



# DEPARTMENT OF CITY PLANNING

## RECOMMENDATION REPORT

### City Planning Commission

<b>Date:</b>	October 13, 2022	<b>Case No.:</b>	CPC-2021-10394-CU-DB-HCA-PHP
<b>Time:</b>	after 8:30 a.m.	<b>CEQA No.:</b>	ENV-2021-10395-CE
<b>Place:</b>	Due to concerns over COVID-19 and continued concerns that meeting in person would present imminent risks to the health and safety of the attendees, the CPC meeting will be conducted entirely telephonically by Zoom [ <a href="https://zoom.us/">https://zoom.us/</a> ].  The meeting's telephone number and access code access number will be provided no later than 72 hours before the meeting on the meeting agenda published at <a href="https://planning.lacity.org/about/commissions-boards-hearings">https://planning.lacity.org/about/commissions-boards-hearings</a> and/or by contacting <a href="mailto:cpc@lacity.org">cpc@lacity.org</a>	<b>Incidental Cases:</b>	N/A
		<b>Related Cases:</b>	N/A
		<b>Council No.:</b>	11 - Bonin
		<b>Plan Area:</b>	Palms – Mar Vista – Del Rey
		<b>Plan Overlay:</b>	Los Angeles Coastal Transportation Corridor Specific Plan
		<b>Certified NC:</b>	Mar Vista
		<b>GPLU:</b>	Medium Residential
		<b>Zone:</b>	R3-1
<b>Public Hearing:</b>	August 17, 2022 October 13, 2022	<b>Applicant:</b>	12737 Mitchell Investment, LLC
<b>Appeal Status:</b>	Off-Menu Density Bonus Housing Incentives and Waivers are not appealable by any party. On-Menu Density Bonus and Conditional Use are appealable to City Council.	<b>Representative:</b>	Jesi Harris, Brian Silveira & Associates
<b>Expiration Date:</b>	October 27, 2022, Subject to the Mayor's Tolling Order		
<b>Multiple Approval:</b>	Yes		
<b>PROJECT</b>	<b>12735-12737 West Mitchell Avenue</b>		
<b>LOCATION:</b>	(legally described as Lot 190; Block None; Del Mar Tract)		
<b>PROPOSED PROJECT:</b>	The project is the construction of a six-story, 67-foot 8-inch tall residential building comprised of 15 dwelling units (including 2 Very Low Income and 1 Low Income units). The project will be approximately 19,047 square feet in floor area with a Floor Area Ratio ("FAR") of 3.99:1. The project will provide 14 parking spaces at-grade. The site is currently improved with a two-story single-family dwelling and triplex which will be demolished for the project. The project will also involve minimal grading of less than 500 cubic yards of soil.		
<b>REQUESTED ACTION:</b>	<ol style="list-style-type: none"><li>1. Pursuant to California Environmental Quality Act ("CEQA") Guidelines, an Exemption from CEQA pursuant to CEQA Guidelines, Article 19, Section 15332 (Class 32), and that there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.</li><li>2. Pursuant to Los Angeles Municipal Code ("LAMC") Section 12.22 A.25(g)(2) and (3), a Density Bonus/Affordable Housing Incentive Program Compliance Review to permit the construction of a Housing Development Project totaling 15 units, reserving 2 units for Very</li></ol>		

Low Income and 1 unit for Low Income Household occupancy for a period of 55 years, with the following requested three (3) On- and Off-Menu Incentives:

- a. A Floor Area Ratio of 3.99:1 in lieu of 3:1 as otherwise permitted in the R3-1 Zone (On-Menu).
- b. A reduction in parking to allow 14 parking spaces in lieu of 21 parking spaces otherwise required by LAMC Section 12.21 A.4 (Off-Menu).
- c. To allow 12 tandem parking spaces in lieu of the 1 individually accessible stall per dwelling unit required by LAMC Section 12.21 A.5(h) (Off-Menu).
3. Pursuant to LAMC Section 12.22 A.25(g), the following four (4) Waivers of Development Standards:
  - a. A height increase to 67 feet 8 inches in lieu of the 45 feet otherwise allowed by the R3-1 Zone.
  - b. A 7.2-foot northerly side yard setback in lieu of the 9 feet otherwise required by the R3-1 Zone.
  - c. A 7.2-foot southerly side yard setback in lieu of the 9 feet otherwise required by the R3-1 Zone.
  - d. To allow 6 compact and 8 standard parking spaces in lieu of the 1 standard parking stall per dwelling unit minimum required by LAMC Section 12.21 A.5(c).
4. Pursuant to LAMC Section 12.24 U.26, a Conditional Use Permit for a 62.5 percent increase in density over the Project site, in lieu of the otherwise permitted 35 percent increase in density allowable under LAMC Section 12.22 A.25.

#### **RECOMMENDED ACTIONS:**

1. **Determine**, that based on the whole of the administrative record, the project is exempt from CEQA pursuant to CEQA Guidelines, Article 19, Section 15332 (Class 32), and that there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies.
2. **Approve**, pursuant to LAMC Section 12.22 A.25(g)(3), a **Density Bonus/Affordable Housing Incentive Program Compliance Review** to permit the construction of a Housing Development Project totaling 15 units, reserving 2 units for Very Low Income and 1 unit for Low Income Household occupancy for a period of 55 years, with the following requested **three (3) On- and Off-Menu Incentives**:
  - a. A Floor Area Ratio of 3.99:1 in lieu of 3:1 as otherwise permitted in the R3-1 Zone (On-Menu).
  - b. A reduction in parking to allow 14 parking spaces in lieu of 21 parking spaces otherwise required by LAMC Section 12.21 A.4 (Off-Menu).
  - c. To allow 12 tandem parking spaces in lieu of the 1 individually accessible stall per dwelling unit required by LAMC Section 12.21 A.5(h) (Off-Menu).

3. **Approve**, pursuant to LAMC Section 12.22 A.25(g)(3), the following **four (4) Waivers of Development Standards**:
  - a. A height increase to 67 feet 8 inches in lieu of the 45 feet otherwise allowed by the R3-1 Zone.
  - b. A 7.2-foot northerly side yard setback in lieu of the 9 feet otherwise required by the R3-1 Zone.
  - c. A 7.2-foot southerly side yard setback in lieu of the 9 feet otherwise required by the R3-1 Zone.
  - d. To allow 6 compact and 8 standard parking spaces in lieu of the 1 standard parking stall per dwelling unit minimum required by LAMC Section 12.21 A.5(c).
4. **Approve**, pursuant to LAMC 12.24 U.26, a **Conditional Use Permit** for a 62.5 percent increase in density over the Project site, in lieu of the otherwise permitted 35 percent increase in density allowable under LAMC Section 12.22 A.25.
5. **Adopt** the attached Findings.

VINCENT P. BERTONI, AICP  
Director of Planning

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**ADVICE TO PUBLIC:** \*The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Written communications may be mailed to the *Commission Secretariat, Room 273, City Hall, 200 North Spring Street, Los Angeles, CA 90012* (Phone No. 213-978-1300). While all written communications are given to the Commission for consideration, the initial packets are sent to the week prior to the Commission's meeting date. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters, assistive listening devices, or other auxiliary aids and/or other services may be provided upon request. To ensure availability of services, please make your request not later than three working days (72 hours) prior to the meeting by calling the Commission Secretariat at (213) 978-1300.

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## PROJECT ANALYSIS

### **PROJECT SUMMARY**

The proposed project is the construction of a new six-story, 67-foot 8-inch tall residential building comprised of 15 dwelling units (including 2 Very Low Income and 1 Low Income units). The project will be approximately 19,047 square feet in floor area with a Floor Area Ratio ("FAR") of 3.99:1.

The primary pedestrian entrance is located along Mitchell Avenue that will provide direct stairway access to the 1 studio unit and 14 two-bedroom units on the upper floors. A secondary pedestrian entrance is through a gate along the southerly side yard that will provide access to the lobby and mailboxes in the enclosed garage. Residential amenities are provided in the form of a roof deck and private balconies.

The project will provide 14 parking spaces in an at-grade parking level that is accessed from both Mitchell Avenue and the rear alley. Out of the 14 parking spaces proposed, 6 will be compact and 12 will be in tandem. The project will also provide 31 long-term and 18 short-term bicycle parking spaces.

The site is currently improved with a two-story single-family dwelling and triplex with a total of four (4) dwelling units which will be demolished for the project. No existing significant trees will be removed. The project will also involve minimal grading of less than 500 cubic yards of soil.

### **BACKGROUND**

#### Subject Property

The project site is located mid-block on the northern side of Mitchell Avenue between Zanja Street and Wade Street. The project site is a relatively flat, rectangular, single lot that is approximately 7,100 square feet of lot area, with approximately 50 feet of frontage along the north side of Mitchell Avenue, a lot depth of approximately 143 feet, and a 15-foot wide alley to the rear. The site is currently improved with a two-story single-family dwelling and triplex. The project site is located within 5.24 kilometers (3.25 miles) of the Santa Monica Fault however it is not located within the Alquist-Priolo Fault Zone, Liquefaction Zone, Landslide Area, Methane Zone, Very High Fire Severity Zone, or Special Grading Area.

#### Zoning and Land Use Designation

The project site is in the Palms – Mar Vista – Del Rey Community Plan, and is designated for Medium Residential land uses, with corresponding zones of R3 and R3(PV). The site is zoned R3-1 and is therefore consistent with the land use designation. The project site is in the Los Angeles Coastal Transportation Corridor Specific Plan ("LACTC", Ordinance Nos. 186,104 and 186,105), and the Project is subject to Department of Transportation clearance of the LACTC. Height District No. 1 limits the Floor Area Ratio ("FAR") to 3:1 and building height to 45 feet with no limit on the number of stories. There is a 15-foot Building Line along Mitchell Avenue established under Ordinance No. 138533.

#### Surrounding Uses

The subject site is in an urbanized area surrounded primarily by multi-family residential uses and some single-family dwellings. Properties immediately adjacent to the site are developed primarily with two- and three-story multi-family residential uses and are similarly zoned R3-1, and surrounding properties are developed or proposed for multi-family buildings up to five stories tall.

Other parcels further south fronting Washington Boulevard are zoned C2-1 and developed with commercial uses including a liquor store, laundromat, as well as a schoolhouse; further southwest is the city boundary for Culver City.

#### Streets and Circulation

Mitchell Avenue, abutting the property to the south, is designated by the Mobility Plan as a Local Street - Standard, with a designated right-of-way width of 60 feet and roadway width of 36 feet, and is currently dedicated to a 60-foot right-of-way width and approximately 40 foot roadway width, with a curb, gutter, sidewalk, and parkway.

#### Public Transit

The subject site is within 500 feet of several bus stops served by the Santa Monica Big Blue Bus 16 line, and Culver CityBus 1 and 2 lines.

#### Relevant Cases and Building Permits

##### Subject Site:

Building Permit No. 21010-10000-02704: On June 1, 2021, a Building Permit application was submitted for a new 5-story, 10-unit (2 Very Low Income), affordable housing apartment to include 4 story Type VA apartment over 1 story Type 1A parking garage. The permit application is pending and the permit was not issued at the time of preparing this report.

##### Surrounding Sites:

The following relevant cases were identified to be within 500 feet of the subject site:

Case No. ZA-2021-9385-ZV-DB-VHCA: On November 21, 2021, an application was filed for a Density Bonus, Conditional Use, and Vesting Tentative Tract Map for a five-story, 40-unit residential development, for a project located at 3984 South Meier Street. The application is pending and was not issued at the time of preparing this report.

Case No. ADM-2021-9120-DB: On November 3, 2021, an application was filed for the construction, use, and maintenance of a new 16,948 square-foot five-story apartment building, for a project located at 12759 West Caswell Avenue. The application is pending and was not issued at the time of preparing this report.

Case No. DIR-2013-2447-DB: On November 12, 2013, the Director of Planning approved a Density Bonus Affordable Housing Incentive Program Review, for a four-story, 45-foot tall residential building comprised of 11 dwelling units, with Density Bonus On-Menu Incentives for: 1) reduced side yard to 5 feet 8 inches and 2) reduced open space by 20 percent, for a project located at 12770 West Caswell Avenue. The project was subsequently appealed by an aggrieved party, which was denied by City Planning Commission on February 21, 2014. The Certificate of Occupancy was issued by the Department of Building and Safety on November 28, 2017.

**REQUESTED ACTIONS****Density Bonus / Affordable Housing Incentives Program**

In accordance with California State Law (including Senate Bill 1818, and Assembly Bills 2280, 2222, and 2556), the applicant is proposing to utilize LAMC Section 12.22 A.25 (Affordable Housing Incentives – Density Bonus) to set aside a minimum of 2 dwelling units for Very Low Income household occupancy for a period of 55 years. Because the applicant is providing 22 percent (2 units) of base dwelling units (9 units) to be affordable for Very Low Income household occupancy, the project is eligible for three (3) Density Bonus Incentives.

***On- and Off-Menu Incentives***

As a result of setting aside 22 percent (2 dwelling units) of the base 9 dwelling units as Restricted Affordable Units for Very Low Income Households, the applicant requests three (3) On- and Off-Menu Density Bonus Incentives, as follows:

- a. A Floor Area Ratio of 3.99:1 in lieu of 3:1 as otherwise permitted in the R3-1 Zone (On-Menu).
- b. A reduction in parking to allow 14 parking spaces in lieu of 21 parking spaces otherwise required by LAMC Section 12.21 A.4 (Off-Menu).
- c. To allow 12 tandem spaces in lieu of the 1 individually accessible stall per dwelling unit required by LAMC Section 12.21 A.5(h) (Off-Menu).

***Waivers of Development Standards***

As mentioned above, a project that provides 22 percent of its base units for Very Low Income Households qualifies for three (3) Incentives, but may request other “waiver[s] or reduction[s] of development standards that will have the effect of physically precluding the construction of a development meeting the [affordable set-aside percentage] criteria of subdivision (b) at the densities or with the concessions or incentives permitted under [State Density Bonus Law]” (Government Code Section 65915(e)(1)), in conjunction with a Density Bonus Project. Given that the project is utilizing all three (3) Density Bonus Incentives, the applicant requests four (4) Waivers of Development Standards, as follows:

- a. A height increase to 67 feet 8 inches in lieu of the 45 feet otherwise allowed by the R3-1 Zone.
- b. A 7.2-foot northerly side yard setback in lieu of the 9 feet otherwise required by the R3-1 Zone.
- c. A 7.2-foot southerly side yard setback in lieu of the 9 feet otherwise required by the R3-1 Zone.
- d. To allow 6 compact and 8 standard parking spaces in lieu of the 1 standard parking stall per dwelling unit minimum required by LAMC Section 12.21 A.5(c).

***Housing Replacement***

On October 9, 2019, the Governor signed into law the Housing Crisis Act of 2019 (SB 330). SB 330 creates new state laws regarding the production, preservation and planning for housing, and establishes a statewide housing emergency until January 1, 2025. During the duration of the statewide housing emergency, SB 330, among other things, creates new housing replacement requirements for Housing Development Projects by prohibiting the approval of any proposed

housing development project on a site that will require the demolition of existing residential dwelling units or occupied or vacant "Protected Units" unless the proposed housing development project replaces those units. The Los Angeles Housing Department (LAHD) has determined, per the Housing Crisis Act of 2019 (SB 330) Determination, dated September 1, 2021, that three (3) residential units need to be replaced with equivalent type, with two (2) units restricted to Very Low Income Households and one (1) unit restricted to Low Income Households (Exhibit C). The LAHD housing replacement requirements are partially satisfied by two (2) Very Low Income units provided through this Density Bonus Affordable Housing Incentives Program. In addition to the set aside units pursuant to Density Bonus, the applicant must provide one (1) additional unit affordable to Low Income Households to comply with the Determination made by the LAHD. This is reflected in the Conditions of Approval.

#### Conditional Use

The City's Density Bonus Ordinance (Ordinance No. 179,581), codified in LAMC Section 12.22 A.25, permits a maximum density increase of up to 35 percent in exchange for setting aside 11 percent of the base density units for Very Low Income Households in accordance with the State Density Bonus Law (Government Code Section 65915). The State Density Bonus Law (Government Code Section 65915(n)) also allows a city to grant a density bonus greater than 35 percent for a development, if permitted by a local ordinance. The City adopted the Value Capture Ordinance (Ordinance No. 185,373), codified in LAMC Section 12.24 U.26, to permit a density increase greater than 35 percent with the approval of a Conditional Use. In exchange for the increased density, the Value Capture Ordinance requires projects to set aside one (1) additional percent of base density units above the 11 percent for Very Low Income Households for every additional 2.5 percent density increase above the 35 percent.

Below is a table showing the requisite percentage of affordable housing units for Very Low Income Households based on the percentage of density increase.

Percentage of Base Density to be Restricted to Very Low Income Households	Percentage of Density Increase Granted
11	35
12	37.5
13	40
14	42.5
15	45
16	47.5
17	50
18	52.5
19	55
20	57.5
21	60
<b>22</b>	<b>62.5</b>

The project site is zoned R3-1, which allows a base density of 9 units on the subject property. The Density Bonus Ordinance allows a density bonus of up to 35 percent in exchange for setting aside 11 percent of the 9 base density units for Very Low Income Households. With the Density Bonus Ordinance, the project would be permitted a total of 13 units on site in exchange for setting aside two (2) units for Very Low Income Households.

The applicant requests a Conditional Use for a density increase in excess of 35 percent pursuant to LAMC Section 12.24 U.26, to allow a 62.5 percent increase in density for a total of 15 dwelling units in lieu of 9 base density dwelling units as otherwise permitted by-right in the R3-1 Zone. In accordance with LAMC Section 12.22 A.25(c)(7), in calculating Density Bonus and Restricted Affordable units any number resulting in a fraction shall be rounded up to the next whole number. As provided in the table above, the applicant is required to set aside 22 percent, or 2 units, of the base 9 density units for Very Low Income Households in order to be granted a 62.5 percent density bonus. The applicant proposes to set aside 2 units for Very Low Income Households for a period of 55 years, which is 22 percent of the base 9 density units. As such, the project satisfies the minimum percentage of base density to be restricted to Very Low Income Households to be eligible for a 62.5 percent density increase.

## **ISSUES**

### Technical Clarification

The Hearing Notice listed existing uses as a two-story apartment, however records with the Los Angeles Housing Department ("LAHD") and Department of Building and Safety ("LADBS") indicate the site is currently improved with a two-story single-family dwelling and triplex.

The Hearing Notice listed a 67 percent increase in density, however the applicant is requesting a 62.5 percent increase in density. The 62.5 percent density increase is lower than the 67 percent originally listed on the Hearing Notice, and does not result in an increase in dwelling units or a change to the amount of restricted affordable units.

The technical clarifications are minor and do not result in any changes to the project plans.

### Parking / Traffic

Staff received several comment letters and public comments at the hearing expressing concerns regarding the parking requests, and the impacts of construction on street parking and circulation.

The applicant requested an Off-Menu Incentive to allow 14 parking spaces in lieu of the 21 spaces, an Off-Menu Incentive to allow 12 tandem parking spaces in lieu of the 1 individually accessible stall per dwelling unit, and a Waiver of Development Standard to allow 6 compact and 8 standard parking spaces in lieu of the 1 standard parking stall per dwelling unit minimum required by LAMC. The reduction in parking and allowance for tandem and compact parking are allowed through Density Bonus and justified under Finding No. 1.

Parking will be unbundled as conditioned, and the applicant has stated that tandem spaces will be leased jointly to the two-bedroom units. All parking spaces are provided at-grade, and the parking garage will be screened from Mitchell Avenue with cedar wood posts and buffered with landscaping to minimize the view of parking. The project will not increase the number of driveways. The project will also provide 31 long-term and 18 short-term bicycle parking spaces, which exceeds LAMC Section 12.21 A.16 bicycle parking requirements in order to utilize the bicycle parking replacement provision of LAMC Section 12.21 A.4.

The Department of Transportation (LADOT) Referral Form dated June 2, 2022 and the Vehicle Miles Traveled (VMT) calculator indicated that the number of daily vehicle trips will be 72 which is under the threshold of 250 or more daily vehicles trips to require VMT analysis. Therefore, the project does not exceed the threshold criteria established by LADOT for preparing a traffic study and will not have any significant impacts related to traffic.

### Height / Massing

The subject site is zoned R3-1, with a Height District No. 1 that permits a maximum FAR of 3:1. The applicant has requested an FAR of 3.99:1 in lieu of the maximum 3:1 through an On-Menu Density Bonus Incentive, for a maximum floor area of 19,047 square feet. While the height of the project is taller than the existing multi-family buildings immediately adjacent to the site, the increase in height is granted through the Density Bonus Ordinance. In addition, the project height will be compatible with surrounding properties that are developed or proposed with multi-family buildings up to five stories tall.

The site's zone permits a maximum height of 45 feet with no limit on the number of stories for residential projects. The applicant has requested an increase in height of 22 feet 8 inches to allow for 67 feet 8 inches through a Waiver of Development Standard per LAMC Section 12.22 A.25. The request for an additional 22 feet 8 inches is needed to construct the number of units that the requested Density Bonus allows.

The project will comply with the 15-foot building line, and there is no transitional height requirement for the project.

### Urban Design Studio

The proposed project was reviewed by the Department of City Planning's Urban Design Studio (UDS) on February 9, 2022. The resulting comments and suggestions focus primarily on the pedestrian experience, 360-degree design, and climate adaptive design. The following includes a discussion of UDS comments and suggestions and the applicant's response.

#### *Pedestrian-First*

- Consider providing a lobby or secure place to receive packages.
- Consider alternative parking layouts such as two-vehicle stackers or automated parking stackers, or provide opportunities for future installation of such systems.

In response to UDS comments, the applicant designated a lobby space in the enclosed parking garage in front of the elevator. The lobby is accessed through a side gate along the southerly yard, and will include unit mailboxes for residents to receive packages. The applicant responded that providing a traditional lobby at the front of the building will require the elevator shaft be shifted closer to the front façade which would jeopardize the front-facing units and balconies, and would add massing to the front façade and be less compatible than the existing design.

The applicant indicated that parking is unbundled, and tandem spaces will be leased jointly to the 2-bedroom units, with the exception of the van accessible and standard accessible stalls that can be leased singularly. The applicant indicated that installing vehicle stackers is not physically feasible due to the physical constraints of the ground floor garage height and the need for structural support beams.

#### *360-Degree Design*

- Consider screening the proposed wall openings for the parking garage.

The applicant introduced cedar wood posts to screen the parking garage openings facing Mitchell Avenue to minimize views of the parking from the street. The cedar wood posts are also applied on the ground floor façade adjacent to the stairway to help create a more identifiable front entrance for the building.

*Climate-Adapted*

- Verify EV charging requirements such as accessible van space, accessible standard space, and side charger access with the Department of Building and Safety.
- Verify tree spacing requirements from the driveway apron and water/gas meters/vaults with Urban Forestry.
- Consider providing a second tree at the roof deck.
- Show areas reserved for future solar installation on the roof, or verify compliance with Title 24 alternatives.
- Verify tolerance of proposed plants (aspidistra elatior and philodendron 'Xanadu') for direct sunlight.
- Verify LID compliance.

The applicant updated the plans to show five (5) EV charging spaces including a fully equipped van accessible EV parking stall at the rear of the garage and future EV standard accessible parking stall at the front of the garage. The applicant indicated that EV charger access is located as close to the front of each parking stall as feasible given the spatial constraints of the floor plan.

The applicant updated the plans to show a total of four (4) trees including two (2) on the rooftop, one (1) on the front yard, and one (1) street tree; street tree spacing and selection is subject to further review by the Urban Forestry Division. The plans were also updated to show 255 square feet of rooftop solar panels, new shrub species (dasyliron and sansevieria), and LID compliance.

**CONCLUSION**

Based on the information submitted to the record, and the surrounding uses and zones, staff recommends that the City Planning Commission approve the project, as recommended, subject to the Conditions of Approval. The project will redevelop an underutilized site with a new multi-family residential project resulting in a net increase of 11 dwelling units, including 2 Very Low Income and 1 Low Income units.

## CONDITIONS OF APPROVAL

### Density Bonus Conditions

1. **Site Development.** Except as modified herein, the project shall be in substantial conformance with the plans and materials submitted by the Applicant, stamped "Exhibit A," and attached to the subject case file. No change to the plans will be made without prior review by the Department of City Planning, West/South/Coastal Project Planning Division, and written approval by the Director of Planning. Each change shall be identified and justified in writing. Minor deviations may be allowed in order to comply with the provisions of the Los Angeles Municipal Code or the project conditions.
2. **Residential Density.** The project shall be limited to a maximum density of 15 residential units including Density Bonus Units.
3. **Affordable Units.** A minimum of two (2) units, that is 22 percent of the base 9 dwelling units, shall be reserved as affordable units for Very Low Income household occupancy, as defined by the State Density Bonus Law 65915 (c)(1) or (c)(2). An additional one (1) unit shall be reserved as an affordable unit for Low Income household occupancy, and to meet the requirements of SB 330 (Government Code section 65915(c)(3)). The Density Bonus Affordable Housing Incentive Program Guidelines also requires a Housing Development to meet any applicable housing replacement requirements of California Government Code Section 65915(c)(3), as verified by the Los Angeles Housing Department (LAHD) prior to the issuance of any building permit. Replacement housing units required per this section may also count towards other On-Site Restricted Affordable Units requirements.
4. **Changes in Restricted Units.** Deviations that change the composition of units shall be consistent with LAMC Section 12.22 A.25 (9a-d) and State Density Bonus Law (Government Code Section 65915).
5. **Housing Requirements.** Prior to issuance of a building permit, the owner shall execute a covenant to the satisfaction of the Los Angeles Housing Department (LAHD). The covenant shall bind the owner to reserve two (2) units available to Very Low Income Households and one (1) unit available to Low Income Households, for sale or rental as determined to be affordable to such households by LAHD for a period of 55 years. Enforcement of the terms of said covenant shall be the responsibility of LAHD. The Applicant will present a copy of the recorded covenant to the Department of City Planning for inclusion in this file. The project shall comply with the Guidelines for the Affordable Housing Incentives Program adopted by the City Planning Commission and any monitoring requirements established by the LAHD. Refer to the Density Bonus Legislation Background and Housing Replacement (SB 330 Determination) Background sections of this determination.
6. **Floor Area Ratio (FAR) (Incentive).** The project shall be limited to a maximum floor area ratio of 3.99:1 per Exhibit "A".
7. **Automobile Parking (Incentive).** The project shall provide a minimum of 14 parking spaces, as shown in Exhibit "A".
8. **Tandem Parking (Incentive).** The project shall be limited to a maximum of 12 tandem parking spaces, as shown in Exhibit "A".
9. **Compact Parking (Waiver).** The project shall be limited to a maximum of 6 compact parking spaces, as shown in Exhibit "A".

10. **Height (Waiver).** The project shall be limited to 67 feet 8 inches in building height per Exhibit "A".
11. **Side Setbacks (Waiver).** The northerly and southerly side yard setbacks shall be no less than 7.2 feet, as shown in Exhibit "A".
12. **Bicycle Parking.** Bicycle parking shall be provided consistent with LAMC 12.21 A.16. The project shall provide a minimum of 31 long term and 18 short term bicycle parking spaces total, as shown in Exhibit "A".

### **Conditional Use Conditions**

13. **Street Improvements.**
  - a. Dedication Required on Alley (North of Mitchell Avenue) – A 2.5-foot wide strip of land along alley frontage to complete a 10-foot wide half alley right-of-way.
  - b. Improvements Required on Mitchell Avenue – Remove the existing improvements and reconstruct a new minimum 5-foot wide concrete sidewalk, integral concrete curb and 2-foot gutter along the property frontage. Repair all broken, off-grade asphalt concrete pavement and construct new driveway to comply with ADA requirements.
  - c. Improvements Required on Alley – Reconstruct the 17.5-foot alley with asphalt pavement adjoining the new property line and reconstruct the 2-foot wide longitudinal concrete gutter. These improvements should suitably transition to join the existing improvements.
14. **Fire.** Submit plot plans for Fire Department approval and review prior to issuance of building permits.
15. **Mechanical Equipment.** All exterior mechanical equipment, including heating, ventilation and air conditioning (HVAC) equipment, satellite dishes, and cellular antennas, shall be screened from public view through the use of architectural elements such as parapets.
16. **Lighting.** All outdoor and parking lighting shall be shielded and down-cast within the site in a manner that prevents the illumination of adjacent public rights-of-way, adjacent properties, and the night sky (unless otherwise required by the Federal Aviation Administration (FAA) or for other public safety purposes).
17. **Lighting Design.** Areas where nighttime uses are located shall be maintained to provide sufficient illumination of the immediate environment so as to render objects or persons clearly visible for the safety of the public and emergency response personnel. All pedestrian walkways, storefront entrances, and vehicular access ways shall be illuminated with lighting fixtures. Lighting fixtures shall be harmonious with the building design. Wall mounted lighting fixtures to accent and complement architectural details at night shall be installed on the building to provide illumination to pedestrians and motorists.
18. **Heat Island Effect.** To reduce the heat island effect, a minimum of 50 percent of the area of pathways, patios, driveways or other paved areas shall use materials with a minimum initial Solar Reflectance value of 0.35 in accordance with ASTM (American Society of Testing Materials) standards.

**19. Electric Vehicle Parking.** All electric vehicle charging spaces (EV Spaces) and electric vehicle charging stations (EVCS) shall comply with the regulations outlined in Sections 99.04.106 and 99.05.106 of Article 9, Chapter IX of the LAMC.

**20. Unbundled Parking.** Residential parking shall be unbundled from the cost of the rental units, with the exception of parking for Restricted Affordable Units.

**21. Landscape Plan.** Revised landscape plans shall be submitted to show the size and location of all plants. The landscape plan shall indicate landscape points for the Project equivalent to 10% more than otherwise required by LAMC 12.40 and Landscape Ordinance Guidelines "O". All open areas not used for buildings, driveways, parking areas, recreational facilities or walks shall be landscaped, including an automatic irrigation system, and maintained in accordance with a final landscape plan prepared by a licensed landscape architect or licensed architect, and submitted for approval to the Department of City Planning. The final landscape plan shall be in substantial conformance with the submitted Landscape Plan, Exhibit "A," and shall incorporate any modifications required as a result of this grant.

**22. Soil Depths.** Shrubs, perennials, and groundcover shall require a minimum soil depth as follows:

- a. A minimum depth with a height ranging from 15 to 40 feet shall be 42 inches.
- b. A minimum depth with a height ranging from 1 to 15 feet shall be 24 to 36 inches.
- c. A minimum depth with a height of less than 1 foot shall be 18 inches.
- d. A minimum depth of an extensive green roof shall be 3 inches.

Trees shall require a 42-inch minimum soil depth.

Further, the minimum amount of soil volume for tree wells on the rooftop or any above grade open spaces shall be based on the size of the tree at maturity:

- e. 220 cubic feet for trees with a canopy diameter ranging from 15 to 19 feet.
- f. 400 cubic feet for trees with a canopy diameter ranging from 20 to 24 feet.
- g. 620 cubic feet for trees with a canopy diameter ranging from 25 to 29 feet.
- h. 900 cubic feet for trees with a canopy diameter ranging from 30 to 34 feet.

## **23. Street Trees.**

a. **Street Trees.** Street trees shall be provided to the satisfaction of the Urban Forestry Division. Street trees may be used to satisfy on-site tree requirements pursuant to LAMC Article Section 12.21.G.3 (Chapter 1, Open Space Requirement for Six or More Residential Units). Per Exhibit A and 12.21.G.3, one (1) Street tree shall be provided.

b. **Required Trees per 12.21 G.2.** As conditioned herein, a final submitted landscape plan shall be reviewed to be in substantial conformance with Exhibit "A." There shall be a minimum of three (3) 24-inch box, or larger, trees on site pursuant to LAMC Section 12.21 G.2. Any required trees pursuant to LAMC Section 12.21 G.2 shown in the public right of way in Exhibit "A" shall be preliminarily reviewed and approved by the Urban Forestry Division prior to building permit issuance. In-lieu fees pursuant to LAMC Section 62.177 shall be paid if placement of required trees in the public right of way is proven to be infeasible due to City determined physical constraints.

c. Project shall preserve all healthy mature street trees whenever possible. All feasible alternatives in project design should be considered and implemented to retain healthy mature street trees. A permit is required for the removal of any street tree and shall be replaced 2:1 as approved by the Board of Public Works and Urban Forestry Division.

d. Plant street trees at all feasible planting locations within dedicated streets as directed and required by the Bureau of Street Services, Urban Forestry Division. All tree

plantings shall be installed to current tree planting standards when the City has previously been paid for tree plantings. The subdivider or contractor shall notify the Urban Forestry Division at: (213) 847-3077 upon completion of construction for tree planting direction and instructions.

