60.87	59.52	<b>60.63</b>		
Day	7am-10pm									
Night	10pm-7am									

# 11.1 Proposed Condition Description

## 11.1.1 PROPOSED AND EXISTING CONTRIBUTING ELEMENTS

### Noise generators

- Carwash dryer blowers. Located on the street side “exit” of the carwash about 7’-10’ within the structure. It will be assumed that the point source will at 90 dbA, as described by manufacturer vendors.
- Vacuums. Located along the west property line about 2/3’s into the property. It will be assumed that the point source will at 85 dbA, as described by manufacturer vendors.
- The Laurel Canyon/Riverside sound wall. Sound coming from both Laurel Canyon Blvd. and Riverside Dr. constitute a significant source of noise within the site.

### Non-building sound barriers

- Masonry wall located at “exit” of carwash. Estimated STC: 50.
- Masonry wall located along the West property line. Estimated STC: 50.
- Masonry wall located along the North property line. Estimated STC: 50.

## 11.1.2 NOISE CRITERIA

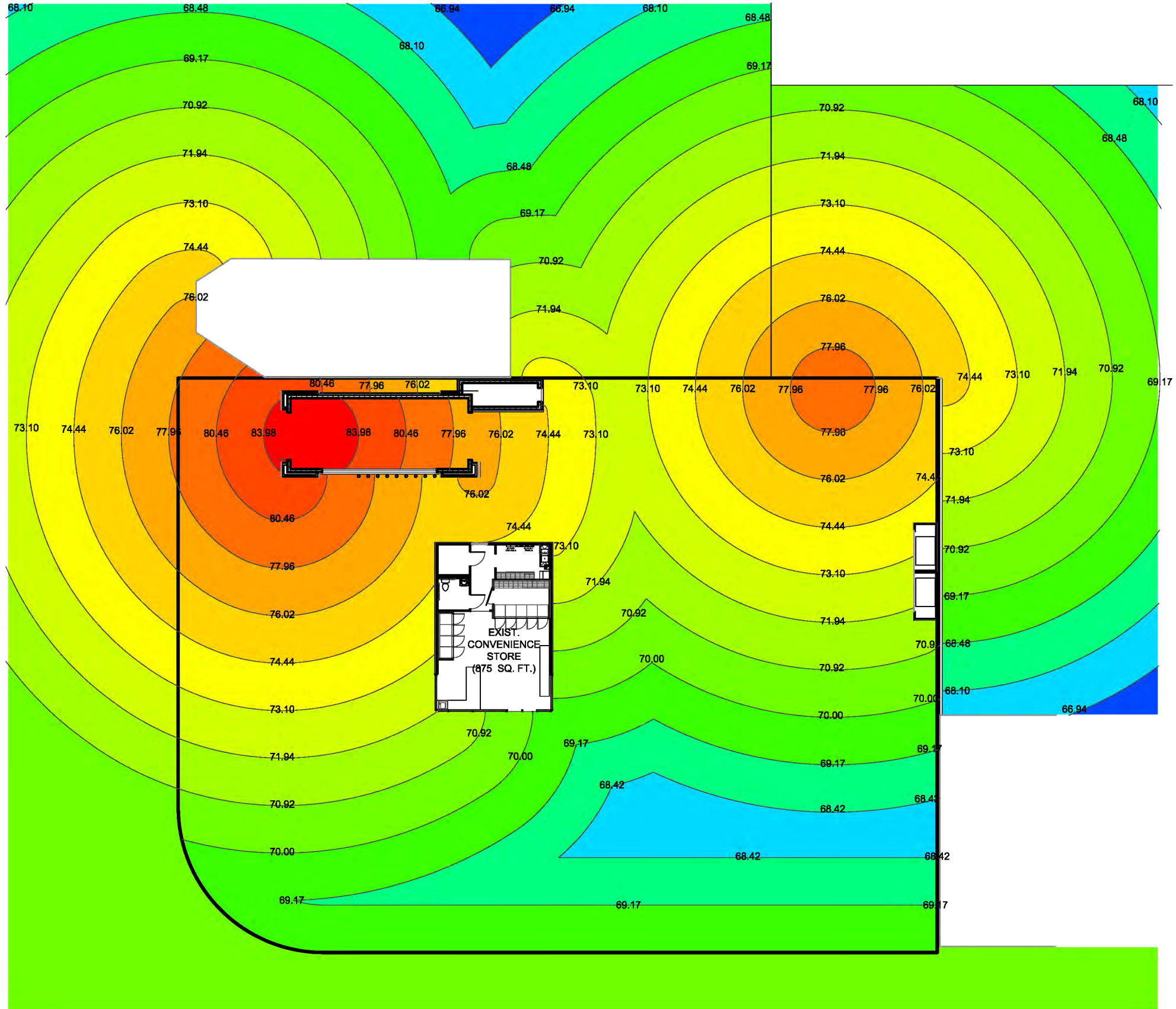
### Exterior Noise Levels – Residential

Existing on-site noise levels were established by on site samplings. A sound map was developed from this material to extend nearby residential locations as required by city planning staff. As this map contained all onsite generating elements to create a standard baseline noise level.

As the only significant variable remaining, a table was developed that used both the on-site noise level and traffic dependent noise levels to create the required Residential Noise Level. This was made possible by data from Google on the traffic levels, and was averaged into 1-hour steps across a typical week in a typical year.

### CALGreen – Non-residential

Section 5.507.4.2 of the 2016 California Green Building Code stipulates that for buildings exposed to a noise level of 65 dB or more when measured as a 1-hour Equivalent Sound Level (Leq), the building façade, including walls, windows, and roofs, shall provide enough sound insulation so that the interior sound level from exterior sources does not exceed 50 dBA during any hour of operation. This applies to non-residential spaces such as retail space, leasing, and amenities. hour of operation. This applies to non-residential spaces such as retail space, leasing, and amenities.



Contour (dba)	Area (sqft)
83.98	259.66
80.46	548.16
77.96	986.10
76.02	1,314.27
74.44	1,769.51
73.10	1,725.18
71.94	1,758.48
70.92	1,566.02
70.00	1,673.82
69.17	3,212.46
68.42	2,144.06
67.72	910.00

Average 73.68 dbA  
 Weighted average 71.96 dbA

Table limited to property boundaries.

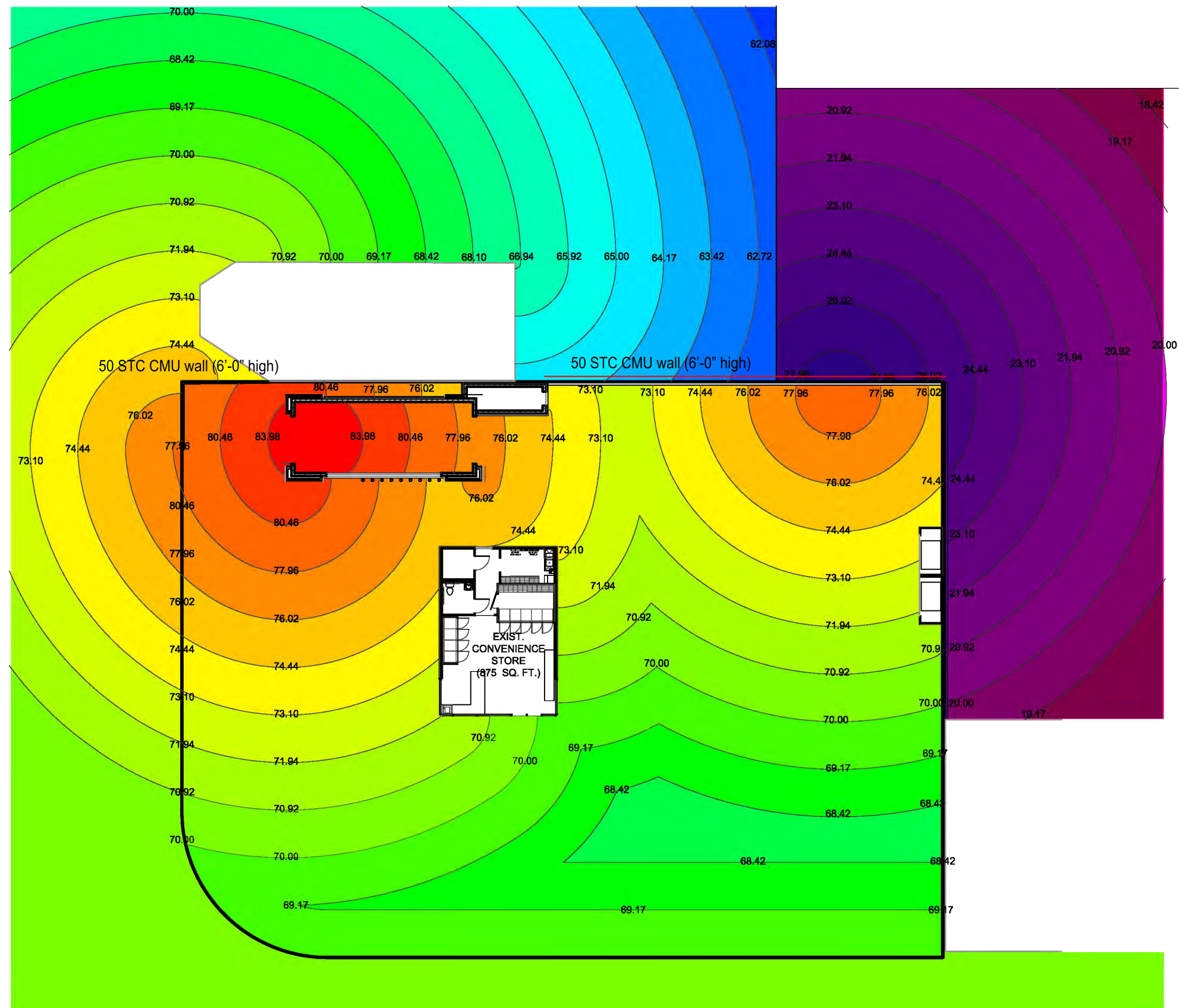
### Proposed Site Plan - No implemented measures

### Peak Noise Levels

Contour (dbA)	Area (sqft)
83.98	259.66
80.46	548.16
77.96	986.10
76.02	1,314.27
74.44	1,769.51
73.10	1,725.18
71.94	1,758.48
70.92	1,566.02
70.00	1,673.82
69.17	3,212.46
68.42	2,144.06
67.72	910.00

Average                    73.68 dbA  
Weighted average        71.96 dbA

Table limited to property boundaries.



## Proposed Site Plan - With implemented Regulatory Compliance Measures

### Peak Noise Levels

## 12.0 Recommendations

Upon reviewing the data and relevant documents MK Design is able to make the following recommendations as a result of the above CNEL Sound study.

During the study it was found that the greatest existing noise element was generated from the Laurel Canyon Blvd./ Riverside Dr. sound wall. During the proposed analysis it was found to be a secondary noise source which still dominated or equaled 25% of the site.

Of the new proposed noise sources only one presented a noise increase off-site, which was the carwash dryer blowers. Most of the noise was already mitigated by the carwash structure and by the sound barrier wall located near the carwash "exit".

The harshest effect from the noise generation occurs at the commercial building frontage located to the west of the property. Peaks calculated at 74 dbA were found with the noise decreasing to about 70 dbA by their driveway. 75 dbA is normally considered the upper threshold of acceptable noise at commercial structures.

The closest residential structure, located to the northwest, is of sufficient distance that street noise was largely dominant over noise generated from the project site with less than 30 dbA contributing at a distance more than 50'.

### **Recommended Regulatory Compliance Measures**

- The sound barrier located near the carwash 'exit' to be increased in height to 1' greater than the carwash exit opening to eliminate all line-of-sight sound propagation. The last 36" near the driveway should be no taller than 36" to allow for traffic sighting.
- The sound barrier located near the carwash 'exit' to be planted as much as possible with 42" high leafy plants, on both side, to minimize "wall crawl" sound propagation.
- Carwash dryer blower should employ any muffler, damper or sound attenuator the carwash manufacturer has available to reduce the initial sound source.
- The sound barrier located along the west property line to be increased to a height of 6' to eliminate all line-of-sight sound propagation.

The net result upon making all Recommended Regulatory Compliance Measures permanent project design features is as follows:

- Noise levels brought on by the new carwash and vacuums will be brought down to acceptable levels in the residential areas during their operation.
- All commercial areas will have acceptable noise levels during carwash and vacuum operation, but not to same extent as residential areas.

- Adjacent commercial and residential uses should actually notice real noise level reduction when carwash and vacuum activities are not taking place, especially at night, as the Recommended Regulatory Compliance Measures would reduce existing traffic noise traveling across the site.

## 13.0 Conclusion

Per the stated recommendations, the carwash equipment and vacuums will be equipped with all manufacturer available options to damper, muffle and otherwise decrease the generated noise, which will bring the dryer blowers and vacuum systems into industry standard noise producing ranges. By implementing the Recommended Regulatory Compliance Measures, which consist of constructing masonry walls along the property edge, the noise levels to adjacent properties will be dramatically reduced.

The closest commercial structure will experience a 7.36 dbA reduction of peak noise levels. This represents a real world reduction of nearly 60%. Non-operation noise levels will also be reduced by about 2 dbA below pre-construction site conditions.

The closest residential structure will experience a 7.11 dbA reduction of peak noise levels. Again, this is a decrease of roughly 60%. Noise levels when neither the carwash or vacuums are operating will have a 2.5 dbA reduction of general noise.

The Recommended Regulatory Compliance Measures reduce the peak noise levels by roughly 60% by the time reaches the property edges.

By averaging the peak noise levels (with the Recommended Regulatory Compliance Measures in place) against the measured existing ambient noise levels our site meets the Minimum Ambient Noise Level (Sec 111.01(a)). As such, we can agree that the project with the Recommended Regulatory Compliance Measures in place does not significantly impact the Ambient Noise Level of the surrounding properties.

## 14.0 Bibliography

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Holger Schulze (2008), *Sound Studies*

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# Appendix A

## LIST OF ACRONYMS AND DEFINITIONS

### **A-Weighting**

A frequency-weighting network used to account for changes in human auditory sensitivity as a function of frequency.

### **Abatement**

The method of reducing the degree of intensity of noise and the use of such a method.

### **Airport Environmental Design Tool (AEDT)**

The Federal Aviation Administration's (FAA), Office of Environment and Energy (AEE-100) has developed the Airport Environmental Design Tool (AEDT) for evaluating aircraft noise impacts in the vicinity of airports. The AEDT replaced the Integrated Noise Model (INM) and has been the FAA's standard tool since 2015 for determining the predicted noise impact in the vicinity of airports. The FAA requires airports use the INM in assessing environmental impacts for soundproofing, evaluating physical improvements to the airfield, analyzing changes to existing or new procedures and in assessing land use compatibility. AEDT utilizes flight track information, aircraft fleet mix, standard and user defined aircraft profiles and terrain as inputs. AEDT produces noise exposure contours that are used for land use compatibility maps. The AEDT program includes built-in tools for comparing contours and utilities that facilitate easy export to commercial Geographic Information Systems. The model also calculates predicted noise at specific sites such as hospitals, schools or other sensitive locations.

### **Annoyance**

Any bothersome or irritating occurrence.

### **CNEL**

Community Noise Equivalent Level. Used in California and is nearly identical to DNL, except that CNEL includes a 5 dB penalty for the evening time period from 7 pm to 10 pm and a 10 dB penalty for the nighttime hours of 10 pm to 7 am.

### **Day-Night Average Sound Level**

#### **(Abbreviation DNL, denoted by the symbol Ldn)**

Twenty-four hour average sound level for a given day, after addition of 10 decibels to levels from midnight to 0700 hours and from 2200 hours to midnight. Ldn is computed as follows:

$$Ldn = LAE + 10 \cdot \log_{10}(N_{day} + 10 \cdot N_{night}) - 49.4 \text{ (dB)}$$

where:

LAE = Sound exposure level in dB (also known as SEL);

N<sub>day</sub> = Number of noise events between 0700 and 2200 hours;

N<sub>night</sub> = Number of noise events between 2200 and 0700 hours; and 49.4 = A normalization constant which spreads the acoustic energy associated with noise events over a 24-hour period, i.e.,

$$10 \cdot \log_{10}(86,400 \text{ seconds per day}) = 49.4 \text{ dB.}$$

### **dBa**

The A-weighted Decibel (dBA) is the most common unit used for measuring environmental sound levels. It adjusts, or weights, the frequency components of sound to conform to the normal response of the human ear at conversational levels. dBA is an international metric that is used for assessing environmental noise exposure of all noise sources.

### **Decibel (dB)**

The Decibel (dB) is the unit used to measure the magnitude or intensity of sound. Decibel means 1/10 of Bel (named after Alexander Graham Bell). The decibel uses a logarithmic scale to cover the very large range of sound pressures that can be heard by the human ear. Under the decibel unit of measure, a 10 dB increase will be perceived by most people to be a doubling in loudness, i.e., 80 dB seems twice as loud as 70 dB.

**Equivalent Sound Level**

(abbreviation TEQ, denoted by the symbol LAeqT or Leq)

Ten times the logarithm to the base ten of the ratio of time-mean-squared instantaneous A-weighted sound pressure, during a stated time interval T, to the square of the standard reference sound pressure.

LAeqT is related to LAE by the following equation:

$$LAeqT = LAE - 10 \cdot \log_{10}(t_2 - t_1) \text{ (dB)}$$

where,

LAE = Sound exposure level in dB

**FAA**

Federal Aviation Administration

**GIS**

Geographic Information Systems. A computer software program to analyze spatial data. Can be especially useful in examining noise distribution over a geographic area.

**Hertz (Hz)**

The Hertz is a unit of measurement of frequency, numerically equal to cycles per second of the measure of the rate of the vibration of the sound. High frequencies can be thought of as having a high pitch; like a whistle; low frequency sounds are more like a rumble of a truck or airplane.

**Intensity**

The sound energy flow through a unit area in a unit time.

**LAE**

See Sound Exposure Level

**Leq or Laeq**

See Equivalent Sound Level

**Ldn**

See Day-Night Average Sound Level

**Line-of-sight**

A direct line between a target and a source. An unobstructed line would be considered having line-of-sight while obstructions would disrupt the line-of-sight.

**Lmax**

See Maximum Noise Level

**Maximum Noise Level**

The maximum noise level, in A-weighted decibels, occurring during an loud single exposure event, e.i. aircraft flyover.

**NMS**

Noise Monitoring Station (locations).

**Noise**

1. Unwanted sound. 2. Any sound not occurring in the natural environment, such as sounds emanating from aircraft, highways, industrial, commercial and residential sources. 3. An erratic, intermittent, or statistically random oscillation.

**Noise Level**

For airborne sound, unless specified to the contrary, the A-weighted sound level.

**Noise Contour**

A Noise Contour is a line on a map that represents equal levels of noise exposure.

**SEL**

See Sound Exposure Level

**SENEL**

Single Event Noise Exposure Level same as Sound Exposure Level

**Sound Exposure Level (abbreviation SEL, denoted by the symbol LAE)**

Over a stated time interval, T (where  $T=t_2-t_1$ ), ten times the base-10 logarithm of the ratio of a given time integral of squared instantaneous A-weighted sound pressure, and the product of the reference sound pressure of 20 micropascals, the threshold of human hearing, and the reference duration of 1 sec. The time interval, T, must be long enough to include a majority of the sound source's acoustic energy. As a minimum, this interval should encompass the 10 dB down points. In addition, LAE is related to LAeqT by the following equation:

$$LAE = LAeqT + 10 \cdot \log_{10}(t_2 - t_1) \text{ (dB)}$$

where, LAeqT = Equivalent sound level in dB (see definition above, also Leq).

**Sound Wall Barrier**

A wall constructed or used in which sound penetration is the primary method of sound propagation.

Walls should be tall enough to prevent line-of-sight to sensitive locations.

**Sound Wall Generator**

A linear field or object considered to act as a uniform noise source.

**Sound Wall terminator**

A linear field or object considered to have a uniform noise contour or sound pressure level at which all sound contours or sound pressure levels equal or less than the field end.

# Appendix B

## BACKGROUND INFORMATION

### 1 INTRODUCTION

This section presents background information on the characteristics of noise and summarizes federal, state and local noise/land use compatibility guidelines. This section also provides the reader with an understanding of the metrics used to assess noise impacts. This section is divided as follows:

- Properties of sound that are important for technically describing sound.
- Acoustic factors influencing human subjective response to sound.
- Potential disturbances to humans and health effects due to sound.
- Sound rating scales used in this study.
- Summary of noise assessment criteria.

### 2 CHARACTERISTICS OF SOUND

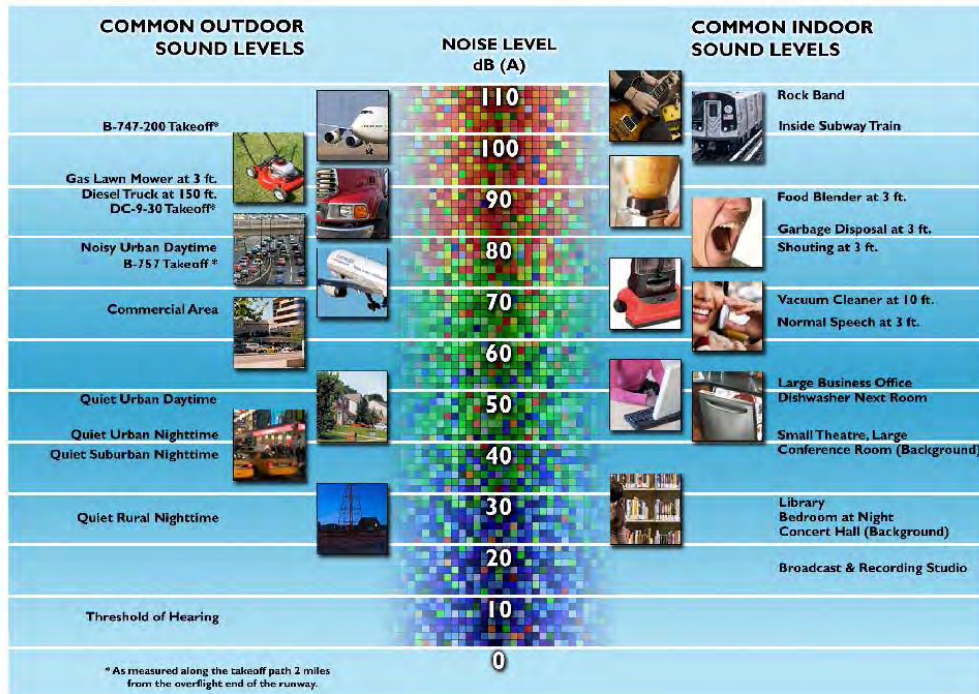
#### 2.1 SOUND LEVEL AND FREQUENCY

Sound can be technically described in terms of the sound pressure (amplitude) and frequency (similar to pitch).

Sound pressure is a direct measure of the magnitude of a sound without consideration for other factors that may influence its perception. The range of sound pressures that occur in the environment is so large that it is convenient to express these pressures as sound pressure levels on a logarithmic scale that compresses the wide range of sound pressures to a more usable range of numbers. The standard unit of measurement of sound is the Decibel (dB), which describes the pressure of a sound relative to a reference pressure.

The frequency (pitch) of a sound is expressed as Hertz (Hz) or cycles per second. The normal audible frequency for young adults is 20 Hz to 20,000 Hz. Community noise, including aircraft and motor vehicles, typically ranges between 50 Hz and 5,000 Hz. The human ear is not equally sensitive to all frequencies, with some frequencies judged to be louder for a given signal than others. As a result of this, various methods of frequency weighting have been developed. The most common weighting is the A-weighted noise curve. The A-weighted decibel scale (dBA) performs this compensation by discriminating against frequencies in a manner approximating the sensitivity of the human ear. In the A-weighted decibel, everyday sounds normally range from 30 dBA (very quiet) to 100 dBA (very loud). Most community noise analyses are based upon the A-weighted decibel scale. Examples of various sound environments, expressed in dBA, are presented in Figure 1.

**Figure 1 Typical A-Weighted Noise Levels**



Source: Landrum & Brown, 1974.

## 2.2 PROPAGATION OF NOISE

Outdoor sound levels decrease as the distance from the source to the receiver increases. This decrease in sound level is a result of wave divergence, atmospheric absorption, and ground attenuation. Sound radiating from a source in an undisturbed manner travels in spherical waves. As the sound wave travels away from the source, the sound energy is dispersed over a greater area, decreasing the sound power of the wave. Spherical spreading of the sound wave reduces the noise level at a rate of 6 dB per doubling of the distance.

Atmospheric absorption also influences the sound levels received by the observer.

The greater the distance traveled, the greater the influence of the atmosphere and the resultant fluctuations. Atmospheric absorption becomes important at distances of greater than 1,000 feet. The degree of absorption varies depending on the frequency of the sound, as well as the humidity and temperature of the air. For example, atmospheric absorption is lowest (i.e., sound carries farther) at high humidity and high temperatures. Absorption effects in the atmosphere vary with frequency. Higher frequencies are more readily absorbed than lower frequencies. Over large distances, lower frequencies become the dominant sound as the higher frequencies are attenuated. Turbulence and gradients of wind, temperature, and humidity also play a significant role in determining the degree of attenuation.

Certain conditions, such as inversions, can channel or focus the sound waves resulting in higher noise levels than would result from simple spherical spreading.

In addition to atmospheric absorption, aircraft noise can also be affected by the physical properties of the surrounding terrain. The magnitude of this terrain-related absorption varies with the angle of the aircraft above the horizon as measured from the observer to the aircraft. Lateral attenuation is influenced by ground reflection, refraction, aircraft shielding, and engine aircraft installation effects. In general, the lower an aircraft is, the greater the lateral attenuation. Lateral attenuation is not considered to be a factor if the angle between the observer and

aircraft, as measured from the horizon, is greater than 60°. In this case, the aircraft is essentially overhead the observer.

## **2.3 DURATION OF SOUND**

Annoyance from a noise event increases with increased duration of the noise event, i.e., the longer the noise event, the more annoying it is. The "effective duration" of a sound is the time between when a sound rises above the background sound level until it drops back below the background level. Psycho-acoustic studies have determined the relationship between duration and annoyance and the amount a sound must be reduced to be judged equally annoying for increased duration. Duration is an important factor in describing sound in a community setting. The relationship between duration and noise level is the basis of the equivalent energy principle of sound exposure. Reducing the acoustic energy of a sound by one-half results in a 3 dB reduction. Doubling the duration of the sound increases the total energy of the event by 3 dB. This equivalent energy principle is based upon the premise that the potential for a noise to impact a person is dependent on the total acoustical energy content of the noise. Defined in subsequent sections of this study, noise metrics such as CNEL, DNL, LEQ and SENEL are all based upon the equivalent energy principle.

## **2.4 CHANGE IN NOISE**

The concept of change in ambient sound levels can be understood with an explanation of the hearing mechanism's reaction to sound. The human ear is a far better detector of relative differences in sound levels than absolute values of levels. Under controlled laboratory conditions, listening to a steady unwavering pure tone sound that can be changed to slightly different sound levels, a person can just barely detect a sound level change of approximately one decibel for sounds in the mid-frequency region. When ordinary noises are heard, a young healthy ear can detect changes of two to three decibels. A five decibel change is readily noticeable while a 10 decibel change is judged by most people as a doubling or a halving of the loudness of the sound. It is typical in environmental documents to consider a 3 dB change as potentially discernable.

## **2.5 MASKING EFFECT**

The ability of one sound to limit a listener from hearing another sound is known as the masking effect. The presence of one sound effectively raises the threshold of audibility for the hearing of a second sound. For a signal to be heard, it must exceed the threshold of hearing for that particular individual and exceed the masking threshold for the background noise.

The masking characteristics of sound depend on many factors including the spectral (frequency) characteristics of the two sounds, the sound pressure levels and the relative start time of the sounds. Masking effect is greatest when the frequencies of the two sounds are similar or when low frequency sounds mask higher frequency sounds. High frequency sounds do not easily mask low frequency sounds.

## **3 FACTORS INFLUENCING HUMAN RESPONSE TO SOUND**

Many factors influence sound perception and annoyance. This includes not only physical characteristics of the sound but also secondary influences such as sociological and external factors. Molino, in the Handbook of Noise Control describes human response to sound in terms of both acoustic and non-acoustic factors. These factors are summarized in Table 1.

Sound rating scales are developed in reaction to the factors affecting human response to sound. Nearly all of these factors are relevant in describing how sounds are perceived in the community. Many non-acoustic parameters play a prominent role in affecting individual response to noise. Background sound, an additional acoustic factor not specifically listed, is also important in describing sound in rural settings. Fields, in his analysis of the effects of

personal and situational variables on noise annoyance, has identified a clear association of reported annoyance and various other individual perceptions or beliefs. In particular, Fields stated:

“There is therefore firm evidence that noise annoyance is associated with: (1) the fear of an aircraft crashing or of danger from nearby surface transportation; (2) the belief that aircraft noise could be prevented or reduced by designers, pilots or authorities related to airlines; and (3) an expressed sensitivity to noise generally.”

Thus, it is important to recognize that non-acoustic factors such as the ones described above as well as acoustic factors contribute to human response to noise.

### **Table 1 Factors that Affect Individual Annoyance to Noise**

#### **Primary Acoustic Factors**

- Sound Level
- Frequency
- Duration

#### **Secondary Acoustic Factors**

- Spectral Complexity
- Fluctuations in Sound Level
- Fluctuations in Frequency
- Rise-time of the Noise
- Localization of Noise Source

#### **Non-acoustic Factors**

- Physiology
- Adaptation and Past Experience
- How the Listener's Activity Affects Annoyance
- Predictability of When a Noise will Occur
- Is the Noise Necessary?
- Individual Differences and Personality

Source: C. Harris, 1979

## **4 SOUND RATING SCALES**

The description, analysis, and reporting of community sound levels is made difficult by the complexity of human response to sound and myriad of sound-rating scales and metrics developed to describe acoustic effects. Various rating scales approximate the human subjective assessment to the "loudness" or "noisiness" of a sound. Noise metrics have been developed to account for additional parameters such as duration and cumulative effect of multiple events.

Noise metrics are categorized as single event metrics and cumulative metrics. Single event metrics describe the noise from individual events, such as one aircraft flyover. Cumulative metrics describe the noise in terms of the total noise exposure throughout the day. Noise metrics used in this study are summarized below:

### **4.1 SINGLE EVENT METRICS**

☒ **Frequency Weighted Metrics (dBA).** In order to simplify the measurement and computation of sound loudness levels, frequency-weighting networks have obtained wide acceptance. The A-weighting (dBA) scale has become the most prominent of these scales and is widely used in community noise analysis. Its advantages are

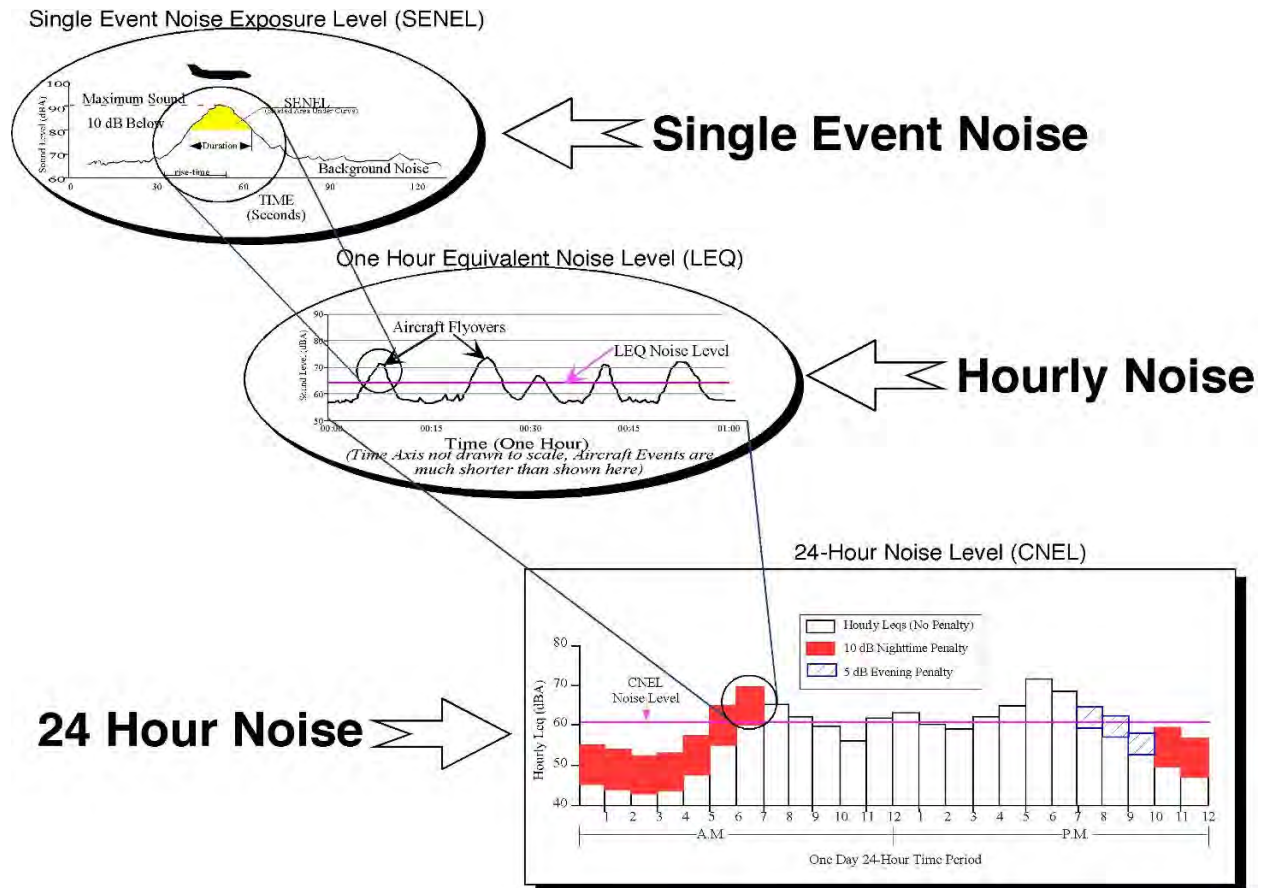
that it has shown good correlation with community response and is easily measured. The metrics used in this study are all based upon the dBA scale.

**Maximum Noise Level.** The highest noise level reached during a noise event is called the "Maximum Noise Level," or Lmax. For example, as an aircraft approaches, the sound of the aircraft begins to rise above ambient noise levels. The closer the aircraft gets the louder it is until the aircraft is at its closest point directly overhead. Then, as the aircraft passes, the noise level decreases until the sound level again settles to ambient levels. Such a history of a flyover is plotted at the top of Figure 3. It is this metric to which people generally instantaneously respond when an aircraft flyover occurs.

**Single Event Noise Exposure Level (SENEL) or Sound Exposure Level (SEL).** Another metric that is reported for aircraft flyovers is the Single Event Noise Exposure Level (SENEL). This metric is essentially equivalent to the Sound Exposure Level (SEL) metric. It is computed from dBA sound levels. Referring again to the top of Figure 3, the shaded area, or the area within 10 dB of the maximum noise level, is the area from which the SENEL is computed. The SENEL value is the integration of all the acoustic energy contained within the event. Speech and sleep interference research can be assessed relative to Single Event Noise Exposure Level data.

The SENEL metric takes into account the maximum noise level of the event and the duration of the event. For aircraft flyovers, the SENEL value is typically about 10 dBA higher than the maximum noise level. Single event metrics are a convenient method for describing noise from individual aircraft events. This metric is useful in that airport noise models contain aircraft noise curve data based upon the SENEL metric. In addition, cumulative noise metrics such as LEQ, CNEL and DNL can be computed from SENEL data.

**Figure 2 Single & Cumulative Noise Metric Definitions**





## 4.2 CUMULATIVE METRICS

Cumulative noise metrics assess community response to noise by including the loudness of the noise, the duration of the noise, the total number of noise events and the time of day these events occur in one single number rating scale.

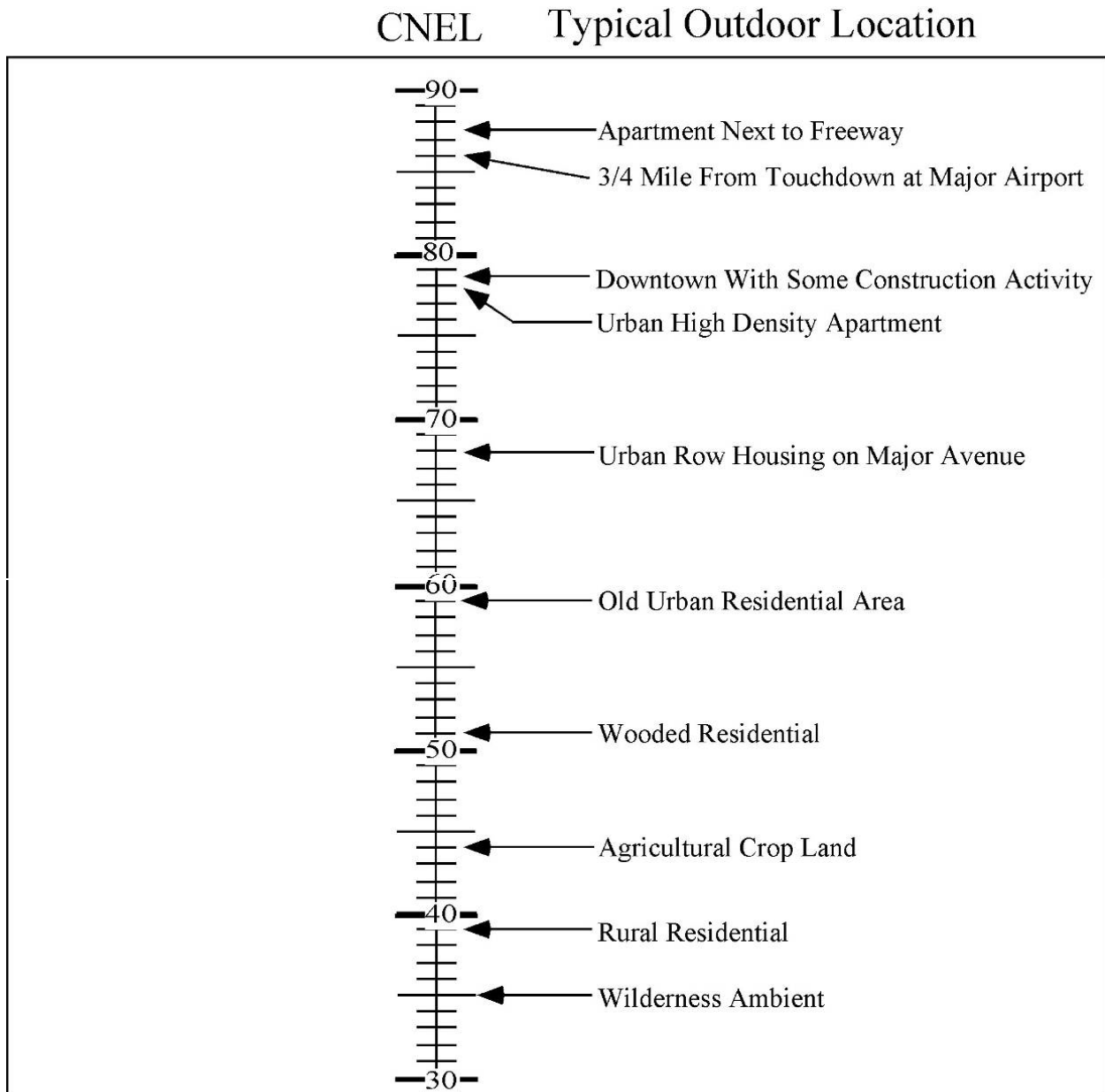
**Equivalent Noise Level (Leq).** Leq is the sound level corresponding to a steady-state, A-weighted sound level containing the same total energy as several SEL events during a given sample period. Leq is the "energy" average noise level during the time period of the sample. It is based on the observation that the potential for noise annoyance is dependent on the total acoustical energy content of the noise. This is graphically illustrated in the middle graph of Figure 3. Leq can be measured for any time period, but is typically measured for 15 minutes, 1 hour or 24-hours. Leq for a one-hour period is used by the Federal Highway Administration for assessing highway noise impacts. Leq for one hour is called Hourly Noise Level (HNL) in the California Airport Noise Regulations and is used to develop Community Noise Equivalent Level (CNEL) values for aircraft operations.

**Community Noise Equivalent Level (CNEL).** CNEL is a 24-hour, time-weighted energy average noise level based on the A-weighted decibel. It is a measure of the overall noise experienced during an entire day. The term "time-weighted" refers to the penalties attached to noise events occurring during certain sensitive time periods. In the CNEL scale, noise occurring between the hours of 7 pm and 10 pm is penalized by approximately 5 dB. This penalty accounts for the greater potential for noise to cause communication interference during these hours, as well as typically lower ambient noise levels during these hours. Noise that takes place during the night (10 pm to 7 am) is penalized by 10 dB. This penalty was selected to attempt to account for the higher sensitivity to noise in the nighttime and the expected further decrease in background noise levels that typically occur in the nighttime.

CNEL is graphically illustrated in the bottom of Figure 2. Examples of various noise environments in terms of CNEL are presented in Figure 3. CNEL is specified for use in the California Airport Noise Regulations and is used by local planning agencies in their General Plan Noise Element for land use compatibility planning.

**Day Night Noise Level (DNL).** The DNL index is very similar to CNEL but does not include the evening (7 pm to 10 pm) penalty that is included in CNEL. It does include the nighttime (10 pm to 7 am) penalty. Typically, DNL is about 1 dB lower than CNEL, although the difference may be greater if there is an abnormal concentration of noise events in the 7 to 10 pm time period. DNL is specified by the Federal Aviation Administration (FAA) for airport noise assessment and by the Environmental Protection Agency (EPA) for community noise and airport noise assessment. The FAA guidelines (described later) allow for the use of CNEL as a substitute to DNL.

Figure 3 Typical Outdoor Noise Levels



Source: Adapted from "Information on Levels of Environmental Noise Requisite to Protect Public Health and Welfare With an Adequate Margin of Safety", EPA, 1974

#### 4.3 EFFECTS OF NOISE ON HUMANS

Noise, often described as unwanted sound, is known to have several adverse effects on humans. From these known adverse effects of noise, criteria have been established to help protect the public health and safety and prevent disruption of certain human activities. These criteria are based on effects of noise on people such as hearing loss (not a factor with typical community noise), communication interference, sleep interference, physiological responses and annoyance. Each of these potential noise impacts on people are briefly discussed in the following narrative:

**Hearing Loss** is generally not a concern in community noise problems, even very near a major airport or a major freeway. The potential for noise induced hearing loss is more commonly associated with occupational noise exposures in heavy industry, very noisy work environments with long term exposure, or certain very loud recreational activities such as target shooting, motorcycle or car racing, etc. The Occupational Safety and Health Administration (OSHA) identifies a noise exposure limit of 90 dBA for 8 hours per day to protect from hearing loss (higher limits are allowed for shorter duration exposures). Noise levels in neighborhoods, even in very noisy neighborhoods, are not sufficiently loud to cause hearing loss.

**Communication Interference** is one of the primary concerns in environmental noise problems. Communication interference includes speech interference and interference with activities such as watching television. Normal conversational speech is in the range of 60 to 65 dBA and any noise in this range or louder may interfere with speech. There are specific methods of describing speech interference as a function of distance between speaker and listener and voice level. Figure 5 shows the relation of quality of speech communication with respect to various noise levels.

**Sleep Interference** is a major noise concern in noise assessment and, of course, is most critical during nighttime hours. Sleep disturbance is one of the major causes of annoyance due to community noise. Noise can make it difficult to fall asleep, create momentary disturbances of natural sleep patterns by causing shifts from deep to lighter stages, and cause awakening. Noise may even cause awakening, which a person may, or may not, be able to recall.

Extensive research has been conducted on the effect of noise on sleep disturbance.

Recommended values for desired sound levels in residential bedroom space range from 25 to 45 dBA, with 35 to 40 dBA being the norm. Some years ago, the National Association of Noise Control Officials published data on the probability of sleep disturbance with various single event noise levels. Based on laboratory experiments conducted in the 1970s, it was determined that a noise event with an interior noise exposure of 75 dBA interior will cause noise induced awakening in 30 percent of the cases.

However, research first published in Britain in the 1990s has shown that the probability for sleep disturbance, when measured in an in-home setting is much less than what had been reported in earlier research that was based on laboratory studies. This research showed that once a person was asleep, it is much more unlikely that they will be awakened by a noise. The significant difference in the British studies is the use of actual in-home sleep disturbance patterns as opposed to laboratory data that had been the historic basis for predicting sleep disturbance. Some of this research has been criticized because it was conducted in areas where subjects had become habituated to aircraft noise. On the other hand, some of the earlier laboratory sleep studies had been criticized because of the extremely small sample sizes of most laboratory studies and because the laboratory was not necessarily a representative sleep environment. A 1994 British sleep study compared the various causes of sleep disturbance using in-home sleep studies. This field study assessed the effects of nighttime aircraft noise on sleep in 400 people (211 women and 189 men; 20-70 years of age; one per household) habitually living at eight sites adjacent to four U.K. airports, with different levels of nighttime flying. The main finding was that only a minority of aircraft noise events affected sleep, and, for most subjects, that domestic and other non-aircraft factors had much greater effects. As shown in the Figure 6, aircraft noise was a minor contributor among a host of other factors that lead to awakening response.

The Federal Interagency Committee on Noise (FICON) in 1992, in a document entitled Federal Interagency Review of Selected Airport Noise Analysis Issues, recommended an interim dose-response curve for sleep disturbance based on laboratory studies of sleep disturbance. In June of 1997, the Federal Interagency Committee on Aviation Noise (FICAN) updated the FICON recommendation with an updated curve based on the more recent in-home sleep disturbance studies which show lower rates of awakening compared to the laboratory studies. The FICAN recommended a curve based on the upper limit of the data presented and therefore considers the curve to

represent the “maximum percent of the exposed population expected to be behaviorally awakened,” or the “maximum awakened.”