24. **Stormwater/irrigation.** The project shall implement on-site stormwater infiltration as feasible based on the site soils conditions, the geotechnical recommendations, and the City of Los Angeles Department of Building and Safety Guidelines for Storm Water Infiltration. If on-site infiltration is deemed infeasible, the project shall analyze the potential for stormwater capture and reuse for irrigation purposes based on the City Low Impact Development (LID) guidelines.
25. **Solar and Electric Generator.** Generators used during the construction process shall be electric or solar powered. Solar generator and electric generator equipment shall be located as far away from sensitive uses as feasible.
26. **Solar-ready Buildings.** The Project shall comply with the Los Angeles Municipal Green Building Code, Section 99.05.211, to the satisfaction of the Department of Building and Safety.
27. **Signage.** There shall be no off-site commercial signage on construction fencing during construction.

#### **Administrative Conditions**

28. **Final Plans.** Prior to the issuance of any building permits for the project by the Department of Building and Safety, the applicant shall submit all final construction plans that are awaiting issuance of a building permit by the Department of Building and Safety for final review and approval by the Department of City Planning. All plans that are awaiting issuance of a building permit by the Department of Building and Safety shall be stamped by Department of City Planning staff "Plans Approved". A copy of the Plans Approved, supplied by the applicant, shall be retained in the subject case file.
29. **Notations on Plans.** Plans submitted to the Department of Building and Safety, for the purpose of processing a building permit application shall include all of the Conditions of Approval herein attached as a cover sheet and shall include any modifications or notations required herein.
30. **Approval, Verification and Submittals.** Copies of any approvals, guarantees or verification of consultations, review of approval, plans, etc., as may be required by the subject conditions, shall be provided to the Department of City Planning prior to clearance of any building permits, for placement in the subject file.
31. **Code Compliance.** Use, area, height, and yard regulations of the zone classification of the subject property shall be complied with, except where granted conditions differ herein.
32. **Department of Building and Safety.** The granting of this determination by the Director of Planning does not in any way indicate full compliance with applicable provisions of the Los Angeles Municipal Code Chapter IX (Building Code). Any corrections and/or modifications to plans made subsequent to this determination by a Department of Building and Safety Plan Check Engineer that affect any part of the exterior design or appearance of the project as approved by the Director, and which are deemed necessary by the Department of Building and Safety for Building Code compliance, shall require a referral of the revised plans back to the Department of City Planning for additional review and sign-off prior to the issuance of any permit in connection with those plans.

**33. Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning.

**34. Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assign. The agreement must be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the file.

**35. Indemnification and Reimbursement of Litigation Costs.**

Applicant shall do all of the following:

- (i) Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
- (ii) Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
- (iii) Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- (iv) Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
- (v) If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the

right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

“City” shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

“Action” shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions includes actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

## FINDINGS

### **DENSITY BONUS/AFFORDABLE HOUSING INCENTIVES COMPLIANCE FINDINGS**

1. **Government Code Section 65915 and LAMC Section 12.22 A.25 state that the Commission shall approve a density bonus and requested incentive(s) unless the Commission finds that:**
  - a. **The incentives do not result in identifiable and actual cost reductions to provide for affordable housing costs as defined in California Health and Safety Code Section 50052.5 or Section 50053 for rents for the affordable units.**

The record does not contain substantial evidence that would allow the City Planning Commission to make a finding that the requested on- and off-menu incentives do not result in actual and identifiable cost reductions to provide for affordable housing costs per State Law. The California Health & Safety Code Sections 50052.5 and 50053 define formulas for calculating affordable housing costs for Very Low, Low, and Moderate Income Households. Section 50052.5 addresses owner-occupied housing and Section 50053 addresses rental households. Affordable housing costs are a calculation of residential rent or ownership pricing not to exceed 25 percent gross income based on area median income thresholds dependent on affordability levels.

Based on the set-aside of 22 percent of base units for Very Low Income households, the applicant is entitled to three (3) Incentives under both the Government Code and LAMC. Therefore, the three (3) On- and Off-Menu requests qualify as the proposed development's Incentives. The remaining requests must be processed as Waivers of Development Standards.

**FAR:** The subject site is zoned R3-1, with a Height District No. 1 that permits a maximum Floor Area Ratio ("FAR") of 3:1. The FAR Increase incentive permits a percentage increase in the allowable Floor Area Ratio equal to the percentage of Density Bonus for which the Housing Development Project is eligible, not to exceed 35 percent. For this project site, an increase of 35 percent over the 3:1 FAR allows an FAR of 4.05:1 or 19,350 square feet. The applicant has requested an FAR of 3.99:1 in lieu of the maximum 3:1 through an On-Menu Density Bonus Incentive, for a maximum floor area of 19,047 square feet. The additional floor area is requested to accommodate larger sized units, including two-bedroom units. The project includes 1 studio unit and 14 two-bedroom units. The requested increase in FAR will allow approximately 4,713 square feet of additional floor area and will enable the construction of affordable units. As set forth on Sheet A0.27 of the project plans, the project's upper residential levels (Levels 5 and 6) would each have floor plates between 3,605 and 3,617 square feet with 3 units at each level. These larger floor plates would not be achievable under the 3:1 base FAR and enable the project to construct the unit mix above. Without the incentive to permit additional floor area, the project would need to remove the uppermost floor containing 3 units, or the average unit size and bedroom count would have to be significantly smaller to construct the number of units that the requested density bonus allows. The ability to develop larger units will increase the revenues from the market-rate units, which will lower the marginal cost of developing the affordable units. The additional floor area will allow certain fixed costs involved in the construction of new residential units to be spread over more floor area thereby reducing the per square foot build cost of the development. The requested incentive will allow the developer to expand the building envelope so the additional units can be constructed, and the overall space dedicated to residential uses

is increased. Therefore, the FAR incentive will result in identifiable and actual cost reductions to provide for affordable housing costs.

FAR by-right	Buildable Lot Area (sf)	Base Floor Area (sf)
3.0:1	4,778	4,778x 3 = <b>14,334</b>

FAR Requested	Requested Floor Area (sf)	Additional Floor Area (sf)
3.99:1	4,778x3.99=19,047	19,047 - 14,334 = <b>4,713</b>

**Parking Reduction:** The applicant requested an Off-Menu Incentive to allow 14 parking spaces in lieu of the 21 spaces required by LAMC Section 12.21 A.4. LAMC Section 12.21 A.4 requires one parking space for each dwelling unit of less than three habitable rooms, one and one-half parking spaces for each dwelling unit of three habitable rooms, and two parking spaces for each dwelling unit of more than three habitable rooms, except that 30 percent of the required automobile parking may be replaced by bicycle parking. The project provides 1 studio unit and 14 two-bedroom units and is therefore required to provide 28 parking spaces, 30 percent of which may be replaced with bicycle parking, for a total of 21 vehicular parking spaces required. The Off-Menu Incentive will allow the developer to expand the Project's building envelope so that the residential units being constructed are of sufficient size, configuration, and quality. Compliance with the requirements of LAMC Section 12.21 A.4 would require the removal of a significant amount of floor area that could otherwise be dedicated to the number, configuration, and livability of affordable housing units. If the project were to expand its parking area by building an additional parking level below grade, it would add a cost of approximately \$670,500 to the project. Similarly, if the project was to construct parking above grade to accommodate the required parking pursuant to LAMC Section 12.21 A.4, it would result in the loss of units or increase the height of the building and also result in financial infeasibility. Therefore, the parking reduction will result in identifiable and actual cost reductions to provide for affordable housing costs.

**Tandem Parking:** The applicant requested an Off-Menu Incentive to allow 12 tandem parking spaces in lieu of the 1 individually accessible stall per dwelling unit required by LAMC Section 12.21 A.5(h). LAMC Section LAMC Section 12.21 A.5(h) requires that each required parking stall within a parking area or garage shall be accessible, except that they may be parked in tandem where the tandem is not more than two cars in depth and is not in parking areas for recreational vehicles or guest parking. The project provides 1 studio unit and 14 two-bedroom units and is therefore required to provide a total of 21 standard non-tandem parking spaces. The Off-Menu Incentive will allow the developer to expand the Project's building envelope so that the residential units being constructed are of sufficient size, configuration, and quality. Compliance with the requirements of LAMC Section LAMC Section 12.21 A.5(h) would require the removal of a significant amount of floor area that could otherwise be dedicated to the number, configuration, and livability of affordable housing units. If the project were to expand its parking area by building an additional parking level below grade, it would add a cost of approximately \$670,500 to the project. Similarly, if the project was to construct parking above grade to accommodate the required parking pursuant to LAMC Section LAMC Section 12.21 A.5(h), it would result in the loss of units or increase the height of the building and also result in financial infeasibility. Therefore, the increased tandem parking will result in identifiable and actual cost reductions to provide for affordable housing costs.

- b. The Incentive will have specific adverse impact upon public health and safety or on any real property that is listed in the California Register of Historical Resources

**and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact without rendering the development unaffordable to Very Low, Low and Moderate Income households. Inconsistency with the zoning ordinance or the general plan land use designation shall not constitute a specific, adverse impact upon the public health or safety (Government Code Section 65915(d)(1)(B) and 65589.5(d)).**

There is no evidence in the record that the proposed density bonus incentive(s) will have a specific adverse impact. A “specific adverse impact” is defined as, “a significant, quantifiable, direct and unavoidable impact, based on objective, identified written public health or safety standards, policies, or conditions as they existed on the date the application was deemed complete” (LAMC Section 12.22.A.25(b)).

The facade of the proposed building which faces Mitchell Avenue is articulated in multiple ways, creating a visually interesting elevation that invites interaction with the street. The structure will also be oriented toward the street with the stairway entrance, windows, and architectural features on street-facing elevations as required. The project does not involve a contributing structure in a designated Historic Preservation Overlay Zone or on the City of Los Angeles list of Historical-Cultural Monuments. The project is not located on a substandard street in a Hillside area or a Very High Fire Hazard Severity Zone. There is no evidence in the record which identifies a written objective health and safety standard that has been exceeded or violated. Based on the above, there is no basis to deny the requested incentives. Therefore, there is no substantial evidence that the project’s proposed incentives will have a specific adverse impact on public health and safety, or on property listed in the California Register of Historic Resources.

**c. The incentives are contrary to state or federal laws.**

There is no evidence in the record that the proposed incentives are contrary to state or federal law.

Following is a delineation of the findings related to the request for three (3) Waivers of Development Standards, pursuant to Government Code Section 65915.

**2. Government Code Section 65915 and LAMC Section 12.22 A.25 state that the Commission shall approve a density bonus and requested Waiver of Development Standard(s) unless the Commission finds that:**

**a. *The waivers or reductions are contrary to state or federal laws.***

There is no evidence in the record that the proposed incentives are contrary to state or federal law.

A project that provides 22 percent of base units for Very Low Income Households qualifies for three (3) Incentives, and may request other “waiver[s] or reduction[s] of development standards that will have the effect of physically precluding the construction of a development meeting the [affordable set-aside percentage] criteria of subdivision (b) at the densities or with the concessions or incentives permitted under [State Density Bonus Law]” (Government Code Section 65915(e)(1)).

Therefore, the requests for the following are recommended as Waivers of Development Standards. Without the below Waivers, the existing development standards would preclude development of the proposed density bonus units and project amenities:

**Height:** The subject site is zoned R3-1, with a Height District No. 1 that permits a maximum height of 45 feet with no limit on the number of stories for residential projects. The applicant has requested an increase in height of 22 feet 8 inches to allow for 67 feet 8 inches through a Waiver of Development Standards per LAMC Section 12.22 A.25. The request for an additional 22 feet 8 inches is needed to construct the number of units that the requested density bonus allows. The limitation on the height would remove two (2) stories from the proposed building, resulting in a loss of six dwelling units total from the upper floors (Level 5 and Level 6). This height limitation would have the effect of physically precluding construction of a development providing 15 dwelling units, of which 2 units will be set aside for Very Low Income households and 1 unit will be set aside for Low Income households. As proposed, the additional height will allow for the construction of the affordable residential units. The requested incentive will allow the project to expand the building envelope so that additional units can be constructed, provide for design efficiencies, and allow the overall space dedicated to residential uses to be increased.

**Side Setbacks:** LAMC Section 12.10 C.2 requires a minimum 5 foot side yard, and requires one additional foot in the width of the side yard for each additional story above the 2nd story. The Project is six stories and would therefore be required to provide 9-foot side yard setbacks. The applicant has requested reduced side yard setbacks of 7.2 feet, a 20 percent reduction, through Waivers of Development Standards per LAMC 12.22 A.25. The requests for the side yard reductions are needed to construct the number of units that the requested density bonus allows. Provision of the reduced 1.8-foot side yards along the approximately 119-foot 6-inch building length accounts for an additional building area of approximately 430 square feet per floor, totaling approximately 2,580 square feet across all six floors. As shown on Sheet A0.00 of the project plans, the unit sizes range from 515 to 1,222 square feet. Without the side yard waivers, the total unit count would be reduced from 15 units to approximately 13 units. Additionally, one of the portions of the building that is proposed to be within the side yard setback area contains the elevator and stairway into the building; moving this circulation core to observe a 9 foot side yard would require reconfiguration that could cause ripples through the building design and would physically preclude the construction of the development and the affordable units. Therefore, provision of the required 9-foot side yard setbacks would physically preclude construction of the project at the permitted density and with the requested FAR incentive, resulting in a loss of approximately two residential dwelling units and 2,580 square feet of floor area.

**Compact Parking:** The applicant requested a Waiver of Development Standard to allow 6 compact and 8 standard parking spaces in lieu of the 1 standard parking stall per dwelling unit minimum required by LAMC Section 12.21 A.5(c). LAMC Section 12.21 A.5(c) allows all parking stalls in excess of one parking stall per dwelling unit may be designed as compact parking stalls to accommodate compact cars. The project proposes 14 parking spaces for 15 units, therefore all 14 parking spaces would be required to be standard stalls. Every standard parking stall provided for dwelling units shall be at least 8 feet 6 inches in width and 18 feet in length; every compact stall shall be at least 7 feet 6 inches in width and 15 feet in length. The project is physically limited from expanding its proposed parking areas to provide all 14 spaces as standard size spaces, due to the physical constraints of the 50-foot lot width, and the need to accommodate for the circulation core, yards, driveways and back-up space, and other building requirements. The Waiver will allow the developer to expand the Project's building envelope so that the residential units being constructed are of sufficient size, configuration, and quality. Compliance with the requirements of LAMC Section 12.21 A.5(c) would require the removal of a significant amount of floor area that could otherwise be dedicated to the number, configuration, and livability of affordable housing

units. If the project were to expand its parking area by building an additional parking level above grade to accommodate the required standard parking stalls pursuant to LAMC Section 12.21 A.5(c), it would result in the loss of three units on the lower residential floor (Level 3). This parking requirement would have the effect of physically precluding construction of a development providing 15 dwelling units, of which 2 units will be set aside for Very Low Income households and 1 unit will be set aside for Low Income households.

These waivers support the applicant's decision to set aside the specified number of dwelling units for Very Low or Low Income Households for 55 years.

- b. The Waiver will have specific adverse impact upon public health and safety or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse Impact without rendering the development unaffordable to Very Low, Low and Moderate Income households. Inconsistency with the zoning ordinance or the general plan land use designation shall not constitute a specific, adverse impact upon the public health or safety.*

There is no evidence in the record that the proposed density bonus Waivers will have a specific adverse impact. A "specific adverse impact" is defined as, "a significant, quantifiable, direct and unavoidable impact, based on objective, identified written public health or safety standards, policies, or conditions as they existed on the date the application was deemed complete" (LAMC Section 12.22.A.25(b)). The project does not involve a contributing structure in a designated Historic Preservation Overlay Zone or on the City of Los Angeles list of Historical-Cultural Monuments. The project is not located on a substandard street in a Hillside area or a Very High Fire Hazard Severity Zone. There is no evidence in the record which identifies a written objective health and safety standard that has been exceeded or violated. Based on the above, there is no basis to deny the requested incentives. Therefore, there is no substantial evidence that the project's proposed incentives will have a specific adverse impact on public health and safety, or on property listed in the California Register of Historic Resources.

## **CONDITIONAL USE FINDINGS**

- 3. The project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city, or region.**

The project site is zoned R3-1, which allows a base density of 9 units on the subject property. The Density Bonus Ordinance allows a density bonus of up to 35 percent in exchange for setting aside 11 percent of the 9 base density units for Very Low Income Households. With the Density Bonus Ordinance, the project would be permitted a total of 13 units on site in exchange for setting aside one (1) unit for Very Low Income Households. And with the approval of the density bonus Conditional Use Permit, the project will be permitted 15 units total.

The State Density Bonus Law (Government Code Section 65915(n)) also allows a city to grant a density bonus greater than 35 percent for a development, if permitted by a local ordinance. The City adopted the Value Capture Ordinance (Ordinance No. 185,373), codified in LAMC Section 12.24 U.26, to permit a density increase greater than 35 percent with the approval of a Conditional Use. In exchange for the increased density, the Value Capture Ordinance requires projects to set aside one (1) additional percent of base density units above the 11 percent for Very Low Income Households for every additional 2.5 percent density increase

above the 35 percent. Below is a table showing the requisite percentage of affordable housing units for Very Low Income Households based on the percentage of density increase.

Percentage of Base Density to be Restricted to Very Low Income Households	Percentage of Density Increase Granted
11	35
12	37.5
13	40
14	42.5
15	45
16	47.5
17	50
18	52.5
19	55
20	57.5
21	60
<b>22</b>	<b>62.5</b>

The applicant requests a Conditional Use for a density increase in excess of 35 percent pursuant to LAMC Section 12.24 U.26, to allow a 62.5 percent increase in density for a total of 15 dwelling units in lieu of 9 base density dwelling units as otherwise permitted by-right in the R3-1 Zone. In accordance with LAMC Section 12.22 A.25(c)(7), in calculating Density Bonus and Restricted Affordable units any number resulting in a fraction shall be rounded up to the next whole number. As provided in the table above, the applicant is required to and is proposing to set aside 22 percent, or 2 units, of the 9 base density units for Very Low Income Households in order to be granted a 62.5 percent density bonus. As such, the project satisfies the minimum percentage of base density to be restricted to Very Low Income Households to be eligible for a 62.5 percent density increase.

According to the 2021 Housing Element of the City of Los Angeles General Plan, 22 percent of total households in the City earn less than \$25,000 a year and 42 percent of all households make less than \$50,000 a year; therefore, almost half of the City's residents are in the Very Low or Low Income Categories. The City has determined that the shortage of affordable housing is an ongoing crisis in Los Angeles. The increased intensity and density of the proposed development will be offset by the project's ability to provide the number of affordable units required by the City's Density Bonus policy. Therefore, the proposed project would provide a service that is essential and beneficial to the community, city and region.

4. **The project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare, and safety.**

The proposed project is the construction of a six-story, 67-foot 8-inch tall residential building comprised of 15 dwelling units (including 2 Very Low Income and 1 Low Income units). The project will be approximately 19,047 square feet in floor area with a Floor Area Ratio ("FAR") of 3.99:1. The project will provide 14 parking spaces at-grade. The site is currently improved with a two-story single-family dwelling and triplex which will be demolished for the project. No existing significant trees will be removed. The project will also involve minimal grading of less than 500 cubic yards of soil.

The subject site is in an urbanized area surrounded primarily by multi-family residential uses and some single-family dwellings. Properties immediately adjacent to the site are developed primarily with two- and three-story multi-family residential uses and are similarly zoned R3-1, and surrounding properties are developed or proposed for multi-family buildings up to five stories tall. Other parcels further south fronting Washington Boulevard are zoned C2-1 and developed with commercial uses including a liquor store, laundromat, as well as a schoolhouse; further southwest is the city boundary for Culver City. The subject site is within 500 feet of several bus stops served by the Santa Monica Big Blue Bus 16 line, and Culver CityBus 1 and 2 lines.

The multi-family development is permitted at this location on the subject site as an allowable use by the underlying R3-1 zone. As provided under Finding Nos. 1 and 2, the project's density, FAR, parking, height, and setbacks are allowed by the underlying zone in combination with Density Bonus law.

The primary pedestrian entrance is located along Mitchell Avenue that will provide direct stairway access to the 1 studio unit and 14 two-bedroom units on the upper floors. A secondary pedestrian entrance is through a gate along the southerly side yard that will provide access to the lobby and mailboxes in the enclosed garage. Residential amenities are provided in the form of a roof deck and private balconies.

The project will provide 14 parking spaces in an at-grade parking level that is accessed from both Mitchell Avenue and the rear alley. Out of the 14 parking spaces proposed, 6 will be compact and 12 will be in tandem. The project will also provide 31 long-term and 18 short-term bicycle parking spaces.

Given the project site's proximity to public transit, the commercial corridor along Washington Boulevard, and the surrounding uses, the project's location, size, height, operations, and other significant features will be compatible with and will not adversely affect adjacent properties, the surrounding neighborhood, or the public health, welfare, and safety.

The subject site is zoned R3-1, with a Height District No. 1 that permits a maximum FAR of 3:1. The applicant has requested an FAR of 3.99:1 in lieu of the maximum 3:1 through an On-Menu Density Bonus Incentive, for a maximum floor area of 19,047 square feet. While the size of the project is larger than the existing multi-family buildings on Mitchell Avenue, the increase in FAR is consistent with the Density Bonus Ordinance.

The site's zone permits a maximum height of 45 feet with no limit on the number of stories for residential projects. The applicant has requested an increase in height of 22 feet 8 inches to allow for 67 feet 8 inches through a Waiver of Development Standard per LAMC Section 12.22 A.25. The request for an additional 22 feet 8 inches is needed to construct the number of units that the requested Density Bonus allows. The project will comply with the 15-foot building line, and there is no transitional height requirement for the project. While the height of the project is taller than the existing multi-family buildings immediately adjacent to the site, the increase in height is granted through the Density Bonus Ordinance. In addition, the project height will be compatible with surrounding properties that are developed or proposed with multi-family buildings up to five stories tall.

The applicant requested an Off-Menu Incentive to allow 14 parking spaces in lieu of the 21 spaces required by LAMC Section 12.21 A.4, an Off-Menu Incentive to allow 12 tandem parking spaces in lieu of the 1 individually accessible stall per dwelling unit required by LAMC Section 12.21 A.5(h), and a Waiver of Development Standard to allow 6 compact and 8 standard parking spaces in lieu of the 1 standard parking stall per dwelling unit minimum required by LAMC Section 12.21 A.5(c). Parking will be unbundled as conditioned, and the

applicant has stated that tandem spaces will be leased jointly to the two-bedroom units. All parking spaces are provided at-grade, and the parking garage will be screened from Mitchell Avenue with cedar wood posts and buffered with landscaping to minimize the view of parking. The project will not increase the number of driveways. The project will also provide 31 long-term and 18 short-term bicycle parking spaces, which exceeds LAMC Section 12.21 A.16 bicycle parking requirements in order to utilize the bicycle parking replacement provision of LAMC Section 12.21 A.4. The Department of Transportation (LADOT) Referral Form dated June 2, 2022 and the Vehicle Miles Traveled (VMT) calculator indicated that the number of daily vehicle trips will be 72 which is under the threshold of 250 or more daily vehicles trips to require VMT analysis. Therefore, the project does not exceed the threshold criteria established by LADOT for preparing a traffic study and will not have any significant impacts related to traffic.

A total of 1,938 square feet of usable open space will be provided, including a 1,188 square foot roof top deck. The project provides fifteen balconies to serve as private open space for individual units, totaling 750 square feet of private open space. The project incorporates landscaping within the front setback along Mitchell Avenue, as well as within the rooftop deck. The project will not remove any significant trees on-site or from the public right-of-way; the project will plant two (2) on the rooftop, one (1) on the front yard, and one (1) street tree; street tree spacing and selection is subject to further review by the Urban Forestry Division of the Department of Public Works, as provided in Exhibit "A".

Trash storage is located within the parking garage along the alley to ensure it will not be visible from the street or affect circulation for surrounding properties.

Therefore, as described above, the project will provide amenities and features that will enhance the surrounding neighborhood rather than further degrade or adversely affect other properties.

**5. The project substantially conforms with the purpose, intent and provisions of the General Plan, the applicable community plan, and any applicable specific plan.**

The Los Angeles General Plan sets forth goals, objectives and programs that guide both Citywide and community specific land use policies. The General Plan is comprised of a range of State-mandated elements, including, Land Use, Transportation, Noise, Safety, Housing and Conservation. The City's Land Use Element is divided into 35 community plans that establish parameters for land use decisions within those sub-areas of the City.

The General Plan is a long-range document determining how a community will grow, reflecting community priorities and values while shaping the future. The project substantially conforms with the following purposes and objectives of the General Plan Elements: Framework Element, Land Use Element (Palms – Mar Vista – Del Rey Community Plan), Housing Element, and Mobility Element. The project site is in the Los Angeles Coastal Transportation Corridor Specific Plan ("LACTC", Ordinance Nos. 186,104 and 186,105), and the Project is subject to Department of Transportation clearance of the LACTC.

**Framework Element**

The Framework Element is a strategy for long-term growth which sets a citywide context to guide the update of the Community Plan and Citywide Elements. The primary objectives of the policies in the Framework Element's Land Use Chapter are to support the viability of the City's residential neighborhoods and commercial districts, and when growth occurs, to encourage sustainable growth in a number of higher-intensity commercial and mixed-use

districts, centers and boulevards and industrial districts particularly in proximity to transportation corridors and transit stations.

The Community Plan Map designates the site for Medium Residential land uses, with corresponding zones of R3 and R3(PV). The Framework Element identifies the Medium Residential land use designation as corresponding to the R3 zone and estimates 30 to 55 dwelling units per acre. The site is zoned R3-1 and is therefore consistent with the land use designation.

Therefore, as a 6-story residential development with a maximum 3.99:1 FAR as allowed by Density Bonus, the proposed project is consistent with the General Plan Framework.

#### Land Use Element – Palms – Mar Vista – Del Rey Community Plan

The project site is in the Palms – Mar Vista – Del Rey Community Plan, and is designated for Medium Residential land uses, with corresponding zones of R3 and R3(PV). The site is zoned R3-1 and is therefore consistent with the land use designation.

Consistent with the Community Plan, the proposed 15-unit residential development, which includes two (2) Very Low Income units and one (1) Low Income unit, adds new multi-family housing and much needed affordable housing to Los Angeles's housing supply, in a neighborhood that is conveniently located to a variety of destinations, community services and amenities, and multi-modal transportation options.

The proposed project aligns with the intent of the Palms – Mar Vista – Del Rey Community Plan including the following:

Goal 1: A safe, secure and high-quality residential environment for all community residents.

Policy 1-1.1: Provide for adequate multi-family residential development.

Policy 1-2.1: Locate higher residential densities near commercial centers and major bus routes where public service facilities and infrastructure will support this development.

Objective 1-4: To promote the adequacy and affordability of multiple-family housing and increase its accessibility to more segments of the population.

Policy 2.3: Pedestrian Infrastructure: Recognize walking as a component of every trip, and ensure high quality pedestrian access in all site planning and public right-of-way modifications to provide a safe and comfortable walking environment.

The proposed project meets the above goal, policies, and objective by providing multi-family dwelling units in a new, safe, and secure building. The proposed project is located within a neighborhood designated for Medium Residential Land Uses, which includes multiple-family residential uses, and is well served by facilities and necessary infrastructure. The site is near a several bus stops, which encourages alternative modes of transportation. Planting new street trees will help achieve the City's goals for high-quality pedestrian access for a safe and comfortable walking environment. The three (3) affordable units will ensure that the proposed project is accessible to lower-income segments of the population.

#### Housing Element 2021 - 2029

The proposed project also conforms with the applicable policies of the Housing Element, including:

Goal 1: A City where housing production results in an ample supply of housing to create more equitable and affordable options that meet existing and projected needs.

Objective 1.2: Facilitate the production of housing, especially projects that include Affordable Housing and/or meet Citywide Housing Priorities.

Policy 1.2.1: Expand rental and for-sale housing for people of all income levels. Prioritize housing developments that result in a net gain of Affordable Housing and serve those with the greatest needs.

Policy 1.3.1: Prioritize housing capacity, resources, policies and incentives to include Affordable Housing in residential development, particularly near transit, jobs, and in Higher Opportunity Areas.

Goal 3: A City in which housing creates healthy, livable, sustainable, and resilient communities that improve the lives of all Angelenos.

Objective 3.2: Promote environmentally sustainable buildings and land use patterns that support a mix of uses, housing for various income levels and provide access to jobs, amenities, services and transportation options.

Policy 3.1.5: Develop and implement environmentally sustainable urban design standards and pedestrian centered improvements in development of a project and within the public and private realm such as shade trees, parkways and comfortable sidewalks.

Policy 3.2.2: Promote new multi-family housing, particularly Affordable and mixed-income housing, in areas near transit, jobs and Higher Opportunity Areas, in order to facilitate a better jobs-housing balance, help shorten commutes, and reduce greenhouse gas emissions.

The proposed project will result in a net increase of 11 new residential units to the City's housing stock and conforms with the applicable provisions of the Housing Element. The applicant has requested deviations from code requirements under the Density Bonus program for increased FAR, reduced parking, increased tandem and compact parking, increased height, and reduced side yard setbacks, thereby allowing the creation of affordable units. Pursuant to Density Bonus requirements, 22 percent (2 units) of the base dwelling units (9 units), will be set aside for Very Low Income units, and an additional 1 unit will be set aside for Low Income units. Additionally, this mixed-income development is in close proximity to public transit options, and a variety of retail, commercial, recreational, and employment opportunities. Locating new housing in this portion of the City will allow residents to have better access to employment centers and places of interest in area. Planting new street trees will help achieve the City's goals for environmentally sustainable urban design standards and pedestrian-oriented improvements.

### *Mobility Plan 2035*

The proposed project also conforms with the following additional policies of the Mobility Plan, including:

Policy 3.1: Access for All: Recognize all modes of travel, including pedestrian, bicycle, transit, and vehicular modes - including goods movement – as integral components of the City's transportation system.

Policy 3.3: Land Use Access and Mix: Promote equitable land use decisions that result in fewer vehicle trips by providing greater proximity and access to jobs, destinations, and other neighborhood services.

The project utilizes Density Bonus incentives for the construction of a residential mixed-income development that provides housing opportunities in proximity to public transit along the Washington Place and Washington Boulevard corridor, and to permit reduced parking, increased tandem parking, and increased compact parking through Density Bonus incentives and waivers, encouraging multi-modal transportation and decreasing vehicle miles traveled in the neighborhood. The site is located along a portion of Mitchell Avenue that is designated by the Mobility Plan as a Local Street. The project will also provide 31 long-term and 18 short-

term bicycle parking spaces, which exceeds LAMC Section 12.21 A.16 bicycle parking requirements in order to utilize the bicycle parking replacement provision of LAMC Section 12.21 A.4.

**6. The project is consistent with and implements the affordable housing provisions of the Housing Element of the General Plan**

The City's Housing Element for 2021-2029 was adopted by the City Council on November 24, 2021. The Housing Element is the City's blueprint for meeting housing and growth challenges. It identifies the City's housing conditions and needs, reiterates goals, objectives, and policies that are the foundation of the City's housing and growth strategy, and provides the array of programs the City has committed to implement to create sustainable, mixed-income neighborhoods across the City.

As provided under Finding No. 5, the proposed Project would be in conformance with the following goals of the Housing Element as described below:

- Goal 1: A City where housing production results in an ample supply of housing to create more equitable and affordable options that meet existing and projected needs.
- Objective 1.2: Facilitate the production of housing, especially projects that include Affordable Housing and/or meet Citywide Housing Priorities.
- Policy 1.2.1: Expand rental and for-sale housing for people of all income levels. Prioritize housing developments that result in a net gain of Affordable Housing and serve those with the greatest needs.
- Policy 1.3.1: Prioritize housing capacity, resources, policies and incentives to include Affordable Housing in residential development, particularly near transit, jobs, and in Higher Opportunity Areas.

In granting a Conditional Use for a 62.5 percent density increase, affordable housing is required beyond the minimum percentage required per the State Density Bonus Law and the City's Density Bonus Ordinance. This ensures that the project provides a proportional amount of affordable housing units compared to the density increase it is seeking. In this case, the project is required to set aside 22 percent (2 units) of the base dwelling units (9 units) for Very Low Income Households in exchange for the 62.5 percent density increase requested. The project proposes to set aside two (2) units for Very Low Income Households, thereby complying with the requisite percentage of affordable housing units for the 62.5 percent density increase. In addition to the set aside units pursuant to Density Bonus, the applicant must provide one (1) additional unit affordable to Low Income Households to comply with the Determination made by the LAHD.