**Physiological Responses** are those measurable effects of noise on people, which are realized as changes in pulse rate, blood pressure, etc. While such effects can be induced and observed, the extent is not known to which these physiological responses cause harm or are a sign of harm. Generally, physiological responses are a reaction to a loud short-term noise such as a rifle shot or a very loud jet over flight.

Health effects from noise have been studied around the world for nearly thirty years. Scientists have attempted to determine whether high noise levels can adversely affect human health—apart from auditory damage—which is amply understood. These research efforts have covered a broad range of potential impacts from cardiovascular response to fetal weight and mortality. Yet, while a relationship between noise and health effects seems plausible, it has remained a difficult effect to quantify—that is, shown in a manner that can be repeated by other researchers while yielding similar results.

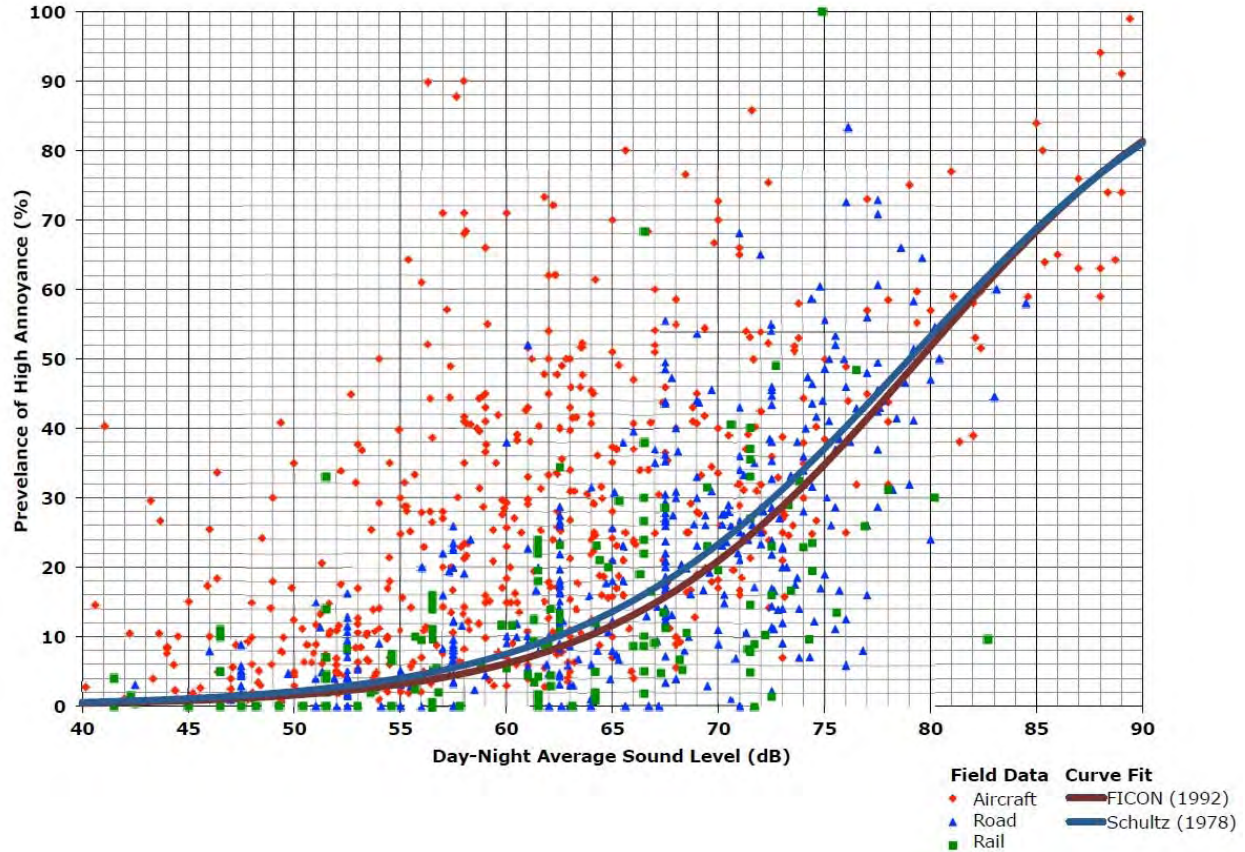
While annoyance and sleep/speech interference have been acknowledged, health effects are also associated with a wide variety of other environmental stressors, including air pollution. Isolating the effects of aircraft noise alone as a source of long-term physiological change has proved to be almost impossible as the effects associated with noise are also the same well-known effects of air pollution. In a review of 30 studies conducted worldwide between 1993 and 1998 [17], a team of international researchers concluded that, while some findings suggest that noise can affect health, improved research concepts and methods are needed to verify or discredit such a relationship. They called for more study of the numerous environmental and behavioral factors than can confound, mediate or moderate survey findings. In 2008, the Airport Cooperative Research Board (ACRP), a part of the National Academies, published a synthesis on the effects of aircraft noise.

The ACRP synthesis concluded, “Despite decades of research, including review of old data and new research efforts, health effects of aviation noise continues to be an enigma. Most, if not all, current research concludes that it is yet impossible to determine causal relations between health disorders and noise exposure, despite well-founded hypotheses.”

**Annoyance** is the most difficult of all noise responses to describe. Annoyance is a very individual characteristic and can vary widely from person to person. What one person considers tolerable can be quite unbearable to another of equal hearing capability. The level of annoyance, of course, depends on the characteristics of the noise (i.e.; loudness, frequency, time, and duration), and how much activity interference (e.g. speech interference and sleep interference) results from the noise. However, the level of annoyance is also a function of the attitude of the receiver. Personal sensitivity to noise varies widely. It has been estimated that 2 to 10 percent of the population is highly susceptible to annoyance from any noise not of their own making, while approximately 20 percent are unaffected by noise. Attitudes are affected by the relationship between the person and the noise source (Is it our dog barking or the neighbor's dog?). Whether we believe that someone is trying to abate the noise will also affect the level of annoyance.

Annoyance levels have been correlated to CNEL levels. Figure 4 relates DNL noise levels to community response from two of these surveys. One of the survey curves presented in Figure 4 is the well-known Schultz curve, developed by Theodore Schultz. It displays the percent of a populace that can be expected to be annoyed by various DNL (CNEL in California) values for residential land use with outdoor activity areas. At 65 dB DNL, the Schultz curve predicts approximately 14 percent of the exposed population reporting themselves to be “highly annoyed.” At 60 dB DNL, this decreases to approximately 8 percent of the population. However, Figure 4 shows that the data used to determine the Schultz curve and updates have a very wide range of scatter, with communities near some airports reporting much higher percentages of population highly annoyed at these noise exposure levels. Annoyance levels have never been correlated statistically to single event noise exposure levels in airport-related studies.

**Figure 4 Percent of Population Highly Annoyed as a Function of DNL**



Source: Schultz (1978) & FICON (1992)

In recent years, there has been the suggestion in Europe and by researchers in the US that the noise dose, response curve for annoyance from aircraft noise is different for aviation noise than it is for road and rail noise. In these studies, it has been suggested that the percentage of the population highly annoyed at 65 DNL is closer to 30 percent of the population and not the 14 percent as suggested by the Schultz curve. The US studies go on further to describe that communities form unique attitudes about noise and differing communities show a wide range of annoyance response for the same noise exposure that can be attributed to non-acoustic factors.

**School Room Effects.** Interference with classroom activities and learning from aircraft noise is an important consideration and the subject of much recent research. Studies from around the world indicate that vehicle traffic, railroad, and aircraft noise can have adverse effects on reading ability, concentration, motivation, and long term learning retention. A complicating factor in this research is the extent of background noise from within the classroom itself. The studies indicating the most adverse effects examine cumulative noise levels equivalent to 65 CNEL or higher and single event maximum noise levels ranging from 85 to 95 dBA. In other studies, the level of noise is unstated or ambiguous. According to these studies, a variety of adverse school room effects can be expected from interior noise levels equal to or exceeding 65 CNEL, and/or 85 dBA SEL.

Some interference with classroom activities can be expected with noise events that interfere with speech. As discussed in other sections of this report, speech interference begins at 65 dBA, which is the level of normal conversation. Typical construction attenuates outdoor noise by 20 dBA with windows closed and 12 dBA with

windows open. Thus, some interference of classroom activities can be expected at outdoor levels of 77 to 85 dBA, the latter being the noise level for the SENEL contours.

APCSV-2019-1481-SPE-SPP-CU-ZV

Exhibits

Exhibit D – Site Photos, Case Referral Form



# INDEX MAP - SITE PHOTOGRAPHS

## LEGEND

PROJECT SITE

**PROJECT SITE:**  
 2155 S. EUCLID AVE.  
 ONTARIO, CA 91762  
 APN: 1050-571-01-0000





① Facing West - South at the east side of project property. East of property, East facade of existing fuel canopy with dispensers/ conc. curb and convenience store with curb (tile and conc. finish on top). And depicting existing handicap parking with conc. ramp.



② Facing West - North at the east side of project property. East of property, East facade of existing fuel canopy with dispensers/ conc. curb. And depicting existing landscape planter along the east and north of property.



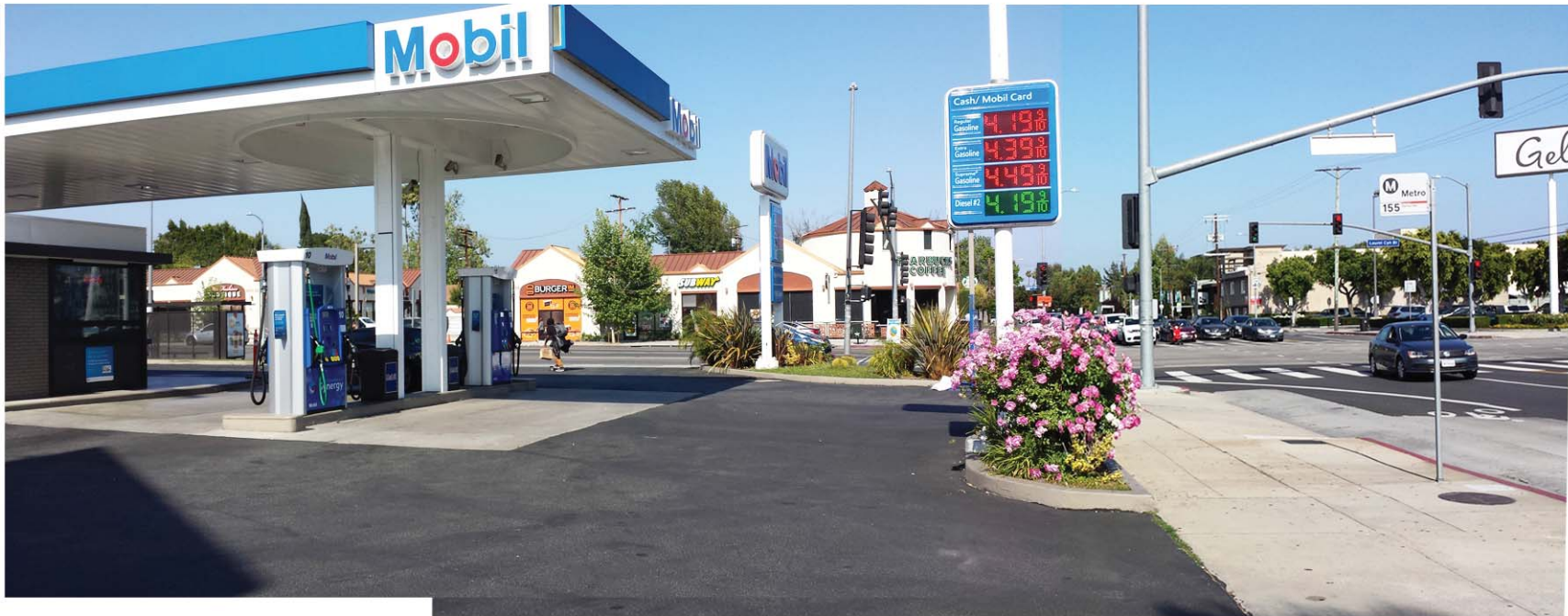
**3** Facing East - South at the Northwest corner of the project property. Northwest of property, west and north facade of existing fuel canopy with dispensers/ conc. curb and North facade of existing convenience store with curb (tile and conc. finish on top). And depicting existing landscape planter with conc. curb. along the North of property.



**4** Facing South - West at the Northwest corner of the project property. Northwest of property, west facade of existing fuel canopy with dispensers/ conc. curb and North and South facade of existing convenience store with conc. curb. Existing landscape planter with conc. curb. along the West of property. And existing fuel vapor canister (to be relocated), storage building (to be removed), and exist. parking along West of property.



5 Facing West - North at the Northwest corner of the project property. Northwest of property, east facade of existing storage building (to be removed) and existing metal trash enclosure (to be removed and relocated).



6 Facing West at the South side of the project property. South of property, South of property, West and South facade of existing fuel canopy with dispensers/ conc. curb and partial South facade of the existing convenience store with curb (tile and conc. finish on top). And planters along the South and Southeast corner of the property where the existing main pole price (at the corner intersection) and second pole price sign (South).



7 Facing North - East at the Southwest corner of the project property. Southwest of property, west facade of existing fuel canopy and West/South facade of existing convenience store with curb (tile and conc. finish on top). And depicting existing landscape planter with conc. curb. along the West of property with 2 existing parking.



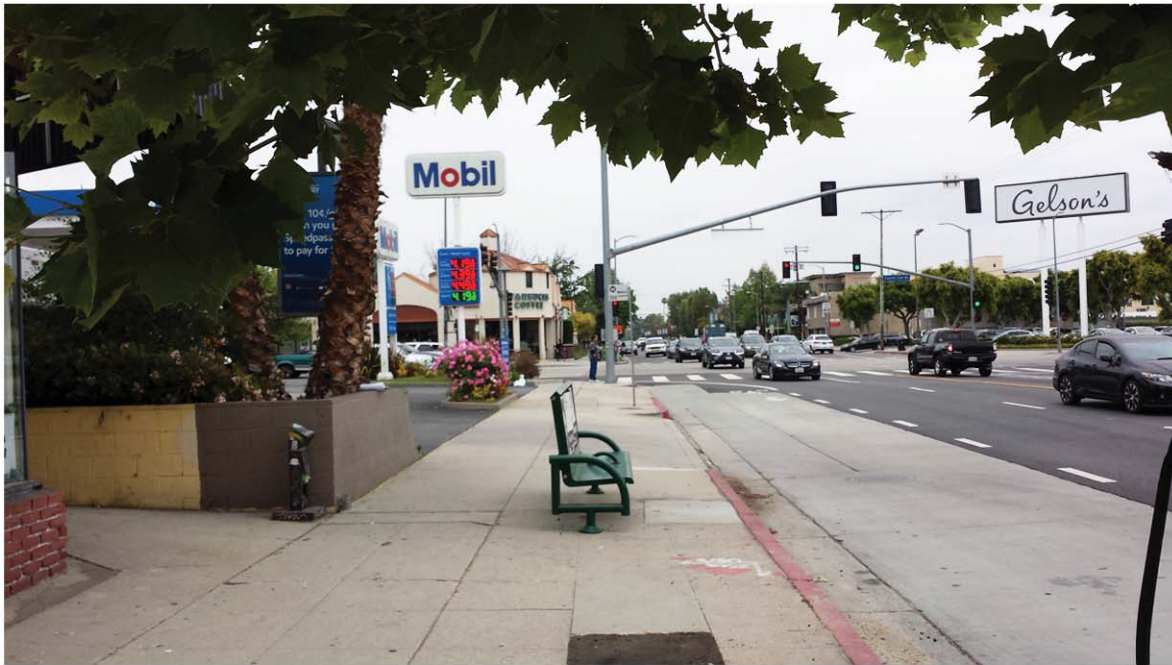
8 Facing North at the South of the project property. South of property, partial South facade of existing convenience store with curb. In addition, depicting existing landscape planter with conc. curb. along the West of property with 2 existing parking, and adjacent commercial (existing Cleaners) Building along the west of property, behind the building is commercial parking for the cleaners (the rest of the length along the west of the property).



9 Facing East at the East of the project property. West of property, partial west facade of existing fuel canopy and West facade of existing convenience store with curb (conc. finish on top).



10 Facing East at the East of the project property. West of property, partial west facade of existing fuel canopy and West facade of existing convenience store with curb (conc. finish on top). In addition, depicting existing landscape planter with conc. curb. along the South of the property with the secondary price pole signage.



11 Facing East along Riverside Dr. at the South of the project property. South of property, existing landscape planter with conc. curb. along the South of the property with the secondary price pole signage. And bus bench in front of the adjacent south commercial property.



12 Facing West at the Southeast corner of the project property. Southeast corner of the property, partial East/South facade of existing fuel canopy and existing convenience store with conc. curb (tile and conc. finish on top). In addition, depicting existing landscape planter with conc. curb. at the Southeast corner at the intersection of Riverside Dr. and Laurel Canyon Rd. of the property. And the secondary price pole signage at South of Prop.



13

Facing North at the Southeast corner of the project property. Southeast corner of the property, partial East facade of existing fuel canopy and existing convenience store with conc. curb (tile and conc. finish on top). In addition, depicting partial existing landscape planter with conc. curb and the main price pole signage at the Southeast corner at the intersection of Riverside Dr. and Laurel Canyon Rd. of the property.



14 Facing South at the North of the project property along Laurel Canyon Blvd at the corner of La Maida st. and Laurel Canyon Blvd.. East facade of the North adjacent commercial building (Citibank). Across Laurel Canyon, East of property, commercial buildings are along Laurel Canyon Blvd.



15 Facing West - North (view down Maida St. at the corner of La Maida st. and Laurel Canyon Blvd.), commercial buildings are along Laurel Canyon Blvd., and multi-residential and single residential are behind the commercial properties.





16 Facing North - East (view down Laurel Canyon Blvd. at the corner of La Maida St. and Laurel Canyon Blvd.), commercial buildings are along Laurel Canyon Blvd. and multi-residential further North the commercial properties start at Huston St. going North along Laurel canyon Blvd.



17 Facing East - South (view down Laurel Canyon Blvd. at the corner of La Maida St. and Laurel Canyon Blvd.), commercial buildings are along Laurel Canyon Blvd. and multi-residential and single residential are behind the commercial properties.



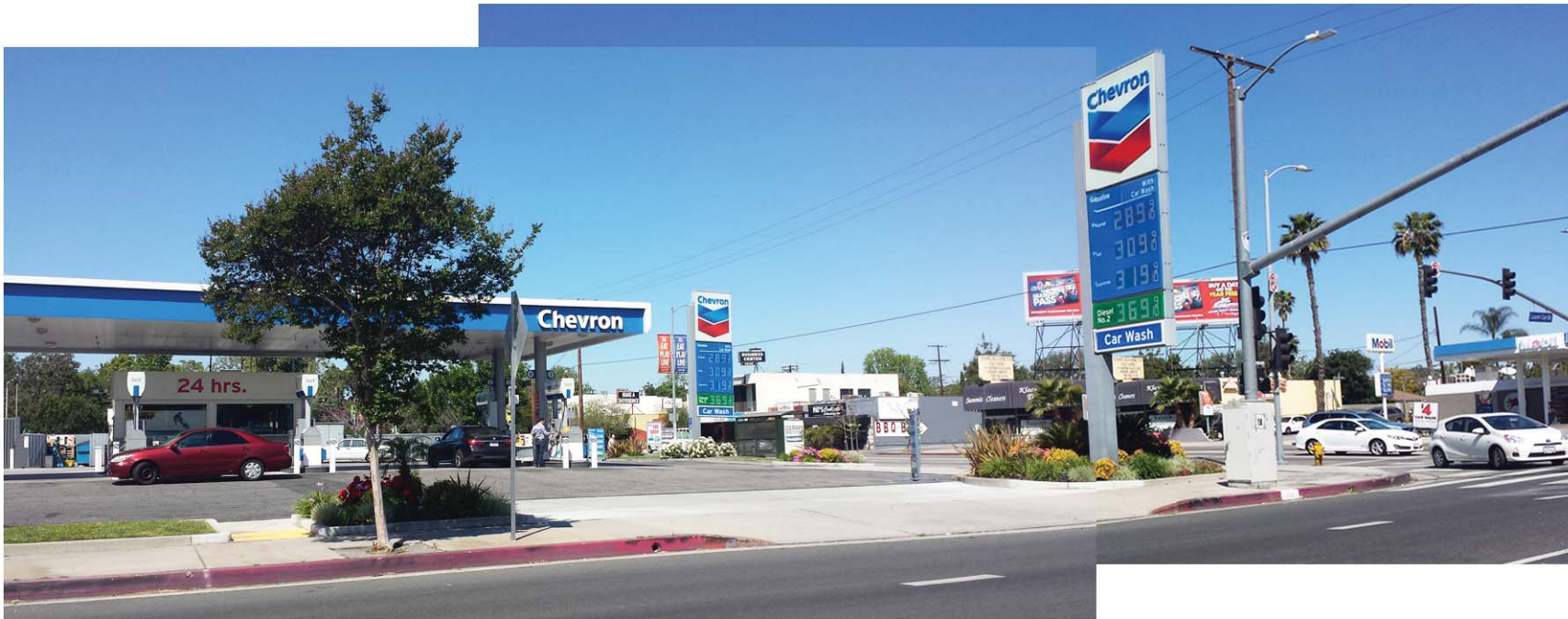
18 Facing North - East (view down Laurel Canyon Blvd. at West of the project property), commercial buildings are along Riverside Dr. and multi-residential and single residential are behind the commercial properties.



19 Facing West - North (view down Laurel Canyon Blvd. at West of the project property), commercial buildings are along Riverside Dr. and multi-residential and single residential are behind the commercial properties.



20 Facing West (view down Laurel Canyon Blvd. at South of the project property across Riverside Dr.), Mobil Gas Station with a small self-operated carwash and commercial buildings are along Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.



21 Facing West (view down Laurel Canyon Blvd. at South of the project property across Riverside Dr.), Mobil Gas Station with a small self-operated carwash.



22

Facing South (view down Riverside Dr. and intersection of Laurel Canyon Blvd. and Riverside Dr. at South of the project property and across Riverside Dr.), Mobil Gas Station with an existing convenience store and a small self-operated carwash and commercial buildings are along Laurel canyon Blvd. and Riverside Dr. with multi-residential and single residential are behind the commercial properties.



23

Facing South - West (view down Riverside Dr. and intersection of Laurel Canyon Blvd. and Riverside Dr. at South of the project property and across Riverside Dr.), Partial Mobil Gas Station and a parking lot next to the Station. And commercial buildings are along Laurel canyon Blvd. and riverside Dr. with multi-residential and single residential are behind the commercial properties.



24 Facing West (view down Riverside Dr. at the intersection of Laurel Canyon Blvd. and Riverside Dr. at the Northeast corner of the street intersection), view of the existing Mobil Station.