The site is currently improved with a two-story single-family dwelling and triplex; all existing structures will be demolished. The Los Angeles Housing Department (LAHD) has determined, per the Housing Crisis Act of 2019 Determination, dated September 1, 2021, that three (3) residential units need to be replaced with equivalent type, with two (2) units restricted to Very Low Income Households and one (1) unit restricted to Low Income Households (Exhibit C). The three (3) units required by the LAHD determination are partially satisfied by the two (2) units set aside for habitation by Very Low Income Households proposed through this Density Bonus request. In addition to the set aside units pursuant to Density Bonus, the applicant must provide one (1) additional unit affordable to Low Income Households to comply with the Determination made by the LAHD. By redeveloping the subject site for the Project, 15 new dwelling units will be made available in the community. The Project will offer studio and two-bedroom apartment types in various sizes. Therefore, the project is in conformance with the affordable housing provisions of the Housing Element.

7. The project contains the requisite number of Restricted Affordable Units, based on the number of units permitted by the maximum allowable density on the date of application, as follows:

- A. 11% Very Low Income Units for a 35% density increase; or
- B. 20% Low Income Units for a 35% density increase; or
- C. 40% Moderate Income Units for a 35% density increase in for-sale projects.

The project may then be granted additional density increases beyond 35% by providing additional affordable housing units in the following manner:

- D. For every additional 1% set aside of Very Low Income Units, the project is granted an additional 2.5% density increase; or
- E. For every additional 1% set aside of Low Income Units, the project is granted an additional 1.5% density increase; or
- F. For every additional 1% set aside of Moderate Income Units in for-sale projects, the project is granted an additional 1% density increase; or
- G. In calculating the density increase and Restricted Affordable Units, each component of any density calculation, including base density and bonus density, resulting in fractional units shall be separately rounded up to the next whole number.

The project site is zoned R3-1, which allows a base density of 9 units on the subject property. The Density Bonus Ordinance allows a density bonus of up to 35 percent in exchange for setting aside 11 percent of the 9 base density units for Very Low Income Households. With the Density Bonus Ordinance, the project would be permitted a total of 13 units on site in exchange for setting aside one (1) unit for Very Low Income Households. The project is permitted additional density increase beyond 35 percent by setting aside one (1) additional percent of base density units above the 11 percent for Very Low Income Households for every additional 2.5 percent density increase above the 35 percent. Below is a table showing the requisite percentage of affordable housing units for Very Low Income Households based on the percentage of density increase.

Percentage of Base Density to be Restricted to Very Low Income Households	Percentage of Density Increase Granted
11	35
12	37.5
13	40
14	42.5
15	45
16	47.5
17	50
18	52.5
19	55
20	57.5
21	60
22	62.5

The applicant requests a Conditional Use for a density increase in excess of 35 percent pursuant to LAMC Section 12.24 U.26, to allow a 62.5 percent increase in density for a total of 15 dwelling units in lieu of 9 base density dwelling units as otherwise permitted by-right in the R3-1 Zone. In accordance with LAMC Section 12.22 A.25(c)(7), in calculating Density Bonus and Restricted Affordable units any number resulting in a fraction shall be rounded up to the next whole number. As provided in the table above, the applicant is required to and is proposing to set aside 22 percent (2 units) of the base dwelling units (9 units) for Very Low Income Households in order to be granted a 62.5 percent density bonus. As such, the project satisfies the minimum percentage of base density to be restricted to Very Low Income Households to be eligible for a 62.5 percent density increase.

**8. The project meets any applicable dwelling unit replacement requirements of California Government Code Section 65915(c)(3).**

On October 9, 2019, the Governor signed into law the Housing Crisis Act of 2019 (SB 330). SB 330 creates new state laws regarding the production, preservation and planning for housing, and establishes a statewide housing emergency until January 1, 2025. During the duration of the statewide housing emergency, SB 330, among other things, creates new housing replacement requirements for Housing Development Projects by prohibiting the approval of any proposed housing development project on a site that will require the demolition of existing residential dwelling units or occupied or vacant "Protected Units" unless the proposed housing development project replaces those units Government Code section 65915(c)(3). The Los Angeles Housing Department (LAHD) has determined, per the Housing Crisis Act of 2019 Determination, dated September 1, 2021, that three (3) residential units need to be replaced with equivalent type, with two (2) units restricted to Very Low Income Households and one (1) unit restricted to Low Income Households (Exhibit C). The three (3) units required by the LAHD determination are partially satisfied by the two (2) units set aside for habitation by Very Low Income Households proposed through this Density Bonus request. In addition to the set aside units pursuant to Density Bonus, the applicant must provide one (1) additional unit affordable to Low Income Households to comply with the Determination made by the LAHD.

**9. The project's Restricted Affordable Units are subject to a recorded affordability restriction of 55 years from the issuance of the Certificate of Occupancy, recorded in a covenant acceptable to the Housing and Community Investment Department, and subject to fees as set forth in Section 19.14 of the Los Angeles Municipal Code.**

The applicant proposes to set aside a total of three (3) units for Restricted Affordable Units. Per the Conditions of Approval, the applicant is required to execute a covenant to the satisfaction of LAHD to make two (2) Restricted Affordable Units available to Very Low Income Households and one (1) Restricted Affordable Unit available to Lower Income Households for rental as determined to be affordable to such households by LAHD for a period of 55 years. The applicant is required to present a copy of the recorded covenant to the Department of City Planning and the proposed project shall comply with any monitoring requirements established by LAHD. Therefore, as conditioned, the project satisfies this finding in regards to subjected restricted affordable units to recorded affordability per LAHD, and is subject to fees as set forth in Section 19.14 of the LAMC.

**10. The project addresses the policies and standards contained in the City Planning Commission's Affordable Housing Incentives Guidelines.**

The City Planning Commission approved the Affordable Housing Incentives Guidelines (CPC-2005-1101-CA) on June 9, 2005. The Guidelines were subsequently approved by City Council

(CF 05-1345) on February 20, 2008, as a component of the City of Los Angeles Density Bonus Ordinance. The Guidelines describe the density bonus provisions and qualifying criteria, incentives available, design standards, and the procedures through which projects may apply for a density bonus and incentives. LAHD utilizes these Guidelines in the preparation of Housing Covenants for Affordable Housing Projects. On April 9, 2010, the City Council adopted updates to the City's Density Bonus Ordinance (CF 05-1345-S1, Ordinance No. 181,142). However, at that time, the Affordable Housing Incentives Guidelines were not updated to reflect changes to the City's Density Bonus Ordinance or more recent changes in State Density Bonus Law located in the Government Code. Therefore, where there is a conflict between the Guidelines and current laws, the current law prevails. Additionally, many of the policies and standards contained in the Guidelines, including design and location of affordable units to be comparable to the market-rate units, equal distribution of amenities, monitoring requirements, and affordability levels, are covered by the State Density Bonus Laws.

The project requests a 62.5 percent density increase above the 9 base density units to permit a total of 15 dwelling units. The project will set aside two (2) units for Very Low Income Households and one (1) unit for Low Income Households. As such, the project is consistent with the State Density Bonus Law and the local Density Bonus Ordinance, which the Affordable Housing Incentives Guidelines implement. Therefore, the project complies with the City Planning Commission's Affordable Housing Incentives Guidelines.

### **CEQA FINDINGS**

The Department of City Planning determined, based on the whole of the administrative record, that the Project is exempt from the California Environmental Quality Act ("CEQA") pursuant to CEQA Guidelines, Article 19, Section 15332 (Class 32), and there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies. The Notice of Exemption and Justification for Environmental Case No. ENV-2021-10395-CE is provided in the case file and attached as Exhibit D.

The project is the construction of a six-story, 67-foot 8-inch tall residential building comprised of 15 dwelling units (including 2 Very Low Income and 1 Low Income units). The project will be approximately 19,047 square feet in floor area with a Floor Area Ratio ("FAR") of 3.99:1. The project will provide 14 parking spaces at-grade. The site is currently improved with a two-story single-family dwelling and triplex which will be demolished for the project. No existing significant trees will be removed. The project will also involve minimal grading of less than 500 cubic yards of soil.

As a residential building, and a project which is characterized as in-fill development, the project qualifies for the Class 32 Categorical Exemption.

### **CEQA Determination – Class 32 Categorical Exemption Applies**

A project qualifies for a Class 32 Categorical Exemption if it is developed on an infill site and meets the following criteria:

**(a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with the applicable zoning designation and regulations.**

The project site is in the Palms – Mar Vista – Del Rey Community Plan, and is designated for Medium Residential land uses, with corresponding zones of R3 and R3(PV). The site is zoned R3-1 and is therefore consistent with the land use designation. The project site is in the Los Angeles Coastal Transportation Corridor Specific Plan ("LACTC", Ordinance Nos. 186,104 and 186,105), and the Project is subject to Department of Transportation clearance of the

LACTC. Height District No. 1 limits the Floor Area Ratio ("FAR") to 3:1 and building height to 45 feet with no limit on the number of stories; however, the proposed project will have a FAR of 3.99:1 and a height of 67 feet 8 inches as permitted by State Density Bonus Law in exchange for providing two (2) units for Very Low Income Households and one (1) unit for Low Income Households for 55 years. There is a 15-foot Building Line along Mitchell Avenue established under Ordinance No. 138533. As demonstrated in the case file, the project is consistent with the General Plan, the applicable Palms – Mar Vista – Del Rey Community Plan designation and policies, and all applicable zoning designations and regulations as permitted by Density Bonus law.

**(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.**

The subject site is wholly within the City of Los Angeles, on a site that is approximately 0.163 acres (7,100 square feet) and is surrounded primarily by multi-family residential uses and some single-family dwellings. Properties immediately adjacent to the site are developed primarily with two- and three-story multi-family residential uses and are similarly zoned R3-1, and surrounding properties are developed or proposed for multi-family buildings up to five stories tall. Other parcels further south fronting Washington Boulevard are zoned C2-1 and developed with commercial uses including a liquor store, laundromat, as well as a schoolhouse; further southwest is the city boundary for Culver City. The subject site is within 500 feet of several bus stops served by the Santa Monica Big Blue Bus 16 line, and Culver CityBus 1 and 2 lines.

**(c) The project site has no value as habitat for endangered, rare or threatened species.**

The site is previously disturbed and surrounded by development and therefore is not, and has no value as, a habitat for endangered, rare or threatened species. The site is currently improved with a single-family dwelling and triplex; all existing structures will be demolished. There are no protected trees or shrubs on the subject site or in the adjacent public right of way that would be removed as verified in the Tree Letter prepared by The Urban Lumberjack dated December 7, 2021.

**(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.**

Regulatory Compliance Measures – The project will be subject to Regulatory Compliance Measures (RCMs), which require compliance with the City of Los Angeles Noise Ordinance, pollutant discharge, dewatering, stormwater mitigations; and Best Management Practices for stormwater runoff. More specifically, RCMs include but are not limited to the following, to ensure the project will not have significant impacts:

- **Regulatory Compliance Measure RC-AQ-1 (Demolition, Grading and Construction Activities): Compliance with provisions of the SCAQMD District Rule 403.** The project shall comply with all applicable standards of the Southern California Air Quality Management District, including the following provisions of District Rule 403:
  - All unpaved demolition and construction areas shall be wetted at least twice daily during excavation and construction, and temporary dust covers shall be used to reduce dust emissions and meet SCAQMD District Rule 403. Wetting could reduce fugitive dust by as much as 50 percent.
  - The construction area shall be kept sufficiently dampened to control dust caused by grading and hauling, and at all times provide reasonable control of dust caused by wind.

- All clearing, earth moving, or excavation activities shall be discontinued during periods of high winds (i.e., greater than 15 mph), so as to prevent excessive amounts of dust.
- All dirt/soil loads shall be secured by trimming, watering or other appropriate means to prevent spillage and dust.
- All dirt/soil materials transported off-site shall be either sufficiently watered or securely covered to prevent excessive amount of dust.
- General contractors shall maintain and operate construction equipment so as to minimize exhaust emissions.
- Trucks having no current hauling activity shall not idle but be turned off.
- **Regulatory Compliance Measure RC-AQ-2:** In accordance with Sections 2485 in Title 13 of the California Code of Regulations, the idling of all diesel-fueled commercial vehicles (weighing over 10,000 pounds) during construction shall be limited to five minutes at any location.
- **Regulatory Compliance Measure RC-AQ-3:** In accordance with Section 93115 in Title 17 of the California Code of Regulations, operation of any stationary, diesel-fueled, compression-ignition engines shall meet specified fuel and fuel additive requirements and emission standards.
- **Regulatory Compliance Measure RC-AQ-4:** The Project shall comply with South Coast Air Quality Management District Rule 1113 limiting the volatile organic compound content of architectural coatings.
- **Regulatory Compliance Measure RC-AQ-5:** The Project shall install odor-reducing equipment in accordance with South Coast Air Quality Management District Rule 1138.
- **Regulatory Compliance Measure RC-AQ-6:** New on-site facility nitrogen oxide emissions shall be minimized through the use of emission control measures (e.g., use of best available control technology for new combustion sources such as boilers and water heaters) as required by South Coast Air Quality Management District Regulation XIII, New Source Review.
- **Regulatory Compliance Measure RC-GEO-1 (Seismic):** The design and construction of the project shall conform to the California Building Code seismic standards as approved by the Department of Building and Safety.
- **Regulatory Compliance Measure RC-NO-1 (Demolition, Grading, and Construction Activities):** The project shall comply with the City of Los Angeles Noise Ordinance and any subsequent ordinances, which prohibit the emission or creation of noise beyond certain levels at adjacent uses unless technically infeasible.

These RCMs will ensure the project will not have significant impacts on noise and water.

Geotechnical - The applicant has submitted a Geotechnical Engineering Exploration Report prepared by Irvine Geotechnical, Inc. dated February 24, 2021, and a Supplemental Geotechnical Engineering Exploration Report dated April 18, 2022. RCMs also include the submittal of the Geology and Soils Report to the Department of Building and Safety (“DBS”), and compliance with a Soils Report Approval Letter (Log No. 116492, dated March 10, 2021) which details conditions of approval that must be followed. In addition, the RCMs require that design and construction of the building must conform to the California Building Code, and grading on site shall comply with the City’s Landform Grading Manual, as approved by the Department of Building and Safety Grading Division.

Traffic - The Project does not exceed the threshold criteria established by LADOT for preparing a traffic study. The Department of Transportation (LADOT) Referral Form dated June 2, 2022 and the Vehicle Miles Traveled (VMT) calculator indicated that the number of daily vehicle trips will be 72 which is under the threshold of 250 or more daily vehicles trips to require VMT analysis. Therefore, the project does not exceed the threshold criteria established by LADOT for preparing a traffic study and will not have any significant impacts related to traffic.

Noise – The Project must comply with the adopted City of Los Angeles Noise Ordinances No. 144,331 and 161,574 and LAMC Section 41.40 as indicated above in RC-NO-1, LAMC Section 112.05, as well as any subsequent Ordinances, which prohibit the emission or creation of noise beyond certain levels. These Ordinances cover both operational noise levels (i.e., post-construction), and any construction noise impacts. The Project does not exceed the threshold criteria for preparing a noise study. As a result of this mandatory compliance, the proposed Project will not result in any significant noise impacts.

Air Quality – There are several Regulatory Compliance Measures listed above (RC-AQ-1 through RC-AQ-6) which regulate air quality-related impacts for projects citywide. The Project does not exceed the threshold criteria for preparing an air quality study; at 15 dwelling units, the Project is well under the screening criteria of 80 units for air quality studies. As a result of this mandatory compliance, the proposed Project will not result in any significant air quality impacts.

**(e) The site can be adequately served by all required utilities and public services.**

The project site will be adequately served by all public utilities and services given that the construction of a multi-family building will be on a site which has been previously developed and is consistent with the General Plan.

Therefore, the project meets all the Criteria for the Class 32 Categorical Exemption.

**CEQA Section 15300.2: Exceptions to the Use of Categorical Exemptions**

There are five (5) Exceptions which must be considered to find a project exempt under Class 32:

**(a) Cumulative Impacts.** *All exemptions for these classes are inapplicable when the cumulative impact of successive projects of the same type in the same place, over time is significant.*

There are other projects proposed, approved, or under construction within proximity to the site, including but not limited to:

- 12759 West Caswell Avenue – construction of a new 5-story 14-unit apartment building
- 3984 South Meier Street – construction of a 5-story, 40-unit apartment building

While there could potentially be a succession of known projects of the same type and in the same place as the subject project, all projects are subject to the citywide Regulatory Compliance measures as noted above, which regulate impacts related to air quality, noise, and geology to a less than significant level. There is no evidence to conclude that significant impacts will occur based on past project approvals or that the proposed Project's impacts are cumulatively considerable when evaluating any cumulative impacts associates with construction noise and transportation/traffic in the surrounding area.

Therefore, in conjunction with citywide RCMs and compliance with other applicable regulations, no foreseeable cumulative impacts are expected.

**(b) Significant Effect Due to Unusual Circumstances.** *A categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances.*

The project proposes a residential building in an area zoned and designated for such development. All adjacent lots are developed with multi-family and single-family residential uses, and the subject site is of a similar size and slope to nearby properties. The project proposes a FAR of 3.99:1 on a site that is permitted to have an FAR of 3.0:1 by the site's zoning. The project is eligible for the FAR 3.99:1 through an On-Menu Density Bonus Incentive. The project size and height is not unusual for the vicinity of the subject site, and is similar in scope to other existing multi-family dwellings and proposed future projects in the area. Furthermore, there is no substantial evidence in the administrative record that this project will cause a significant impact. Thus, there are no unusual circumstances which may lead to a significant effect on the environment, and this exception does not apply.

(c) **Scenic Highways.** *A categorical exemption shall not be used for a project which may result in damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway.*

The only State Scenic Highway within the City of Los Angeles is the Topanga Canyon State Scenic Highway, State Route 27, which travels through a portion of Topanga State Park. State Route 27 is located approximately 8.7 miles northwest of the subject site. Therefore, the subject site will not create any impacts within a designated state scenic highway, and this exception does not apply.

(d) **Hazardous Waste Sites.** *A categorical exemption shall not be used for a project located on a site which is included on any list compiled pursuant to Section 65962.5 of the Government Code*

According to Envirostor, the State of California's database of Hazardous Waste Sites, neither the subject site, nor any site in the vicinity, is identified as a hazardous waste site. Therefore, the project is not identified as a hazardous waste site, or in the vicinity of a hazardous waste site, and this exception does not apply.

(e) **Historical Resources.** *A categorical exemption shall not be used for a project which may cause a substantial adverse change in the significance of a historical resource.*

The project site is not listed in the National Register of Historic Places, California Register of Historical Resources, the Los Angeles Historic-Cultural Monuments Register, and/or any local register, and was not found to be a potential historic resource based on the City's HistoricPlacesLA website or SurveyLA, the citywide survey of Los Angeles. As such, the Project would have no impact on historical resources. Based on this, the project will not result in a substantial adverse change to the significance of a historic resource and this exception does not apply.

## PUBLIC HEARING AND COMMUNICATIONS

### PUBLIC HEARING

The public hearing was held on August 17, 2022 at approximately 10:00 a.m. Due to concerns over COVID-19, the Public Hearing was conducted in a virtual format. The hearing was conducted by the Hearing Officer, Connie Chauv, on behalf of the City Planning Commission in taking testimony for Case No. CPC-2021-10394-CU-DB-HCA-PHP and ENV-2021-10395-CE. All interested parties were invited to attend the public hearing at which they could listen, ask questions, or present testimony regarding the project. The purpose of the hearing was to obtain testimony from affected and/or interested parties regarding this application. Interested parties are also invited to submit written comments regarding the request prior to the hearing. The environmental analysis was among the matters to be considered at the hearing. The hearing notice was mailed on July 18, 2022, and was posted on-site on August 3, 2022, in accordance with LAMC noticing requirements. The courtesy notice was mailed on September 16, 2022, published in the newspaper on September 16, 2022, and was posted on-site on September 29, 2022, in accordance with LAMC noticing requirements.

The public hearing was attended by the applicant's representatives (Jesi Harris and Brian Silveira) and approximately six (6) other members from the community. There were four (4) speakers who provided general comment at the hearing.

Applicant Presentation. The applicant's representative described the site location, project description, requested entitlements, and community outreach. Specifically, the applicant noted the following:

- Site is 1 block north of Washington Boulevard/Place and 4 blocks south of Venice Boulevard. It is surrounded by residential and commercial and several bus stops. Venice Boulevard is slated for a fully protected bike lane in the Mobility Plan. There is an existing separated bike lane on Washington Boulevard, and three different bus lines in the area, so the project is designed to be transit-facing.
- The site is currently surrounded on both sides by multi-family residential buildings.
- The project is a mixed-income building with 13 market-rate, 2 Very Low Income, and 1 Low Income units. The project is providing 22% of base density as Very Low Income Units.
- The project is not maxing out under density bonus, as they could take a larger density bonus for an additional Very Low Income unit. Typical developers want to max out on Very Low Income units to take a larger bonus, but the community wants more units that are moderately affordable.
- The requests for reduced yards and increased FAR are on-menu. The requests for height and parking are off-menu.
- The project will use passive heating/cooling techniques for a climate adaptive design, with windows that can get sunlight to warm units but also passively cool the building.
- The project coalesces with citywide development goals in the Housing Element. Applicant wants to create a range of affordable housing including market rate. There is a diversity in type of units with mostly two-bedroom units with studio market-rate units.
- The site is in a high resource area based on Planning's Fair Share Housing Report which focuses on dispersal of affordable housing development in the city. Development will meet goals of spreading diversity of low income and mixed income development.
- Most of Mar Vista is zoned for single-family, with just small portions zoned for multi-family uses. The project height and density takes advantage of the limited space zoned for multifamily housing in order to preserve the single-family neighborhoods.
- Based on community feedback, the applicant made the following changes to the project:
  - Darkened building materials so as to not reflect light into nearby residents.

- Integrated a Low Income Unit that was previously Very Low Income.
- Incorporated native drought resistant landscaping
- Incorporated lobby into parking garage for receiving packages.
- Incorporated energy star appliances and solar roof deck.

Public Comments in Support:

- Has been engaged with the developer throughout their proposal. Compliments them on engaging the community. Encourages support of the project.
- There are 27 low income long-term minority tenants that have been displaced by a separate project 3 doors down to the west; the developer reached out to tenants to try to accommodate them if they qualify for low income housing.
- Applicant did everything they asked for regarding sustainability, and will be involved in sustainable issues like window coverings. Did muted colors on outside to reduce heat island.
- Neighborhood is all R3. This is where density bonus belongs. Need affordable housing. There are 8 projects in 1 block area including Bastion development on Washington.
- They listened to the community and is proposing a gorgeous building and is taking Mar Vista to the next level.
- They added a Low Income unit to address requests for moderate housing. Every other developer puts all Extremely Low Income units which is detriment.
- This should be the status quo moving forward. Need more developers to do what they did. Can't wait for it to be built.

Public Comments in Opposition:

- This is a 6-story building in the middle of a neighborhood that is no more than 3 stories. Nothing else in the area is 6 stories. Might be a beautiful building and design but will take away views, cut out sunlight, and stick out in the neighborhood.
- Tenants are concerned about a 6-story building next door with waivers for density, setbacks, parking.
- Parking is terrible; there is not enough parking in the area to accommodate what's there. Residents in a two-bedroom unit will have two cars; there is not enough spaces for the number of units. People don't use transportation.
- There are multiple buildings going up in few block radius. There is another 4-5 story building on corner of Wade/Matteson that is blocking views.
- Not sure where there is room for trash cans.
- Objects to waivers and this development the way it is, but is not against building something that makes sense. Giving waivers is a disservice to the community. The parking, density, and traffic should not be allowed.
- No idea why MVCC is allowing this and saying it's okay. Not sure if they are really checking with neighbors who live there, or if anyone has driven during nighttime or during street clean-up.

Applicant's Response to Public Comments and Staff Questions:

- Los Angeles suffers from a housing shortage for both market-rate and affordable units because the zoning has been historically too low for an urban area. A 3-story building is not enough when it's close to transit, commercial, and amenities. Building taller than the neighborhood because they want to provide an adequate number of units to meet the need. In providing 2 Very Low Income and 1 Low Income units, they have to reduce the marginal costs of providing those units. This is consistent with Density Bonus policy.
- Project is intentionally designed to provide less parking. Providing parking increases the cost of development and reduces the amount of space available to residential units. Los Angeles has prioritized spaces for vehicles over people, but the project is trying to take a step in the other direction and encourage alternative modes of transportation.

Subterranean parking costs over \$150 per square foot with rising building costs and inflation. The savings from less parking will be passed onto renters. There is parking for 8 distinct units with 6 sets of tandem spaces and 2 standard spaces, so over half of units will have parking.

- Mobility Plan includes enhancements for Venice, Washington, and other streets nearby including a fully protected bike lane on Venice Boulevard that stretches all the way to the coast. The project gives options for alternative transportation.
- Project is providing unbundled parking so people who don't need parking don't have to pay for it. Parking will be rented separately from the rental of the dwelling unit. The project provides 6 pairs of tandem spaces and 2 standard spaces. Will rent both tandem spaces for one unit. Residents have the option to rent a parking space. If additional parking spaces are still available, then they can be leased.
- Site is in a good position in the neighborhood that is community-facing commercial and walkable distance to 3 bus lines that can provide access to other transportation sources like Expo Line that can reach the rest of the city.
- Trash cans are stored in the garage and will be accessed through the alleyway to the rear, which is consistent with trash pick-up now.
- Applicant is requesting a 20% setback reduction to increase the building envelope which are on-menu and consistent with Density Bonus policy to incentivize affordable housing. Projects need different development standards to accommodate affordable housing. Zoning geared towards low density has not worked well. The project is meeting front and rear setback requirements.
- Housing Department will require a 55 year covenant for the affordable units that go on the title and permanent record. The units will have to remain affordable even when the building is sold, changes hands, or tenants move out. This is enforced by the city.
- Lobby is integrated into the ground floor parking. Residents enter through the stairway on the side of the building along the south side yard, and come upstairs to the 2nd floor to access internal stairways and elevator for upper floors. Lobby enhances the privacy of the residents, and protects packages from being exposed/visible. Lobby will be climate controlled.
- VMT calculator generated by LADOT indicated the project will not have a significant impact based on screening criteria. The project results in approximately 50 net vehicle trips which is below the 250 daily trip threshold.
- Project will provide 2.5-foot dedication along alley and improvements to both Mitchell and the alley per BOE requirements.
- Project will provide 2 street trees and 2 rooftop trees, with a total of 4 trees.
- Tree report from certified arborist included an inventory of trees and confirmed no protected trees/shrubs.

## WRITTEN CORRESPONDENCE

The Mar Vista Community Council submitted a letter in opposition to the project on June 8, 2022.

Planning Staff has received seven (7) written correspondences from adjacent neighbors expressing concerns about the project. Their comments are included in Exhibit E and summarized as follows:

- Opposed to the demolition and construction because of the high-rise structural plan, ultimate increase in local traffic, and creation of an additional building site on a street already saturated with construction.
- Many projects are currently going on in this immediate area over the last few years. There are four new construction sites on the same block with two more in the works, including the new triangle business park on Zanja/Washington, two other buildings on the corner of

Zanja/Mitchell, demolition of a single-family home on Caswell Avenue, and corner of Frances/Mitchell. There are two construction sites within only 50 feet of the proposed project, and three buildings being demolished for additional new rental properties, for a total of 5 new buildings within a block that will bring additional density and cars.

- The neighborhood is extremely congested and competitive now. Putting up a building that does not provide adequate parking is going to highly increase the parking issues. Residents and guests have to park as far as two or three blocks away or drive around for 10 minutes to search for parking. There is not enough street parking available for residents and guests of the current buildings, especially not including the nearly completed buildings. People currently park in the alleys, on driveways, and on sidewalks.
- The project has 15 units but the applicant is requesting to minimize building parking from 21 spaces to merely 14. The project will put at least 15 more cars on the street which will fill up all of one side of the street and part of the other.
- Charging a separate rental fee may cause people to decide to park on the street. Low income tenants may not be able to afford the \$300 monthly parking fee.
- The 12 tandem parking spaces provided are a major issue. The tandem spaces cannot be allocated to two different residents living in different units.
- Every new building should have at least 2 parking spaces per unit, plus spaces for guest parking, to accommodate roommates and families with 2 vehicles. This is the reality given the high rents and people needing roommates to split rent plus likelihood of couples.
- Adding more people and hoping they won't have cars for transportation or will ride bikes is not logical and not working in real life. Tenants seldom use bikes for shopping or during workdays. Only 1-2 bikers use the bike lanes on Venice Boulevard every 3-4 minutes.
- Concerned about the lack of safety of the neighborhood during the time before demolition. Noticed a spike in homelessness, hazardous waste, trash, and crime around the current construction sites on the block. Have written and called law enforcement about trespassers and homeless dwelling in the abandoned structures on the block.
- Development process will create a noticeable rise in noise/shaking, debris, and traffic. Concerns about compacting from this development will impact neighboring building's foundation and subterranean parking.
- The project is too tall and big overall. It is out of place with the rest of the properties in the area that are no more than 3 or 4 stories high. It is unsightly for a building that is over 20 feet taller than what is permitted in the zone. It will cause obstructed views and blocked sunlight. Height waiver would impact quality of life of others.
- Opposed to all the waivers requested by this developer. The project will violate all the building department requirements for setbacks and parking for this property.
- Neighbors have rights to live in a peaceful and pleasant environment as the new residents of this new building want.
- There should be proper setbacks for light, air breeze, and to keep the privacy of residents and neighbors.
- There should be noise abatement for any rooftop HVAC machinery.
- There are two fig trees at least 1 lemon tree.
- The units should be available for under \$1000 and \$1500 per month, but it's unclear who will enforce that they will be low income.
- The plans should show trash cans and recyclable containers. Neighbors use my recyclable containers.