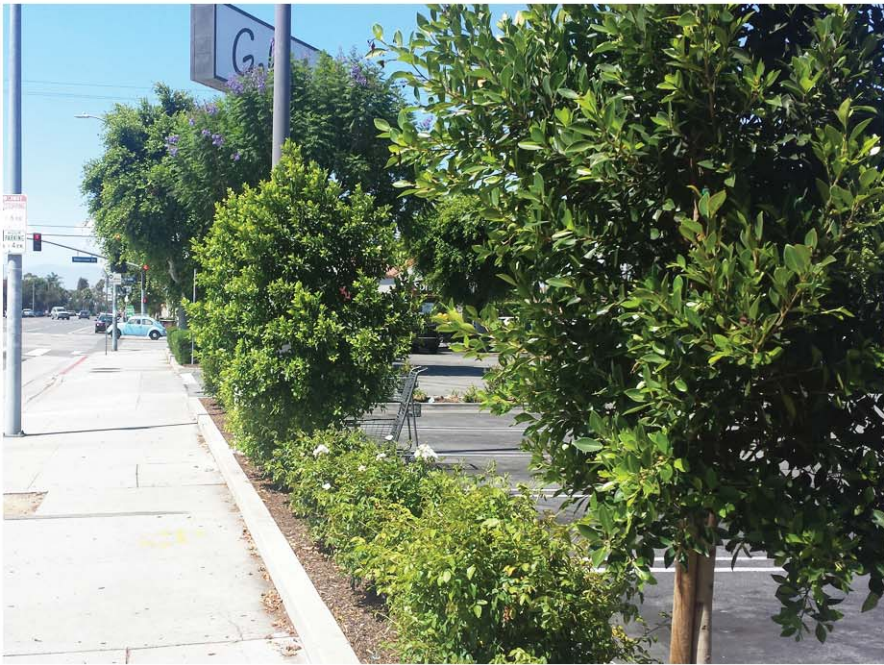
25 Facing Northwest (view down Laurel Canyon Blvd. at the intersection of Laurel Canyon Blvd. and Riverside Dr. and at the Northeast corner of the street intersection.), view of the existing Mobil Station and commercial buildings are along Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.



**26** Facing Southwest (view down Laurel Canyon Blvd.), partial view of the existing Mobil Station and commercial buildings that are along Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.



**27** Facing East - South (view down Laurel Canyon Blvd.), view of the existing market and commercial parking lot with commercial buildings that are along Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.



28

Facing North - East (view down Laurel Canyon Blvd.), view of the existing market commercial parking lot with commercial buildings that are along Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.



29

Facing East - South (view down Laurel Canyon Blvd.), view of the existing market with commercial buildings that are along Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.

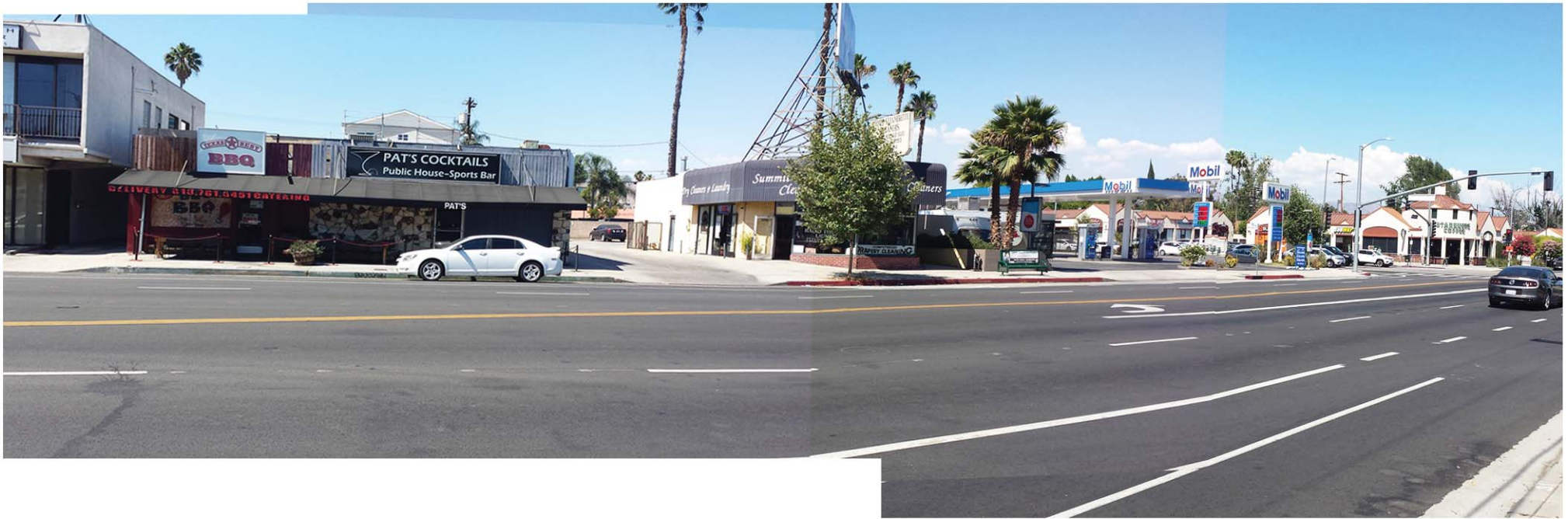


30 Facing South - West (view down Laurel Canyon Blvd.), view of the 76 Gas station with commercial buildings that are along Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.



31 Facing East - South (view down Riverside Dr.), view of the Gelson's Market and commercial buildings that are along Riverside Dr. and Laurel canyon Blvd. with multi-residential and single residential are behind the commercial properties.





32 Facing North - East (view down Riverside Dr.), view of the existing project Mobil Gas station with commercial buildings that are along Riverside Dr. with multi-residential and single residential are behind the commercial properties.



33 Facing North - West (view down Riverside Dr.), view of the existing commercial buildings that are along Riverside Dr. with multi-residential and single residential are behind the commercial properties.

**PLANNING CASE REFERRAL FORM (PCRF)**  
 City of Los Angeles, Bureau of Engineering (BOE) / Department of City Planning (DCP)

Reference Number: 201800379

**Part I. To be Completed by Applicant**

DCP Case Number  /  No

Applicant **KEN KANG** address **535 W ALLEN AVE, UNIT 23  
SAN DIMAS, CA 91773**  
 Phone **626.374.3834** email **KANGDESIGN@HOTMAIL.COM**

Owner **NADER HATTAR** address **4801 N LAUREL CANYON BLVD  
LOS ANGELES, CA 91607**

Project Address **4801 N LAUREL CANYON BLVD** APN **2356008021**  
 Engineering District **Valley**

Project description (attach ZIMAS map with highlighted parcel(s))  
**GAS STATION/SERVICE STATION(E) WITH A SMALL CONVENIENCE STORE(E, 736 SQ FT). WILL INSTALL A  
 NEW SMALL SELF OPERATED CAR WASH WITH CAR WASH STORAGE(875 SQ FT).**

Is there a tract or parcel map being filed in conjunction with this:  Yes  No  
 If yes, Tract Map No. \_\_\_\_\_ Parcel Map No. \_\_\_\_\_

Has the Tract/Parcel report been prepared and submitted to DCP by BOE  Yes  No  
 If yes, please refer to the Tract or Parcel map conditions, if not, then

Is any part of this project on a corner lot?  Yes  No

Dedication Required:  Yes  No

Street Widening Required:  Yes  No

Other Improvements Required:  Yes  No

If yes, please list preliminary required improvements:  
 INSTALL STREET TREES AND TREE WELLS AS REQUIRED BY URBAN FORESTRY DIVISION.  
 INSTALL STREET LIGHTS AS REQUIRED BY BUREAU OF STREET LIGHTING.

**APC 19V-2019-1481**

**Part II. To be Completed by BOE Staff**

What is/are the street classification(s) for the adjacent streets (list all)?  
 RIVERSIDE DR - AVENUE I  
 LAUREL CANYON BLVD - AVENUE I

Does the project front an intersection of two major or secondary highways?  Yes  No  
 If yes, additional dedication may be required for dual left-turn pockets. If no, how far is the project from the nearest major/secondary intersection? Additional dedication may be required if within the standard flare section. Dedication and improvements are to be consistent with Standard Street Dimensions. See Standard Plan S-470-1.

Apparent width of existing half right of way (street centerline to property line):  
 RIVERSIDE DR: 50 FT  
 LAUREL CANYON BLVD: 50 ft

Standard dimension for half right of way (from S-470-1), (street centerline to property line):  
 RIVERSIDE DR: 50 FT  
 LAUREL CANYON BLVD: 50 ft

Apparent width of existing half roadway (street centerline to curb face):  
 RIVERSIDE DR: 37 FT  
 LAUREL CANYON BLVD: 37 ft

Standard street dimension for half roadway (street centerline to curb face):  
 RIVERSIDE DR: 35 FT  
 LAUREL CANYON BLVD: 35 ft

Is the lot connected to the sewer?  Yes  No

Distance from subject lot to nearest main line sewer Date: 07/26/2018  
 19 ft

Is the subject lot(s) within the hillside ordinance boundary?  Yes  No

**Preliminary Required Improvements:**

Planning Case Referral Form Recommendation:  
 Dedication Required:  Yes  No  
 Street Widening Required:  Yes  No  
 Other Improvements Required:  Yes  No  
 If yes, please list preliminary required improvements:  
 INSTALL STREET TREES AND TREE WELLS AS REQUIRED BY URBAN FORESTRY DIVISION.  
 INSTALL STREET LIGHTS AS REQUIRED BY BUREAU OF STREET LIGHTING.

**NOTE:** The information on this PCRf is only a "preliminary recommendation" by BOE, which provides the applicant with a general understanding of what **may** be required by BOE. If the PCRf Recommendations for Dedication or Street Widening is marked "Yes", a formal investigation and engineering report will be required. The engineering report will be provided after submittal of all documentation and payment of fees. Measurements and statements contained herein may be adjusted in the engineering report.

**Street Trees:** If the PCRf Recommendation for Street Widening is marked "Yes", Street tree removals may be required. All street tree removals must be approved by the Board of Public Works. Applicant shall contact the Urban Forestry Division at (213) 847-3077 before proceeding with the Master Land Use Application.

In all cases, the Applicant will be required to close any unused driveways; remove and reconstruct broken, off-grade, or bad order concrete curb, gutter, driveways or sidewalk,; and install/replace public improvements, such as driveway aprons and access ramps, to meet ADA requirements.

Applicants with PCRf Recommendation of "Yes" for Dedication or Street Widening are advised to submit the following documents and pay the BOE investigation fee.

1. BOE investigation fee.
2. Two (2) copies of the Planning Master Land Use Application.
3. Two (2) copies of the project site plan.
4. Two (2) copies of the radius map.
5. Picture of the existing building, sidewalk, curb, and gutter.

Due to the possible implications that dedications and improvements may have on the development of a project, applicants that do not pay the BOE investigation fee for the preparation of a detailed engineering report may have their application placed on hold until such information is provided. Questions and concerns regarding the engineering report may be presented at the hearing.

Prepared by: Esmeralda Bou

Date: 07/26/2018

Exhibit E – Public Comments



Andrew Jorgensen <andrew.jorgensen@lacity.org>

## Request For Information - 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

7 messages

**Ariel Soltani** <arielsoltani1@gmail.com>  
To: Andrew.Jorgensen@lacity.org

Fri, Apr 30, 2021 at 10:38 AM

Hi Andrew:

I am a resident of Valley Village and recently received a notice from the City of Los Angeles Planning Department that another car wash is going to be built in our neighborhood at [4801 N. Laurel Canyon Blvd.](#) (APCSV-2019-1481-SPE-SPP-CU-ZV)

I would like to ask if I can have the opportunity to review your Planning File for this application before the upcoming Hearing.

I am concerned that too many car washes are being built in our community.

Thank you,  
Ariel

**Andrew Jorgensen** <andrew.jorgensen@lacity.org>

Fri, Apr 30, 2021 at 10:49 AM

To: Ariel Soltani <arielsoltani1@gmail.com>

Cc: Claudia Rodriguez <claudia.rodriguez@lacity.org>, Miguel Hernandez <miguel.hernandez@lacity.org>

Hi Ariel,

Certainly; first, there are some materials already available to the public through our website. Click the following link and then "Initial Submittal Documents." That will allow you to view the materials as initially submitted.

<https://planning.lacity.org/pdiscaseinfo/search/encoded/Mjl3OTg50>

This should provide a good overview of the request, but if you need more information, we can make the physical file available after next Tuesday (I would need to go into the office and verify the case file has all the most recent documents inside). Miguel could then assist with reviewing as per our COVID protocols.

Best,

Andrew

[Quoted text hidden]

--



LOS ANGELES  
CITY PLANNING

**Andrew Jorgensen, AICP**

*pronouns: he/him/his*

City Planner

**Los Angeles City Planning**

6262 Van Nuys Blvd

Van Nuys, CA 91401

Planning4LA.org

T: (818) 374-9904



---

**Ariel Soltani** <arielsoltani1@gmail.com>

Sun, May 2, 2021 at 2:55 PM

To: Andrew Jorgensen &lt;andrew.jorgensen@lacity.org&gt;

Cc: Claudia Rodriguez &lt;claudia.rodriguez@lacity.org&gt;, Miguel Hernandez &lt;miguel.hernandez@lacity.org&gt;

Hi Andrew:

Thank you for the submittal information.

Additionally, can your staff provide me with the following information.

1. A copy of the Acoustical Study
2. Letters from the Valley Village Neighborhood Council including conditions of approval.
3. The site appears to be too tight for the construction of a new car wash. The queuing lane has the potential of blocking parking space 3,4,5.
4. Have the Plans been revised since the initial submittal?
5. Has the internal circulation vehicle trips etc. been reviewed by the City of Los Angeles, DOT.

Thank you,

Ariel

[Quoted text hidden]

---

**Andrew Jorgensen** <andrew.jorgensen@lacity.org>

Mon, May 3, 2021 at 2:48 PM

To: Ariel Soltani &lt;arielsoltani1@gmail.com&gt;

Cc: Claudia Rodriguez &lt;claudia.rodriguez@lacity.org&gt;, Miguel Hernandez &lt;miguel.hernandez@lacity.org&gt;

Hi Ariel,

Please see attached for final sound study. For your remaining questions, I'll check the case file at the office tomorrow.

Best,

Andrew

[Quoted text hidden]

---

 **4801 Laural Canyon Blvd.- Los Angeles - CNEL study - Final Report.pdf**  
19067K

---

**Andrew Jorgensen** <andrew.jorgensen@lacity.org>

Tue, May 4, 2021 at 12:07 PM

To: Ariel Soltani &lt;arielsoltani1@gmail.com&gt;

Cc: Claudia Rodriguez &lt;claudia.rodriguez@lacity.org&gt;, Miguel Hernandez &lt;miguel.hernandez@lacity.org&gt;

Hi Ariel,

Please see most recent plans, dated 9-22-19, which appears to be a revision from the initial filing.

I have noted your comment regarding the spaces and printed for the case file.

With regards to traffic assessments I don't see any notes in the case file; however, this would not be required as the proposed scope of work can be categorized as the addition of an accessory use to existing gas station, which is categorically exempt from CEQA per 15303(e).

Best,

Andrew

[Quoted text hidden]

---

**Ariel Soltani** <arielsoltani1@gmail.com>  
To: Andrew Jorgensen <andrew.jorgensen@lacity.org>

Sun, May 30, 2021 at 7:10 PM

Hi Andrew,

I hope you're having a nice weekend. Thanks for providing this information. I'm upset that I was unable to attend the public hearing a few weeks back. However, please let this email serve as my opposition to the carwash.

As I'm sure it was mentioned in the meeting, there are simply too many car washes in the neighborhood. The approval of a carwash at this location would simply encourage the 76 gas station down the street to request approval for a car wash as well. It is a domino effect that cannot be stopped. And, if the city is simply relying on fairness norms to govern their decision, that should not be enough.

Thanks for your time and I hope the city makes the right decision in denying approval of this wash.

Best,  
Ariel

[Quoted text hidden]

---

**Andrew Jorgensen** <andrew.jorgensen@lacity.org>  
To: Ariel Soltani <arielsoltani1@gmail.com>

Tue, Jun 1, 2021 at 8:56 AM

Thank you Ariel, in receipt of your comment.

[Quoted text hidden]





Andrew Jorgensen <andrew.jorgensen@lacity.org>

---

## Public Concern

2 messages

---

**eddie halaoui** <eddie\_halaoui@yahoo.com>  
To: andrew.jorgensen@lacity.org

Sun, May 30, 2021 at 11:04 PM

Hello,

I am a valley village resident and am emailing you regarding the Mobil car wash on Laurel canyon and riverside. I am concerned about idle vehicle emissions and extreme noise from the carwash blowers. I'm concerned that this carwash will decrease property values in the immediate neighborhood therefore, I'm against to the carwash.

Thank you,  
Eddie

---

**Andrew Jorgensen** <andrew.jorgensen@lacity.org>  
To: eddie halaoui <eddie\_halaoui@yahoo.com>

Tue, Jun 1, 2021 at 8:43 AM

Hi Eddie,

Thank you for your email. I am in receipt and will add to the case file.

Best,

Andrew

[Quoted text hidden]

--



LOS ANGELES  
CITY PLANNING

**Andrew Jorgensen, AICP**

*pronouns: he/him/his*

City Planner

**Los Angeles City Planning**

6262 Van Nuys Blvd

Van Nuys, CA 91401

Planning4LA.org

T: (818) 374-9904





Andrew Jorgensen &lt;andrew.jorgensen@lacity.org&gt;

---

**Mobil CarWash - Laurel Canyon and Riverside**

2 messages

**Tiffany Lorane** <tiff1or12@yahoo.com>

Mon, May 31, 2021 at 6:15 PM

To: "andrew.jorgensen@lacity.org" &lt;andrew.jorgensen@lacity.org&gt;

Hello,

My name is Tiffany Lorane. I am a resident at [4901 Laurel Canyon Boulevard, Valley Village](#). I received a notice that another carwash is being built in the neighborhood.

I raised concern about the carwash that was built on Laurel Canyon and Coldwater several years back but my concern was clearly ignored as that station ended up building a wash. Now, I am expressing concern against this project and I hope this time the city listens. ALL the neighbors agree that we do NOT want or need another car wash!

Have you seen how small the lot is? If this car wash is anything like the one across the street, it will have a long line. The lot is simply too small for it to handle a line of cars. This is a safety hazard as nearly 5-6 cars will line up and block traffic. Terrible idea, please reject the owners proposal!! Give him approval to build something else, anything! Possibly a bigger convenience store.

Thank you,  
Tiff

---

**Andrew Jorgensen** <andrew.jorgensen@lacity.org>

Tue, Jun 1, 2021 at 8:49 AM

To: Tiffany Lorane &lt;tiff1or12@yahoo.com&gt;

Hi Tiffany,

Thank you for your comment; I am in receipt and will add to the case file.

Best,

Andrew

[Quoted text hidden]

--

LOS ANGELES  
CITY PLANNING**Andrew Jorgensen, AICP***pronouns: he/him/his*

City Planner

**Los Angeles City Planning**

6262 Van Nuys Blvd

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T: (818) 374-9904





Andrew Jorgensen <andrew.jorgensen@lacity.org>

---

## PROPOSED CAR WASH AT LAUREL CANYON AND RIVERSIDE

2 messages

---

**Bruce A** <brucenote@hotmail.com>

Wed, Jun 23, 2021 at 5:20 PM

To: "andrew.jorgensen@lacity.org" <andrew.jorgensen@lacity.org>

Dear Mr. Jorgensen,

I wanted to write you regarding the proposed 24 car wash on the gas station site at 4801-4815 N. Laurel Canyon Blvd.

I live nearby and am opposed to this development. There is already a car wash of this type right across the street in another gas station, so it is totally unnecessary to have another car wash only about 250 feet from the existing car wash on the other side of Riverside Dr.

Additionally, there was not proper notification given of the hearing regarding this development.

As far as I know, the only neighborhood notification of this hearing was a small sign near the bottom of the door of the service station, which no one would likely notice even if they did shop at the station. This really does not seem to be proper notification to neighbors in the area. Something should be mailed to neighbors or posted on their doors (at the expense of the gas station if need be).

Please contact me at 818 850-9171 so that we can discuss this in more detail.

Thank you.

Best Regards,

Bruce Ablin

(P.S., you sound like a nice guy on your outgoing voicemail message).

---

**Andrew Jorgensen** <andrew.jorgensen@lacity.org>

Thu, Jun 24, 2021 at 8:06 AM

To: Bruce A <brucenote@hotmail.com>

Hi Bruce,

Thanks for your email, I'm in receipt and will place in the case file, as well as adding you to the interested parties list to receive notification when the case goes to commission.

Best,

Andrew

[Quoted text hidden]

--



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CITY PLANNING

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*pronouns: he/him/his*

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Andrew Jorgensen <andrew.jorgensen@lacity.org>

**4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)**

17 messages

**eolivo@olivo-law.com** <eolivo@olivo-law.com> Wed, Aug 25, 2021 at 5:09 PM  
To: apcsouthvalley@lacity.org  
Cc: Andrew Jorgensen <andrew.jorgensen@lacity.org>, Miguel Hernandez <miguel.hernandez@lacity.org>, Claudia Rodriguez <claudia.rodriguez@lacity.org>

Please see the attached comment letter regarding the above-referenced matter.

Eduardo Olivo  
Attorney at Law  
OLIVO & ASSOCIATES  
[13181 Crossroads Parkway North](#)  
[Suite 340](#)  
[Industry, CA 91746](#)  
Tel: 562-697-2440  
Fax: 562-697-2443

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 **Comment Letter Final .pdf**  
230K

**Planning APC South Valley** <apcsouthvalley@lacity.org> Thu, Aug 26, 2021 at 8:56 AM  
To: eolivo@olivo-law.com  
Cc: Andrew Jorgensen <andrew.jorgensen@lacity.org>, Miguel Hernandez <miguel.hernandez@lacity.org>, Claudia Rodriguez <claudia.rodriguez@lacity.org>

Hello,

The Commission office are accepting two pages only at this time. Please resend your submissions and I will upload them for the Commission consideration, otherwise your documents are non compliant.

Thank you,

[Quoted text hidden]

--



**Etta Armstrong, Commission  
Executive Assistant I  
Department of City Planning**

**T:** (213) 978-1128  
200 N. Spring St., Room 272  
Los Angeles, CA. 90012

---

**eolivo@olivo-law.com** <eolivo@olivo-law.com>

Thu, Aug 26, 2021 at 11:37 AM

To: Planning APC South Valley <apcsouthvalley@lacity.org>

Cc: Andrew Jorgensen <andrew.jorgensen@lacity.org>, Miguel Hernandez <miguel.hernandez@lacity.org>, Claudia Rodriguez <claudia.rodriguez@lacity.org>

These are my comments for the record. I will send another shorter version, but I do not believe that you can legally deny written comments because they exceed more than two pages. We have a right to provide public comments per the Ralph M. Brown Act and I am doing so by submitting them in written form. We are also required to address legal issues that may be raised in subsequent litigation and that is what I am doing as well. A shorter version will follow, but I am asking that my prior letters be made part of the record.