## **EXHIBIT A**

### **PROJECT PLANS**

**CPC-2021-10394-CU-DB-HCA-PHP**

# M I T C H E L L A V E



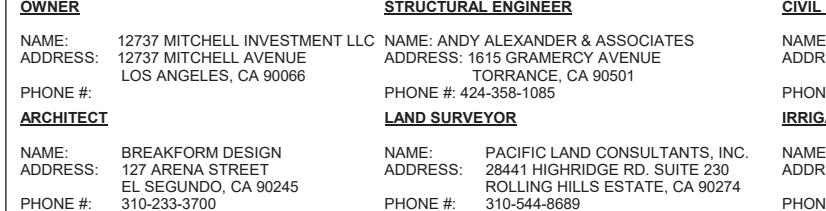
breakformdesign



CODE	
BUILDING CODE:	2020 LABC, (TITLE 24, PART 2.5) BASED ON THE 2018 IRC (INCLUDES ACCESSIBILITY)
STRUCTURAL:	2020 LABC, VOL 2 (TITLE 24, PART 2, VOL 2) BASED ON THE 2018 IBC WITH ASCE 7-16
MECHANICAL CODE:	2019 CA MECHANICAL CODE, (TITLE 24, PART 4) BASED ON THE 2018 UNIFORM MECHANICAL CODE
PLUMBING CODE:	2019 CA PLUMBING CODE (TITLE 24, PART 5) BASED ON THE 2018 UNIFORM PLUMBING CODE
ELECTRICAL CODE:	2019 CA ELECTRICAL CODE (TITLE 24, PART 3) BASED ON THE 2017 NATIONAL ELECTRIC CODE
ENERGY CODE:	2019 CA ENERGY CODE (TITLE 24, PART 6) 2020 L.A. CITY GREEN BUILDING CODE



OWNER	STRUCTURAL ENGINEER	CIVIL ENGINEER
NAME: 12737 MITCHELL INVESTMENT LLC	NAME: ANDY ALEXANDER & ASSOCIATES	NAME:
ADDRESS: 12737 MITCHELL AVENUE LOS ANGELES, CA 90066	ADDRESS: 1615 GRAMERY AVENUE TORRANCE, CA 90501	ADDRESS:
PHONE #:	PHONE #: 424-358-1085	PHONE #:
ARCHITECT	LAND SURVEYOR	IRRIGATION
NAME: BREAKFORM DESIGN	NAME: PACIFIC LAND CONSULTANTS, INC.	NAME:
ADDRESS: 127 ARENA STREET EL SEGUNDO, CA 90245	ADDRESS: 28441 HIGHRIDGE RD. SUITE 230 ROLLING HILLS ESTATE, CA 90274	ADDRESS:
PHONE #: 310-233-3700	PHONE #: 310-544-8689	PHONE #:



ABBREVIATIONS & SYMBOLS	
&	And
<	Angle
@	At
C.L.	Centerline
Ø	Diameter or Round
+	Perpendicular
#	Number
(E)	Existing
AB	Anchor Bolt
A/C	Air Conditioning
A.C.	Asphaltic Concrete
ACT	Acoustical Tile
ACOUS	Acoustical
ADJ	Adjustable
A.F.F.	After Finish Floor
ALT	After Alternative
ALUM	Aluminum
ANOD	Anodized
A.P.	Access Panel
APPROX	Approximate
ARCH	Architectural
ASPH	Asphalt
BD	Board
BITUM	Bituminous
BLDG	Building
BLK	Block
BLKG	Blocking
BM	Beam
BOT	Bottom
BU.R.	Built Up Roofing
CAB	Cabinet
CARP	Carpet
C.B.	Catch Basin
CEM	Cement
CEER	Ceramic
C.I.	Cast Iron
CLG	Ceiling
CLO	Closet
CLR	Clear
CMU	Concrete Masonry Unit
CNTR	Counter
COL	Column
CONC	Concrete
CONN	Connection
CONSTR	Construction
CONT	Continuous
CONTR	Contractor
CORR	Corridor
C.T.	Center
C.TSK	Countersunk
C.W.	Ceramic Tile
CTSK	Countersunk
EXIST	Existing
FLUOR	Fluorescent
E.O.S.	Edge of Slab
EO.	Edge of
EQUIP	Equipment
FUT	Furring, Furred
FUT.	Furring
FR	Frame
FULL	Full Size
FT	Foot, Feet
FTG	Footing
FURR	Furring, Furred
FUT.	Furring
Gauge	Gauge
JAN	Janitor
JST	Joint
PTD.	Depth
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PTD.	Detail
PTD.	Exterior
PTD.	Drinking Fountain
PTD.	Dimension
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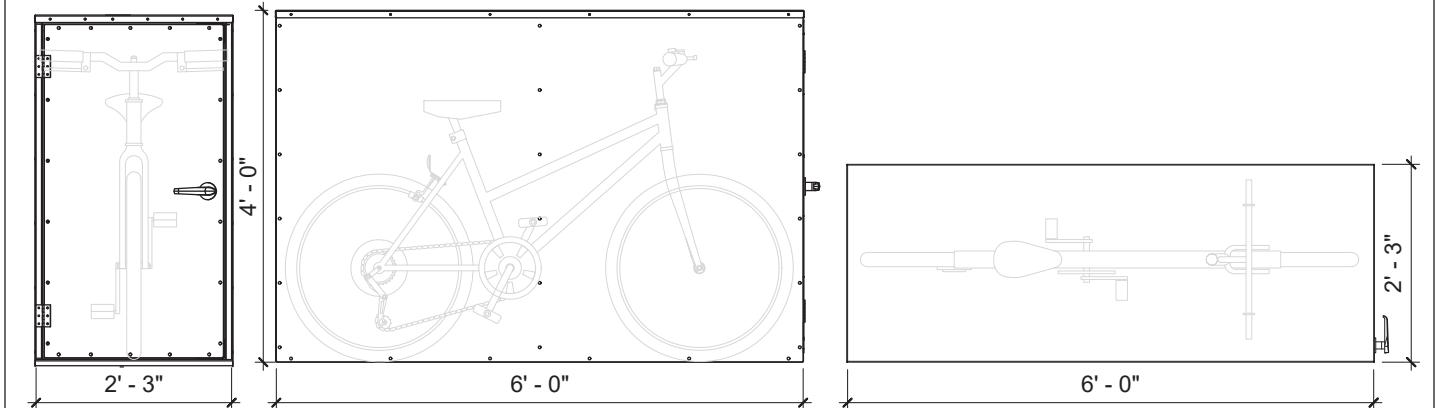
109 eucalyptus drive, el segundo, ca 90245  
[o] 310.322.3700

12737 MITCHELL AVE  
LOS ANGELES  
CA 90066



REVISIONS  
GENERAL DETAILS  
DRAWN CR  
CHECKED PNK  
DATE 4/20/2022 10:15:50 AM  
SCALE As indicated  
JOB # 20-A015

A0.20



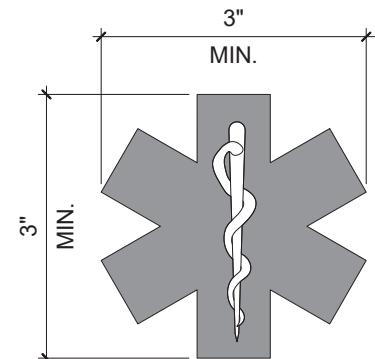
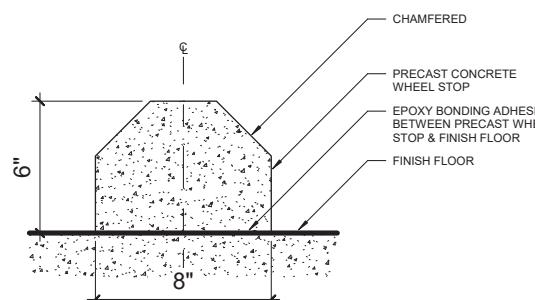
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SIDE ELEVATION

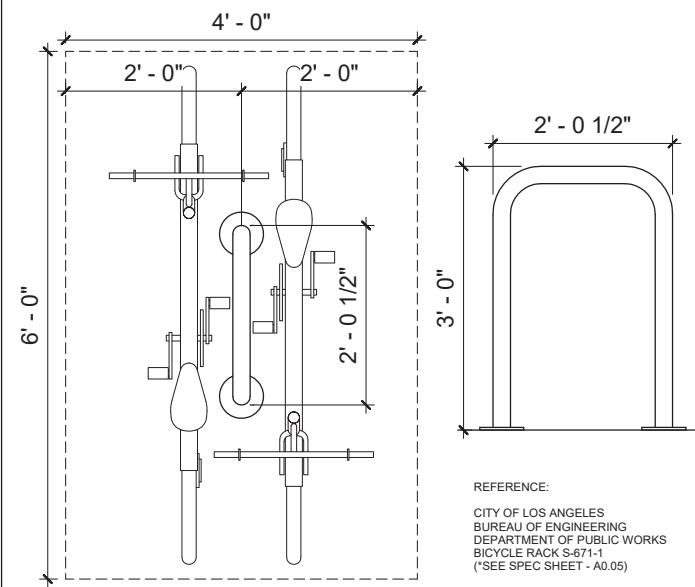
PLAN VIEW

DETAIL - BICYCLE PARKING LONG TERM LOCKER TYP.  
1" = 1'-0"

3



NOTE:  
INTERNATIONAL SYMBOL FOR MEDICAL SERVICES (STAR OF LIFE). THE SYMBOL SHALL NOT BE LESS THAN 3 INCHES HIGH & SHALL BE PLACED INSIDE OF THE HOISTWAY DOOR FRAME.



REFERENCE:  
CITY OF LOS ANGELES  
BUREAU OF ENGINEERING  
DEPARTMENT OF PUBLIC WORKS  
BICYCLE RACK S-671-1  
(\*SEE SPEC SHEET - A-05)

DETAIL - BICYCLE PARKING SHORT TERM TYP.  
1" = 1'-0"

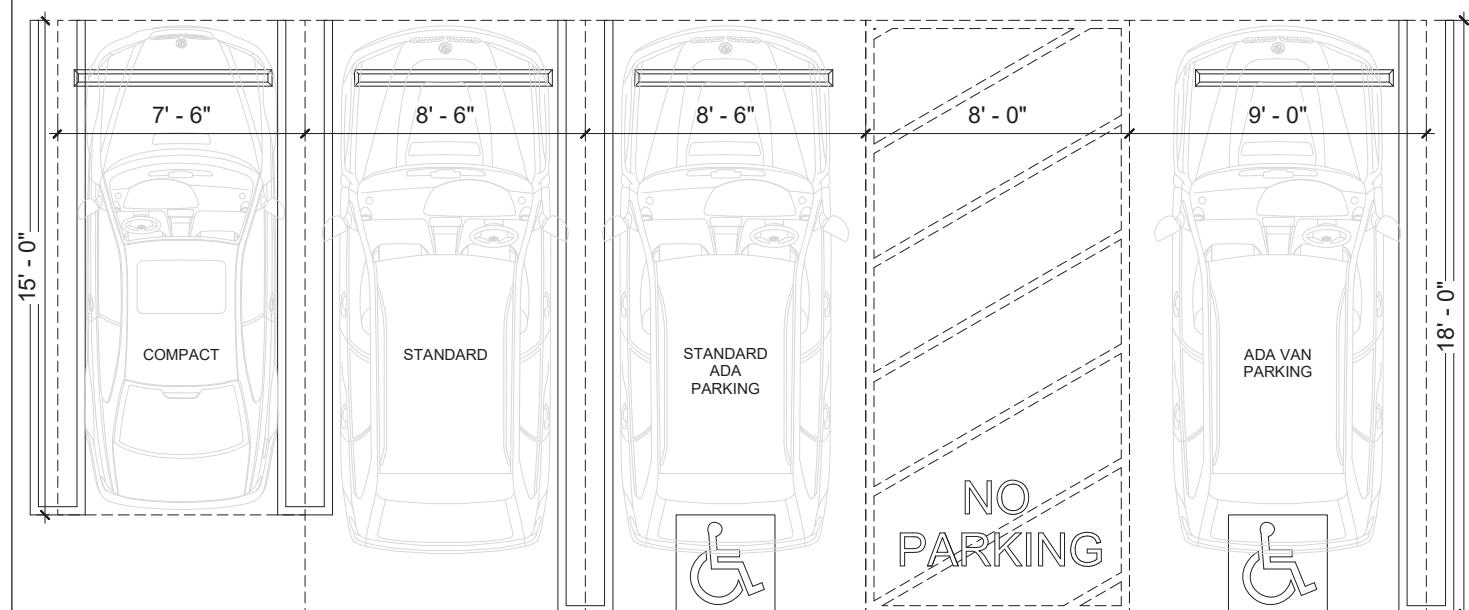
2

DETAIL - WHEEL STOP  
3" = 1'-0"

7

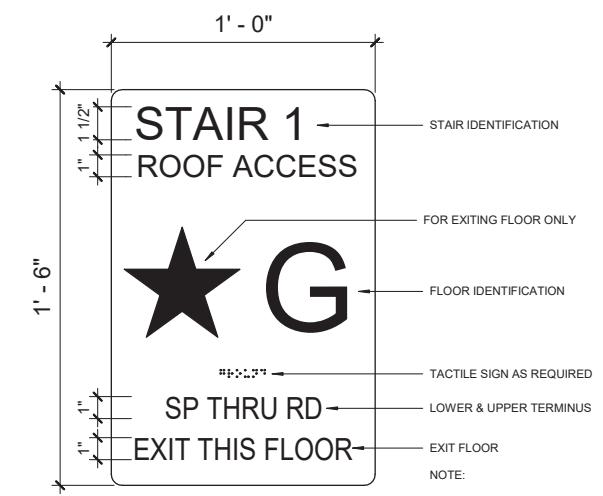
DETAIL - MEDICAL SERVICES SYMBOL  
12" = 1'-0"

5



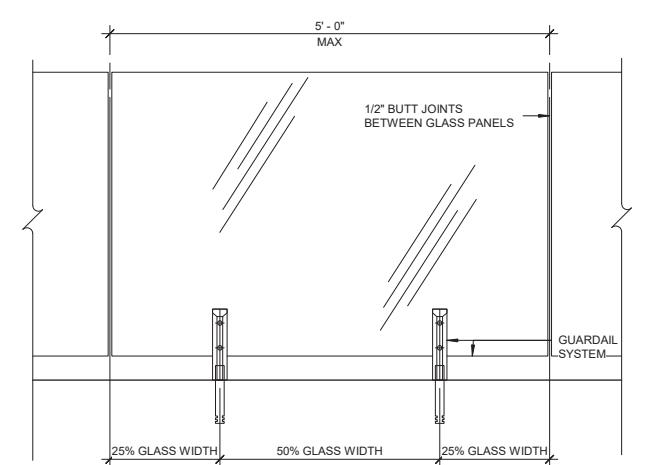
DETAIL - TYPICAL PARKING  
3/8" = 1'-0"

6



DETAIL - FLOOR I.D. SIGN TYP.  
3" = 1'-0"

4



DETAIL - GLASS GUARDRAIL  
1" = 1'-0"

1

# breakform design

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109 eucalyptus drive, el segundo, ca 90245  
[o] 310.322.3700

22737 MITCHELL AVE  
LOS ANGELES  
CA 90066

*GEND*

ASPHALT CONCRETE PAVEMENT  
 BENCH MARK  
 CONCRETE BLOCK WALL  
 CONCRETE BLOCK RETAINING WALL  
 CONCRETE  
 Curb DRIVEWAY APRON  
 EDGE OF PAVEMENT  
 FINISHED FLOOR  
 FINISHED GRADE  
 FIRE HYDRANT  
 FINISHED SURFACE  
 FLOW LINE  
 DRAIN INVERT  
 IRRIGATION CONTROL VALVE  
 LIGHT POLE  
 EDGE OF GLITTER  
 ELECTRIC METER  
 ELECTRIC VALVE  
 GAS METER  
 PULL BOX  
 POURED CONC. RETAINING WALL  
 POWER POLE  
 ROCK RETAINING WALL  
 SEWER MANHOLE  
 STORM DRAIN MANHOLE  
 TOP OF CURB  
 TOP OF DRAIN GRATE  
 TOP OF WALL  
 WATER MANHOLE  
 WATER VALVE  
 --- OVERHANG  
 - EASEMENT LIMIT  
 CONCRETE SURFACE  
 TREE AND TRUNK DIAMETER  
 WOOD FENCE

## TOPOGRAPHIC NOTES

TOUR LINES SHOWN ON THIS TOPOGRAPHIC SURVEY  
ARE GENERATED FROM RANDOM SPOT ELEVATIONS. THE  
EXPECTED LEVEL OF ACCURACY IS  $\pm 1\%$  OF THE CONTOUR  
INTERVAL. ISOLATED ELEVATIONS AS PLACED ON THE PLAT  
ARE PHYSICALLY MEASURED AT APPROXIMATELY THE  
LOCATIONS SHOWN. THEIR EXPECTED LEVEL OF ACCURACY  
IS  $\pm 0.10$  FOOT.

SPECIFIC ELEVATIONS ARE REQUIRED, THE USER SHOULD  
CONTACT PACIFIC LAND CONSULTANTS, INC., TEL: (310)544-8689  
SAME. INTERPOLATIONS MADE FROM THIS TOPOGRAPHIC  
VEYING SHOULD BE DONE IN ACCORDANCE WITH STANDARD  
PRACTICES, AND LIABILITY FOR INTERPOLATIONS  
NOT ASSUMED BY PACIFIC LAND CONSULTANTS, INC.

### SURVEY CERTIFICATION

BY CERTIFY THAT I AM A REGISTERED LAND SURVEYOR OF THE  
OF CALIFORNIA, THAT THIS PLAT CONSISTING OF ONE SHEET  
ONLY REPRESENTS A SURVEY MADE UNDER MY SUPERVISION IN  
SEPTEMBER, 2020, THAT ALL MONUMENTS SHOWN HEREON ACTUALLY  
AND THEIR POSITIONS ARE CORRECTLY SHOWN. THIS SURVEY  
DOES NOT INCLUDE EASEMENTS EXCEPT THOSE SPECIFICALLY  
MENTIONED HEREON.

COPHER W. VASSALLO P.L.S. 8418 DATE  
RATION EXPIRES 12-31-2022



## EXISTING SITE SURVEY

*PLEASE NOTE*

IF THIS DRAWING IS PROVIDED IN AN ELECTRONIC FORMAT (VIA EMAIL OR ON COMPUTER DISC) AS A COURTESY TO OUR CLIENT, THE DELIVERY OF THE ELECTRONIC FILE DOES NOT CONSTITUTE THE DELIVERY OF OUR PROFESSIONAL WORK PRODUCT. IN THE EVENT THE ELECTRONIC FILE IS ALTERED, THE PRINT MUST BE REFERRED TO FOR THE ORIGINAL AND CORRECT SURVEY INFORMATION. PACIFIC LAND CONSULTANTS, INC. SHALL NOT BE RESPONSIBLE FOR ANY MODIFICATIONS MADE TO THE ELECTRONIC FILE OR FOR ANY PRODUCTS DERIVED FROM THE ELECTRONIC FILE WHICH ARE NOT REVIEWED, SIGNED AND SEALED BY PACIFIC LAND CONSULTANTS, INC.

## UNDERGROUND UTILITIES

ALL INFORMATION SHOWN HEREON REGARDING UNDERGROUND UTILITIES WAS TAKEN FROM VISIBLE SURFACE EVIDENCE OR SOURCES NOT CONNECTED WITH THIS COMPANY AND WHILE SAID INFORMATION IS BELIEVED CORRECT, NO LIABILITY IS ASSUMED FOR THE ACCURACY OR COMPLETENESS OF SAID DATA.

### BENCHMARK

FD L+P  
ASSUMED EL: 100.00'

LEGAL DESCRIPTION

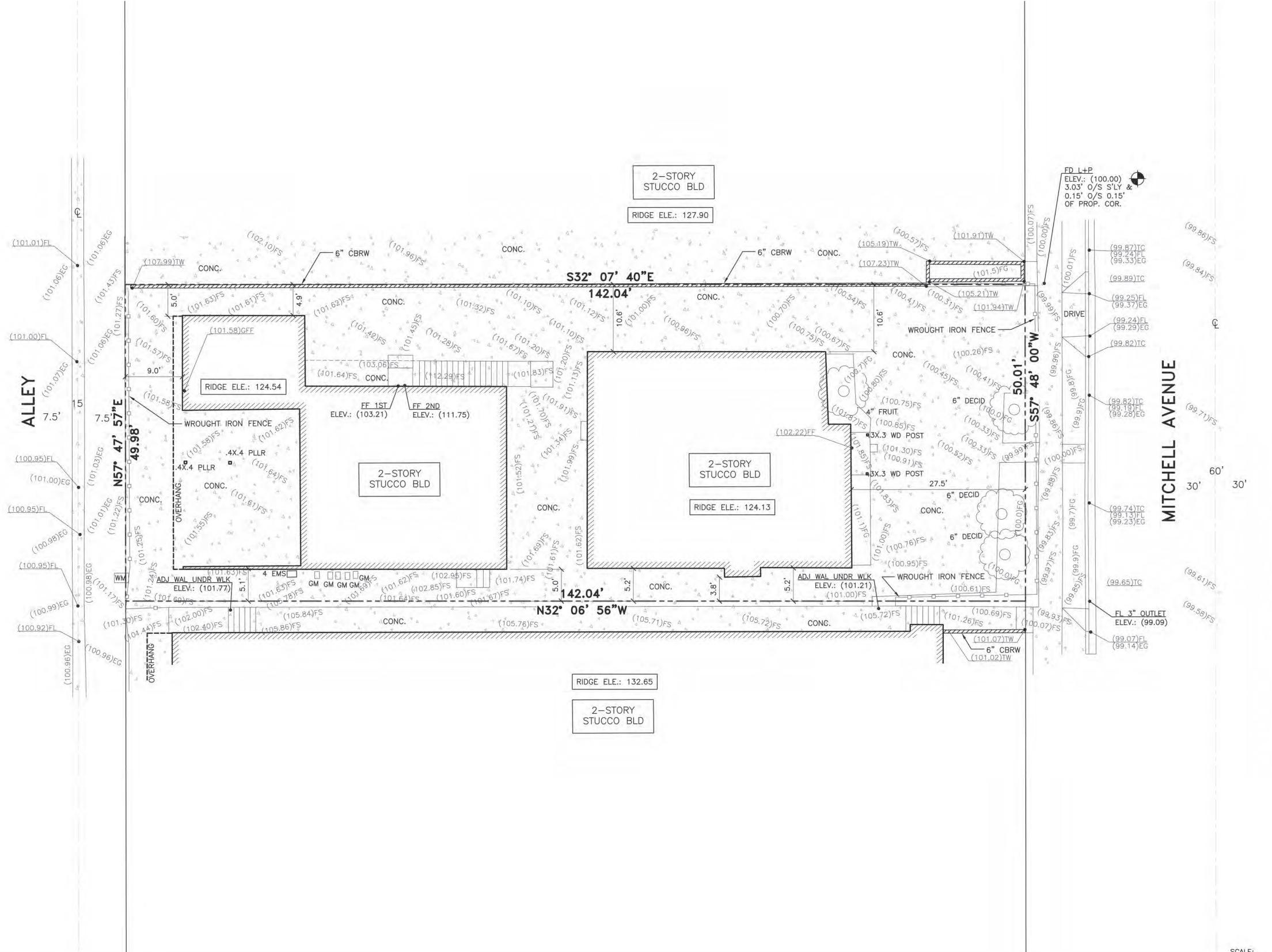
LOT 190 IN DEL MAR TRACT, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 10, PAGE 76 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.



PACIFIC LAND CONSULTANTS, INC.  
3441 HIGHRIDGE RD. SUITE 230  
OLLING HILLS ESTATES, CA 90274  
(310) 544-8689

NAME: 20118LS.DWG  
E: 12-16-2020  
LE: 1/8"=1'-0"  
NO. 20118  
EFT 1 GE 1

A0.26



A scale bar with markings for 0, 4, 8, FEET, and 16. Below the bar, the text "1/8\" data-bbox="111 80 300 125" data-label="Text"/> = 1'-0" data-bbox="111 80 300 125" data-label="Text"/>

AREA = 7,100.87 SQ.FT. APN= 4236-019-026

# breakform design

109 eucalyptus drive, el segundo, ca 90245  
[o] 310.322.3700

12737 MITCHELL AVE  
LOS ANGELES  
CA 90066



REVISIONS

SQUARE FOOTAGE BREAKDOWNS

DRAWN CR

CHECKED PNK

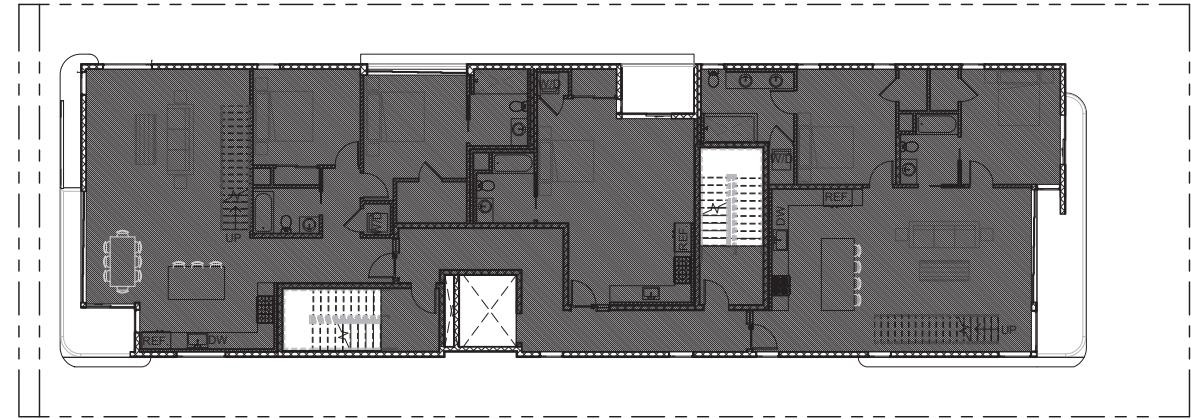
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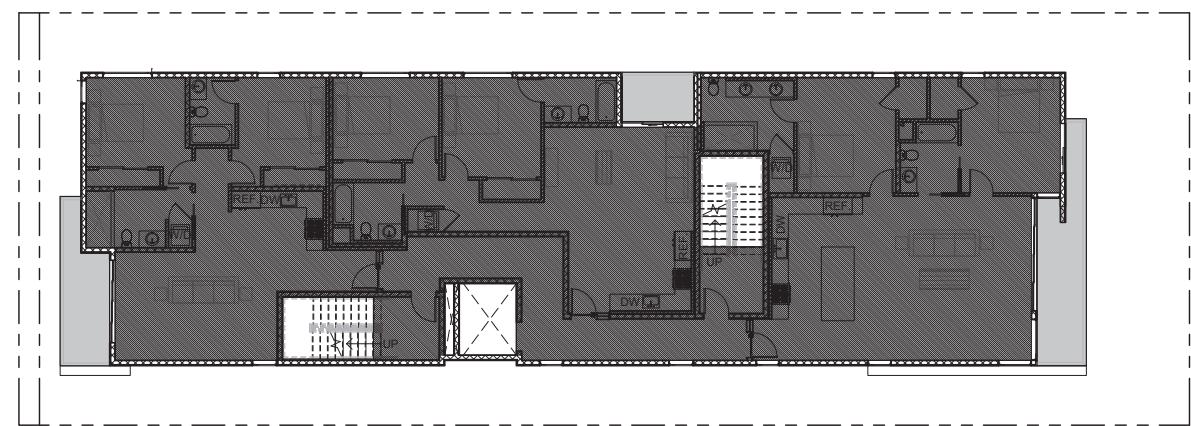
JOB # 20-A015

A0.27

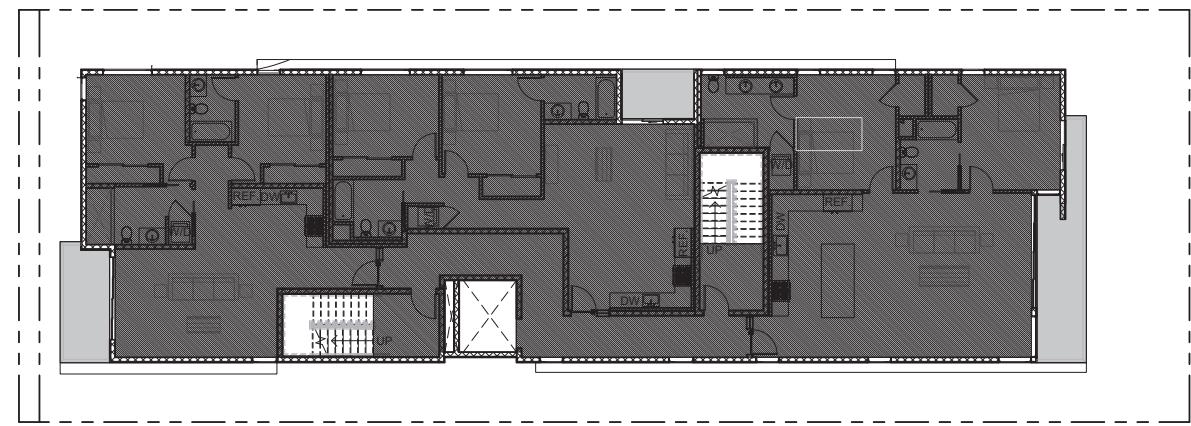
F.A.R. CALCULATIONS	
FIRST FLOOR -COVERED	N/A N/A
SECOND FLOOR -COVERED	3,467 SF 241 SF
THIRD FLOOR -COVERED	3,617 SF 253 SF
FOURTH FLOOR -COVERED	3,617 SF 282 SF
FIFTH FLOOR -COVERED	3,617 SF 296 SF
SIXTH FLOOR -INCLUDED	3,604 SF 52.5 SF
<b>TOTAL PROVIDED</b>	<b>19,046.5 SF</b>



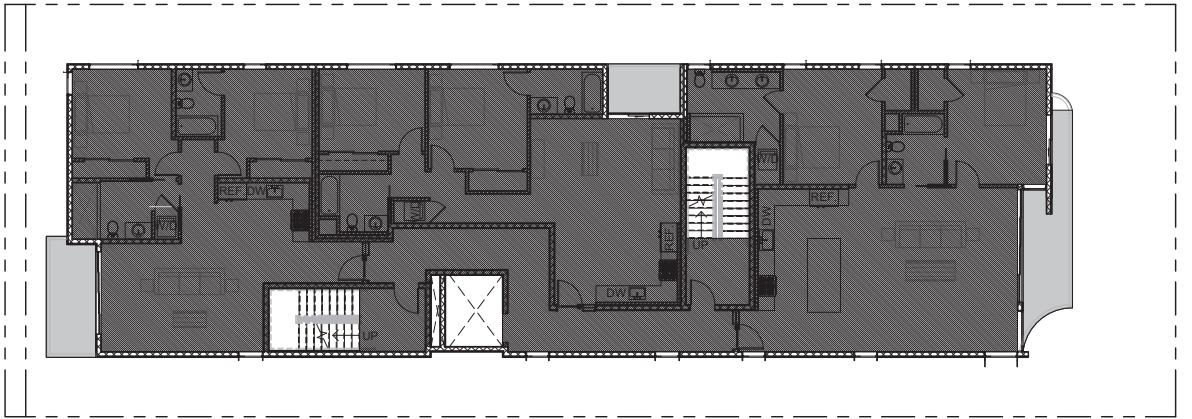
ZONING CODE - SIXTH FLOOR  
3/32" = 1'-0" 6



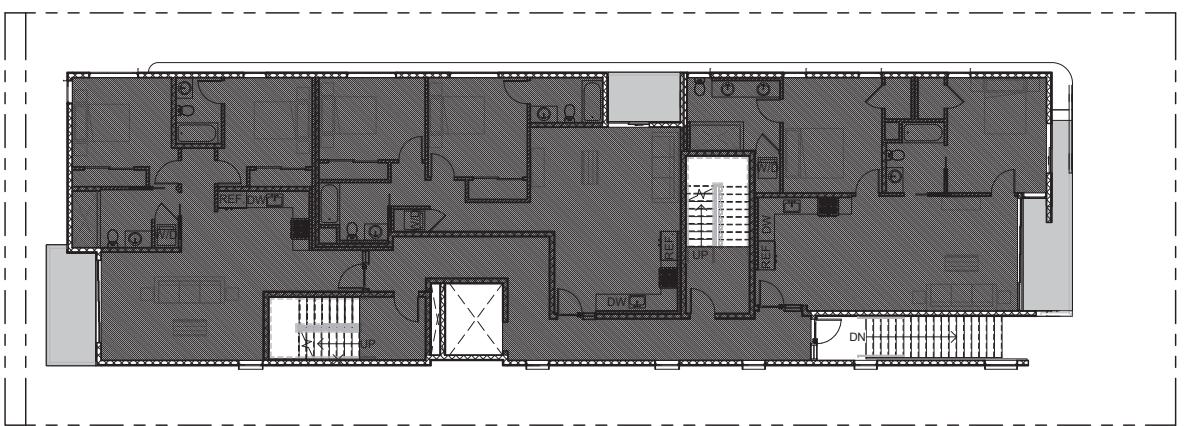
ZONING CODE - FIFTH FLOOR  
3/32" = 1'-0" 5



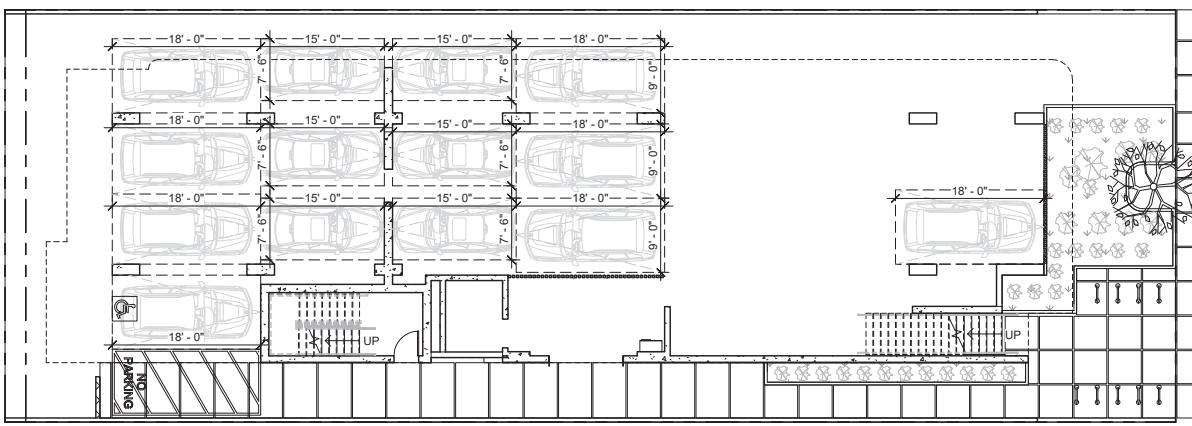
ZONING CODE - FOURTH FLOOR  
3/32" = 1'-0" 4



ZONING CODE - THIRD FLOOR  
3/32" = 1'-0" 3



ZONING CODE - SECOND FLOOR  
3/32" = 1'-0" 2



ZONING CODE - FIRST FLOOR  
3/32" = 1'-0" 1

ALLEY

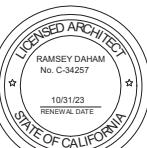
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1

# breakform design

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[o] 310.322.3700

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LOS ANGELES  
CA 90066



REVISIONS

SQUARE FOOTAGE  
BREAKDOWNS

DRAWN CR

CHECKED PNK

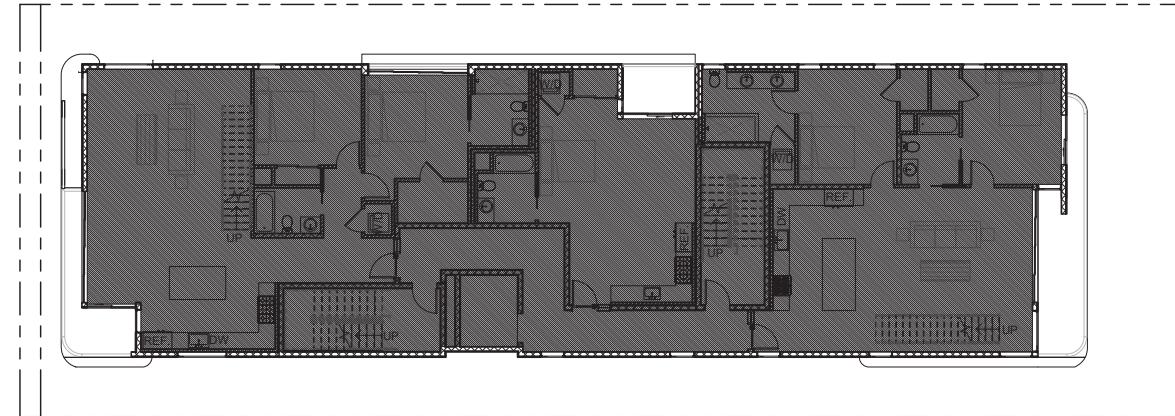
DATE 4/20/2022 10:16:27 AM

SCALE 3/32" = 1'-0"

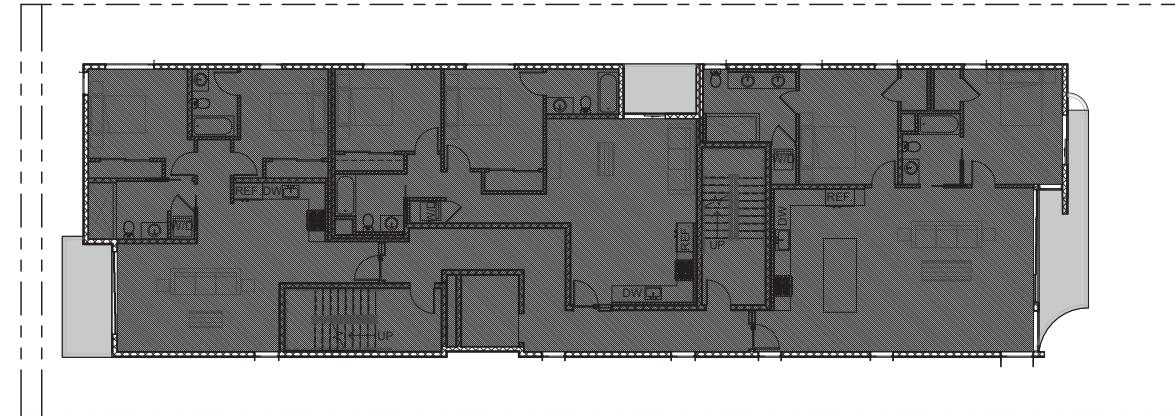
JOB # 20-A015

A0.28

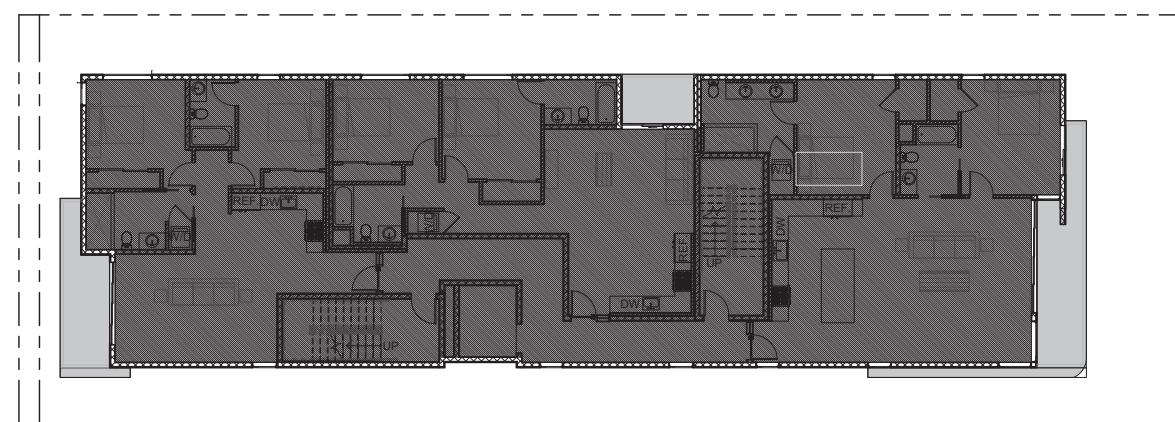
BUILDING CODE FLOOR AREA	
FIRST FLOOR	4,470 SF
SECOND FLOOR	4,102 SF
THIRD FLOOR	4,114 SF
FOURTH FLOOR	4,200 SF
FIFTH FLOOR	4,197 SF
SIXTH FLOOR	3,861 SF
<b>TOTAL PROVIDED</b>	<b>24,944 SF</b>



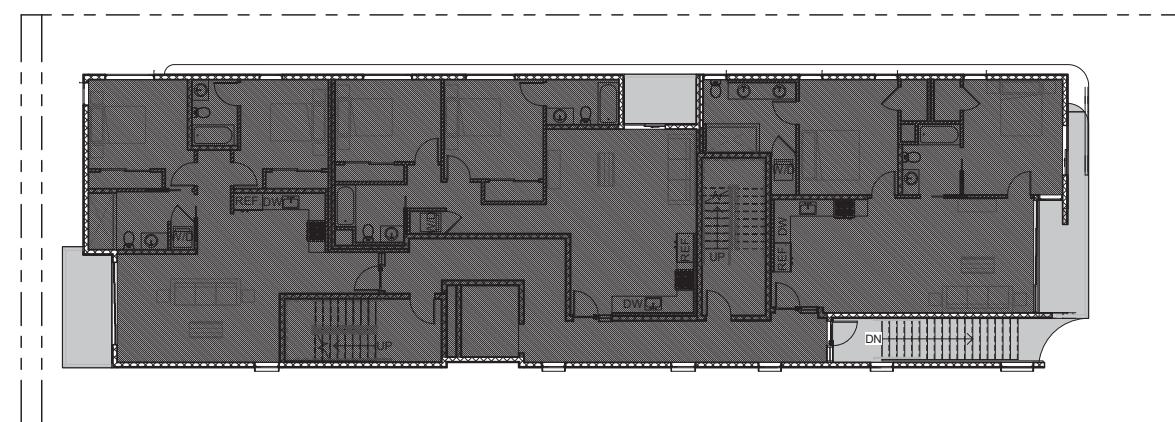
BUILDING CODE - SIXTH FLOOR  
3/32" = 1'-0" 6



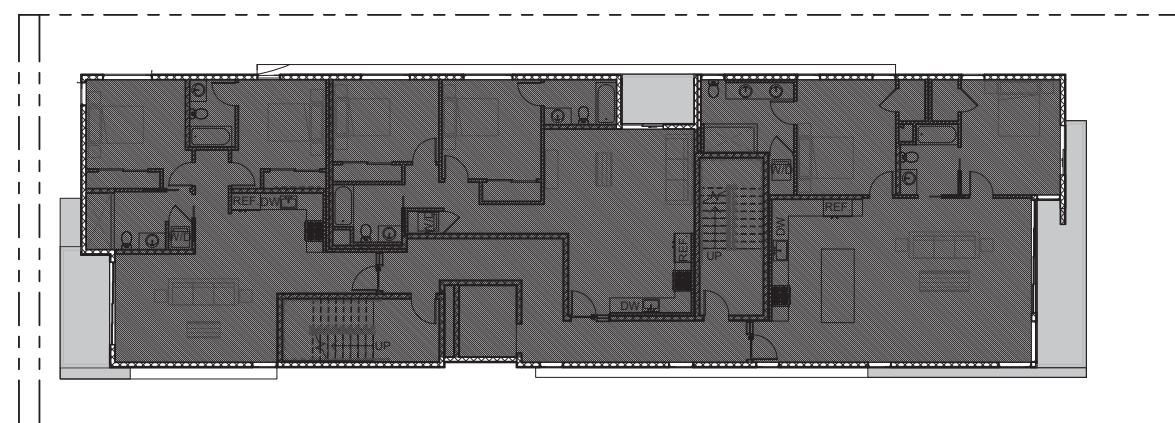
BUILDING CODE - THIRD FLOOR  
3/32" = 1'-0" 3



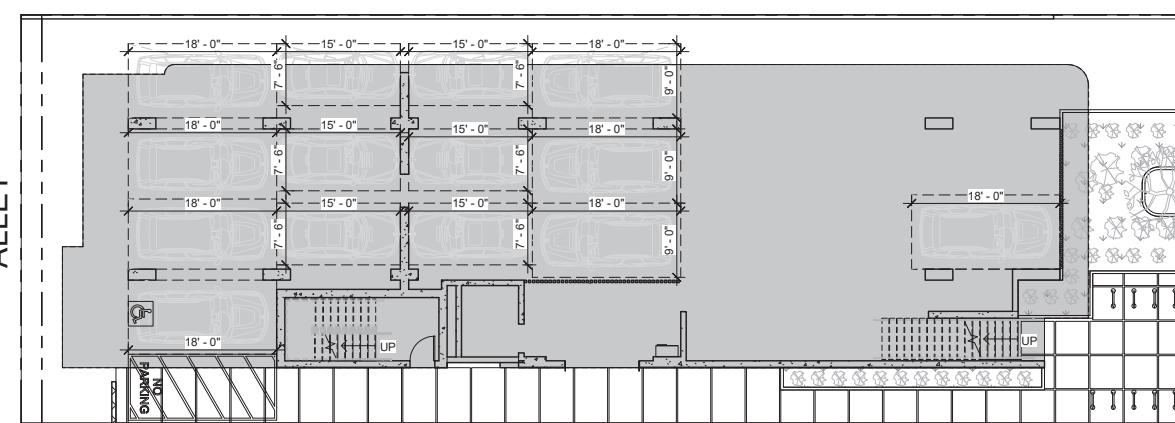
BUILDING CODE - FIFTH FLOOR  
3/32" = 1'-0" 5



BUILDING CODE - SECOND FLOOR  
3/32" = 1'-0" 2



BUILDING CODE - FOURTH FLOOR  
3/32" = 1'-0" 4

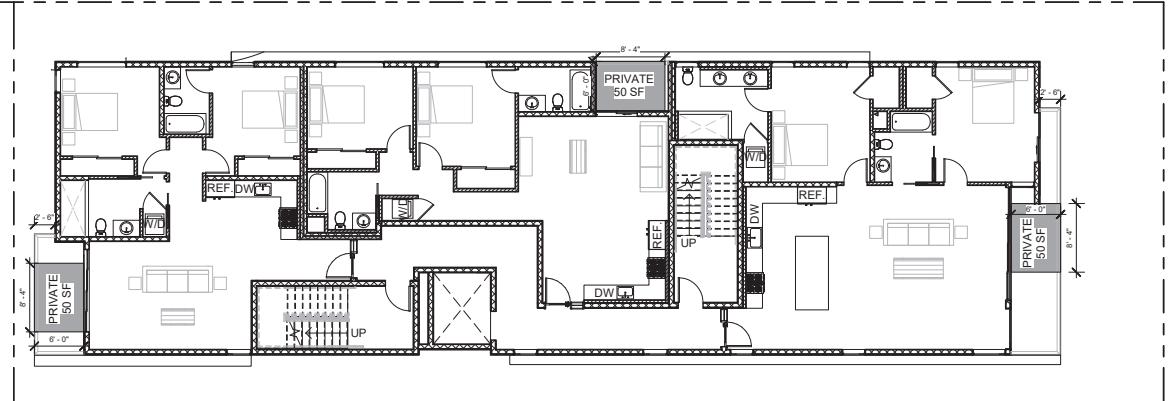


BUILDING CODE - FIRST FLOOR  
3/32" = 1'-0" 1

ALLEY

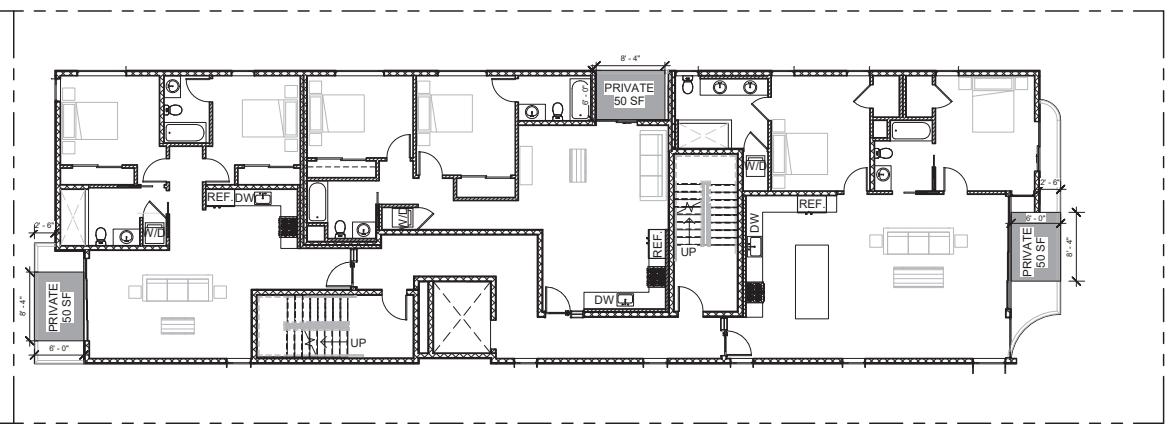
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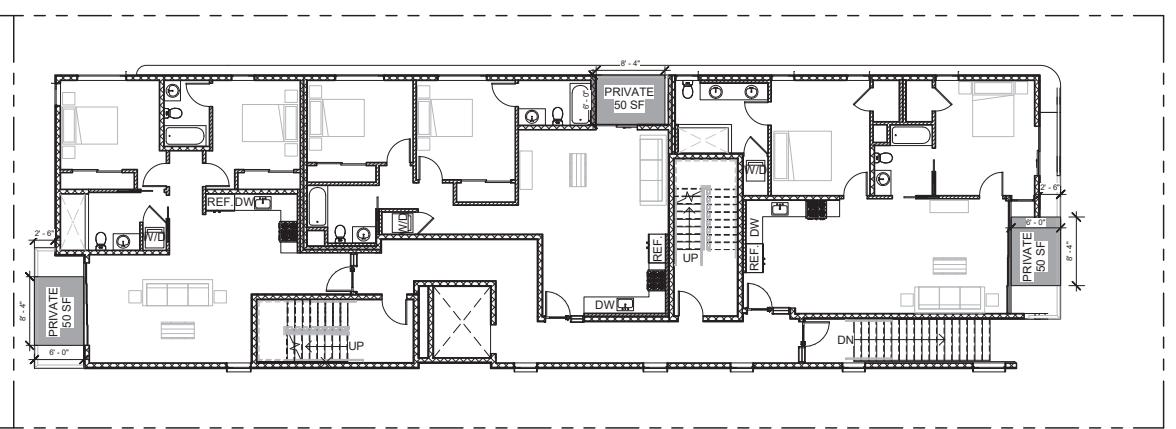
OPEN SPACE - FOURTH FLOOR  
3/32" = 1'-0"

4



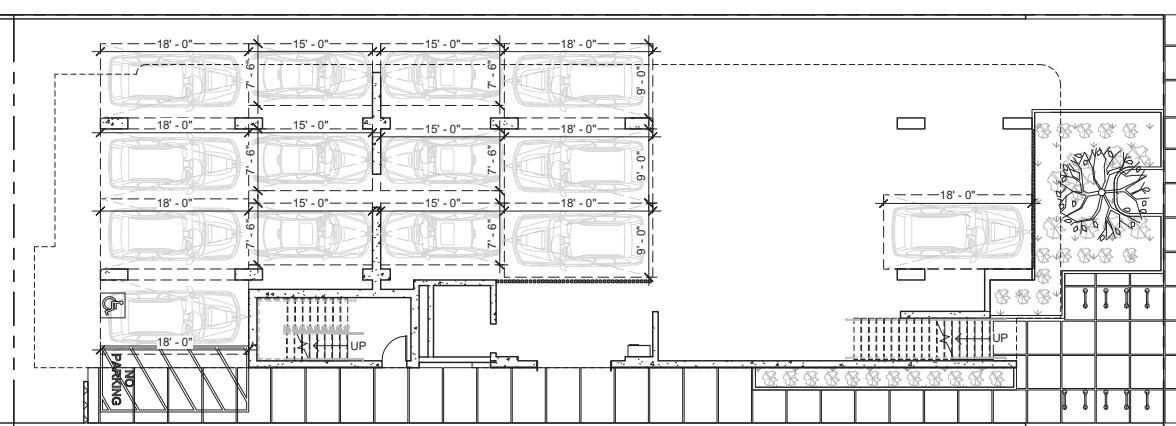
OPEN SPACE - THIRD FLOOR  
3/32" = 1'-0"

3



OPEN SPACE - SIXTH FLOOR  
3/32" = 1'-0"

6



OPEN SPACE - FIFTH FLOOR  
3/32" = 1'-0"

5

ALLEY

18'-0"

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# breakform design

109 eucalyptus drive, el segundo, ca 90245  
[o] 310.322.3700

12737 MITCHELL AVE  
LOS ANGELES  
CA 90066

## REVISIONS

2

## F.A.R. CALCULATIONS

DRAWN CR

CHECKED PNK

DATE 4/20/2022 10:17:03 AM

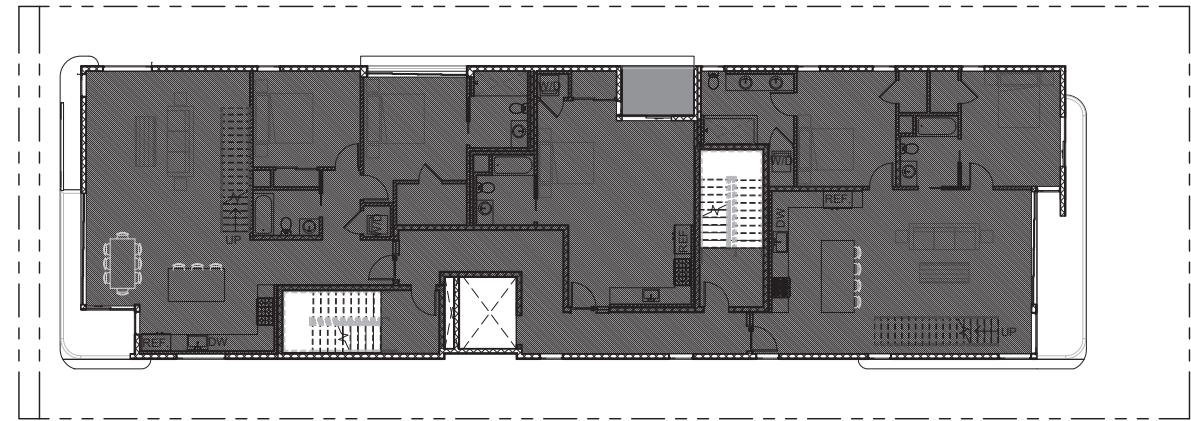
SCALE 3/32" = 1'-0"

JOB # 20-A015

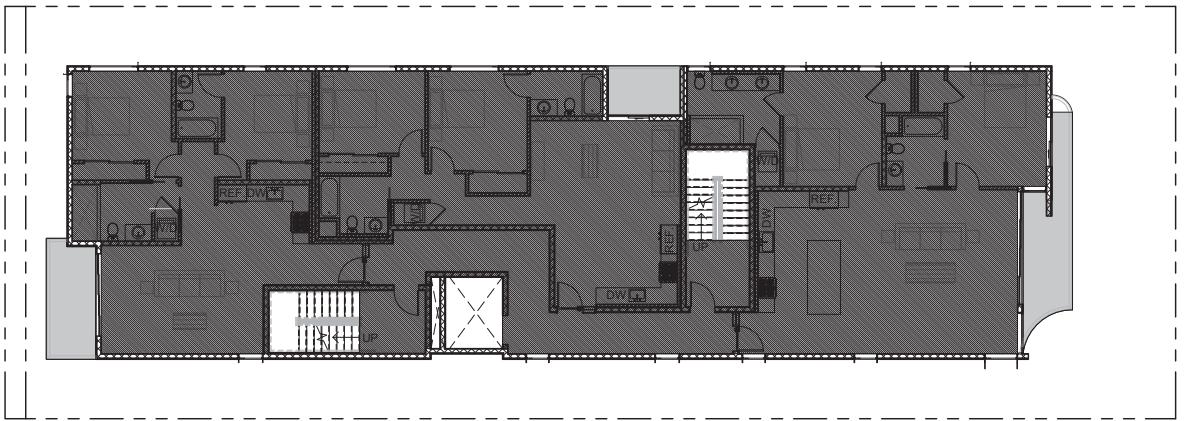
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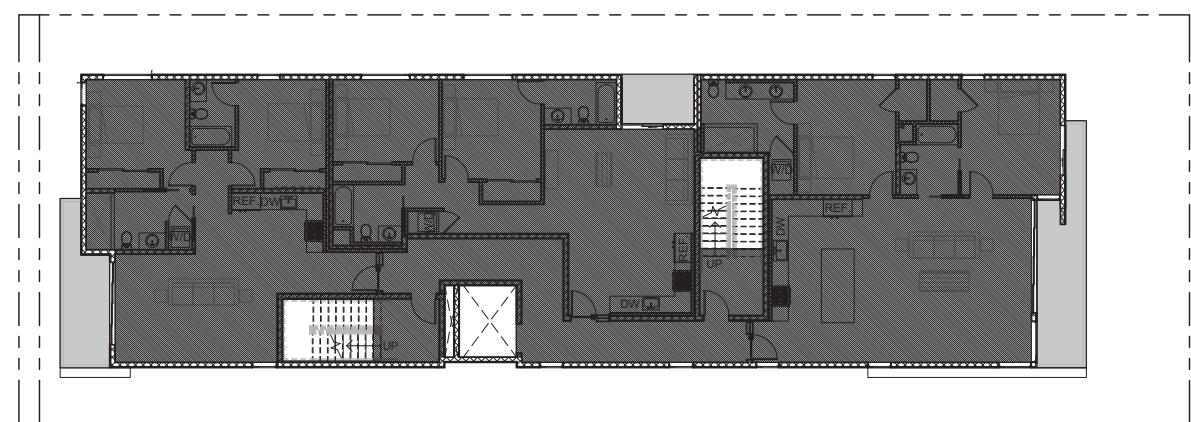
F.A.R. CALCULATIONS	
FIRST FLOOR -COVERED	N/A N/A
SECOND FLOOR -COVERED	3,467 SF 241 SF
THIRD FLOOR -COVERED	3,617 SF 253 SF
FOURTH FLOOR -COVERED	3,617 SF 282 SF
FIFTH FLOOR -COVERED	3,617 SF 296 SF
SIXTH FLOOR -INCLUDED	3,604 SF 52.5 SF
<b>TOTAL PROVIDED</b>	<b>19,046.5 SF</b>



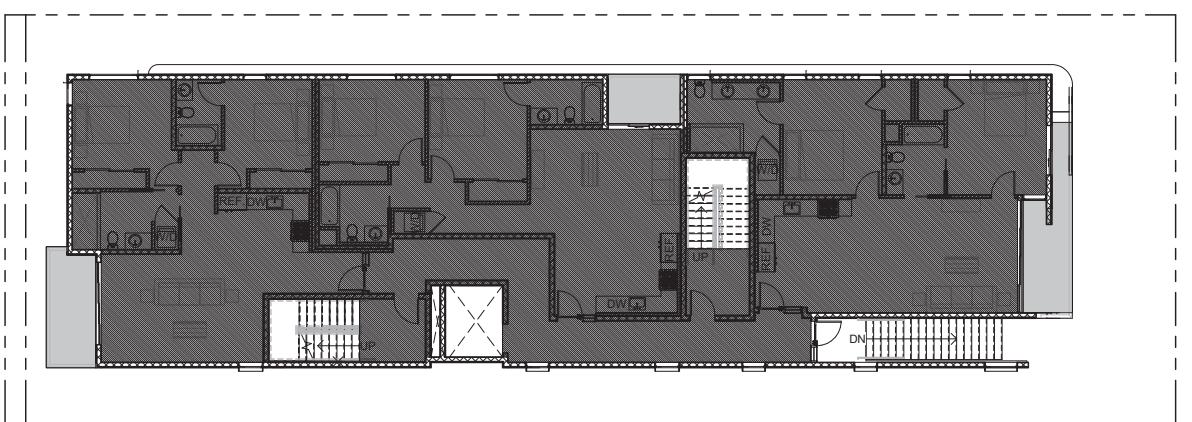
F.A.R. - SIXTH FLOOR  
3/32" = 1'-0" 6



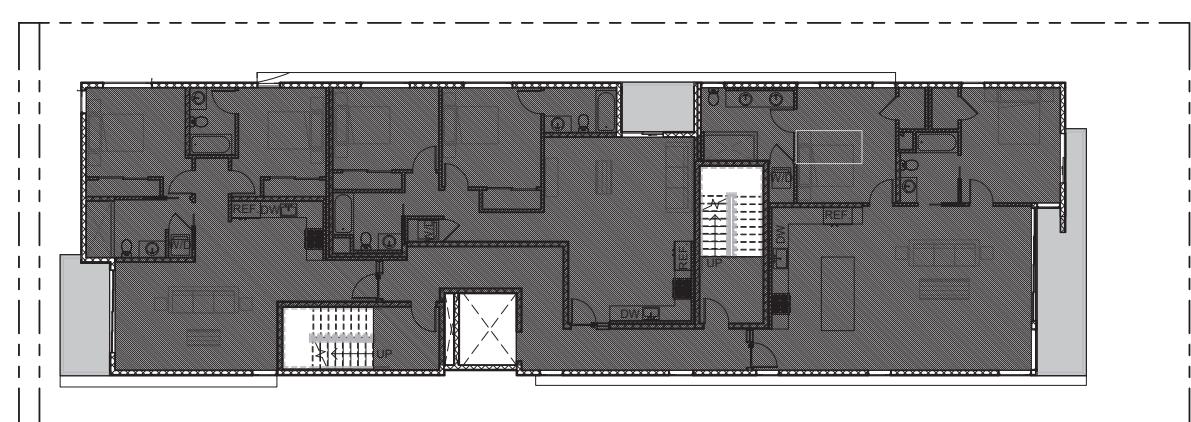
F.A.R. - THIRD FLOOR  
3/32" = 1'-0" 3



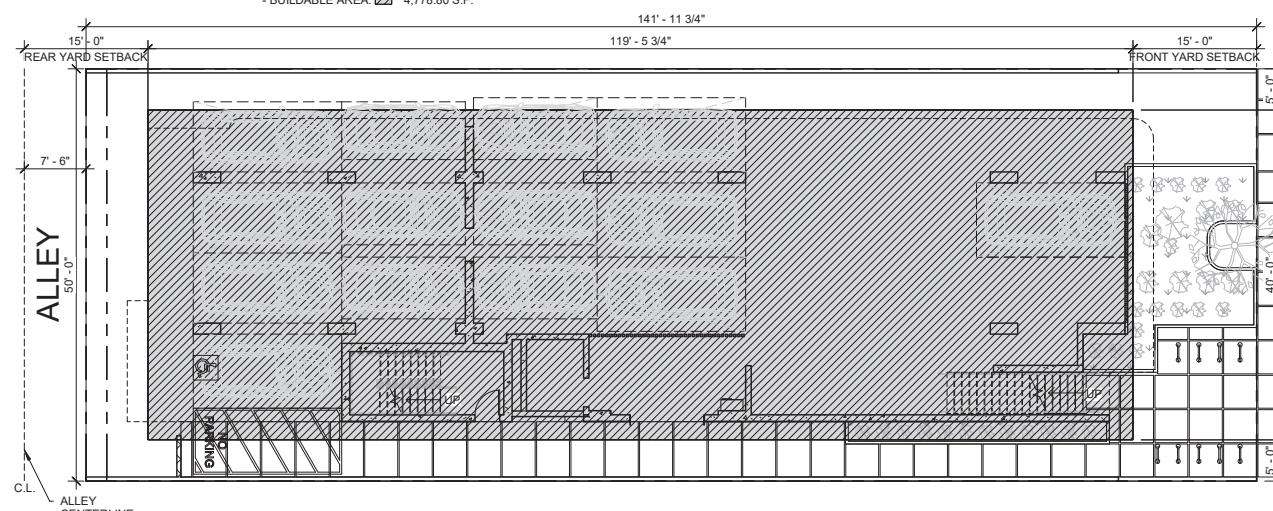
F.A.R. - FIFTH FLOOR  
3/32" = 1'-0" 5



F.A.R. - SECOND FLOOR  
3/32" = 1'-0" 2



F.A.R. - FOURTH FLOOR  
3/32" = 1'-0" 4



F.A.R. - FIRST FLOOR  
3/32" = 1'-0" 1

# breakform design

109 eucalyptus drive, el segundo, ca 90245  
[o] 310.322.3700

12737 MITCHELL AVE  
LOS ANGELES  
CA 90066

REVISIONS	
No.	Date

## PROPOSED SITE PLAN

DRAWN CR  
CHECKED PNK  
DATE 6/3/2022 1:57:16 PM  
SCALE As indicated  
JOB # 20-A015

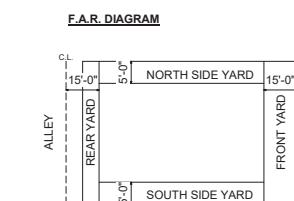
A1.00

1

	FLOOR TYPE
—	1 HR
- - -	2 HR
	SMOKE DETECTOR
	CARBON MONOXIDE
	EXHAUST FAN (50 CFM INTERMITTENT OR 35 CFM CONTINUOUS)
	STANDPIPE
	ILLUMINATED EXIT SIGN
	6' - 0" WOOD FENCE
	3' - 6" WOOD FENCE
	MASTER BEDROOM
	BEDROOM
	MASTER BATHROOM
	BATHROOM
	LIVING ROOM
	KITCHEN
	DINING ROOM
	POWDER ROOM
	CLOSET
	WALK IN CLOSET
	LAUNDRY ROOM
	BALCONY
	ENTRY

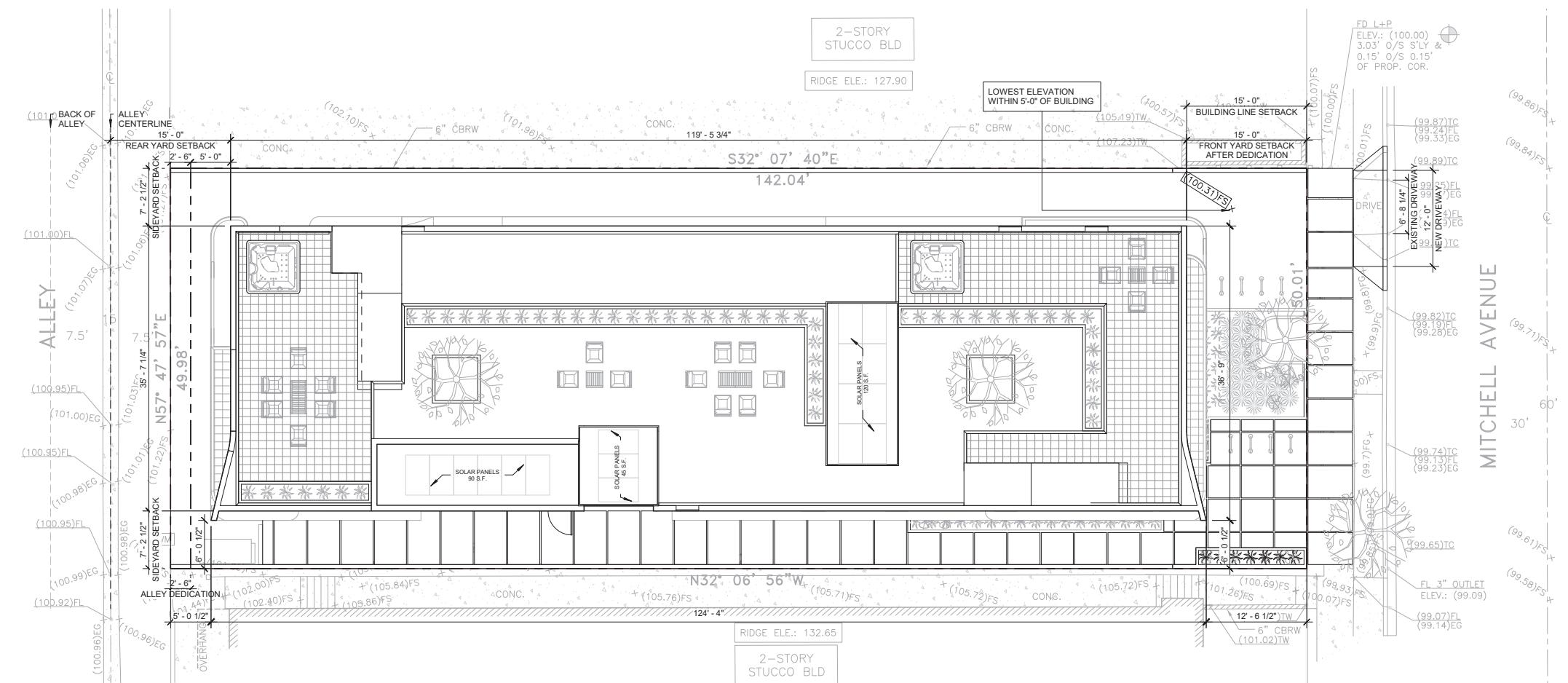
	WINDOW TAG (SEE PAGE A0.11 FOR SCHEDULE)
	DOOR TAG (SEE PAGE A0.10 FOR SCHEDULE)
	WALL TYPE
	PROPERTY LINE
	ACCESSIBLE ROUTE
	HOSE BIB
	EXTERIOR WALL (SEE WALL TYPES FOR SPECIFICS)
	INTERIOR WALL (SEE WALL TYPES FOR SPECIFICS)
	NONE STRUCTURAL INTERIOR WALL

## FLOOR PLAN LEGEND



- PERMIT A 20% DECREASE IN THE REQUIRED NORTH SIDE YARD TO ALLOW A 7.2-FOOT SIDE YARD SETBACK IN LIEU OF THE 9 FEET REQUIRED BY THE R3-1 ZONE PURSUANT TO LAMC 12.10 C3.
- PERMIT A 20% DECREASE IN THE REQUIRED SOUTH SIDE YARD TO ALLOW A 7.2-FOOT SIDE YARD SETBACK IN LIEU OF THE 9 FEET REQUIRED BY THE R3-1 ZONE PURSUANT TO LAMC 12.10 C3.