Thank you.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

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Industry, CA 91746

Tel: 562-697-2440

Fax: 562-697-2443

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[Quoted text hidden]

---

**eolivo@olivo-law.com** <eolivo@olivo-law.com>

Thu, Aug 26, 2021 at 11:58 AM

To: Planning APC South Valley <apcsouthvalley@lacity.org>

Cc: Andrew Jorgensen <andrew.jorgensen@lacity.org>, Miguel Hernandez <miguel.hernandez@lacity.org>, Claudia Rodriguez <claudia.rodriguez@lacity.org>

Please see the attached comment letter. It is 2 pages long. Please assure that the letter is made part of the record.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

Suite 340

Industry, CA 91746

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---

**From:** eolivo@olivo-law.com <eolivo@olivo-law.com>

**Sent:** Thursday, August 26, 2021 11:37 AM

**To:** 'Planning APC South Valley' <apcsouthvalley@lacity.org>

**Cc:** 'Andrew Jorgensen' <andrew.jorgensen@lacity.org>; 'Miguel Hernandez' <miguel.hernandez@lacity.org>; 'Claudia Rodriguez' <claudia.rodriguez@lacity.org>

**Subject:** RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

These are my comments for the record. I will send another shorter version, but I do not believe that you can legally deny written comments because they exceed more than two pages. We have a right to provide public comments per the Ralph M. Brown Act and I am doing so by submitting them in written form. We are also required to address legal issues that may be raised in subsequent litigation and that is what I am doing as well. A shorter version will follow, but I am asking that my prior letters be made part of the record.

Thank you.

Eduardo Olivo

Attorney at Law

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**From:** [etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org) <[etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org)> **On Behalf Of** Planning APC South Valley

**Sent:** Thursday, August 26, 2021 8:56 AM

**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)

**Cc:** Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>; Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>

**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

Hello,

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[Quoted text hidden]

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 **Comment Letter Rev Sum.pdf**  
204K

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[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com) <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)>

Thu, Aug 26, 2021 at 12:12 PM

To: Planning APC South Valley <[apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org)>

Cc: Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>, Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>, Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>

I would also point out that, per the Ralph M. Brown Act, I had requested that Mr. Jorgensen provide me with notice of the meeting and a copy of the agenda once it was posted. He failed to do so, although he advised that he would comply with the request. This delayed my preparation of the comments. I have not received a response to my request for an explanation as to why copies were not provided.

Please also advise me of the name of the person that has sent the email on behalf of the Planning Commission.

Thank you.

Eduardo Olivo  
Attorney at Law  
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**Cc:** 'Andrew Jorgensen' <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>; 'Miguel Hernandez' <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; 'Claudia Rodriguez' <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>  
**Subject:** RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

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Thank you.

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**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

Hello,

[Quoted text hidden]

[Quoted text hidden]

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[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com) <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)>

Thu, Aug 26, 2021 at 12:23 PM

To: Planning APC South Valley <[apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org)>, Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

Cc: Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>, Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>

Pursuant to the Public Records Act, I hereby request that you provide me with the following documents:

1. All communications, including emails, sent to the applicant or any representative of the applicant by Mr. Jorgensen or City staff regarding the above-referenced project.
2. All communications, including emails, that were received by the City from the applicant regarding the above-referenced project.
3. Copies of all communications from the City to any person regarding notices of the Planning Commission meeting date for the above-referenced project.

If I need to send this request to any other person, please advise me of the name and email address for that person.

Thank you.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

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**Subject:** RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

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**Subject:** RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

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Attorney at Law

OLIVO & ASSOCIATES

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**From:** [etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org) <[etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org)> **On Behalf Of** Planning APC South Valley  
**Sent:** Thursday, August 26, 2021 8:56 AM  
**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)  
**Cc:** Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>; Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>  
**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

Hello,

[Quoted text hidden]

[Quoted text hidden]

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**Andrew Jorgensen** <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)> Thu, Aug 26, 2021 at 1:56 PM  
To: [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)  
Cc: Planning APC South Valley <[apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org)>, Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>, Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>, Beatrice Pacheco <[beatrice.pacheco@lacity.org](mailto:beatrice.pacheco@lacity.org)>

Hi Eduardo,

Please contact Beatrice Pacheco, our Department Records supervisor, cc'd here.

Best,

Andrew



**Andrew Jorgensen, AICP**  
*pronouns: he/him/his*  
City Planner, Southeast Valley  
**Los Angeles City Planning**  
6262 Van Nuys Blvd  
Van Nuys, CA 91401  
Planning4LA.org  
T: (818) 374-9904



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---

**Beatrice Pacheco** <[beatrice.pacheco@lacity.org](mailto:beatrice.pacheco@lacity.org)> Thu, Aug 26, 2021 at 1:59 PM  
To: [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)  
Cc: Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

Hello, Mr. Olivo:

Your PRA request was received today, and our Department will respond accordingly.



**Beatrice Pacheco**  
Chief Clerk  
**Los Angeles City Planning**  
**Records Management**  
221 N. Figueroa St., Room 1450  
Los Angeles, CA. 90012  
Planning4LA.org  
T: (213) 847-3732 | F: (213) 269-4127



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---

**Beatrice Pacheco** <beatrice.pacheco@lacity.org>  
To: Andrew Jorgensen <andrew.jorgensen@lacity.org>  
Cc: Lourdes Sanchez <lourdes.sanchez@lacity.org>

Thu, Aug 26, 2021 at 2:11 PM

Hi, Andrew, thanks for copying me. Our response is due on 9/7 due to the holiday on 9/6.

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---

**eolivo@olivo-law.com** <eolivo@olivo-law.com>  
To: Beatrice Pacheco <beatrice.pacheco@lacity.org>  
Cc: Andrew Jorgensen <andrew.jorgensen@lacity.org>

Thu, Aug 26, 2021 at 2:45 PM

Thank you.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

Suite 340

Industry, CA 91746

Tel: 562-697-2440

Fax: 562-697-2443

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---

**Planning APC South Valley** <apcsouthvalley@lacity.org>

Thu, Aug 26, 2021 at 2:58 PM

To: eolivo@olivo-law.com

Cc: Kimberly Huangfu <kimberly.huangfu@lacity.org>, Andrew Jorgensen <andrew.jorgensen@lacity.org>

Hello Mr. Olivo,

On the first page of the South Valley agenda under Day of Hear Submissions states how many pages are acceptable. I have uploaded your two page document in the google drive under Day of Submission. Sorry for any inconvenience this might have caused.

On Wed, Aug 25, 2021 at 5:09 PM <eolivo@olivo-law.com> wrote:

[Quoted text hidden]

[Quoted text hidden]

**Planning APC South Valley** <apcsouthvalley@lacity.org>

Thu, Aug 26, 2021 at 3:01 PM

To: eolivo@olivo-law.com

Cc: Kimberly Huangfu <kimberly.huangfu@lacity.org>, Andrew Jorgensen <andrew.jorgensen@lacity.org>

Additional information regarding the Commission Rules and Operating Procedures can be found at our planning website at [www.planning.lacity.org](http://www.planning.lacity.org). In particular is [Rule 4.3.c. of the Commission Rules and Operating Procedures](#)

[Quoted text hidden]

**eolivo@olivo-law.com** <eolivo@olivo-law.com>

Thu, Aug 26, 2021 at 3:04 PM

To: Planning APC South Valley <apcsouthvalley@lacity.org>

Cc: Kimberly Huangfu <kimberly.huangfu@lacity.org>, Andrew Jorgensen <andrew.jorgensen@lacity.org>

That may be true, but I had asked for notice and a copy of the agenda, which is allowed by the Ralph M. Brown Act. Staff advised me that they would comply, but they did not. I was therefore delayed. My 5 page submission would have been provided within 48 hours but for staff's failure to comply with their obligation to provide me with the notice and a copy of the agenda. Regardless, all documents submitted should be part of the administrative record. I am not sure what a judge will decide about the City's policy of limiting comments; I think it violates the Brown Act.

Thank you.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

Suite 340

Industry, CA 91746

Tel: 562-697-2440

Fax: 562-697-2443

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**From:** [etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org) <[etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org)> **On Behalf Of** Planning APC South Valley

**Sent:** Thursday, August 26, 2021 2:58 PM

**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)

**Cc:** Kimberly Huangfu <[kimberly.huangfu@lacity.org](mailto:kimberly.huangfu@lacity.org)>; Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

Hello Mr. Olivo,

[Quoted text hidden]

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**eolivo@olivo-law.com** <eolivo@olivo-law.com>

Thu, Aug 26, 2021 at 3:06 PM

To: Planning APC South Valley <apcsouthvalley@lacity.org>

Cc: Kimberly Huangfu <kimberly.huangfu@lacity.org>, Andrew Jorgensen <andrew.jorgensen@lacity.org>

I once again ask that all of my submittals be presented to the Planning Commission and be made part of the record. My understanding is that the rules that you cite have not been enforced regarding other Planning Commission matters.

Please also provide me with your name.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

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Tel: 562-697-2440

Fax: 562-697-2443

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**From:** [etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org) <[etta.armstrong@lacity.org](mailto:etta.armstrong@lacity.org)> **On Behalf Of** Planning APC South Valley

**Sent:** Thursday, August 26, 2021 3:01 PM

**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)

**Cc:** Kimberly Huangfu <[kimberly.huangfu@lacity.org](mailto:kimberly.huangfu@lacity.org)>; Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)

Additional information regarding the Commission Rules and Operating Procedures can be found at our planning website at [www.planning.lacity.org](http://www.planning.lacity.org). In particular is [Rule 4.3.c. of the Commission Rules and Operating Procedures](#)

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---

**Planning APC South Valley** <apcsouthvalley@lacity.org>

Thu, Aug 26, 2021 at 3:12 PM

To: [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)

Cc: Kimberly Huangfu <[kimberly.huangfu@lacity.org](mailto:kimberly.huangfu@lacity.org)>, Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

My name is Etta Armstrong, Commission Executive Assistant. I take advice from our City Attorney. If you have any additional questions please direct them to our City Attorney, Kimberly Huangfu who is cc'd on this email.

[Quoted text hidden]

**Kimberly Huangfu** <kimberly.huangfu@lacity.org> Thu, Aug 26, 2021 at 3:23 PM  
To: Etta Armstrong <etta.armstrong@lacity.org>  
Cc: Irene Gonzalez <irene.gonzalez@lacity.org>, Terry Kaufmann Macias <terry.kaufmann-macias@lacity.org>, Andrew Jorgensen <andrew.jorgensen@lacity.org>

**ATTORNEY-CLIENT PRIVILEGED AND CONFIDENTIAL COMMUNICATION**

Hi Etta,  
No need to respond any further. Let's just have the meeting tonight. He can say what he wants during his one minute.

**Kimberly A. Huangfu | Deputy City Attorney**  
Land Use Division | City of Los Angeles City Attorney's Office  
200 North Main Street | Room 701 | City Hall East  
Los Angeles, California 90012  
[kimberly.huangfu@lacity.org](mailto:kimberly.huangfu@lacity.org)

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\*\*\*\*\*

**eolivo@olivo-law.com** <eolivo@olivo-law.com> Thu, Aug 26, 2021 at 3:29 PM  
To: Planning APC South Valley <apcsouthvalley@lacity.org>  
Cc: Kimberly Huangfu <kimberly.huangfu@lacity.org>, Andrew Jorgensen <andrew.jorgensen@lacity.org>

Thank you. I appreciate that.

Ms. Huangfu:

I am attaching the previous submittals and the email exchange I had with Mr. Jorgensen regarding my request for notice and the agenda per the Brown Act. I again request that all documents submitted be made part of the administrative record.

Thank you.



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----- Forwarded message -----

From: <eolivo@olivo-law.com>

To: "Andrew Jorgensen" <andrew.jorgensen@lacity.org>

Cc: "Miguel Hernandez" <miguel.hernandez@lacity.org>, "Claudia Rodriguez" <claudia.rodriguez@lacity.org>

Bcc:

Date: Wed, 25 Aug 2021 12:14:15 -0700

Subject: RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Mr. Jorgensen:

I requested notice of the hearing on the above-referenced matter and a copy of the agenda. You have failed to provide that to me as requested. I have learned today that the matter may be scheduled for this Thursday. Is that correct? Can you tell me why you did not provide the notice that I requested?

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

Suite 340

Industry, CA 91746

Tel: 562-697-2440

Fax: 562-697-2443

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---

**From:** eolivo@olivo-law.com <eolivo@olivo-law.com>

**Sent:** Wednesday, June 23, 2021 10:41 AM

**To:** 'Andrew Jorgensen' <andrew.jorgensen@lacity.org>

**Cc:** 'Miguel Hernandez' <miguel.hernandez@lacity.org>; 'Claudia Rodriguez' <claudia.rodriguez@lacity.org>

**Subject:** RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Thank you!

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

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**From:** Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

**Sent:** Wednesday, June 23, 2021 10:16 AM

**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)

**Cc:** Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>

**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Hi Edward,

The case has not yet been scheduled, but I'm currently anticipating early August. Nothing further has been added to the case file from when I dropped it off last month.

Best,

Andrew

On Tue, Jun 22, 2021 at 5:07 PM <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)> wrote:

Hi Andrew:

I wanted to check in to see if a hearing has been scheduled yet with the Planning Commission regarding the above-referenced matter. If not, do you have an estimate as to when it will be set? Please remember to provide me with notice of the hearing and agendas.

Also, we did copy the file. If documents have been included in the file after May 27, 2021, I would like to get copies.

Thank you!

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

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Tel: 562-697-2440

Fax: 562-697-2443

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**From:** Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

**Sent:** Monday, May 17, 2021 4:06 PM

**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)

**Cc:** Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>

**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Thanks Eduardo, I will add you to our interested parties list. Do you still want to review the case file?

1 - As we discussed, you can submit comments up to and including the day of the hearing.

2 - As a matter of practice, we do not typically record our hearings and I did not record today's

3 - Will be available to the public ~10 days before commission

4 - See case file

5 - Will do

6 - This is more difficult. I will add all comments to the case file, but I won't be able to proactively send you the comments I receive. You may need to just check-in when you want to review comments.

Best,

Andrew

On Mon, May 17, 2021 at 3:45 PM <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)> wrote:

Hi Andrew:

I would like to formally request that I be provided all future notices, agendas and other information provided to the public regarding the above-referenced project. I would like the following, among other things:

1. Information regarding public comments/written comment due dates in connection with the upcoming planning commission hearing in July or August, as you mentioned during the hearing today;
2. If meeting minutes or an audio recording was made of the staff hearing today, please provide a copy;
3. The staff report that is prepared regarding the project;
4. Copies of all documents related to the project, including applications, plans, etc.;
5. All meeting agendas and notices of public hearings regarding the project. I am of course interested in the planning commission meeting that is going to be scheduled in the near future;
6. Copies of all written comments provided regarding the project.

I will of course pay all proper copying costs for such matters. I prefer electronic delivery of documents, if possible. Thank you!

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

Suite 340

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Tel: 562-697-2440

Fax: 562-697-2443

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**From:** Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>  
**Sent:** Friday, May 14, 2021 12:55 PM  
**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)  
**Cc:** Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>  
**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Great, I'll give you a call at 2:30.

On Fri, May 14, 2021 at 11:42 AM <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)> wrote:

Andrew:

I do have time after 2. Let me know what works best for you.

Thank you!

Eduardo Olivo  
Attorney at Law  
OLIVO & ASSOCIATES  
13181 Crossroads Parkway North  
Suite 340  
Industry, CA 91746  
Tel: 562-697-2440  
Fax: 562-697-2443

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**From:** Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>  
**Sent:** Thursday, May 13, 2021 9:07 AM  
**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)  
**Cc:** Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>  
**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Hi Eduardo,

Certainly, happy to help - do you have any time after 2 tomorrow afternoon?

Best,

Andrew

On Wed, May 12, 2021 at 5:10 PM <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)> wrote:

Andrew:

Also, I am familiar with zoning and various processes in different cities. Los Angeles is unique. Is it possible to get on the phone with you or Miguel/Claudia to ask a few basic questions about the hearing coming up and the subsequent hearing by the planning commission?

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

Suite 340

Industry, CA 91746

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Fax: 562-697-2443

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**From:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com) <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)>

**Sent:** Wednesday, May 12, 2021 5:07 PM

**To:** 'Andrew Jorgensen' <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

**Cc:** 'Miguel Hernandez' <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; 'Claudia Rodriguez' <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>

**Subject:** RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Andrew:

Has the hearing notice itself been superseded? Can you tell me what documents have been superseded and are they (other than plans) available electronically? If I have to send someone to Van Nuys, can I do that Friday? And if so, are you able to provide copies on that same day? If not, I have a portable scanner that can be used if we have an outlet and a table/desk or counter to work on. Please advise

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

13181 Crossroads Parkway North

Suite 340

Industry, CA 91746

Tel: 562-697-2440

Fax: 562-697-2443

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**From:** Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)>

**Sent:** Monday, May 10, 2021 2:14 PM

**To:** [eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)

**Cc:** Miguel Hernandez <[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)>; Claudia Rodriguez <[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)>

**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Hi Eduardo,

I believe some documents on the website may have been superseded in the physical file

Staff reports would not be prepared until after the initial staff hearing, prior to the Commission hearing for a decision. I will prepare notes at the initial hearing that will go into the staff report and the commission will prepare and agenda and minutes for its meeting. The initial hearing notice is attached

Let me know if you want to schedule a time for the file.

Best,

Andrew

On Mon, May 10, 2021 at 2:04 PM <eolivo@olivo-law.com> wrote:

I am particularly interested in any and all staff reports/analysis, notices and meeting minutes/agendas re the project.

Thank you.

Eduardo Olivo  
Attorney at Law  
OLIVO & ASSOCIATES  
13181 Crossroads Parkway North  
Suite 340  
Industry, CA 91746  
Tel: 562-697-2440  
Fax: 562-697-2443

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---

**From:** eolivo@olivo-law.com <eolivo@olivo-law.com>  
**Sent:** Monday, May 10, 2021 2:00 PM  
**To:** 'Miguel Hernandez' <miguel.hernandez@lacity.org>; 'Andrew Jorgensen' <andrew.jorgensen@lacity.org>  
**Cc:** 'Claudia Rodriguez' <claudia.rodriguez@lacity.org>  
**Subject:** RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Hi Andrew and Miguel:

I have access to the documents that are on the City's website – they are apparently all of the application submittal documents, but I am not sure. I would need all other documents. If they are a few, maybe they can be emailed. If they are more than that, I will need to set up the appointment to review in person. Please advise.



Eduardo Olivo  
Attorney at Law  
OLIVO & ASSOCIATES  
13181 Crossroads Parkway North  
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**From:** Miguel Hernandez <miguel.hernandez@lacity.org>  
**Sent:** Monday, May 10, 2021 1:08 PM  
**To:** Andrew Jorgensen <andrew.jorgensen@lacity.org>  
**Cc:** eolivo@olivo-law.com; Claudia Rodriguez <claudia.rodriguez@lacity.org>  
**Subject:** Re: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)

Hi Andrew,

If someone would like to review a case file they can reach out to me for an appointment. At the time of the appointment copies may be requested at a cost. It is \$.10 for each 8.5 x 11 & 8.5 x 14, \$.25 for each 11 x 17, and \$8.25 for any oversize plans.

Best,



**Miguel Hernandez**  
Senior Administrative Clerk  
**Los Angeles City Planning**  
6262 Van Nuys Blvd., Room 430  
Los Angeles, CA 91401  
Planning4LA.org  
T: (818) 374-9918 | F:



On Mon, May 10, 2021 at 11:01 AM Andrew Jorgensen <[andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)> wrote:

Hi Eduardo,

If you want certain items from the file, I can possibly scan/send those to you, but if you want to review the entire file, it would be best for us to make it available to you physically.

Miguel, could you confirm the process for that? Would I route it downtown to Records Management or could we make it available at the Van Nuys office?

Best,

Andrew

On Wed, May 5, 2021 at 10:55 AM <[eolivo@olivo-law.com](mailto:eolivo@olivo-law.com)> wrote:

Good morning, Mr. Jorgensen:

I represent a few individuals who are interested in the above-referenced project being processed by the City of Los Angeles. I need to obtain a copy of all of the City of Los Angeles project documents, including all staff reports, relevant meeting minutes, CEQA notices, etc. Please advise as to the process that the City is using currently to provide the documents. I understand that there is a hearing date on May 17, so I need the documents as soon as possible so that my clients may provide pertinent comments and testimony regarding the project.

I very much appreciate your time and attention to this request.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

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--



**Andrew Jorgensen, AICP**

*pronouns: he/him/his*

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--



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[Message clipped] [View entire message](#)

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**3 attachments**

 **Comment Letter Final .pdf**  
230K

 **Comment Letter 2 MD.pdf**  
1660K

 **RE: 4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV).eml**  
141K

# OLIVO & ASSOCIATES

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August 25, 2021

## VIA EMAIL AND U.S. MAIL

[apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org)

cc: [andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)

[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)

[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)

South Valley Area Planning Commission  
CITY OF LOS ANGELES

Re: **South Valley Area Planning Commission August 26, 2021 Agenda Item No. 5**  
**4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)**

Dear Planning Commissioners and City Staff:

My office represents interested parties and residents of the City of Los Angeles who oppose the above-referenced project. As discussed below, the approvals requested by the applicant and City staff are illegal and must be denied.

Initially, I note that on May 17 and June 22, 2021, pursuant to Government Code section 54954.1, I requested that City staff provide me with notice of the scheduled meeting by the Planning Commission for the project and with a copy of the agenda. City staff failed to provide the notice or the agenda.

The Planning Commission is considering, among other things, the following:

1. Whether the “project” is categorically exempt from the California Environmental Quality Act (CEQA) by CEQA Guidelines Section 15303;
2. Whether, pursuant to Los Angeles Municipal Code (LAMC) Section 11.5.7.F (Specific Plan Exceptions) to allow, among other things:
  - An accessory car wash use, although such use is not permitted by the C4 use limitations of Section 5.B. of the Valley Village Specific Plan;
  - Pursuant to LAMC Section 12.24.W.27, a Conditional Use to allow a Commercial Corner Development with the following deviations from the standards of Los Angeles Municipal Code Section 12.22.A.23;
  - Pursuant to LAMC Section 12.27, a Zone Variance from "Q" Conditions of Ordinance No. 165,108 to allow a car wash where otherwise prohibited.

A. The “Project” Is Not Correctly Defined And Is Subject To CEQA

The Planning Commission is required to comply with the requirement of California Environmental Quality Act (“CEQA”) ( Public Resources Code (“PRC”) Sections 21000, et. seq.) in connection with its consideration of the “project” being considered.

PRC Section 20180 (a) provides that:

Except as otherwise provided in this division, this division shall apply to discretionary projects proposed to be carried out or approved by public agencies, including, but not limited to, the enactment and amendment of zoning ordinances, the issuance of zoning variances, the issuance of conditional use permits, and the approval of tentative subdivision maps unless the project is exempt from this division.

CEQA defines a “project” as an activity that (1) is a discretionary action by a governmental agency and (2) will either have a direct or reasonably foreseeable indirect impact on the environment. (PRC § 21065.) The test for whether an action constitutes a “project” must take place in the abstract. The courts have established that a proposed activity is a CEQA project if, by its general nature, the activity is capable of causing a direct or reasonably foreseeable indirect physical change in the environment. This determination is made without considering whether, under the specific circumstances in which the proposed activity will be carried out, these potential effects will actually occur. Government agencies examining whether an action constitutes a project under CEQA should be sure to focus on whether the activity could, in general, have a direct or indirect environmental impact and not on whether the action is likely to have specific impacts.

Pursuant to Section 15378 of the CEQA Guidelines, a “project” means the whole of the action. In this case, City staff appears to define the “project” as being limited to the actual construction of the car wash. The staff report does not consider that the project includes the approval of a specific plan exception, approval of a conditional use and a zoning variance. The failure to properly define the project prevents a proper analysis and violates CEQA.

B. The Categorical Exemption Established by CEQA Guideline 15303 Is Not Applicable To The Project

CEQA Regulation 15303 exempts from CEQA projects that consist of construction and location of limited numbers of new, small facilities or structures; installation of small new equipment and facilities in small structures; and the conversion of existing small structures from one use to another where only minor modifications are made in the exterior of the structure. Staff is requesting that the Commission find that the project in this case is exempt pursuant to subsection (e) of Regulation 15303: “Accessory (appurtenant) structures including garages, carports, patios, swimming pools, and fences.”

As discussed above, the “project” is not properly defined; the full scope of the project does not fall within the 15303 exemption. Even if the scope of the project could be limited to the construction of the car wash, such a project would not constitute a small structure such as a garage, carport, patio, etc. The use of the exemption is improper.

C. The Use Of A Variance To Approve A Use That Is Not Permitted By The City's Zoning Ordinance, Is Illegal

The establishment of a car wash at the subject site is not permitted by the City's zoning ordinances. Staff states, in part, that:

Pursuant to Ordinance 165,108 (Sub Area 6740) adopted in 1989, 'the use of the property shall be limited to the uses existing upon the effective date of this ordinance and thereafter to those of the C1.5-1-VL Zone.' This ordinance, then, in essence restricts the zoning of this previously-C2 lot to C1.5 for the determination of whether a use is allowed on the lot; car washes are first allowed in the C2 zone and therefore not allowed in the C1.5 zone.

Staff advises that the applicant must apply for and receive both a Specific Plan Exception and Zone Variance.

A variance is a permit to build a structure or engage in an activity that would not otherwise be allowed under the zoning ordinance; it cannot, however, be granted to allow a *use* unauthorized by the zoning ordinance. (Government Code § 65906.) Typically, variances provide relief from regulations such as those governing setbacks, height, square footage, and density. A variance may be granted "only when, because of the special circumstances applicable to the property, including size, shape, topography, location or surroundings, the strict application of the zoning ordinance deprives such property of privileges enjoyed by other property in the vicinity and under identical zoning classification." (Government Code § 65906.)

"A zoning variance, and by analogy a specific plan exception, must be 'grounded in conditions peculiar to the particular lot as distinguished from other property' in the specific plan area. Unnecessary hardship therefore occurs where the natural condition or topography of the land places the landowner at a disadvantage vis-à-vis other landowners in the area, such as peculiarities of the size, shape or grade of the parcel." *Committee to Save the Hollywood Specific Plan v. City of Los Angeles* (2008) 161 Cal.App.4<sup>th</sup> 1168, 1183, citations omitted. The courts have also discerned in the hardship requirement an additional finding that the hardship be substantial, and that the exception sought must be in harmony with the intent of the zoning laws. (*Id.*)

Further, the special circumstances pertaining to the property must be such that the property is distinct in character from comparable nearby properties. In *Topanga Assn. for a Scenic Community v. County of Los Angeles* (1974) 11 Cal.3d 506, the landowner obtained a zoning variance to build a 93-space mobile home park on 28 acres in Topanga Canyon on property zoned for light agricultural and single-family residences. *Id.* at 510. Applying Government Code section 65906, the *Topanga* found insufficient evidence supported the grant of the variance because there was no evidence concerning comparable neighborhood properties, and therefore concerning whether the variance was necessary to bring the landowner into parity with other parties holding property in the same area. *Id.* at 521.



LAMC Section 11.5.7 establishes specific plan procedures. Subsection F sets forth the findings required for approving exceptions to the specific plan. Among other things, that subsection requires a finding, like a variance, “[t]hat there are exceptional circumstances or conditions applicable to the subject property involved or to the intended use or development of the subject property that do not apply generally to other property in the specific plan area.” LAMC § 11.5.7 F (b).

In this case, the proposed findings states that the “strict application of the specific plan creates an unnecessary hardship because it limits the improvement and expansion of an existing legal use to include a service on site which will mitigate total vehicle trips and be desirable to the public convenience.” The staff report also points out that the current use as a gas station and convenience store were established prior to the establishment of the Specific Plan in 1993. Staff then argues that allowing for customers to receive a car wash at this location further allows the applicant to develop a use which is typically accessory to such gas station uses. The fact the use preexisted the 1993 Specific Plan, which now does not permit the use, means that the use is a “legal nonconforming use.” It is well established that a legal nonconforming use may not be expanded. *Hansen Brothers Enterprises, Inc. v. Board of Supervisors of Nevada County* (1996) 12 Cal.4<sup>th</sup> 533, 551. Staff is directly advocating the expansion if a legal nonconforming use. This is improper.

The proposed findings in the staff report indicate that: “The unique location (subject to a Specific Plan which prohibits redevelopment or expansion of the legally existing, previously-established use) and existing development of the site are special circumstances applicable to the subject property that do not apply generally to other property in the same zone and vicinity.” Staff states that these circumstances include the large size of the site, history of automotive uses, location on a corner, and adjacency to the nearby Freeway. Staff argues that, in essence, the site is uniquely capable of accommodating the proposed car wash and providing needed access and parking for the proposed incidental use without impeding access to or interference with the existing refueling operation.

The analysis is not focused on the actual characteristics of the subject site. The proposed findings points out that the proposed use is not allowed in the subject zone. The proposed findings then indicate that, because the site already has an existing automotive use and is big enough and located near a freeway, it can handle the proposed use. This is not a proper variance or specific plan exemption analysis. These findings do not support the granting of a variance or a specific plan exemption.

Staff also refers to the adjacent gas station. The fact that the City may have previously allowed a car wash on another property, does not justify the failure in this case to properly consider the legal variance requirements. There is no evidence that relates to the other site that justifies the failure to properly analyze the proposed project in this case.

The intended use is not allowed by the City’s zoning ordinances. If the applicant or the City desire to proceed with the use, they are required to seek a change in the zoning ordinance and comply with all of legal requirements necessary for such a change. Such requirements cannot be avoided by use of the variance procedure.

For all of the above reasons, the Planning Commission should deny the proposed project.

Sincerely,

*Eduardo Olivo*

Eduardo Olivo  
Attorney at Law



Andrew Jorgensen <andrew.jorgensen@lacity.org>

**4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)**

1 message

**eolivo@olivo-law.com** <eolivo@olivo-law.com>

Wed, Aug 25, 2021 at 5:18 PM

To: [apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org)

Cc: Andrew Jorgensen <andrew.jorgensen@lacity.org>, Miguel Hernandez <miguel.hernandez@lacity.org>, Claudia Rodriguez <claudia.rodriguez@lacity.org>

Please see the attached regarding the above-referenced project.

Thank you.

Eduardo Olivo

Attorney at Law

OLIVO & ASSOCIATES

[13181 Crossroads Parkway North](#)

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**Comment Letter 2 MD.pdf**

1660K

**OLIVO & ASSOCIATES**

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CITY OF INDUSTRY, CALIFORNIA 91746

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August 25, 2021

**VIA EMAIL AND U.S. MAIL**

[apcsouthvalley@lacity.org](mailto:apcsouthvalley@lacity.org)

cc: [andrew.jorgensen@lacity.org](mailto:andrew.jorgensen@lacity.org)

[miguel.hernandez@lacity.org](mailto:miguel.hernandez@lacity.org)

[claudia.rodriguez@lacity.org](mailto:claudia.rodriguez@lacity.org)

South Valley Area Planning Commission  
CITY OF LOS ANGELES

Re: **South Valley Area Planning Commission August 26, 2021 Agenda Item No. 5**  
**4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV)**

Dear Planning Commissioners and City Staff:

As previously advised by separate letter, my office represents interested parties and residents of the City of Los Angeles who oppose the above-referenced project.

MD Acoustics has prepared a peer review of the MK Design Study that was provided for the project. MD Acoustics concludes that the car wash has the potential to exceed the City's noise ordinance to the surrounding commercial and residential properties. [A copy of the MD Acoustics report is attached as Exhibit 1.] The findings are important to the surrounding properties and should also be considered by the Commission. I ask that the MD Acoustics study be made part of the record.

Sincerely,

*Eduardo Olivo*

Eduardo Olivo  
Attorney at Law

Enclosure

# **EXHIBIT 1**

May 17, 2021

AZ Office

4960 S. Gilbert Road, Ste 1-461  
Chandler, AZ 85249  
p. (602) 774-1950

CA Office

1197 Los Angeles Avenue, Ste C-256  
Simi Valley, CA 93065  
p. (805) 426-4477

**Subject: 4801 Laurel Canyon Car Wash – Acoustical Peer Review – City of Los Angeles, CA**

Dear Mr. Pouldar:

MD Acoustics, LLC (MD) is pleased to provide a peer review of the MK Design Sound Study (dated 1/7/21) for the proposed 4801 Laurel Canyon Car Wash project, in the City of Los Angeles, CA. This review provides our input as it relates to the noise study and highlights areas where the study inadequately evaluates the noise.

**1.0 Comments for the Report**

MD has specific comments relating to the report that should be addressed in this analysis as outlined below:

7.0 Methodology

*Pg. 10*

1. Noise measurements were taken at a height of 36"-48" above finish grade. Section 111.02(a) of the Los Angeles Municipal Code states that the microphone shall be located four to five feet above the ground. Subsequently, the noise model should be calculated four to five feet above the ground.
2. The baseline measurements were based off 60 second samples. Section 111.01(a) of the Los Angeles Municipal Code states that ambient noise shall be averaged over a period of at least 15 minutes.
3. Streets and roads were modelled as a "uniform sound wall" of 60 dBA or 70 dBA. It is unclear how the propagation of street noise was calculated. Line sources such as roads should have a 3 dB drop-off over a doubling of distance. There is no distance given for the 60 dBA and 70 dBA assumptions or reference for these levels. The modeling approach is unclear.

*Pg. 11*

4. Peak is defined in the report as measurements of short duration. Peak sound pressure is a specific metric which is not generally analyzed for environmental noise. It appears that in this report, peak refers to the Leq levels measured during the loudest minute of the car wash cycle. The metrics should be more clearly defined to determine what each calculation and measurement is referring to.
5. Ambient Noise Level is defined in the report as levels averaged across 15 minutes. As stated in Section 111.01(a) of the Los Angeles Code, ambient noise is taken exclusive of the particular noise

sources to be measured. The report includes the operational noise within the ambient noise levels when only the existing road noise should be included. The metrics should be more clearly defined to determine what each calculation and measurement is referring to.

6. The equation presented appears to linearly average sound levels when they should be logarithmically averaged.

## 8.2 Samples Tables

*Pg.13*

7. It appears that a sound level meter was not used which satisfies the requirements for a Type S2A meter as defined by ANSI S1.4. This type of meter is required by Section 111.01(l) of the Los Angeles Municipal Code. It appears that a sound level meter app and internal microphone was used which is insufficient at performing baseline readings as there is no method to calibrate an internal phone mic. ANSI standards are clear and specific on the use of pistonphone calibrators and use on Type 2 (or higher) hardware.
8. The metric of CNEL is mentioned in this section and throughout the report, but CNEL levels are never calculated or presented.

## 9.1 Traffic Level Description

*Pg.15*

9. There is no reference or distance given for the expected sound levels given based on traffic flow. It appears to be based on sampled data from Location 3. When comparing Location 3 traffic levels versus sound levels, this methodology is anywhere from 0.27 to 6.33 dB off from the measured data. In traffic noise, a difference of 6 dB corresponds to a quadrupling of cars. The standard Federal Highway Administration's Traffic Noise Model or a similar established standard should be used for predicting noise from traffic.

## 11.1 Traffic Level Description

*Pg.19*

10. There is no source given for the blower and vacuum data. It is unknown if these levels refer to sound pressure or sound power levels. No distance is given in relation to these sources. It is not indicated if the reference level is for the vacuum nozzle or turbine. Blower noise levels can vary greatly depending on the manufacturer, so it is important to note which blowers are being modelled. It is also unknown how many blowers have been modeled and what height they are modeled at. These noise sources are the main noise producers on the project site and there is no way to determine if these noise sources were properly evaluated as no reference level has been provided.

## 11.2 Noise Criteria

11. There is no mention of the Los Angeles Municipal Code, the Land Use Noise Compatibility Matrix, or CEQA in this section. These must be addressed as a part of a Los Angeles noise study. The CEQA

assessment should include an analysis of both construction and operational noise. Construction noise is never mentioned in the report.

### 11.3 Proposed Site Plan – With implemented Regulatory Compliance Measures, Peak Noise Levels

Pg. 21

12. The proposed 6' CMU wall appears to have a noise reduction of 50 dB which is a significant over assumption. Assuming a height of 3 ft for the vacuum, this wall would have a maximum reduction of 14 dB (See Appendix A). It appears that the STC rating of the wall was used to calculate overall reduction, which does not take into account sound that travels above the wall. More information on how to determine noise exposure based on distance and barrier insertion loss can be found within Federal Transit Authority Manual (Table 4-28) and HUD calculator.

### 12.0 Recommendations

Pg. 22

13. The report states that 75 dBA is normally considered the upper threshold of acceptable noise at commercial structures, but no source is given. The threshold for powered equipment within 500 ft of a residence is given in Section 112.04 of the Los Angeles Municipal Code. Machinery may not cause the noise level on the premises of any other occupied property to exceed the ambient noise level by more than 5 decibels. CEQA also requires that operational noise must not increase by 3 dBA to or within the normally or clearly unacceptable categories or 5 dBA or greater. The report does not calculate the overall increase from the existing condition or the existing and operational CNEL levels.
14. The report does not clearly label the receptors at the commercial and residential sites or address all surrounding sites.

### 13.0 Conclusions

Pg. 24

15. It is unclear in this section how the report compares the operational noise levels to the existing condition. The report does not specify at which receptor the operational levels are averaged with the measured existing levels.
16. Most of the measured existing levels (8/10) already exceed the Minimum Ambient Noise Level given by Section 111.03 of the Municipal Code. It is unclear what regulation the noise levels comply with.
17. The existing measurement in Location 5 was 65.8 dBA. The calculated noise levels at that location were 74 dBA. Adding that to the existing condition gives an operational maximum condition of 74.6 dBA. Assuming a condition of 65.8 dBA for 10 minutes and 74.6 dBA for 5 minutes as assumed in the report, the overall operational Leq level is 70.9 dBA, which is 5.1 dB over the existing condition. This



calculation, however, cannot be verified as the assumptions used (which have been taken from the report) were not obtained by the required methods and standards.

## **2.0 Communications**

The review team had no communications with the project applicant or the preparer of the noise study.

## **3.0 Conclusion**

The car wash has the potential to exceed the City's noise ordinance to the surrounding commercial and residential properties according to the report. MD cannot verify the existing measurements or calculations in this study. MD's qualifications are provided in Appendix B. MD is pleased to provide this peer review of the MK Design Sound Study for the 4801 Laurel Canyon car wash. If you have any questions regarding this analysis, please do not hesitate to call us at (805) 426-4477.

Sincerely,  
MD Acoustics, LLC



Claire Pincock  
Consultant



Mike Dickerson, INCE  
Principal

**Appendix A:  
Calculations**

## Barrier Performance Module

This module provides to the user a measure on the barrier's effectiveness on noise reduction. A list of the input/output variables and their definitions, as well as illustrations of different scenarios are provided.

### Calculator

[View Day/Night Noise Level Calculator \(/programs/environmental-review/dnl-calculator/\)](/programs/environmental-review/dnl-calculator/)

[View Descriptions of the Input/Output variables.](#)

**Note:** Tool tips, containing field specific information, have been added in this tool and may be accessed by hovering over the Input and Output variables with the mouse.

**WARNING:** If there is direct line-of-sight between the Source and the Observer, the module will report erroneous attenuation. "Direct line-of-sight" means if the 5' tall Observer can see the noise Source (cars, trucks, trains, etc.) over the Barrier (wall, hill/excavation, building, etc.), the current version of Barrier Performance Module will not accurately calculate the attenuation provided. In this instance, there is unlikely to be any appreciable attenuation.

**Note:** Barrier height must block the line of sight

### Input Data

H	<input type="text" value="6"/>	R <sup>1</sup>	<input type="text" value="2"/>
S	<input type="text" value="3"/>	D <sup>1</sup>	<input type="text" value="2"/>
O	<input type="text" value="5"/>	α	<input type="text" value="180"/>

### Output Data

h	2	R	3
D		FS	
	1		13.8618

**Reduction From Barrier (dB):**

**-13.8618**

Refresh

**Note:** If you have separate Road and Rail DNL values, please enter the values below to calculate the new combined Road/Rail DNL :

**Road DNL:**

**Rail DNL:**

Calculate

**Combined Road/Rail DNL with Barrier Reduction:**

## Input/Output Variables

### Input Variables

The following variables and definitions from the barrier being assessed are the input required for the web-based barrier performance module:

- H = Barrier Height
- S = Noise Source Height
- O = Observer Height (known as the receiver)
- $R^1$  = Distance from Noise Source to Barrier
- $D^1$  = Distance from the Observer to the Barrier
- $\alpha$  = Line of sight angle between the Observer and the Noise Source, subtended by the barrier at observer's location

### Output Variables

Definitions of the output variables from the mitigation module of the Day/Night Noise Level Assessment Tools as part of the Assessment Tools for Environmental Compliance:

- h = The shortest distance from the barrier top to the line of sight from the Noise source to

**Appendix B:**  
**Reviewers Resumes**

# CLAIRE PINCOCK

## ACOUSTICAL CONSULTANT



Claire Pincock graduated from Brigham Young University in the top 5% of her graduating class with a Bachelor of Science degree in Applied Physics with an emphasis in acoustics and a minor in Mathematics. Prior to her joining MD Acoustics, she was a research assistant at BYU.

Her research was focused on speech acoustics, specifically on how speech radiates from the mouth.

<https://www.physics.byu.edu/thesis/archive/2017>

In 2017, Claire presented her research with Dr. Timothy Leishman on speech directivity at the Acoustical Society of America, spring Boston conference where she placed 2nd in the student competition on speech acoustics.

<http://asa.scitation.org/doi/abs/10.1121/1.4987652>



### EDUCATION

Brigham Young University  
B.S., Applied Physics (Acoustics), 2017  
Magna Cum Laude

### AFFILIATIONS AND AWARDS

Acoustical Society of America (ASA)  
BYU Acoustic Research Group  
American Institute of Physics (AIP)

### REPRESENTATIVE PROJECT EXPERIENCE

#### Architectural Acoustics Assessment

- Vista Del Mar Residential Development, Dana Point, CA
- Hampton Inn, Santa Ana, CA

#### Noise and Vibration Assessment

- Vineland Gas Station and Car Wash, Los Angeles, CA
- Lakeview Terrace Gas Station & Car Wash, Los Angeles, CA
- Van Buren Blvd Express Car Wash, Riverside, CA
- Broadway Gas Station and Car Wash, Los Angeles, CA
- Clinton Keith Service Station and Car Wash, Murrieta, CA
- Old Middlefield Car Wash, Mountain View, CA
- Riverside Car Wash and Retail, Jurupa Valley, CA
- Estrella Commons Residential Development, Goodyear, AZ
- Blackstone & McKinley Multi-Family Development, Fresno, CA
- Haven at 26th Mixed Residences, Rancho Cucamonga, CA
- Berylwood Park Skatepark, Simi Valley, CA
- Marshalls – Del Amo Plaza, Torrance, CA
- Desert Land Ventures, Desert Hot Springs, CA
- Ellis Commons Senior Housing Blasting, Perris, CA

# CLAIRE PINCOCK

## ACOUSTICAL CONSULTANT —

Continued



- Canyon Creek Blasting, San Diego, CA
- Pico Blvd Multi-Family Development Cat32, Los Angeles, CA
- Cyrus One Trane Chillers, Chandler, AZ
- Warner Ave Vault Resonance, Huntington Beach, CA
- Baypoint Preparatory Academy, San Jacinto, CA
- Amani Apartments, Los Angeles, CA
- Kawana Springs Apartments, Santa Rosa, CA
- Jillson and Harbor Residential Developments, Commerce, CA
- Adams Blvd Car Wash and Service Station, Los Angeles, CA

### Vibration Assessments

- Canyon Creek Blasting, San Diego, CA
- Warner Ave Vault Resonance, Huntington Beach, CA
- Ellis Commons Senior Housing Blasting, Perris, CA

### Construction Noise and Vibration

- Mae Boyar Park Community Building, Oak Park, CA
- Broadway Gas Station and Car Wash, Los Angeles, CA
- Clinton Keith Service Station and Car Wash, Murrieta, CA

### Noise Reviews/Updates

- Mt. San Antonio College Facilities and projects Noise Review, Walnut, CA
- French Valley Airport Development, Riverside County, CA
- Cameron Ave Surf Thru Express Car Wash Noise Compliance, Visalia, CA

### Court Cases

- Sanctuary at Deseret Ridge, Scottsdale, AZ

### Ordinances

- Flagstaff Parks Noise Ordinance, Flagstaff, AZ

### Facilities Acoustics Qualifications/Certification

- Ft. Huachuca Anechoic Chamber Qual/Cert, AZ
- MD Acoustics Anechoic Chamber Qualification

# MIKE DICKERSON JR., INCE PRINCIPAL

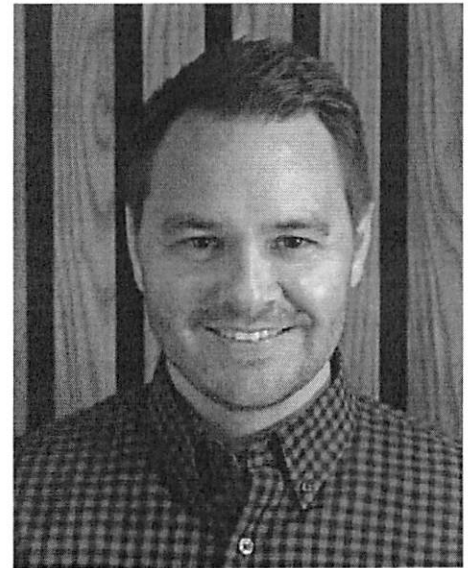


Mike Dickerson has a passion for the science of sound and vibration and has worked professionally in acoustical engineering since 2002. He received his Bachelor of Science degree in Physics, emphasizing in acoustics from Brigham Young University in Utah. He is currently a member of the Institute of Noise Control Engineers (INCE). Motivated by professional growth and opportunity, Mr. Dickerson formed his own acoustical engineering firm, MD Acoustics in 2012.

Mr. Dickerson's versatile experience includes leading and assisting in the design and review of many facets of acoustical engineering and air quality projects, including but not limited to air/noise assessments, ceiling/floor assembly design, architectural design, acoustical product design, vibration analysis and noise mitigation strategies.

Prior to starting his own consulting firm, Mr. Dickerson worked for Sony Entertainment, Parsons, and RK Engineering. He has successfully completed over 4,200 acoustical/air quality assessment reports for various engineering companies, municipalities and other agencies (both public and private). His strategic project planning and cost effective management solutions enabled him to excel in the field of Acoustics and project management.