- LOT AREA: 7,100.80 S.F.  
- BUILDABLE AREA: 4,778.80 S.F.

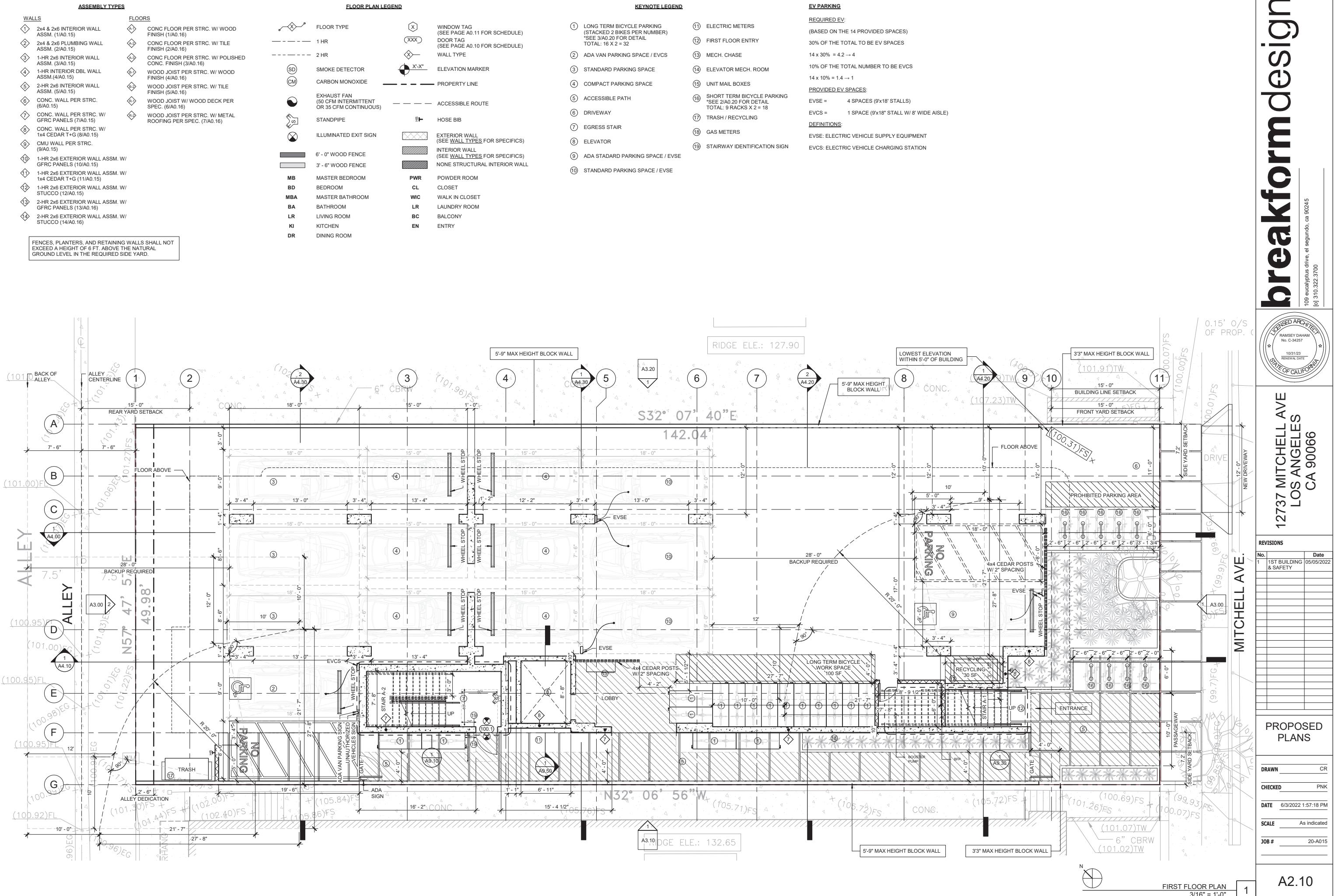


PROPOSED SITE PLAN  
1/8" = 1'-0"

1

# breakform design

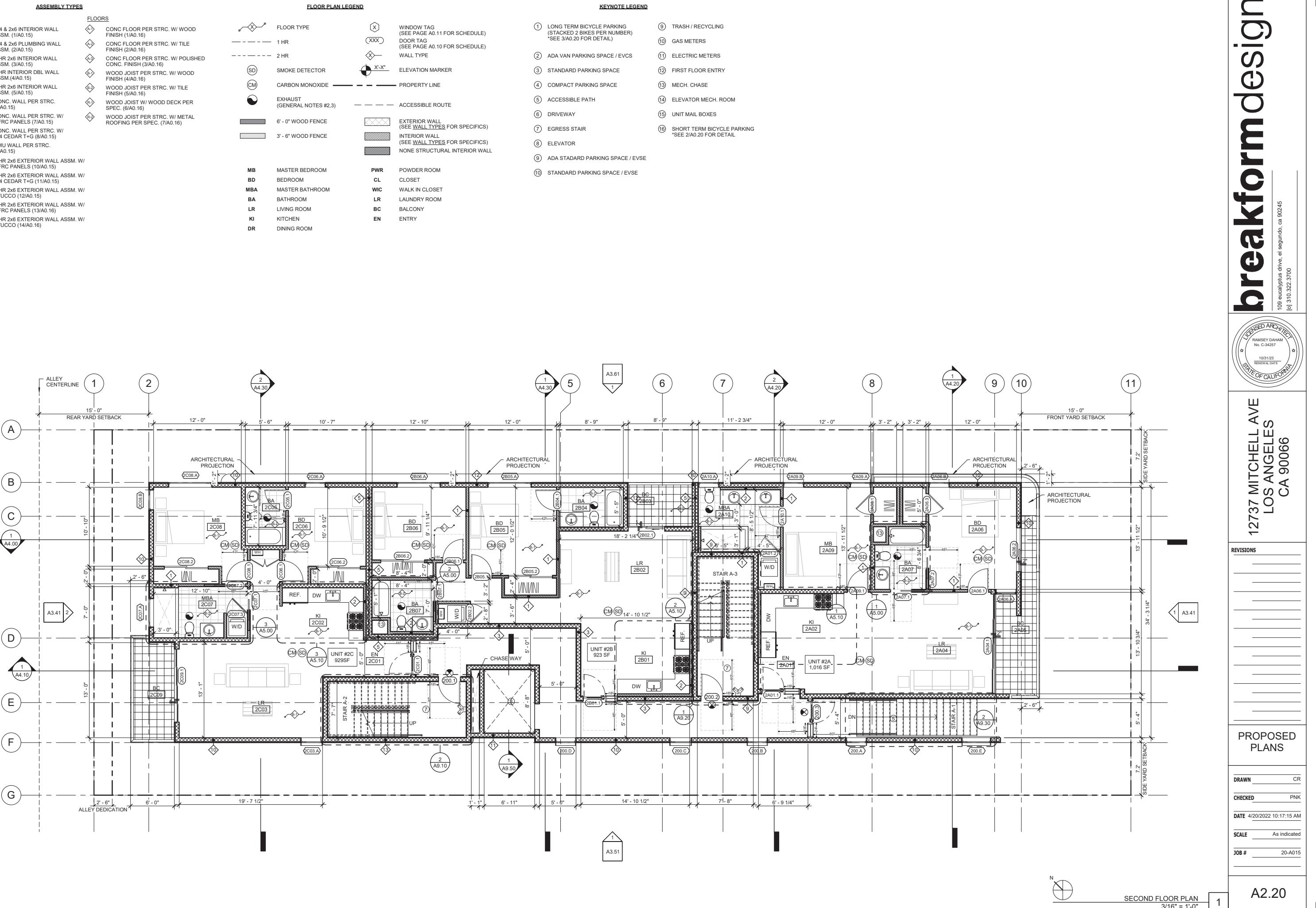
109 eucalyptus drive, el segundo, ca 90245  
[o] 310.322.3700



# breakform design

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[o] 310.322.3700

12737 MITCHELL AVE  
LOS ANGELES  
CA 90066



# breakform design

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[o] 310.322.3700

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CA 90066

## PROPOSED PLANS

DRAWN CR

CHECKED PNK

DATE 4/20/2022 10:17:21 AM

SCALE As indicated

JOB # 20-A015

1

A2.30

3/16" = 1'-0"

## ASSEMBLY TYPES

- WALLS
  - 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.15)
  - 2x4 & 2x6 PLUMBING WALL ASSM. (2/A0.15)
  - 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.15)
  - 1-HR INTERIOR DBL WALL ASSM. (4/A0.15)
  - 2-HR 2x6 INTERIOR WALL ASSM. (5/A0.15)
  - CONC. WALL PER STRC. (6/A0.15)
  - CONC. WALL PER STRC. W/ GFRC PANELS (7/A0.15)
  - CONC. WALL PER STRC. W/ 1x4 CEDAR T+G (8/A0.15)
  - CMU WALL PER STRC. (9/A0.15)
  - 1-HR 2x6 EXTERIOR WALL ASSM. W/ GFRC PANELS (10/A0.15)
  - 1-HR 2x6 EXTERIOR WALL ASSM. W/ 1x4 CEDAR T+G (11/A0.15)
  - 1-HR 2x6 EXTERIOR WALL ASSM. W/ STUCCO (12/A0.15)
  - 2-HR 2x6 EXTERIOR WALL ASSM. W/ GFRC PANELS (13/A0.16)
  - 2-HR 2x6 EXTERIOR WALL ASSM. W/ STUCCO (14/A0.16)

## FLOORS

- CONC FLOOR PER STRC. W/ WOOD FINISH (1/A0.16)
- CONC FLOOR PER STRC. W/ TILE FINISH (2/A0.16)
- CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (3/A0.16)
- WOOD JOIST PER STRC. W/ WOOD FINISH (4/A0.16)
- WOOD JOIST PER STRC. W/ TILE FINISH (5/A0.16)
- WOOD JOIST W/ WOOD DECK PER SPEC. (6/A0.16)
- WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (7/A0.16)

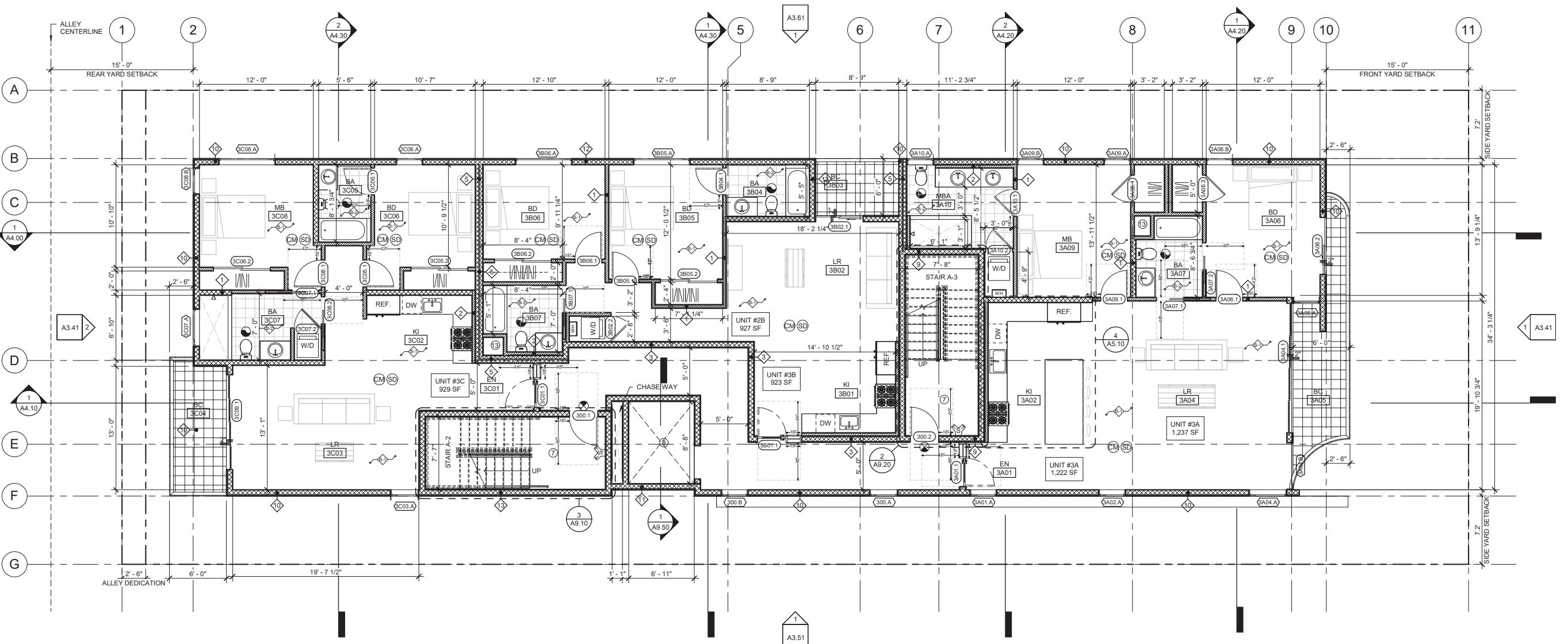
## FLOOR PLAN LEGEND

- FLOOR TYPE
  - 1 HR
  - 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST (GENERAL NOTES #2,3)
- PROPERTY LINE
- ACCESIBLE ROUTE
- 6' - 0" WOOD FENCE
- 3' - 6" WOOD FENCE
- EXTERIOR WALL (SEE WALL TYPES FOR SPECIFICS)
- INTERIOR WALL (SEE WALL TYPES FOR SPECIFICS)
- NONE STRUCTURAL INTERIOR WALL

## KEYNOTE LEGEND

- LONG TERM BIKE PARKING (STACKED 2 BIKES PER NUMBER)  
\*SEE 3/A.20 FOR DETAIL
- TRASH / RECYCLING
- DOOR TAG (SEE PAGE A0.10 FOR SCHEDULE)
- GAS METERS
- WALL TYPE
- ELEVATION MARKER
- ADA VAN PARKING SPACE / EVCS
- ELECTRIC METERS
- STANDARD PARKING SPACE
- FIRST FLOOR ENTRY
- COMPACT PARKING SPACE
- MECH. CHASE
- ACCESSIBLE PATH
- DRIVEWAY
- EGRESS STAIR
- ELEVATOR
- ADA STADARD PARKING SPACE / EVSE
- STANDARD PARKING SPACE / EVSE
- UNIT MAIL BOXES
- SHORT TERM BIKE PARKING \*SEE 2/A.20 FOR DETAIL

- MB MASTER BEDROOM
- PWR POWDER ROOM
- BD BEDROOM
- CL CLOSET
- MBA MASTER BATHROOM
- WIC WALK IN CLOSET
- BA BATHROOM
- LR LAUNDRY ROOM
- LR LIVING ROOM
- BC BALCONY
- KI KITCHEN
- EN ENTRY
- DR DINING ROOM



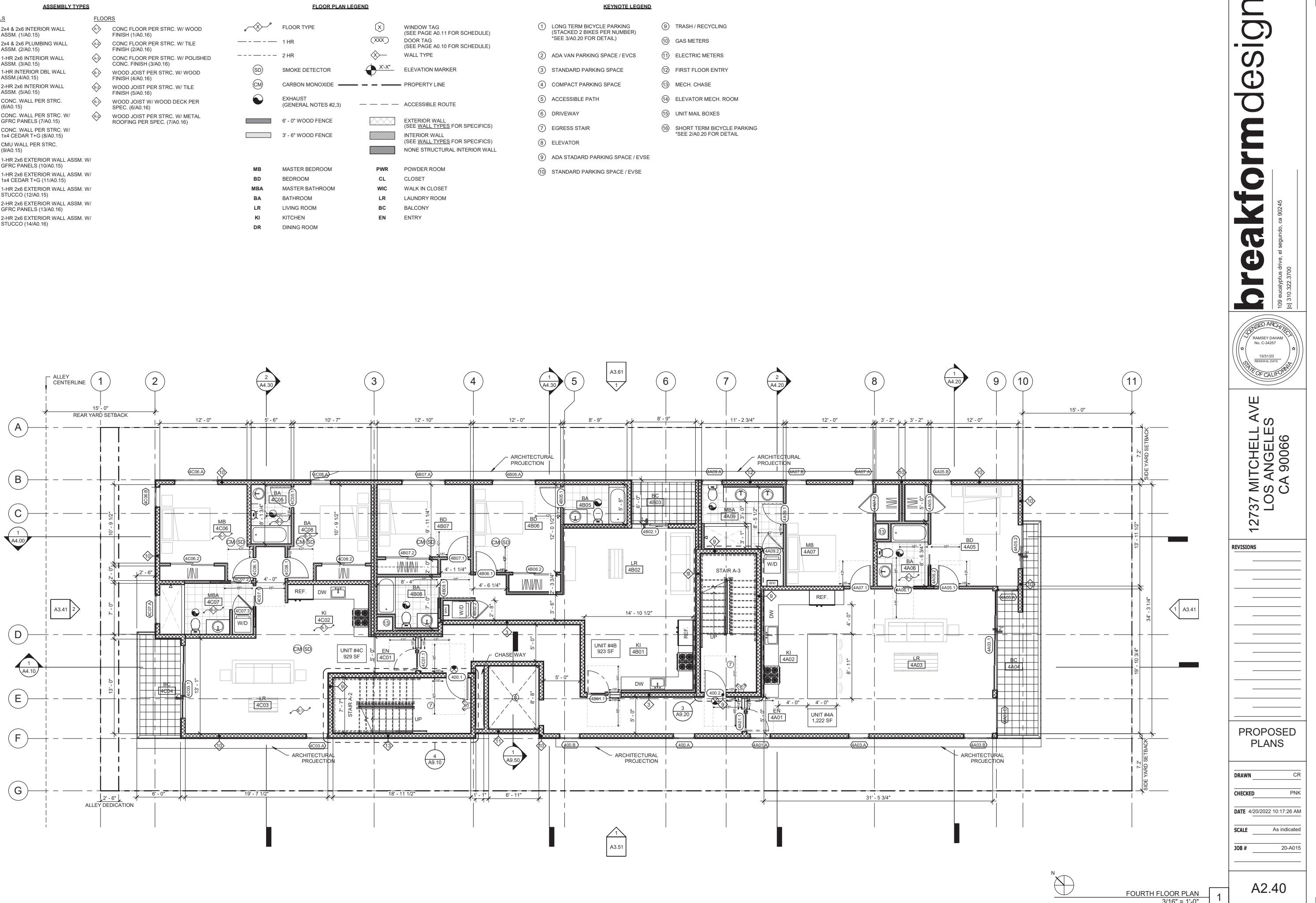
THIRD FLOOR PLAN

3/16" = 1'-0"

1

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[o] 310.322.3700



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LOS ANGELES  
CA 90066

## PROPOSED PLANS

DRAWN CR

CHECKED PNK

DATE 4/20/2022 10:17:31 AM

SCALE As indicated

JOB # 20-A015

FIFTH FLOOR PLAN  
3/16" = 1'-0"

1



A2.50

## ASSEMBLY TYPES

WALLS	FLOORS
2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.15)	CONC FLOOR PER STRC. W/ WOOD FINISH (1/A0.16)
2x4 & 2x6 PLUMBING WALL ASSM. (2/A0.15)	CONC FLOOR PER STRC. W/ TILE FINISH (2/A0.16)
1-HR 2x6 INTERIOR WALL ASSM. (3/A0.15)	CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (3/A0.16)
1-HR INTERIOR DBL WALL ASSM. (4/A0.15)	WOOD JOIST PER STRC. W/ WOOD FINISH (4/A0.16)
2-HR 2x6 INTERIOR WALL ASSM. (5/A0.15)	WOOD JOIST PER STRC. W/ TILE FINISH (5/A0.16)
CONC. WALL PER STRC. (6/A0.15)	WOOD JOIST W/ WOOD DECK PER SPEC. (6/A0.16)
CONC. WALL PER STRC. W/ GFRC PANELS (7/A0.15)	WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (7/A0.16)
CONC. WALL PER STRC. W/ 1x4 CEDAR T+G (8/A0.15)	
CMU WALL PER STRC. (9/A0.15)	
1-HR 2x6 EXTERIOR WALL ASSM. W/ GFRC PANELS (10/A0.15)	
1-HR 2x6 EXTERIOR WALL ASSM. W/ 1x4 CEDAR T+G (11/A0.15)	
1-HR 2x6 EXTERIOR WALL ASSM. W/ STUCCO (12/A0.15)	
2-HR 2x6 EXTERIOR WALL ASSM. W/ GFRC PANELS (13/A0.16)	
2-HR 2x6 EXTERIOR WALL ASSM. W/ STUCCO (14/A0.16)	

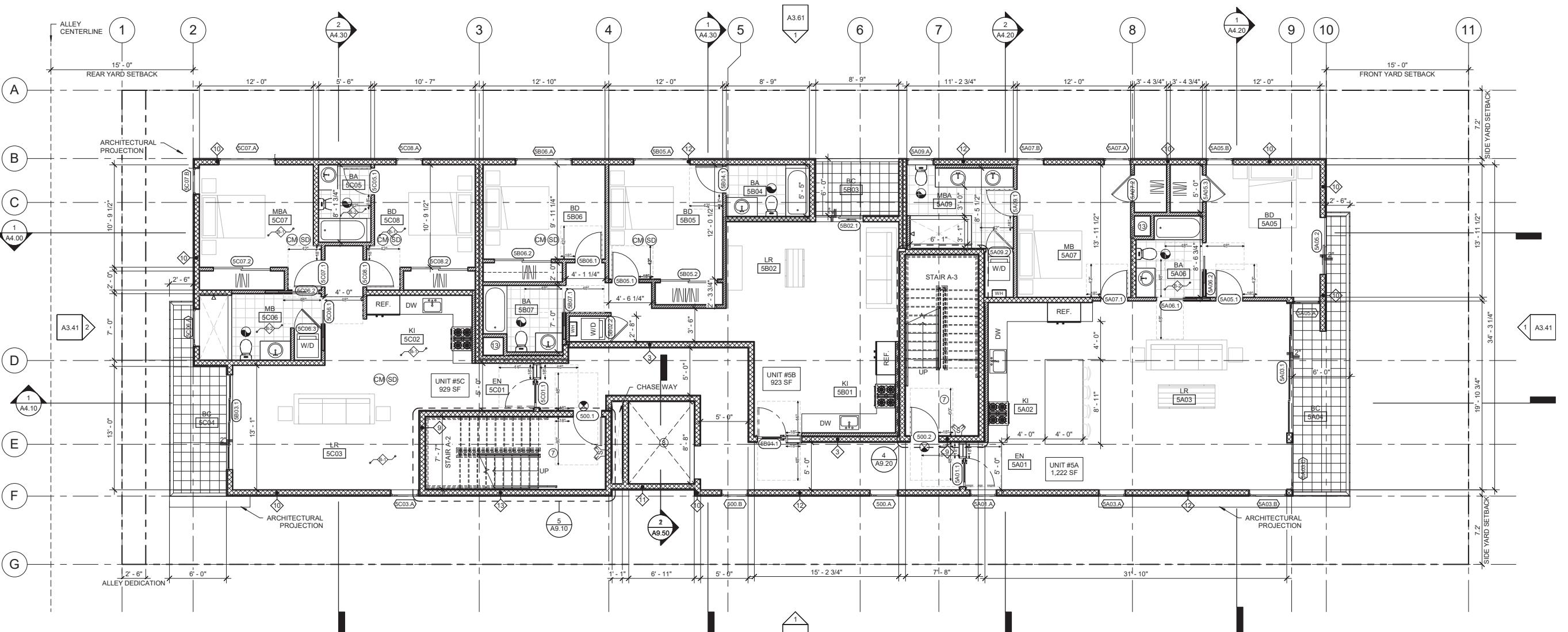
## FLOOR PLAN LEGEND

	FLOOR TYPE
	1 HR
	2 HR
	SMOKE DETECTOR
	CARBON MONOXIDE
	EXHAUST (GENERAL NOTES #2,3)
	6' - 0" WOOD FENCE
	3' - 6" WOOD FENCE

## KEYNOTE LEGEND

	WINDOW TAG (SEE PAGE A0.11 FOR SCHEDULE)
	DOOR TAG (SEE PAGE A0.10 FOR SCHEDULE)
	WALL TYPE
	ELEVATION MARKER
	PROPERTY LINE
	ACCESSIBLE ROUTE
	EXTERIOR WALL (SEE WALL TYPES FOR SPECIFICS)
	INTERIOR WALL (SEE WALL TYPES FOR SPECIFICS)
	NONE STRUCTURAL INTERIOR WALL

MB	MASTER BEDROOM
BD	BEDROOM
MBA	MASTER BATHROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER ROOM
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY



1

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CA 90066

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LOS ANGELES  
CA 90066



## ASSEMBLY TYPES

WALLS	FLOORS
① 2x4 & 2x6 INTERIOR WALL ASSM. (1/A.0.15)	④ CONC FLOOR PER STRC. W/ WOOD FINISH (1/A.0.16)
② 2x4 & 2x6 PLUMBING WALL ASSM. (2/A.0.15)	⑤ CONC FLOOR PER STRC. W/ TILE FINISH (2/A.0.16)
③ 1-HR 2x6 INTERIOR WALL ASSM. (3/A.0.15)	⑥ CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (3/A.0.16)
④ 1-HR INTERIOR DBL WALL ASSM. (4/A.0.15)	⑦ WOOD JOIST PER STRC. W/ WOOD FINISH (4/A.0.16)
⑤ 2-HR 2x6 INTERIOR WALL ASSM. (5/A.0.15)	⑧ WOOD JOIST PER STRC. W/ TILE FINISH (5/A.0.16)
⑥ CONC. WALL PER STRC. (6/A.0.15)	⑨ WOOD JOIST W/ WOOD DECK PER SPEC. (6/A.0.16)
⑦ CONC. WALL PER STRC. W/ GFRC PANELS (7/A.0.15)	⑩ WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (7/A.0.16)
⑧ CONC. WALL PER STRC. W/ 1x4 CEDAR T+G (8/A.0.15)	
⑨ CMU WALL PER STRC. (9/A.0.15)	
⑩ 1-HR 2x6 EXTERIOR WALL ASSM. W/ GFRC PANELS (10/A.0.15)	
⑪ 1-HR 2x6 EXTERIOR WALL ASSM. W/ 1x4 CEDAR T+G (11/A.0.15)	
⑫ 1-HR 2x6 EXTERIOR WALL ASSM. W/ STUCCO (12/A.0.15)	
⑬ 2-HR 2x6 EXTERIOR WALL ASSM. W/ GFRC PANELS (13/A.0.16)	
⑭ 2-HR 2x6 EXTERIOR WALL ASSM. W/ STUCCO (14/A.0.16)	

## FLOORS

- CONC FLOOR PER STRC. W/ WOOD FINISH (1A/0.16)
- CONC FLOOR PER STRC. W/ TILE FINISH (2A/0.16)
- CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (3A/0.16)
- WOOD JOIST PER STRC. W/ WOOD FINISH (4A/0.16)
- WOOD JOIST PER STRC. W/ TILE FINISH (5A/0.16)
- WOOD JOIST W/ WOOD DECK PER SPEC. (6A/0.16)
- WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (7A/0.16)

#### FLOOR PLAN LEGEND

**FLOOR TYPE**

**WINDOW TAG  
(SEE PAGE A0.11 FOR SCHEDULE)**

**DOOR TAG  
(SEE PAGE A0.10 FOR SCHEDULE)**

**WALL TYPE**

**SMOKE DETECTOR**

**CARBON MONOXIDE**

**ELEVATION MARKER**

**PROPERTY LINE**

**EXHAUST  
(GENERAL NOTES #2,3)**

**ACCESSIBLE ROUTE**

**6' - 0" WOOD FENCE**

**3' - 6" WOOD FENCE**

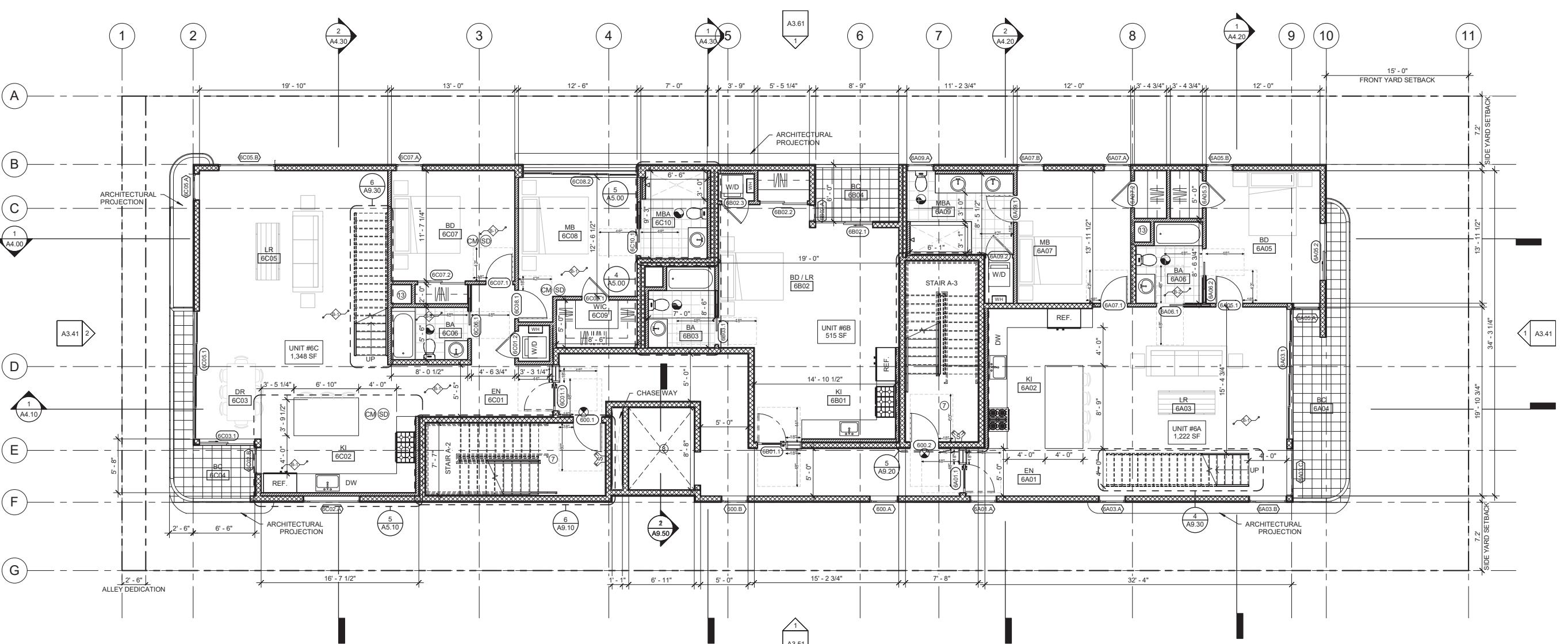
**EXTERIOR WALL  
(SEE WALL TYPES FOR SPEC)**

**INTERIOR WALL  
(SEE WALL TYPES FOR SPEC)**

**NONE STRUCTURAL INTERIOR**

KEYNOTE LEG

<ol style="list-style-type: none"> <li>① LONG TERM BICYCLE PARKING (STACKED 2 BIKES PER NUMBER) *SEE 3/A0.20 FOR DETAIL</li> <li>② ADA VAN PARKING SPACE / EVCS</li> <li>③ STANDARD PARKING SPACE</li> <li>④ COMPACT PARKING SPACE</li> <li>⑤ ACCESSIBLE PATH</li> <li>⑥ DRIVEWAY</li> <li>⑦ EGRESS STAIR</li> <li>⑧</li> </ol>	<ol style="list-style-type: none"> <li>⑨ TRASH / RECYCLING</li> <li>⑩ GAS METERS</li> <li>⑪ ELECTRIC METERS</li> <li>⑫ FIRST FLOOR ENTRY</li> <li>⑬ MECH. CHASE</li> <li>⑭ ELEVATOR MECH. ROOM</li> <li>⑮ UNIT MAIL BOXES</li> <li>⑯ SHORT TERM BICYCLE PARKING *SEE 2/A0.20 FOR DETAIL</li> </ol>
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**SIXTH FLOOR PLAN**  
3/16" = 1'-0"

A2.60





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Architectural elevation diagram showing the height of the building from the first floor to the top of the guardrail. The diagram includes horizontal lines for each floor plan and a vertical line for the height. Labels indicate the top of each floor plan and the top of the guardrail. The height is marked as 9'-11 1/2".