In 2011, Mr. Dickerson was asked to present a paper at the Acoustical Society of America Seattle, Washington Conference on his research and work on noise and vibration in Cockpit Door Modules. MD continues to work closely with many engineering disciplines and provides quality results.



## EDUCATION

Brigham Young University  
B.S., Physics (Acoustics), 2005

## AFFILIATIONS AND AWARDS

Institute of Noise Control Engineers (INCE)  
Acoustical Society of America (ASA)  
BYU Acoustic Research Group  
Association of Environmental Planners (AEP)

## REPRESENTATIVE PROJECT EXPERIENCE

### Architectural Acoustics Assessment/Design

- Wells Fargo Corporate Office, Chandler, AZ
- Peoria Sports Complex Seattle Mariners Lobby, Peoria, AZ
- Intel Corporation CH5 216/217, Chandler, AZ
- Mammoth Rock and Bowl, Mammoth Lakes, CA
- Rubios Restaurant, San Diego, CA
- Americana at Brand, Glendale, CA
- 6300 Hollywood Blvd Retail Space, Hollywood, CA
- Calvary Church Renovations, Santa Ana, CA

### Noise and Vibration Assessment

- Central Metal Incorporated Reclamation Plant Expansion, Los Angeles, CA
- Sonora Commons North Gateway Transfer Station, Phoenix, AZ
- Hotel and Water Park Development, Garden Grove, CA
- Longbow Development Aircraft Noise, Mesa, AZ
- La Ventilla Development I 10 Freeway, Goodyear, AZ
- Wells Park Pump Improvement, Chino Hills, CA



# MIKE DICKERSON JR., INCE

## PRINCIPAL — Continued



- Hog Wash Car Wash, Phoenix, AZ
- Ellis Commons Senior Housing Blasting, Perris, CA
- Canyon Creek Blasting, San Diego, CA

### Oil and Gas Noise and Vibration Assessment

- La Goleta Storage Field Enhancement, Santa Barbara County, CA
- Whittier Workman Mill Road Oil Rig Drilling Operation, Los Angeles County, CA
- La Goleta Storage Compressor Vibration, Santa Barbara County, CA
- Mills Station Excavation Noise and Vibration, Ventura, CA

### Highway/Airport/Rail Noise and Vibration

- Caltrans Yucaipa Bridge Box Culvert Improvement, Yucaipa, CA
- Caltrans SR 110 Freeway Expansion, Los Angeles, CA
- Jackson Hole Airport Noise Contours and Flight Path Evaluation, Jackson Hole, WY
- Light Rail Transit Exposition Blvd to Culver Dr, Los Angeles, CA

### Telecommunications

- T Mobile Telecommunication Tower, Calabasas, CA
- T Mobile Telecommunication Tower, Malibu, CA
- Verizon Wireless Telecommunication Tower, Santa Clarita
- Sprint Pinto Lake Cell Tower, Santa Cruz, CA
- ATT Cell Tower, Dover, MA
- ATT Cell Tower, Poway, CA

### Construction Noise and Vibration

- La Goleta Storage Field Enhancement (1 yr construction monitoring), Santa Barbara County, CA
- Westin Bonaventura Hotel, Los Angeles, CA
- Railroad Canyon Road Expansion, Riverside, CA

### Industrial Noise and Vibration Assessment

- Hexcel Corporation Dust Collector Noise & Vibration, Casa Grande, AZ
- ABB Inverter Operation Noise and Vibration, Tempe, AZ
- Pacific Scientific Shaker Room, Chandler, AZ

### Air Quality & Greenhouse Gas Assessment

- Golden Gate National Park Recreation Area, San Mateo, CA
- RAW removal of Contaminated Soils AQ/GHG, Glendora, CA

- Glass House Pharms Cultivation Facility, Cathedral City, CA
- Murrieta Car Wash and Service Station, Murrieta, CA

### Semiconductor Noise/Vibration Evaluations

- NXP Semiconductor Building M Vibration Isolation Design/Modifications
- NXP Semiconductor PCW Pump Isolation Vibration Isolation Design
- NXP Semiconductor HVAC Duct Silencer and Acoustical Design
- NXP Semiconductor Demising Wall Evaluation and Redesign
- CyrusOne Data Center Chiller Noise Evaluation and Noise Abatement

### Mechanical Equipment (HVAC) Noise and Vibration Assessment

- Kaiser Hospital AHU Louver Replacement Acoustics Review/Design, San Francisco, CA
- Northern Arizona University Communication Building Redesign of Mechanical Systems Noise and Vibration Abatement Assessment and Design Recommendations, Flagstaff, AZ
- UCLA Call Center HVAC Equipment Noise and Vibration Design Review and Recommendations, Agora Hills, CA

### Film and Entertainment Noise and Vibration Assessment

- Revolt Studio, Hollywood, CA
- University of Arizona, HSIB, Acoustics Evaluation and Design, Tucson, AZ
- The Van Buren, Phoenix, AZ
- Down to Shop Studios, Los Angeles, CA

### Court Cases

- Sanctuary at Deseret Ridge, Scottsdale, AZ
- Westfield Mall, Topanga Canyon Blvd., Woodland Hills, CA
- Mobile Radio Patent infringement, Calfee Attorneys
- Foghorn Noise Acoustic Evaluation and Noise Review, Oxnard, CA

### Ordinances

- Flagstaff Parks Noise Ordinance, Flagstaff, AZ
- Queen Creek Noise and Vibration for Well Drilling, Queen Creek, AZ



Andrew Jorgensen <andrew.jorgensen@lacity.org>

**4801 N. Laurel Canyon Blvd. (APCSV-2019-1481-SPE-SPP-CU-ZV (South Valley Area Planning Commission Meeting for August 26, 2021 - Agenda Item No. 5)**

2 messages

**Tiffany Lorane** <tiff1or12@yahoo.com> Thu, Aug 26, 2021 at 2:50 PM  
To: "apcsouthvalley@lacity.org" <apcsouthvalley@lacity.org>  
Cc: Andrew Jorgensen <andrew.jorgensen@lacity.org>

Hello,

Please find attached a petition I started concerning the new carwash proposal at [4801 N Laurel Canyon Blvd](#). As you can see, many residents are opposed to this construction. Please do not allow for this as it will detrimentally affect our community!

Thank you,

Tiffany

**4801 Laurel Canyon - Petition in Opposition.pdf**  
485K

**Andrew Jorgensen** <andrew.jorgensen@lacity.org> Thu, Aug 26, 2021 at 4:24 PM  
To: Blake Lamb <blake.lamb@lacity.org>, Claudia Rodriguez <claudia.rodriguez@lacity.org>



**Andrew Jorgensen, AICP**  
*pronouns: he/him/his*  
City Planner, Southeast Valley  
**Los Angeles City Planning**  
6262 Van Nuys Blvd  
Van Nuys, CA 91401  
Planning4LA.org  
T: (818) 374-9904



[Quoted text hidden]

**4801 Laurel Canyon - Petition in Opposition.pdf**  
485K

# Stop the Carwash!

I am a resident of the neighborhood and I oppose the new **24 hour carwash** at 4801 Laurel Canyon Blvd (APCSV-2019-1481-SPE-SPP-CU)

Signature

Date

08/20/2021

Margaret Flynn 12121 La Maida St Valley Villas Ca

08/21/2021

Berianne Willis "

" 8/21/2021

" 8/21/2021

" 8/21/2021

Jeff Pithla 12138 La Maida St.

8/21/2021

Maia Chang "

" 8/21/2021

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# Local Business





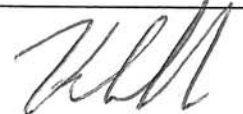


Nader's Mobil

4801 Laurel Canyon BLVD

Valley Village, CA 91607

Please show your support to our Car Wash Project by signing this petition so we can continue to provide a valuable service to the local community.



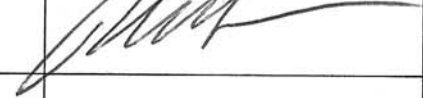



I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
William Ovasapyan	4814 Laurel Canyon State Farm Insurance	Ovasapyanwilliam@gmail.com	747-241-2471	
Gevork Alebyan	4814 Laurel Canyon State Farm Insurance	GevorkAlebyan@yahoo.com	818-263-9610	
Shayan Hakakian	4814 Laurel Canyon State Farm Insurance	shayanhakakian@yahoo.com	818-699-3142	
Marieta Ghantarchyan	4814 Laurel Canyon Blvd State Farm Insurance	marieta.ghantarchyan@gmail.com	213-880-7070	
Lechchik Hakannisyan	4814 Laurel Canyon State Farm Ins	lchchik200@gmail.com	818-522-1477	
Andrey Postikyan	4814 Laurel Canyon State Farm	andreypostikyan@gmail.com	818-599-9696	
Avedis Kegeyan	4814 Laurel Canyon State Farm	AvedKegeyan14@gmail.com	818-434-0466	

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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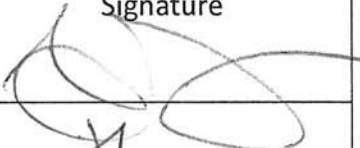


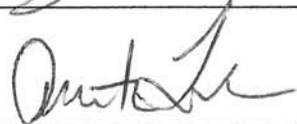
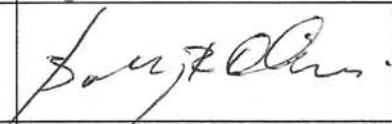

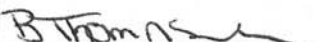

I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Jonathan Martinez	4803 Laurel Canyon Valley Village CA 91607	Johnny6566@ <del>va</del> yahoo.com	(818) 445-6566	
DAVID MORRISON	12729 RIVERSIDE DR VALLEY VILLAGE 91607		818 384 9286	
Neil Cohen	5017 Radford Ave Valley Village 91607		818-618-8820	
Glenn Ryan	12244 Riverside Valley Village CA		818 980-5339	Glenn M Ryan
ALEX HERNANDEZ	4843 LAUREL CANYON VALLEY VILLAGE	ALEX@PAROOTABESCO	818 415 9710	
PEARL	11939 ALBERT ST VALLEY VILLAGE			
JOSHUA B	1264 HERTZMAN			

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

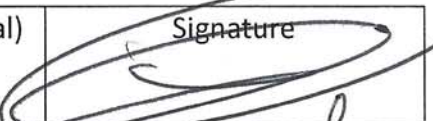


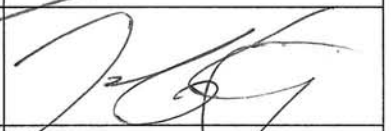

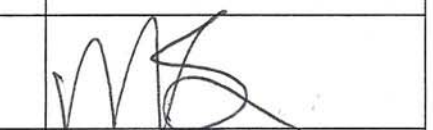
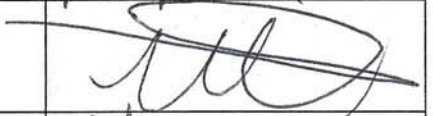

Name	Address	Email	Phone Number (Optional)	Signature
Tori McAnkey	12104 nuston			
John Arenas	5056 Whitsett			
YANA STEIN	4741 Laurel cym			
Auto Tolkoff	11652 Marmite St			
Bobby Oliverz	11935 Kling St #23 Valley Village CA 91607			
Lori Nuzzolo	4701 Vantage Ave Valley Village CA 91607			
Becky Thompson	19009 Sherman Way #47 Reseda 91335	beckyjean0103@gmail.com		
Robert Bell	11911 Magnolia #23 Valley Village 91607	vegas663000@stglobal.net		

# Local Business in the Area

Nader's Mobil  
 4801 Laurel Canyon BLVD  
 Valley Village, CA 91607

Please show your support to our Car Wash Project by signing this petition so we can continue to provide a valuable service to the local community.

I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

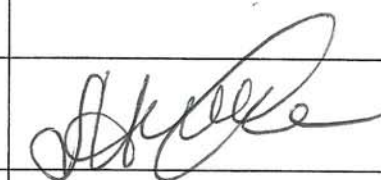



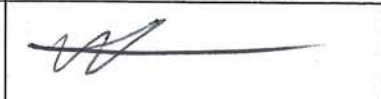

Name	Address	Email	Phone Number (Optional)	Signature
Tolin O'Brien	4853 Laurel Canyon Blvd	Tolin.OBRIEN@gowireless.com	818 432 3030	
Camron Fenger	12629 Riverside PL	TheRane1@gmail.com	818 915 6973	
Anthony Chin	12623 Riverside Dr	antchill@yahoo.com	210-849-8805	
JOHN CASCIARELLI	321 S SHREVEBOURNE LA, CA 90048	JOHNLC@AOL.COM	818-376-2326	
Harin Clarke	12125 Riverside Dr #1 Valley Village	Harin228@yahoo.com	818 618 2474	
Matt Schwartz	12135 Riverside V.V. 91607	QISHAKTI@hotmail.com	818 430 1906	
Jasmine Bennett	11126 Huston St 91601	JBennett70@yahoo.com	N/A	
Rhonda Davis	11124 Huston St 91601	—	N/A	

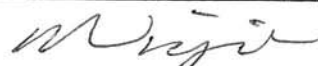
# Local Business

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

Please show your support to our Car Wash Project by signing this petition so we can continue to provide a valuable service to the local community.

I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Anna Mamaryuk	12139 Riverside Drive Valley Village, CA 91607	anna@yandex.com	(818) 763-3937	
Eteri Dokholian	↓	↓	↓	
Brenda Cervantes	12139 Riverside Dr. 91607	cervantesb42@yahoo.com	818 432-4014	
Armaan Mirza	9907 White Oak 1800 Lane Ave Apt 119 Sprint store	ArmaanCS@icloud.com	661 425 5913	
Isidro Pinedo	4800 Laurel Canyon Valley Village CA 91607	isidro.p@gmail.com		IP
Tati Somaro	↓ ↓ SPRINT ↓	Tati.Somaro1@gmail.com		
Naren Horduzich	4804 Laurel canyon Blvd Valley Vlg ca 91607	store2036@theups store.com	818-509-0802	




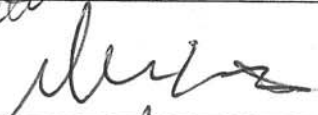

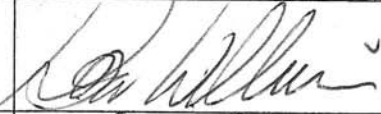
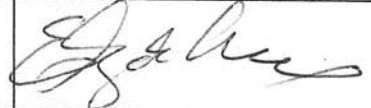
Virginia Mayozzin 4246 Rossmore Ave Los Angeles Veemayozzin521@gmail.com (323) 812-6570 



Nader's Mobil  
4801 Laurel Canyon BLVD  
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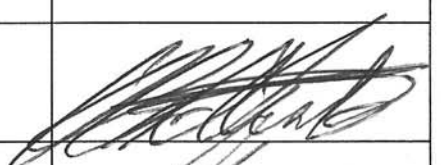



I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Bizilla	5700 WAREHOUSE VALLEY VILLAGE	CHONCHONCUP@ YAHOO.COM		
RYAN BROOKMAN	4924 Laurel Canyon Blvd Valley Village	rbizilla13@ Gmail.com	818 648 5641	
Chris Wooten	4950 Laurel Canyon Blvd.	cwooten18@live.com	409-789-3048	
Dan Hwang	12206 Magnolia Blvd	dano908@msn.com	808-220-9068	
ARTHUR SARK	12208 Riverside Valley Village	Asarkis7@1986@yahoo.com		
DON Williams	4951 WAREHOUSE VALLEY VILLAGE 91607		518 335-4625	
Lisa Charleson	4829 Beeman Ave	LisaCharleson@yaho.com	818 621 2369	

Nader's Mobil  
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Valley Village, CA 91607

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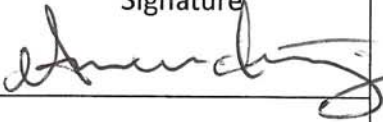




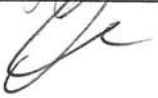
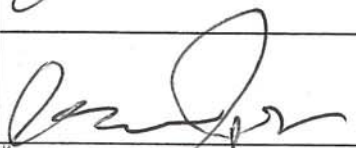

I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Jose Rosario	11701 S Antares Ave	Josecoconuts@Icloud		
Jim Delaney	4629 Fulton #203	JPD173350@gmail.com		
Mark Traysiman	4822 Van Ness Ave.	MTraysiman@gmail.com	— 11 —	
Sharon Anthony	11936 Magnolia Blvd.	probuidoffice sharon@gmail.com		
ALAN ALTICHAL	4804 LAUREL CANYON 9/1007	ALISCHULCA@AOL com	—	
John Hostlake	5210 WILKINSON AV.	XR130X@EMAIL.COM		
GREG COVATTA	1901 KIRBY LN	GREG COVATTA 2 grad.	—	
ROS LAWE	12144 HEATSOAK	ROS@LAWFIRM.COM		

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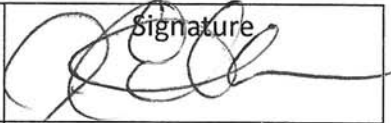
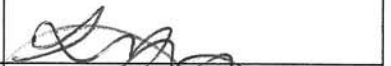


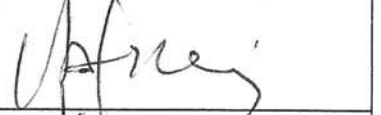
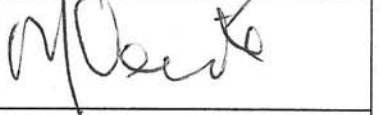
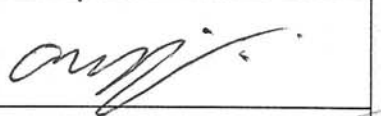

I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Mercy Quinonez	Sherman Village Hospital Riverside OR 97507	Mercyquinonez269@gmail.com	816-450-9954	
Haitham	11840 Chandler Blvd	Haithamabouhamdan@hotmail.com		
Rock Chan	14062 Mergale St	rickc@gmail.com		
Rodolfo	11-014 A	Arvin1234	(515) 445-6432	
David	18347 Camino bello St	Dalascabes@gmail.com		
Moat Lissim	18363 Ventura Blvd	moat.lissim2@gmail.com	8d 676 7922	
Chris James	12235 Tiara Street	Chris James 62 @ Blue Dot @ Gmail	518 605-0880	
Mark Sandstrom	11255 Moor Park St. #18	sandstrom.mark@gmail		

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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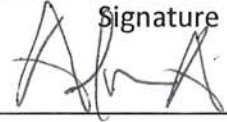





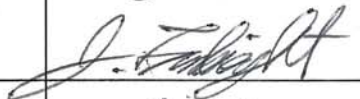
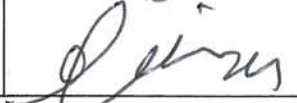
I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Ana Cross-Frye	10000 Simpson Ave NH, CA. 91607	ana.costanza@gmail.com	213-944-2008	
Youngjean Song	11925 King St. Apt 207 Valley Village CA 91607	songyoungjean@hotmail.com	213-665-3371	
Heir Schmidt	5101 Whitsett Ave Valley Village 91607	heir.schmidt@gmail.com	—	
Sharon Clarke	11839 Magnolia Blvd #2 V.V. CA 91607	Shashalobebugey@gmail.com		
Mauricio Capaldi	5257 RADFORD AVE	MCAPALDI1978@hotmail.com		
Marco Verdier	5051 Whitsett Ave	Kavakine@gmail.com		
Anthony Villalobos	2325 Ocean View Ave Los Angeles, CA 90057	va86609@gmail.com		
William Caci	2541/2 Oak St. Apt 5 Santa Monica, CA 90405	cyberbilly04@gmail.com		

Nader's Mobil  
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Valley Village, CA 91607

Please show your support to our Car Wash Project by signing this petition so we can continue to provide a valuable service to the local community.