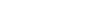
Labels from top to bottom:

- TOP OF ELEVATOR ACCESS
- TOP OF STAIR ACCESS
- TOP OF GUARDRAIL
- ROOF PLAN 64' - 2"
- SIXTH FLOOR PLAN 52' - 6"
- FIFTH FLOOR PLAN 42' - 2 1/4"
- FOURTH FLOOR PLAN 31' - 10 1/2"
- THIRD FLOOR PLAN 21' - 6 3/4"
- SECOND FLOOR 11' - 9"
- FIRST FLOOR

Vertical height dimension: 9'-11 1/2"

**SOUTH ELEVATION**

#### ELEVATION LEGEND

				GFRC PANELS SEE A.05 FOR SPECS		PROPERTY
			CEDAR T+G 1x6 (CLEAR VERTICAL GRAIN W/ CLEAR COAT) SEE A.05 FOR SPECS		1 HR	
			STUCCO (LIGHT GRAY) SEE A.05 FOR SPECS		2 HR	
	DARK GRAY METAL		WINDOW TAG			
	BISCOTTI COLORED METAL		DOOR TAG			
	X-X'	ELEVATION				

This architectural floor plan illustrates the layout of a building across six floors, from the First Floor to the Sixth Floor. The plan includes detailed room dimensions, setbacks, and access points. Key features include:

- Setbacks:** R.Y. SETBACK (15'-0") and F.Y. SETBACK (15'-0") are indicated on the left and right respectively.
- Access Points:** Points 1 through 11 are marked along the top edge, with specific elevations for elevator and stair access.
- Rooms and Units:** Various rooms and units are labeled with codes such as 6C03.1, C02.A, 6C03.3, 4C03.A, 6C03.A, 200.D, 300.A, 400.A, 500.A, 600.B, 600.A, 500.B, 400.B, 200.C, 200.B, 200.A, 200.E, 3A01.A, 3A03.A, 3A03.B, 3A05.A, 3A05.B, 3A04.A, and 3A03.E.
- Floor Levels:** The plan shows the height of the building above grade, with the ROOF PLAN at 64'-2" and the GROUND LEVEL at 6'-0".
- Vertical Dimensions:** Vertical dimensions are provided for each floor, ranging from 11'-9" for the First Floor to 13'-5 1/2" for the Sixth Floor.
- Alley Dedication:** An ALLEY DEDICATION of 2'-6" is shown on the left side.
- Lowest Point:** The LOWEST POINT is indicated as 6'-0" within 5' of the building.
- Plots:** P.L. plots are shown at the bottom of the plan.

ALLEY  
21

1

**SOUTH ELEVATION**

A3.10

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ELEVATIONS

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JOB # 20-A015

ALLEY C.L.

NORTH ELEVATION

3/16" = 1'-0"

1

TAG

ELEVATION LEGEND

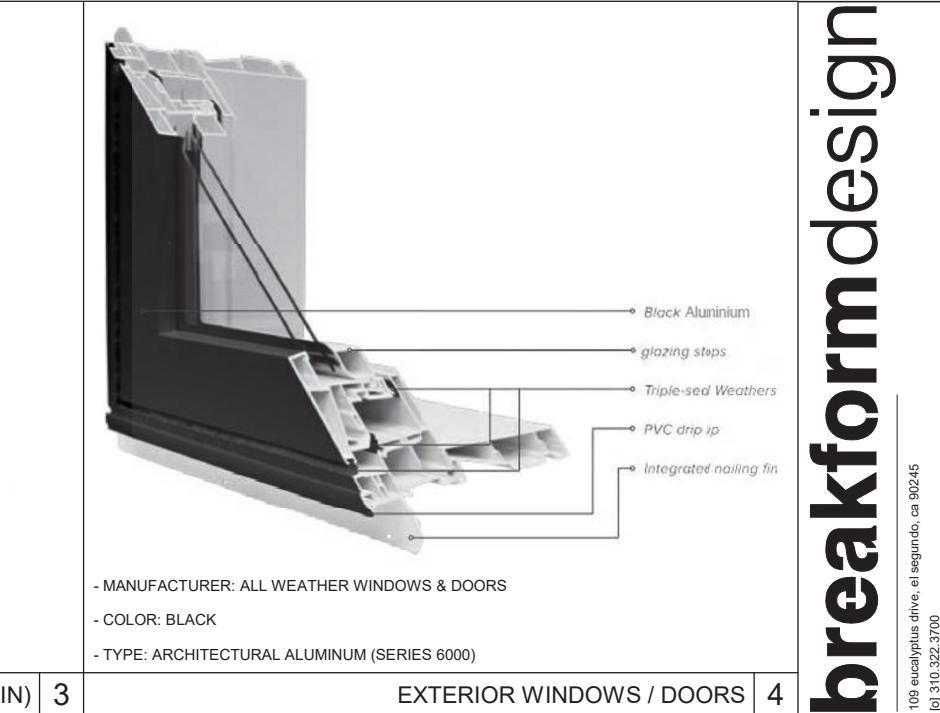
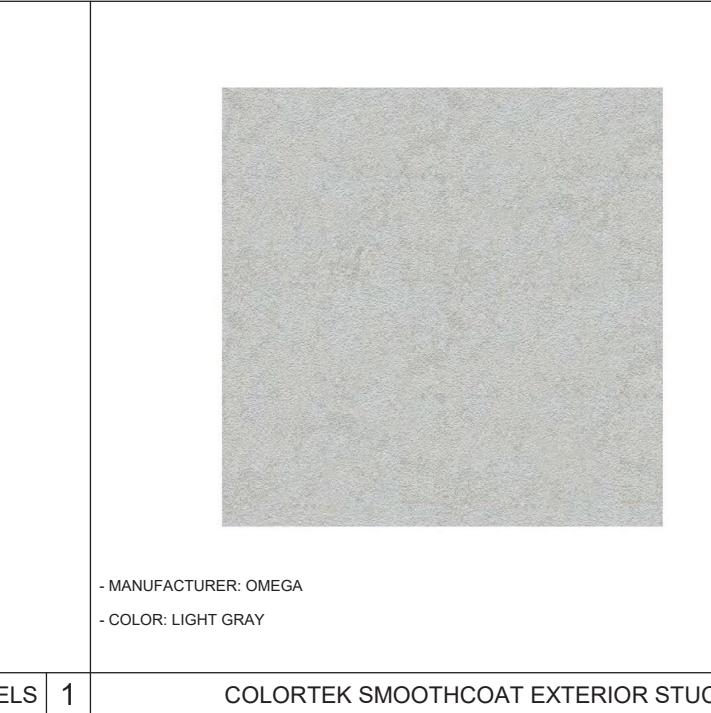
7	13	GFRC PANELS SEE A.05 FOR SPECS	PROPERTY LINE (PL)
8	11	CEDAR T+G 1x6 (CLEAR VERTICAL GRAIN W/ CLEAR COAT) SEE A.05 FOR SPECS	1 HR
9	14	STUCCO (LIGHT GRAY) SEE A.05 FOR SPECS	2 HR
10		WINDOW TAG	
11		DARK GRAY METAL	
12		BISCOTTI COLORED METAL	
13		X-X" ELEVATION MARKER	



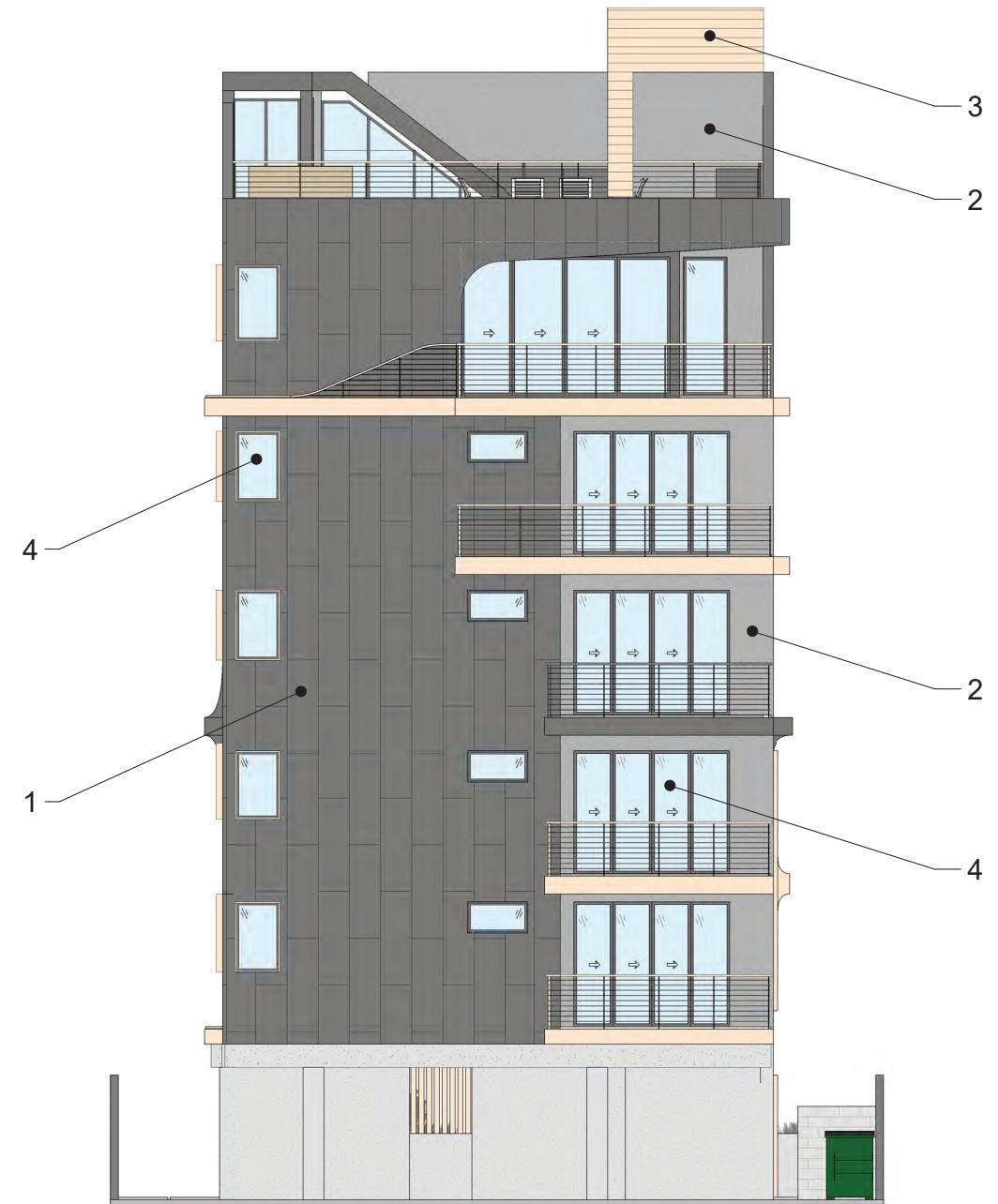


# breakform design

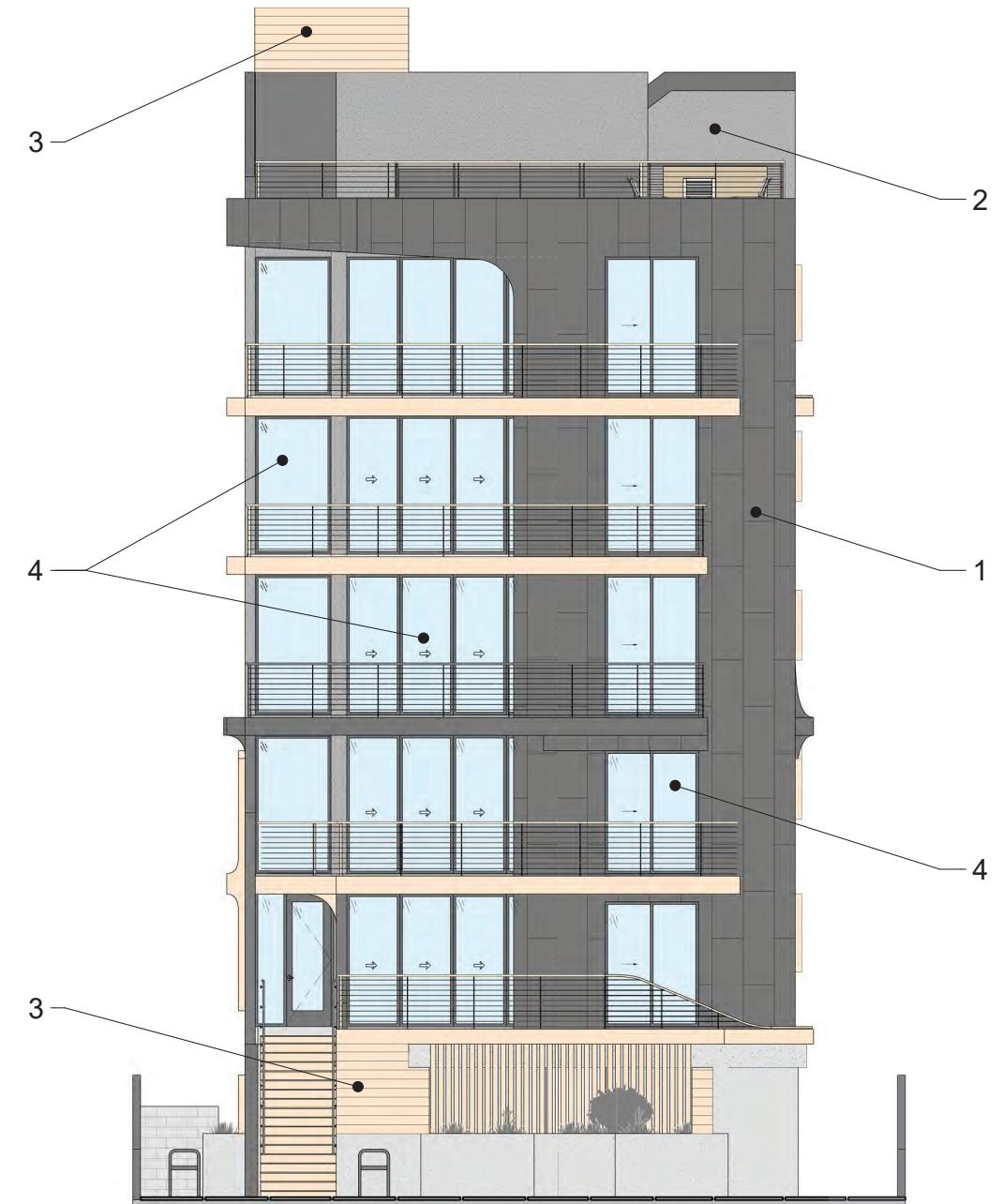
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GFRC PANELS	1	COLORTEK SMOOTHCOAT EXTERIOR STUCCO	2	CEDAR T+G 1x6 (CLEAR VERTICAL GRAIN)	3	EXTERIOR WINDOWS / DOORS	4
-------------	---	-------------------------------------	---	--------------------------------------	---	--------------------------	---



WEST ELEVATION MATERIALS  
3/16" = 1'-0"



EAST ELEVATION MATERIALS  
3/16" = 1'-0"



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REVISIONS  
[REVISIONS]

ELEVATION  
MATERIALS

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DATE 4/20/2022 10:11:15 AM

SCALE 3/16" = 1'-0"

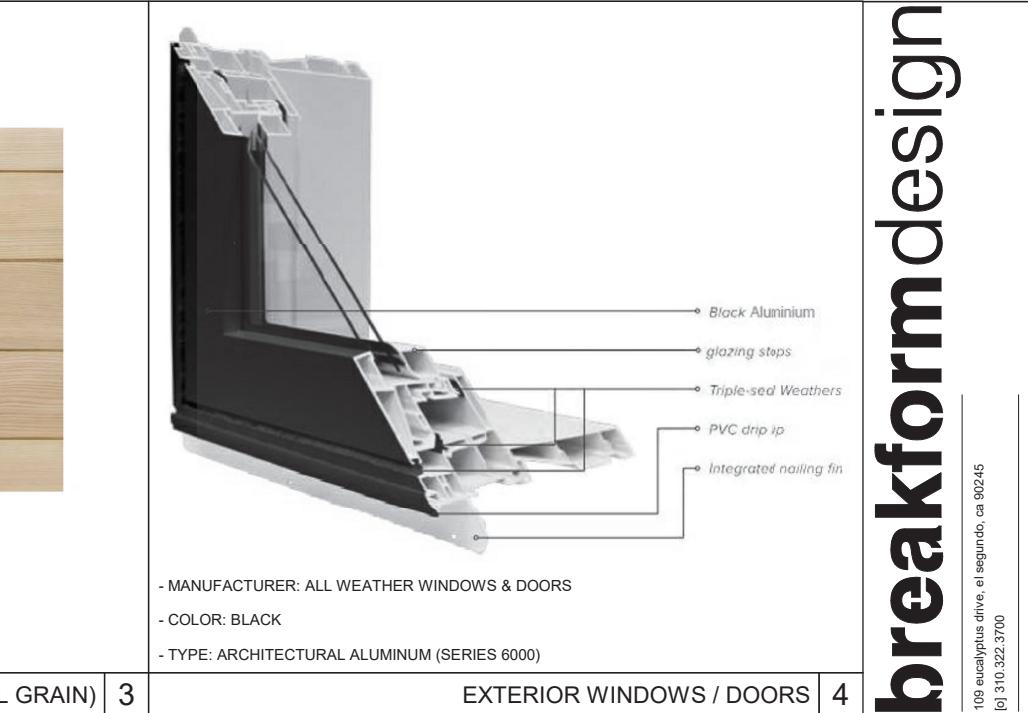
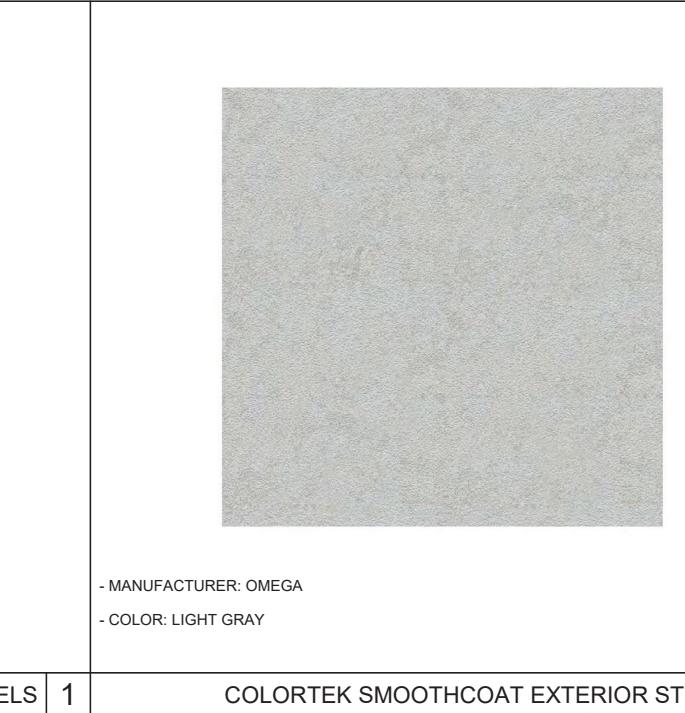
JOB # 20-A015

A3.41



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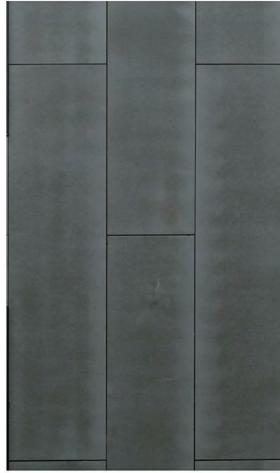
GFRC PANELS	1	COLORTEK SMOOTHCOAT EXTERIOR STUCCO	2	CEDAR T+G 1x6 (CLEAR VERTICAL GRAIN)	3	EXTERIOR WINDOWS / DOORS	4
-------------	---	-------------------------------------	---	--------------------------------------	---	--------------------------	---





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- MANUFACTURER: RIEDER  
- COLOR: GRAY  
- ORIENTATION: VERTICAL



- MANUFACTURER: OMEGA  
- COLOR: LIGHT GRAY



- COLOR: CLEAR COAT  
- ORIENTATION: HORIZONTAL



- MANUFACTURER: ALL WEATHER WINDOWS & DOORS  
- COLOR: BLACK  
- TYPE: ARCHITECTURAL ALUMINUM (SERIES 6000)

GFRC PANELS

1

COLORTEK SMOOTHCOAT EXTERIOR STUCCO

2

CEDAR T+G 1x6 (CLEAR VERTICAL GRAIN)

3

EXTERIOR WINDOWS / DOORS

4

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REVISIONS

ELEVATION  
MATERIALS

DRAWN CR

CHECKED PNK

DATE 4/20/2022 10:12:26 AM

SCALE 3/16" = 1'-0"

JOB # 20-A015



A3.61

NORTH ELEVATION MATERIALS  
3/16" = 1'-0"

1

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**break!**  
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## SECTIONS

DRAWN \_\_\_\_\_ CR \_\_\_\_\_

**CHECKED** \_\_\_\_\_ **PNK** \_\_\_\_\_

---

**DATE** 4/20/2022 10:19:42 AM

**FLOOR**  0"

POINT   
0.31 = 0"

---

---

Section 4

= 1'-0"

The diagram is titled "SECTION LEGEND" in bold capital letters. It contains five entries, each with a symbol on the left and text on the right:

- A dashed line symbol followed by "1 HR".
- A dashed line symbol followed by "2 HR".
- A vertical line with a horizontal tick symbol followed by "ELEVATION MARKER".
- A dashed line symbol followed by "PROPERTY LINE".
- A dashed line symbol followed by "EXISTING GRADE".



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**break!**  
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#### REVISIONS

## SECTIONS

---

BRAWN

HECKED PNK

DATE 4/20/2022 10:19:46 AM

CALE As Indicated

A4 10

The diagram is a section legend. It includes a dashed line symbol followed by the text "1 HR", a dashed line symbol followed by the text "2 HR", a circle with a cross inside symbol followed by the text "ELEVATION MARKER", a dashed line symbol followed by the text "PROPERTY LINE", and a dashed line symbol followed by the text "EXISTING GRADE".



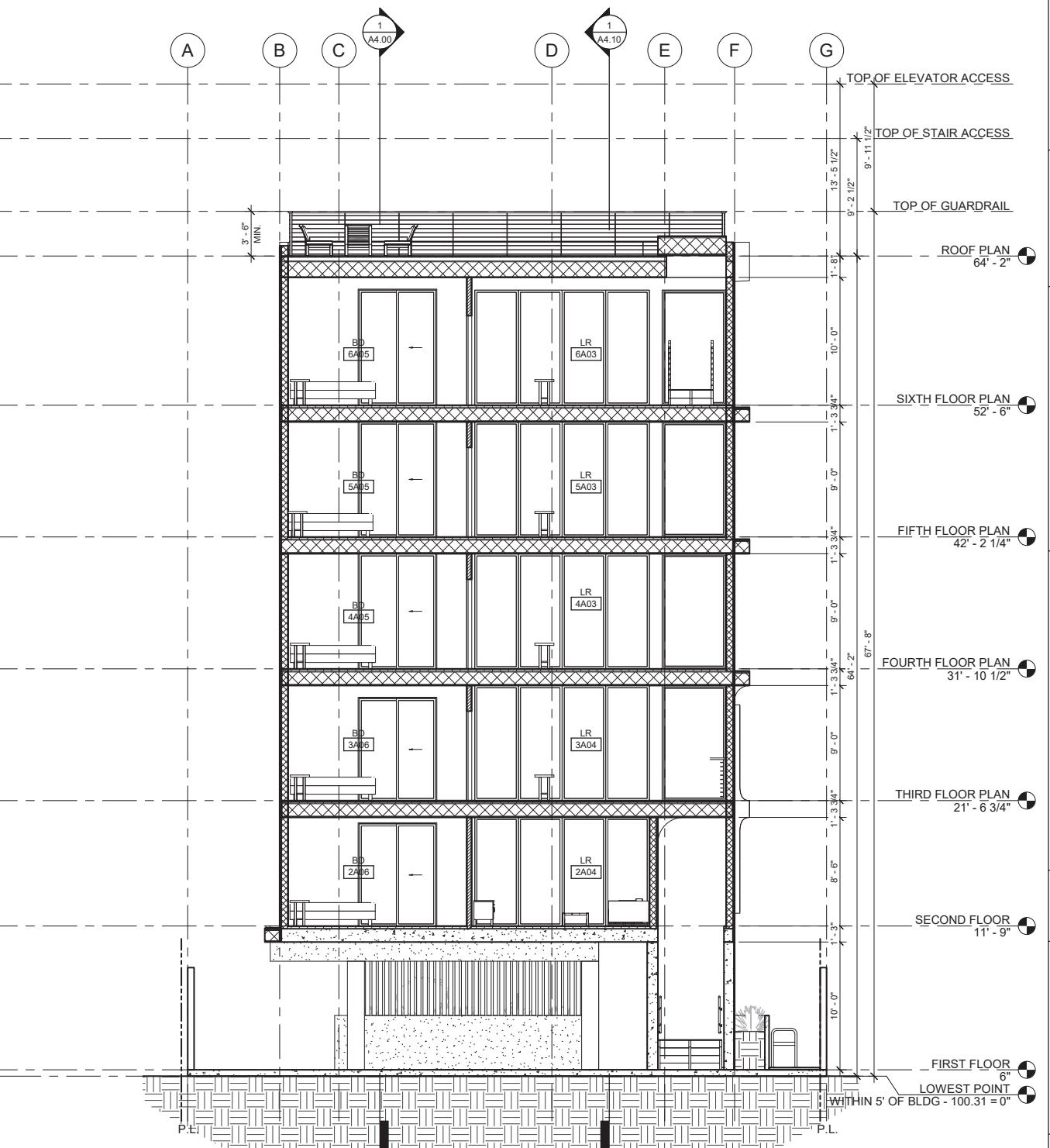
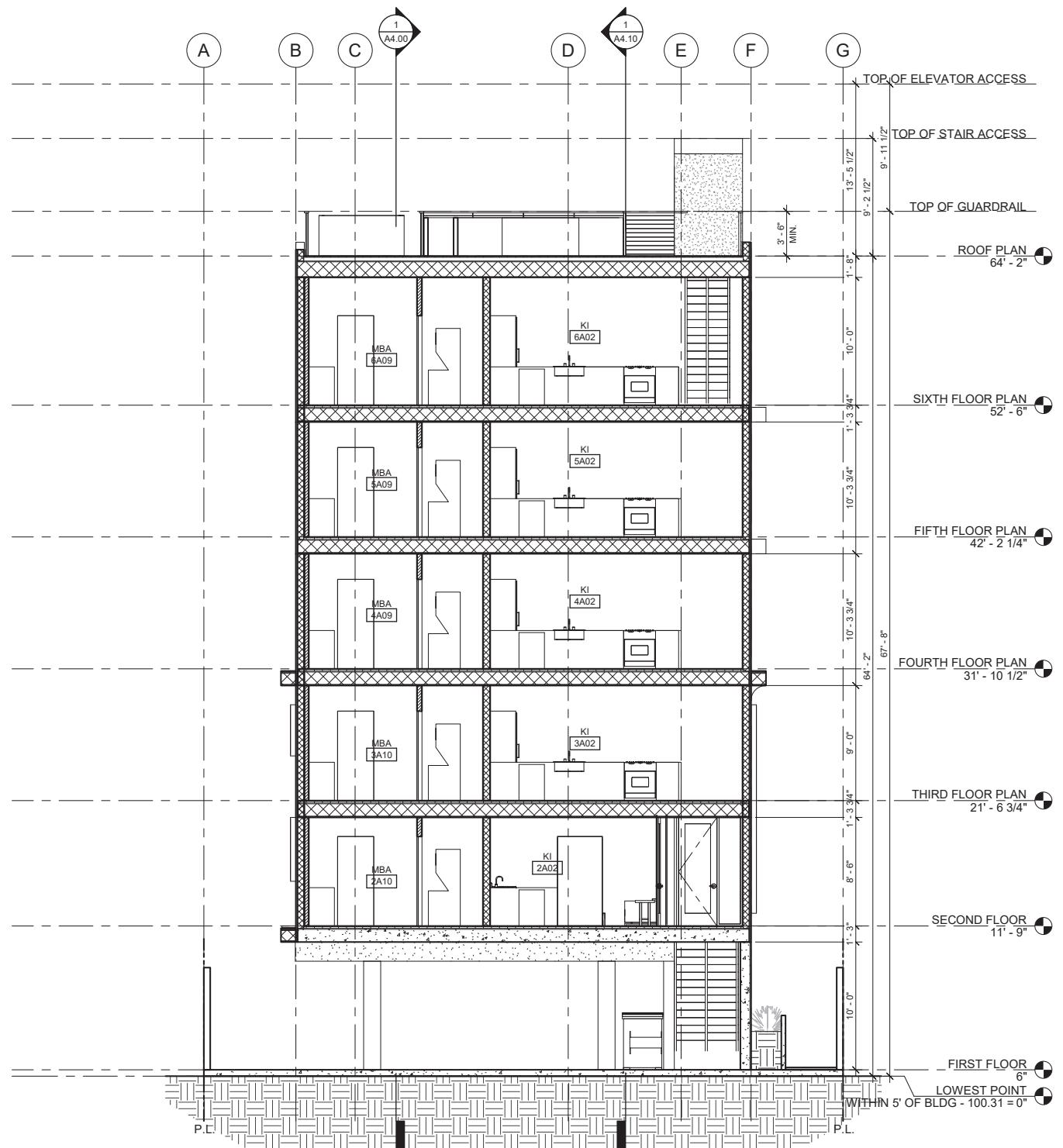
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SECTION LEGEND