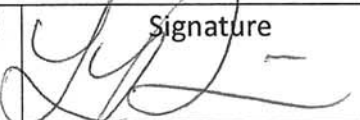
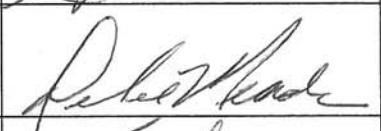
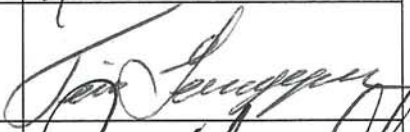
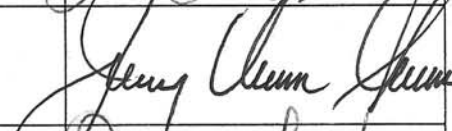
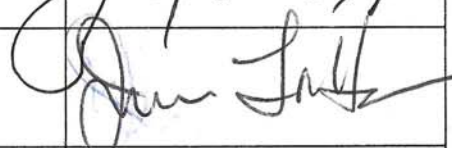
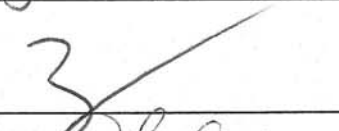

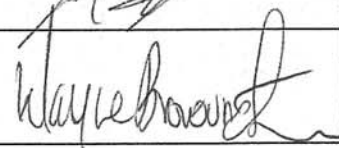
I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Alvaro Antonio	4706 Vantage Ave	A1220@icloud.com		
Jessica Garcia	4706 Vantage Ave	sweetjamil320@gmail.com		
Brandon Delgado	11450 Riverside			
Sean Samson	1244 Riverside Dr.	Slamson22@yahoo.com		
Dave Weinberger	11815 Hesby St	dave@kuschotmail.com		
Alexis Twine	2456 Magnolia	stom28912@gmail.com		
Jesse Lambright	11117 Hartsook St	JLamb317@live.com		
Gary Wilson	5050 Coldwater Cyn.	iwadren@ earthlink.net		

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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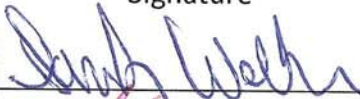
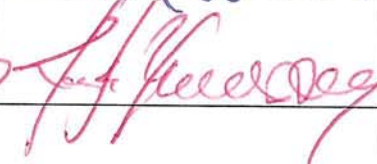


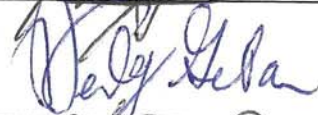

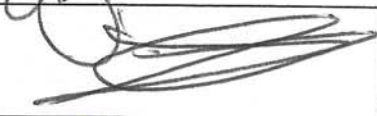

I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Lori DAVIES	12155 Riverside Dr Valley Village, CA 91607	Lorid@ISUVANSA.COM	818-763-9365	
Debra Meade	12155 Riverside Dr Valley Village, CA 91607	Debra@ISUVANSA.COM	818-763-9365	
Tina Bevorgyan	12155 Riverside Dr Valley Village CA	Tinuy@isuvansa.com	818-763-9365 ext 113	
JERRY VANDER SANDE	12155 Riverside Valley Village	jerryvse@isuvansa.com	818 763-9365	
Jamos L. Itta	12155 Riverside Dr Valley Village, CA 91607	rossmura@yoh.com	818 763 9365 ext 115	
Mahsa Zarifyan	12155 Riverside Dr Valley Village CA 91607	MAHSAZ@ ISUVANSA.COM	818 -763-9365 X 127	
Josh Esquith	12155 Riverside Dr Valley Village, CA 91607	joshe@isuvansa.com	818-763-9365	
Wayne Brown	12155 Riverside Dr Valley Village, 91607	WAYNB@ISUVANSA, com	818 763-9365 #123	

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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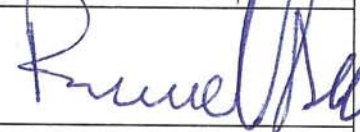






I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Sarah Wahnou	Valley Village Av 4829 Whitsett St		818-300-7246	
✓ ERANOSIAN KREKOS	5916 Wilkinson Street Valley Village		818/422-0407	
Arnet	5227 HERMITAGE AVE PRIVATE	Arnet@Roadrunner.com	818/3954563	
John Boney	RIVERSIDE + RADFORD	Jboney24@msn.com	818-554-0495	
Wendy Gebauer	5001 Laurelgrove	wendyinmaadi @yahoo.com	818-308-7009	
Leo	Krubsack	leonard notes @hotmail.com	818 6347253	
Torin Obrien	4853 Laurel Canyon Verizon 91607	Obrien.torin@gmail.com	661 212 7592	
Jamil HANAD	4822 Laurel Canyon BLVD		818 980 8555	

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
DAVID MANIBANH	5216 Corteen Pl.		447-468-9755	
Rick Dukov	4956 GENTRY Ave. Valley Village CA	dubavartsov @gmail.com	218-508-6305	
ALAN DACH	12348 RIVERDALE	adach@stfrad.com	916-437-1300	
JOHN BLACKWOOD	12360 RIVERSIDE, DA VALLEY VILLAGE, CA		818-763-4732	
Anthony Lois	4443 Coldwater Cyn Studio City CA 91604		818-769-1736	
M. Wierfeld	12133 La Manda St. V. Village, Ca.		(818) 769-8845	
JACOB EDWARDS	12202 Morrison St VALLEY VILLAGE CA 91607		(818) 390-2134	
Sami Mazumder	12244 Riverside Dr, Valley Village, CA		818 861 5654	



Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Pedro Zolich	4110 S. Laurel Blvd		310 390 1578	Pedro
S ZIMMERMAN	19002 Foothills		661 298 7640	
Alan Drey	3760 Avenida Del Sol San Diego, CA			
Austin Johnson	515 Whitesett Ave	ath	714 319 3983	
Banzuela	12339 Emerald St			
Panna P	4835 Redwood Canyon Ave			
Philip Buller	5310 Ames		323 266 4646	Philip Buller
D Beauville	5330 Ben Ave Valley Village, CA 91607			


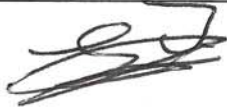
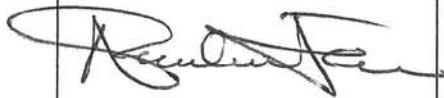






# Local Business

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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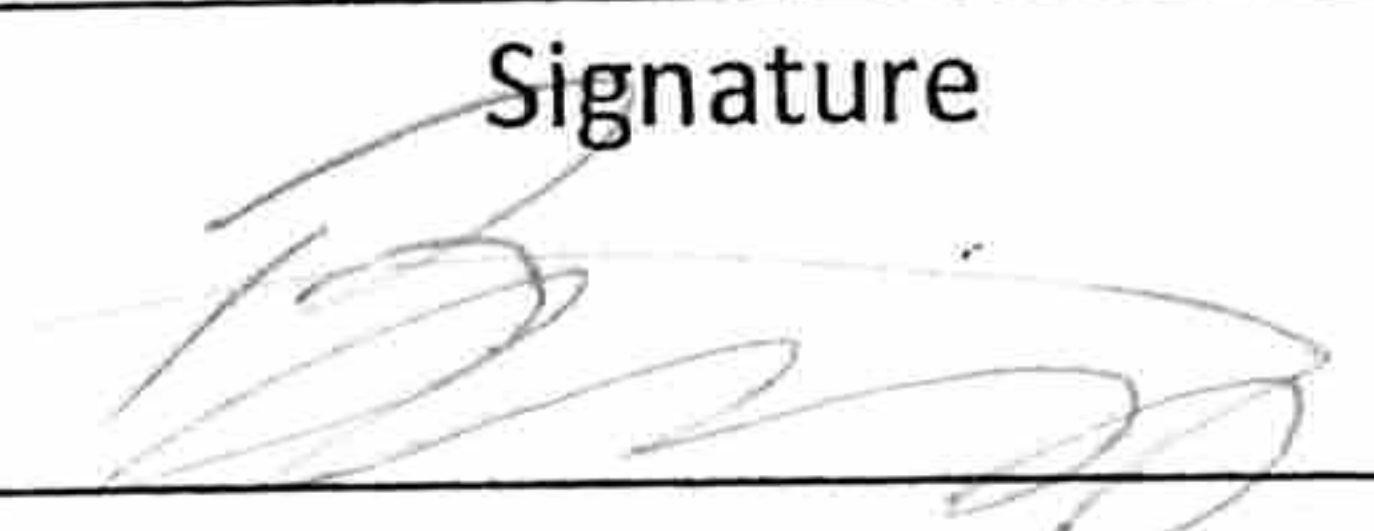

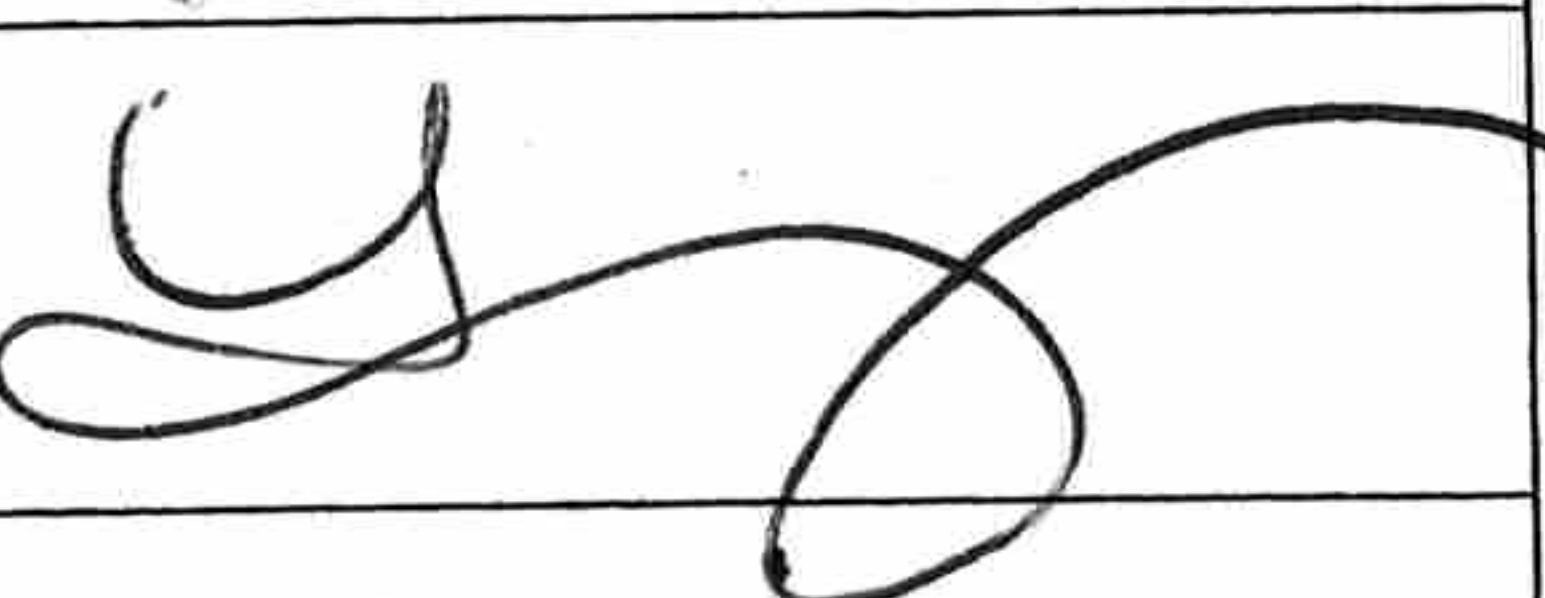
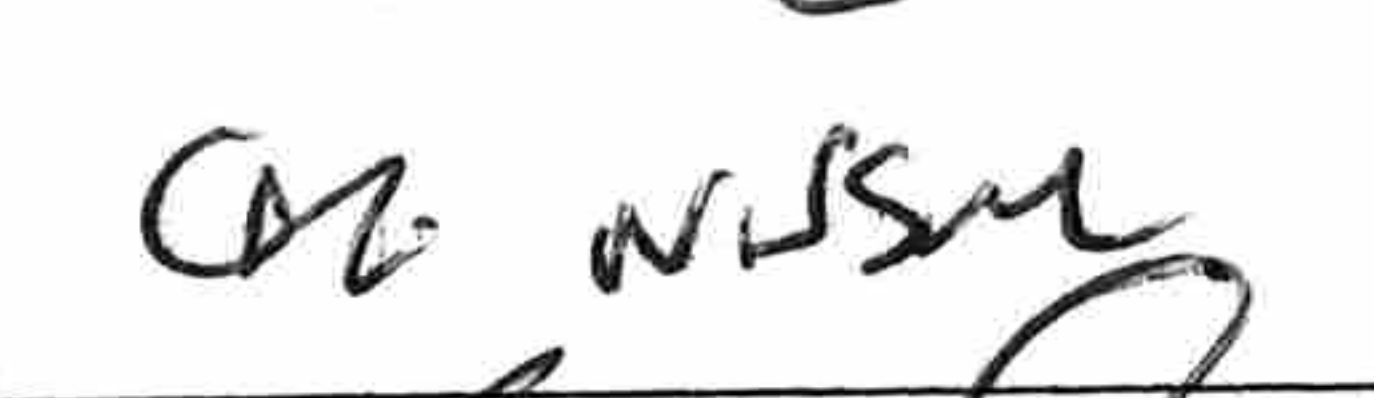

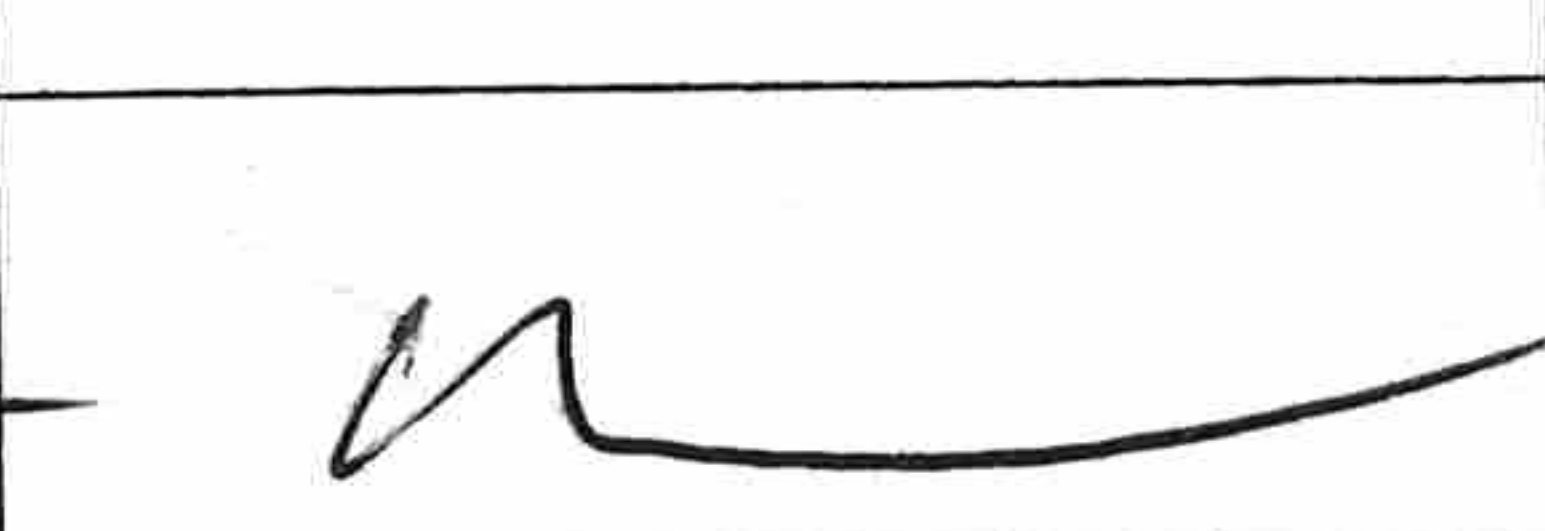
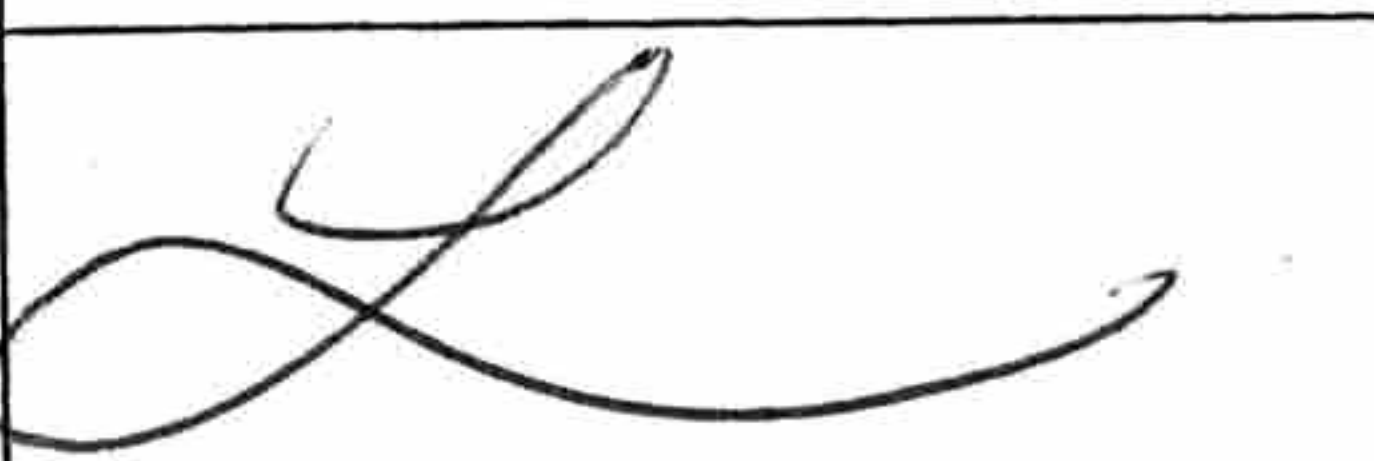

I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
JOE-IC E SLON	4822 LAUREL CANYON BLVD VALLEY VILLAGE, 91607	DANIELLESPIZZA@ HOTMAIL.COM	8189808555	
Emil Bejanian	4820 Laurel canyon BLVD valley village 91607	Emil @Dejucats .com	818-752-1140	
Rubina John	4820 Laurel canyon Blvd. Valley Village 91607	Rubinajohn@gmail.com	8186050026	
Christie Nguyen	4808 Laurel Cyn Valley Village		818-7478882	
SIMON BTIMAZIAN	12041 RIVERSIDE DR VALLEY VILLAGE, CA 91607	SMOKESANDPAPERWAGE @GMAIL.COM	(818) 691-3341	
Arman Adamyan	12043 Riverside Dr Valley Village CA 91607	aa.agunphkwoey @yahoo.com	(818) 308-7538	
Vernika Mirzayan	4800- Laurel gn 91607	Vernikam@gmail.com		

Nader's Mobil  
4801 Laurel Canyon BLVD  
Valley Village, CA 91607

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I support this car wash project located at 4801 Laurel Canyon BLVD Valley Village, CA 91607

Name	Address	Email	Phone Number (Optional)	Signature
Bryan Watten	11940 Werrington St. Valley Village, CA 91607	BryanWatten@gmail.com	747-800-1054	
Paul Gonzalez	11940 Werrington St. Valley Village, CA 91607	8399487@gmail.com	323-275-4505	
TORI McConkey	2927 Surfside Ave	TORIMcconkey@gmail.com		
ORLI NISSAN	4419 Rhodes	ORISNISA@gmail.com	818-299-3781	
KAREN	12629 Riverside Dr	aghrkyan888@yahoo.com		
MALCOLM WISS	4838 (RADFORD) AVE V.V.		310 989-2657	
Lesley Cerwin	Huston St.	LESLEYCerwin@gmail.com	310/245-2386	
Philip Reichman	12244 Riverside Dr. #102	PhilipReichman@outlook.com	(818) 625-4223	

Name: JUDI STOLLER

Email: \_\_\_\_\_

Address: 11201 HORTENSE ST

WEST TOLUCA LAKE

Phone (optional): 818-761-2974

I (Print Name) Judi Stoller support the Carwash Project here at **Nader's Mobil, 4801 Laurel Canyon BLVD Valley Village, CA 91607.** I am a member of the community and believe it to be a valuable asset to the neighborhood.

Thank you!

X Judi Stoller  
Date: 1-7-2022

Name: ALAN DASHEFSKY

Email: thelastdash1@gmail.com

Address: \_\_\_\_\_

12343 1/2 RIVERSIDE DR.

Phone (optional): \_\_\_\_\_

I (Print Name) ALAN DASHEFSKY support the Carwash Project here at **Nader's Mobil, 4801 Laurel Canyon BLVD Valley Village, CA 91607.** I am a member of the community and believe it to be a valuable asset to the neighborhood.

Thank you!

X 

Date: 1-7-2022

Name: PAUL

Email: epauledwards@yahoo.com

Address: 12202 Marisa St.

Valley Village CA 91607

Phone (optional): 818 384 9198

I (Print Name) Paul Edwards support the Carwash Project here at **Nader's Mobil, 4801 Laurel Canyon BLVD Valley Village, CA 91607.** I am a member of the community and believe it to be a valuable asset to the neighborhood.

Thank you!

X Paul Edwards

Date: 1/7/22

Name: Ryan Nizam

Email: RyanNizam@gmail.com

Address: 11837 Hartsook Street

Phone (optional): \_\_\_\_\_

I (Print Name) Ryan Nizam support the Carwash Project here at **Nader's Mobil, 4801 Laurel Canyon BLVD Valley Village, CA 91607**. I am a member of the community and believe it to be a valuable asset to the neighborhood.

Thank you!

X 

Date: 1-7-22

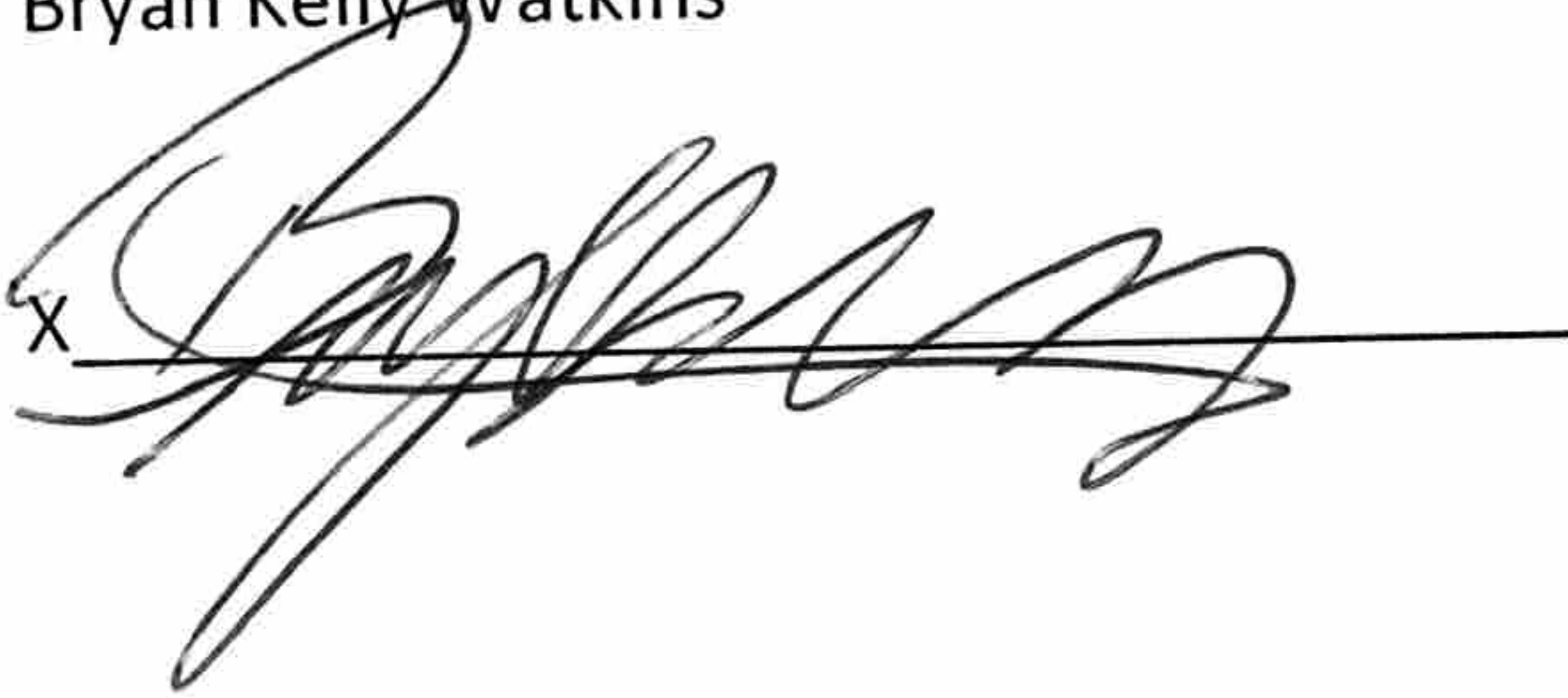


Bryan Kelly Watkins  
11940 Weddington st Unit 11  
Valley Village, CA 91607  
Bryankellywatkins@gmail.com

I Bryan Watkins live at 11940 Weddington st Unit 11 in Valley Village. Located a few blocks away from Nader Mobil, 4801 Laurel Canyon BLVD Valley Village, CA 91607 and as a member of the community I would love to have another carwash in the neighborhood! I believe it would be beneficial to the community due to the fact that at any given time that I go to the Carwash across the street there is a minimum of 5 to 7 cars in the line causing traffic and delay.

Thank you for your time!

Bryan Kelly Watkins

A handwritten signature in black ink, appearing to read "Bryan Kelly Watkins", is written over a horizontal line. A small "x" is visible at the beginning of the line.

Paul Gonzalez  
11940 Weddington st Unit 11  
Valley Village, CA 91607

Hello, my name is Paul Gonzalez and I am writing this letter to show my support in building this carwash at Nader Mobil, 4801 Laurel Canyon BLVD Valley Village, CA 91607. I have been a member of this community for many years and fully encourage this project. It would be great to have a secondary option here in the neighborhood to choose from.

Thank you!

Paul Gonzalez

X

A handwritten signature in black ink, consisting of a large, rounded loop on the left side and a long, horizontal, slightly wavy line extending to the right.