- 1 HR
- 2 HR
- ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



## SECTIONS

DRAWN CR

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DATE 4/20/2022 10:19:49 AM

SCALE As indicated

JOB # 20-A015

A4.20

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**SECTION LEGEND**

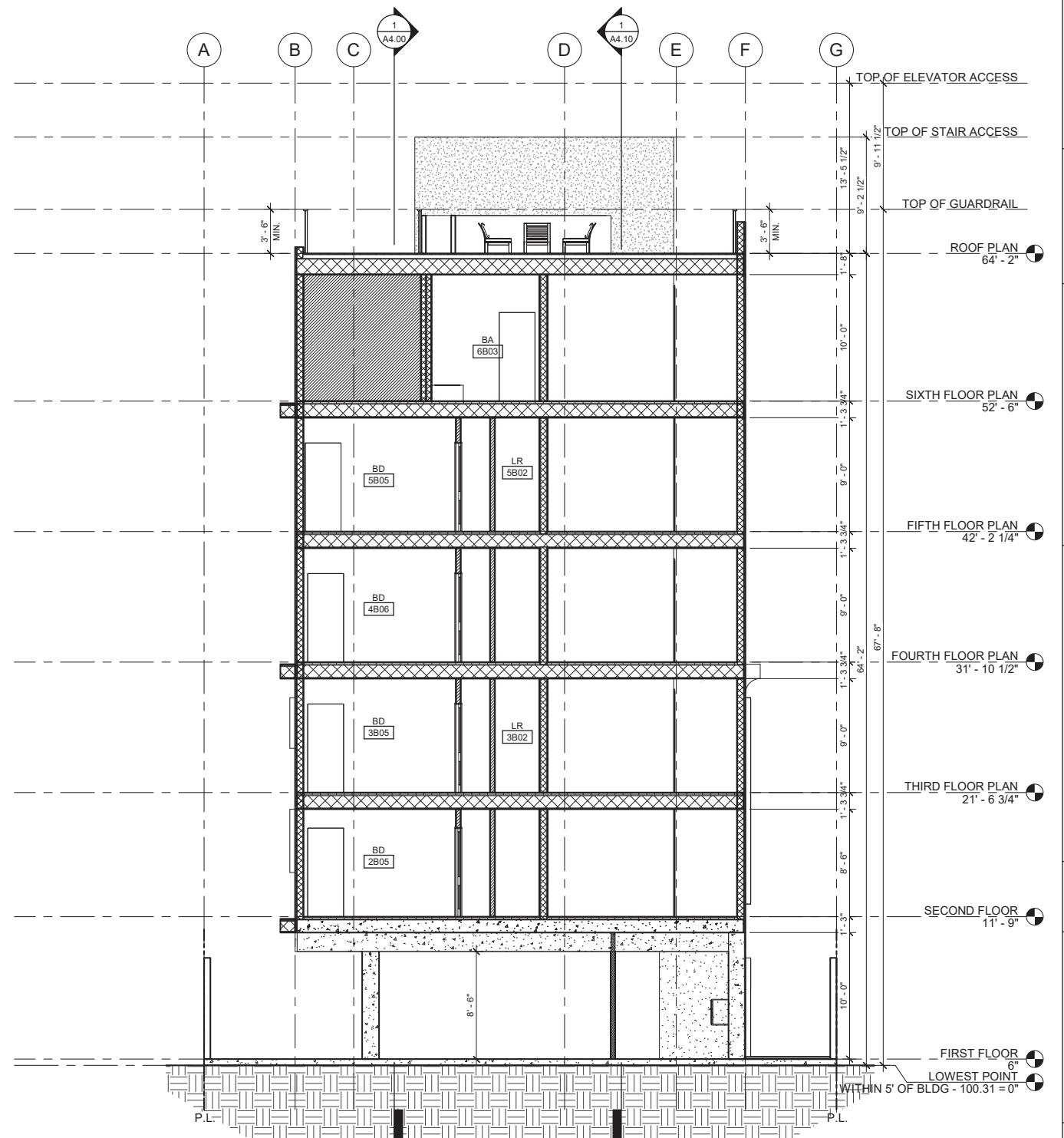
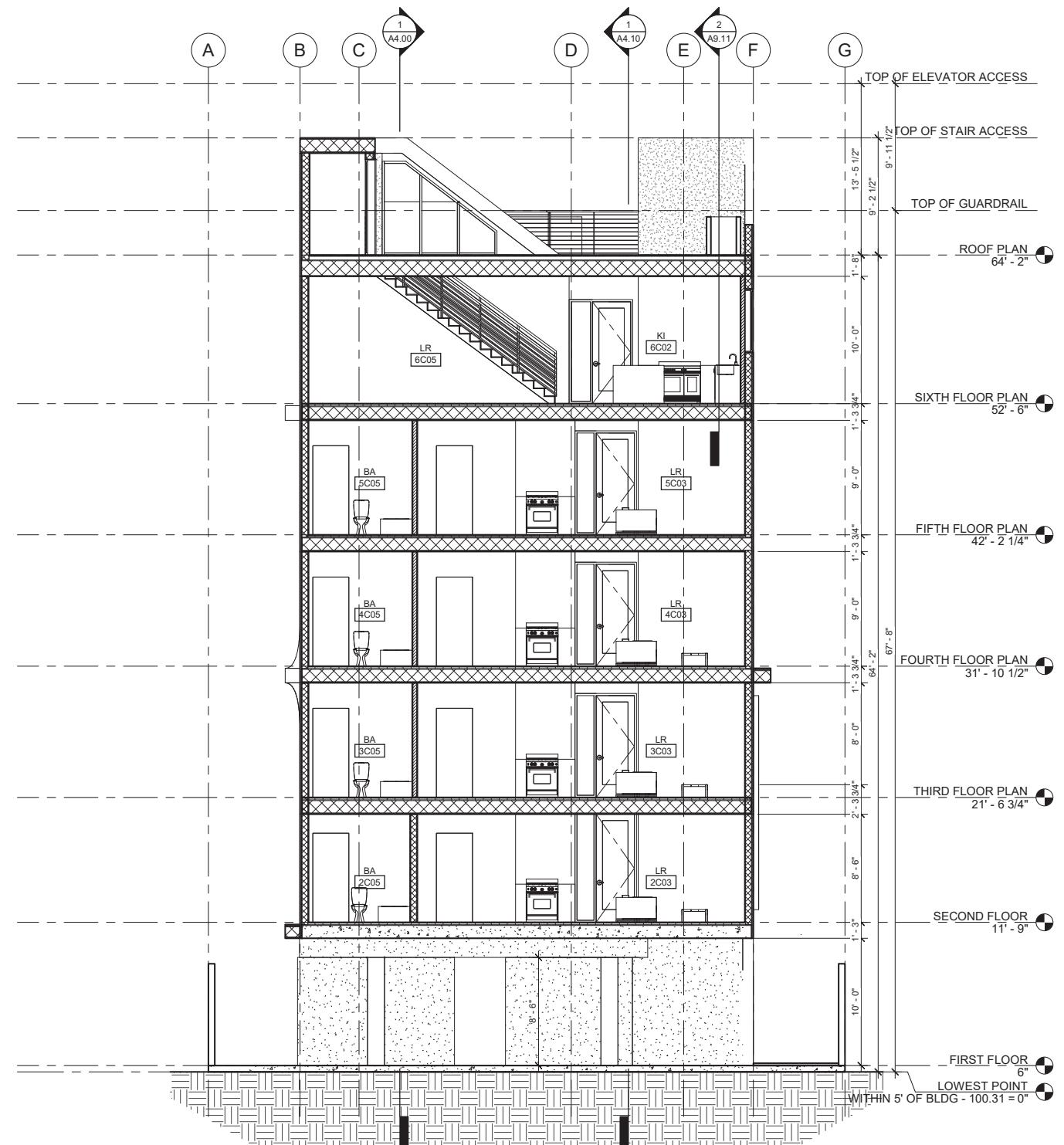
— 1 HR

— 2 HR

— ELEVATION MARKER

— PROPERTY LINE

— EXISTING GRADE



Section 6  
3/16" = 1'-0" 2

ction 5  
≡ 1'-0"

A4.30

### PLANTING NOTES

1. QUANTITIES GIVEN FOR PLANT MATERIALS SPECIFIED FOR "ON CENTER" SPACING ARE SHOWN FOR CONVENIENCE ONLY AND ARE SUBORDINATE TO THE SPACING GIVEN. VERIFY AND SUPPLY SUFFICIENT NUMBER OF PLANTS TO FULFILL SPACING REQUIREMENTS.

2. ALL HEADER AND BAMBOO ROOT BARRIERS SHALL BE LOCATED BY THE ARCHITECT ON SITE.

3. CONTRACTOR SHALL INSTALL PLANT MATERIAL IN ACCORDANCE WITH THE SPECIFICATIONS, DRAWINGS AND DETAILS.

4. CONTRACTOR SHALL PROVIDE A MAINTENANCE PERIOD OF NOT LESS THAN 90 DAYS COMMENCING AT THE DATE OF FINAL ACCEPTANCE. SUCH MAINTENANCE SHALL INCLUDE ALL CARE PERTAINING TO ALL WORK INSTALLED AS PART OF THESE CONTRACT DOCUMENTS.

5. THE CONTRACTOR SHALL MAINTAIN A QUALIFIED SUPERVISOR ON THE SITE AT ALL TIMES DURING CONSTRUCTION THROUGH COMPLETION OF PICK-UP WORK.

6. THE CONTRACTOR SHALL VERIFY ALL PLANT MATERIAL QUANTITIES LISTED FOR CONVENIENCE OF CONTRACTOR. ACTUAL NUMBER OF SYMBOLS SHALL HAVE PRIORITY OVER QUANTITIES DESIGNATED.

7. REMOVE ALL DEBRIS, WEEDS, EXCESS MATERIAL AND ROCKS LARGER THAN 1" IN DIAMETER FROM PLANTING AREAS PRIOR TO PREPARATION & AGAIN PRIOR TO PLANTING.

8. SEE DETAILS AND SPECIFICATIONS FOR STAKING METHOD, PLANT PIT DIMENSIONS, SOIL PREPARATION, AND BACKFILL REQUIREMENTS.

9. ALL PLANT MATERIALS SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

10. FINAL LOCATION OF ALL PLANT MATERIAL SHALL BE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT.

11. CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT 48 HOURS PRIOR TO COMMENCEMENT OF WORK TO COORDINATE PROJECT OBSERVATION SCHEDULES.

12. GROUNDCOVER PLANTING SHALL BE CONTINUOUS UNDER ALL TREES AND SHRUBS. GROUNDCOVER SHALL BE PLANTED ACCORDING TO SPACING ON PLANT LEGEND.

13. TREES SHALL BE LOCATED A MINIMUM OF 5' FROM WALLS, OVERHEADS, WALKS, HEADERS, AND OTHER TREES WITHIN THE PROJECT. IF CONFLICTS ARISE BETWEEN SIZE OF AREAS AND PLANS, CONTRACTOR TO CONTACT LANDSCAPE ARCHITECT FOR RESOLUTION. FAILURE TO MAKE SUCH CONFLICTS KNOWN TO THE LANDSCAPE ARCHITECT WILL RESULT IN CONTRACTOR'S LIABILITY TO RELOCATE THE MATERIALS.

14. ALL PLANTING AREAS SHALL BE LOOSENED TO A DEPTH OF 8". APPLY 4 C.Y. OF ORGANIC AMENDMENT AND 15 LBS. OF 10-10-10 FERTILIZER PER 1000 S.F. AND BLEND WITH THE TOP 6" OF SOIL. THIS AMENDMENT IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.

15. FOR ALL TREES AND SHRUB PLANTING, THE FOLLOWING PREPARED SOIL MIX SHALL BE USED FOR BACKFILL IN THE PLANTERS. THIS MIX IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.

SITE SOIL - 6 PARTS BY VOLUME  
ORGANIC AMENDMENT - 4 PARTS BY VOLUME  
SOIL CONDITIONER / FERTILIZER 10-10-10-LB. PER C.Y. OF MIX  
IRON SULFATE - 2 LBS. PER C.Y. OF MIX

16. TURF IS NOT ALLOWED ON SLOPES GREATER THAN 25% WHERE THE TOE OF THE SLOPE IS ADJACENT TO AN IMPERMEABLE HARDSCAPE..

17. RECIRCULATING WATER SYSTEMS SHALL BE USED FOR WATER FEATURES.

18. A MINIMUM 3-INCH LAYER OF MULCH SHALL BE APPLIED ON ALL EXPOSED SOIL SURFACES OF PLANTING AREAS EXCEPT TURF AREAS, CREEPING OR ROOTING GROUNDCOVER, OR DIRECT SEEDING APPLICATIONS WHERE MULCH IS CONTRAINDICATED.

19. FOR SOILS LESS THAN 6% ORGANIC MATTER IN THE TOP 6 INCHES OF SIL. COMPOST AT A RATE OF A MINIMUM OF FOUR CUBIC YARDS PER 1,000 SQUARE FEET OF PERMEABLE AREA SHALL BE INCORPORATED TO A DEPTH OF SIX INCHES INTO THE SOIL.

20. I AGREE TO COMPLY WITH THE REQUIREMENTS OF THE WATER EFFICIENT LANDSCAPE ORDINANCE AND SUBMIT A COMPLETE LANDSCAPE DOCUMENTATION PACKAGE THAT COMPLYS WITH THE PERFORMANCE APPROACH.

21. AT THE TIME OF FINAL INSPECTION THE PERMIT APPLICANT MUST PROVIDE THE OWNER OF THE PROPERTY WITH A CERTIFICATE OF COMPLETION, CERTIFICATE OF INSTALLATION, IRRIGATION SCHEDULE AND SCHEDULE OF LANDSCAPE AND IRRIGATION MAINTENANCE.

PROJECT DESCRIPTION:

NEW 15 UNIT APARTMENT  
SIX STORY BUILDING WITH PARKING ON GROUND FLOOR

PROJECT ADDRESS:

12737 MITCHELL AVE.  
LOS ANGELES, CA 90066

LOT:

190

TRACT:

DELMAR

BLOCK:

NONE

ASSESSOR'S PARCEL NUMBER:

4236019026

ZONE:

R3-1

LOT SIZE:

7,100.8 SQ. FT.

"THE SUBDIVIDER SHALL RECORD A COVENANT AND AGREEMENT SATISFACTORY TO THE ADVISORY AGENCY GUARANTEEING THAT:

A. THE PLANTING AND IRRIGATION SYSTEM SHALL BE COMPLETED BY THE DEVELOPER/BUILDER PRIOR TO THE CLOSE OF ESCROW OF 50 PERCENT OF THE UNITS OF THE PROJECT OR PHASE.

B. SIXTY DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION, THE LANDSCAPE PROFESSIONAL SHALL SUBMIT TO THE HOMEOWNERS/PROPERTY OWNERS ASSOCIATION A CERTIFICATE OF SUBSTANTIAL COMPLETION (12.40 G LAMC.)

C.

C. THE DEVELOPER/BUILDER SHALL MAINTAIN THE LANDSCAPING AND IRRIGATION FOR 60 DAYS AFTER COMPLETION OF THE LANDSCAPE AND IRRIGATION INSTALLATION.

D. THE DEVELOPER/BUILDER SHALL GUARANTEE ALL TREES AND IRRIGATION FOR A PERIOD OF SIX MONTHS AND ALL OTHER PLANTS FOR A PERIOD OF 60 DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION."

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REVISIONS

LANDSCAPE PLAN

DRAWN CR

CHECKED PNK

DATE 4/20/2022 10:24:49 AM

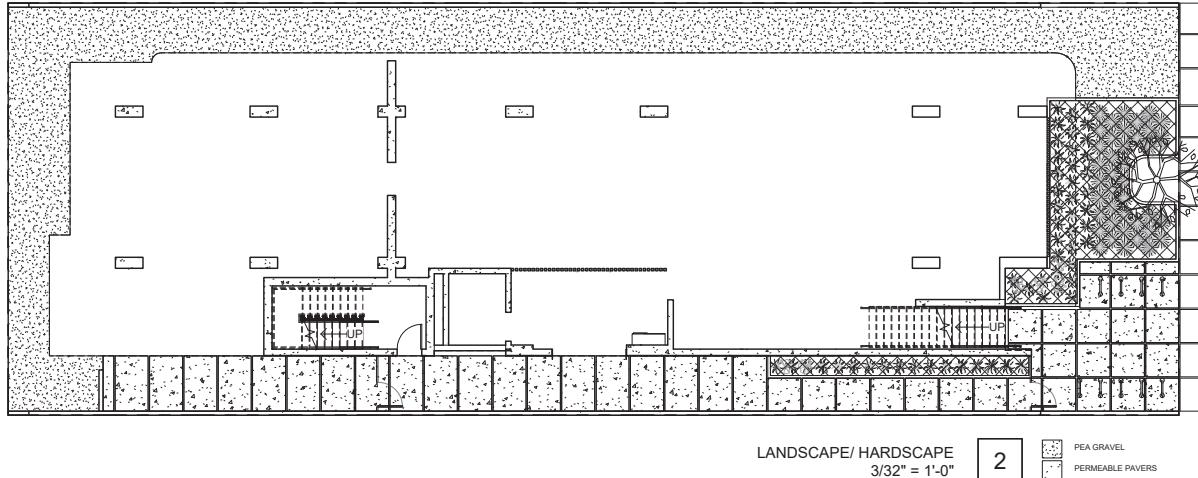
SCALE As indicated

JOB # 20-A015

L1.00

1

3/16" = 1'-0"



PLANTING LEGEND								
SYMBOL	QTY.	SIZE	SPACING	BOTANICAL NAME / COMMON NAME	NATIVE	WUCOLS WATER USE TYPES	WATER USE VALUES	HYDRO ZONE
	55	2'-3"	1'	SANSEVIERIA SPP. / MOTHER-IN-LAW'S TONGUE	NO	LOW	0.3	1
	40	1'	6'-9"	DASYLIRION SPP. / DESERT SPOON	NO	VERY LOW	0.2	1
	2	15'-25'	N/A	CERCIS SILIQUASTRUM	NO	MODERATE	0.4	1

DESIGN FOR: PLANTER BOX (PER LID APPENDIX F)	
ATOTAL =	7,100.8 SF = 0.163 AC.
85 <sup>TH</sup> PERCENTILE, 24-HR RAINFALL =	1.10 IN. = 0.0916 FT.
AI = IMPERVIOUS AREA =	4,526 SF
AP = PERVIOUS AREA =	2,574 SF
CATCHMENT AREA (SF) =	(4,526 SF x 0.9) + (2,574 SF x 0.1) = 4,330.8 SF
VM (FT <sup>3</sup> ) =	(0.0916) x (4,330.8 SF) = 396.70128 FT <sup>3</sup>
V <sub>DESIGN</sub> =	1.5 x 396.70 = 595.05 FT <sup>3</sup>
KSAT DESIGN =	2.5 in/hr
AMIN (FT <sup>2</sup> ) =	$\frac{595.05}{(3 \times 2.5 + 1)} = \frac{595.05}{1.625} = 366.18 SF$
AREA PROVIDED: 303 SF + 74 SF = 377 SF	

LANDSCAPE	
TURF AREA	0 S.F.
PLANT AREA	262 S.F.
TOTAL	262 S.F.
HARDSCAPE	
PERVIOUS	1,187 S.F.
PERMEABLE (PAVERS)	1,026 S.F.
TOTAL	2,213 S.F.

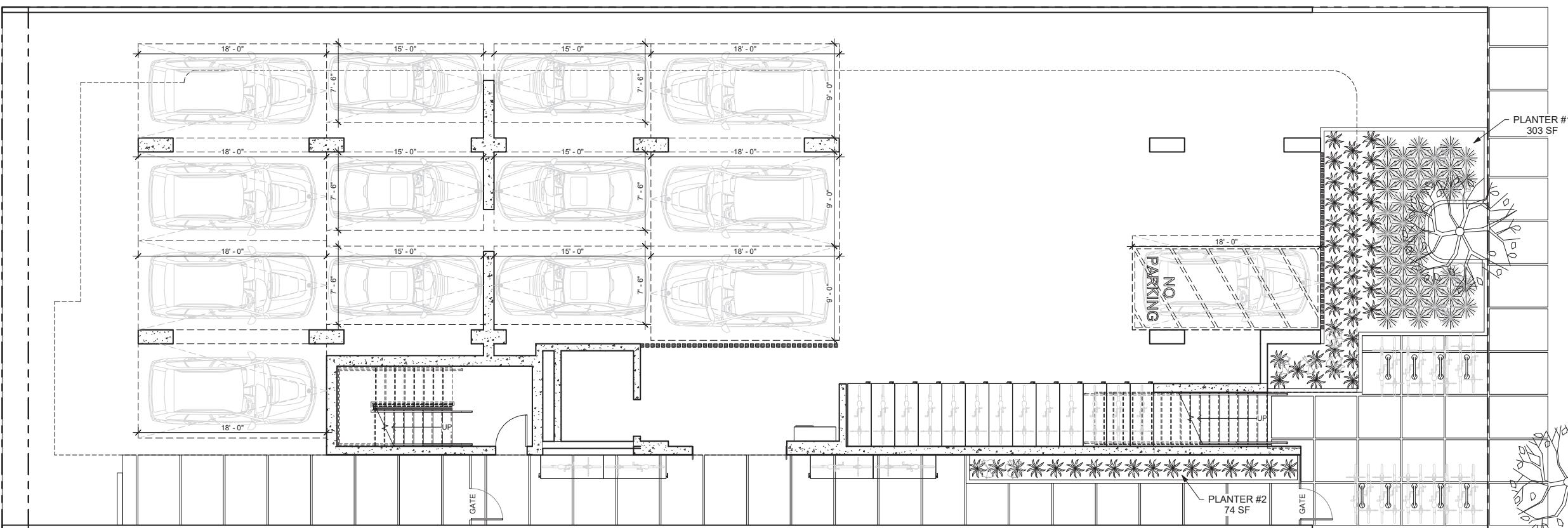
DASYLIRION SPP. / DESERT SPOON



SANSEVIERIA SPP. / MOTHER-IN-LAW'S TONGUE



CERCIS SILIQUASTRUM



LANDSCAPE PLAN

3/16" = 1'-0"

### PLANTING NOTES

1. QUANTITIES GIVEN FOR PLANT MATERIALS SPECIFIED FOR "ON CENTER" SPACING ARE SHOWN FOR CONVENIENCE ONLY AND ARE SUBORDINATE TO THE SPACING GIVEN. VERIFY AND SUPPLY SUFFICIENT NUMBER OF PLANTS TO FULFILL SPACING REQUIREMENTS.
2. ALL HEADER AND BAMBOO ROOT BARRIERS SHALL BE LOCATED BY THE ARCHITECT ON SITE.
3. CONTRACTOR SHALL INSTALL PLANT MATERIAL IN ACCORDANCE WITH THE SPECIFICATIONS, DRAWINGS AND DETAILS.
4. CONTRACTOR SHALL PROVIDE A MAINTENANCE PERIOD OF NOT LESS THAN 90 DAYS COMMENCING AT THE DATE OF FINAL ACCEPTANCE. SUCH MAINTENANCE SHALL INCLUDE ALL CARE PERTAINING TO ALL WORK INSTALLED AS PART OF THESE CONTRACT DOCUMENTS.
5. THE CONTRACTOR SHALL MAINTAIN A QUALIFIED SUPERVISOR ON THE SITE AT ALL TIMES DURING CONSTRUCTION THROUGH COMPLETION OF PICK-UP WORK.
6. THE CONTRACTOR SHALL VERIFY ALL PLANT MATERIAL QUANTITIES LISTED FOR CONVENIENCE OF CONTRACTOR. ACTUAL NUMBER OF SYMBOLS SHALL HAVE PRIORITY OVER QUANTITIES DESIGNATED.
7. REMOVE ALL DEBRIS, WEEDS, EXCESS MATERIAL AND ROCKS LARGER THAN 1" IN DIAMETER FROM PLANTING AREAS PRIOR TO PREPARATION & AGAIN PRIOR TO PLANTING.
8. SEE DETAILS AND SPECIFICATIONS FOR STAKING METHOD, PLANT PIT DIMENSIONS, SOIL PREPARATION, AND BACKFILL REQUIREMENTS.
9. ALL PLANT MATERIALS SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
10. FINAL LOCATION OF ALL PLANT MATERIAL SHALL BE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT.
11. CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT 48 HOURS PRIOR TO COMMENCEMENT OF WORK TO COORDINATE PROJECT OBSERVATION SCHEDULES.
12. GROUNDCOVER PLANTING SHALL BE CONTINUOUS UNDER ALL TREES AND SHRUBS. GROUNDCOVER SHALL BE PLANTED ACCORDING TO SPACING ON PLANT LEGEND.

13. TREES SHALL BE LOCATED A MINIMUM OF 5' FROM WALLS, OVERHEADS, WALKS, HEADERS, AND OTHER TREES WITHIN THE PROJECT. IF CONFLICTS ARISE BETWEEN SIZE OF AREAS AND PLANS, CONTRACTOR TO CONTACT LANDSCAPE ARCHITECT FOR RESOLUTION. FAILURE TO MAKE SUCH CONFLICTS KNOWN TO THE LANDSCAPE ARCHITECT WILL RESULT IN CONTRACTOR'S LIABILITY TO RELOCATE THE MATERIALS.

14. ALL PLANTING AREAS SHALL BE LOOSENED TO A DEPTH OF 8". APPLY 4 C.Y. OF ORGANIC AMENDMENT AND 15 LBS. OF 10-10-10 FERTILIZER PER 1000 S.F. AND BLEND WITH THE TOP 6" OF SOIL. THIS AMENDMENT IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.

15. FOR ALL TREES AND SHRUB PLANTING, THE FOLLOWING PREPARED SOIL MIX SHALL BE USED FOR BACKFILL IN THE PLANTERS. THIS MIX IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.

SITE SOIL - 6 PARTS BY VOLUME  
ORGANIC AMENDMENT - 4 PARTS BY VOLUME  
SOIL CONDITIONER / FERTILIZER 10-10-10-LB. PER C.Y. OF MIX

IRON SULFATE - 2 LBS. PER C.Y. OF MIX

16. TURF IS NOT ALLOWED ON SLOPES GREATER THAN 25% WHERE THE TOE OF THE SLOPE IS ADJACENT TO AN IMPERMEABLE HARDCAPE..

17. RECIRCULATING WATER SYSTEMS SHALL BE USED FOR WATER FEATURES.

18. A MINIMUM 3-INCH LAYER OF MULCH SHALL BE APPLIED ON ALL EXPOSED SOIL SURFACES OF PLANTING AREAS EXCEPT TURF AREAS, CREEPING OR ROOTING GROUNDCOVER, OR DIRECT SEEDING APPLICATIONS WHERE MULCH IS CONTRAINDED.

19. FOR SOILS LESS THAN 6% ORGANIC MATTER IN THE TOP 6 INCHES OF SIL, COMPOST AT A RATE OF A MINIMUM OF FOUR CUBIC YARDS PER 1,000 SQUARE FEET OF PERMEABLE AREA SHALL BE INCORPORATED TO A DEPTH OF SIX INCHES INTO THE SOIL.

20. I AGREE TO COMPLY WITH THE REQUIREMENTS OF THE WATER EFFICIENT LANDSCAPE ORDINANCE AND SUBMIT A COMPLETE LANDSCAPE DOCUMENTATION PACKAGE THAT COMPLYS WITH THE PERFORMANCE APPROACH.

DATE \_\_\_\_\_ SIGNED \_\_\_\_\_

21. AT THE TIME OF FINAL INSPECTION THE PERMIT APPLICANT MUST PROVIDE THE OWNER OF THE PROPERTY WITH A CERTIFICATE OF COMPLETION, CERTIFICATE OF INSTALLATION, IRRIGATION SCHEDULE AND SCHEDULE OF LANDSCAPE AND IRRIGATION MAINTENANCE.

PROJECT DESCRIPTION:

NEW 15 UNIT APARTMENT  
SIX STORY BUILDING WITH PARKING ON GROUND FLOOR

### IRRIGATION NOTES

1. CONTRACTOR IS TO AUGMENT EXISTING IRRIGATION SYSTEM. CONTRACTOR SHALL REPAIR OR REPLACE ANY EXISTING LANDSCAPE AND IRRIGATION SYSTEM DAMAGED FROM CONSTRUCTION TO AN ACCEPTABLE LANDSCAPE CONDITION WITH A FULLY FUNCTIONAL AND EFFICIENT IRRIGATION SYSTEM PER THE CONTAINED CONDITIONS.

2. ALL NEW TREES REQUIRE INDIVIDUAL POP-UP STREAM BUBBLERS, MIN. 2 PER TREE, WITHIN 4' OF TREE. TREE IRRIGATION SHALL BE ON A SEPARATE VALVE.

3. SPRAY OR ROTOR HEADS SHALL BE ON POP-UPS: 6" FOR LAWN, LOW GROUNDCOVER OR PARKED CAR OVERHANG AREAS, 12" FOR SHRUB AREAS. HEADS ON RISERS ARE ONLY ALLOWED ADJACENT TO WALLS WITH LIMITED SPACE FOR POP-UPS.

4. LOCATE SPRAY HEADS 24" FROM NON-PERVIOUS PAVING TO PREVENT OVERSPRAY. EXCEPTION ALLOWED IF ADJACENT SURFACE IS PERMEABLE OR IF USING ALTERNATIVE TECHNOLOGY IRRIGATION. ROTATOR OR ROTARY HEADS MAYBE LOCATED 12" FROM PAVING.

5. CONTRACTOR SHALL REPLACE ANY EXISTING IRRIGATION CONTROLLER WITH A MODULE AND SENSOR TO PROVIDE WEATHER BASED INFORMATION THAT WILL AUTOMATE THE IRRIGATION RUNTIMES BASED ON WEATHER. SEE HUNTER SOLAR SYNC, RAINBIRD ET MANAGER OR EQUIVALENT.

6. THE PLANTING AND IRRIGATION SYSTEM SHALL BE COMPLETED BY THE DEVELOPER/BUILDER PRIOR TO THE CLOSE OF ESCROW OF 50 PERCENT OF THE UNITS OF THE PROJECT OR PHASE.

7. SIXTY DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION, THE LANDSCAPE PROFESSIONAL SHALL SUBMIT TO THE HOMEOWNERS/PROPERTY OWNERS ASSOCIATION A CERTIFICATE OF SUBSTANTIAL COMPLETION (12.40 G LAMC.)

8. THE DEVELOPER/BUILDER SHALL GUARANTEE ALL TRESS AND IRRIGATION FOR A PERIOD OF SIX MONTHS AND ALL OTHER PLANS FOR A PERIOD OF 60 DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION.

### STATEMENTS AND CERTIFICATION

1. I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLANS.



SANSEVIERIA SPP. / MOTHER-IN-LAW'S TONGUE



CERCIS SILIQUASTRUM



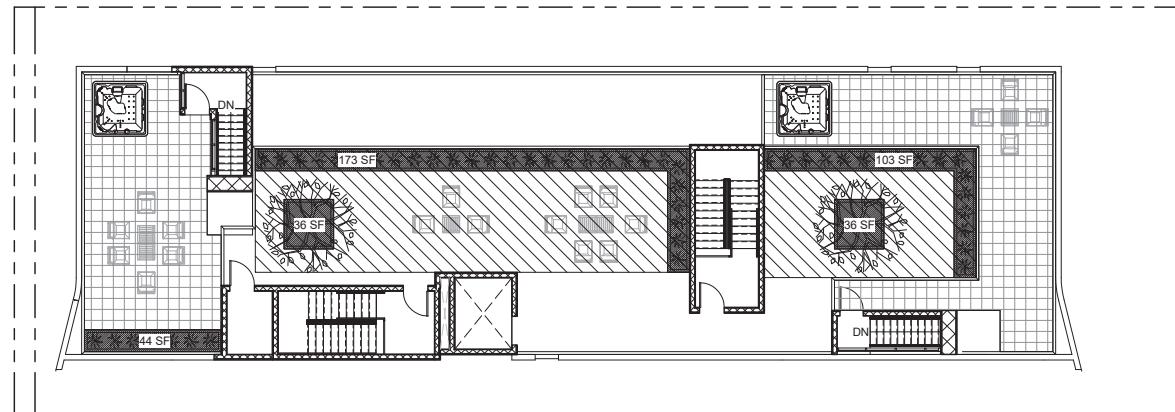
### PLANTING LEGEND

SYMBOL	QTY.	SIZE	SPACING	BOTANICAL NAME / COMMON NAME	NATIVE	WUCOLS WATER USE TYPES	WATER USE VALUES	HYDRO ZONE
	52	2'-3"	1'	SANSEVIERIA SPP. / MOTHER-IN-LAW'S TONGUE	NO	LOW	0.3	1
	2	15'-25'	N/A	CERCIS SILIQUASTRUM	NO	MODERATE	0.4	1

COMMON OPEN SPACE: 778 SF + 400 SF = 1,188 SF

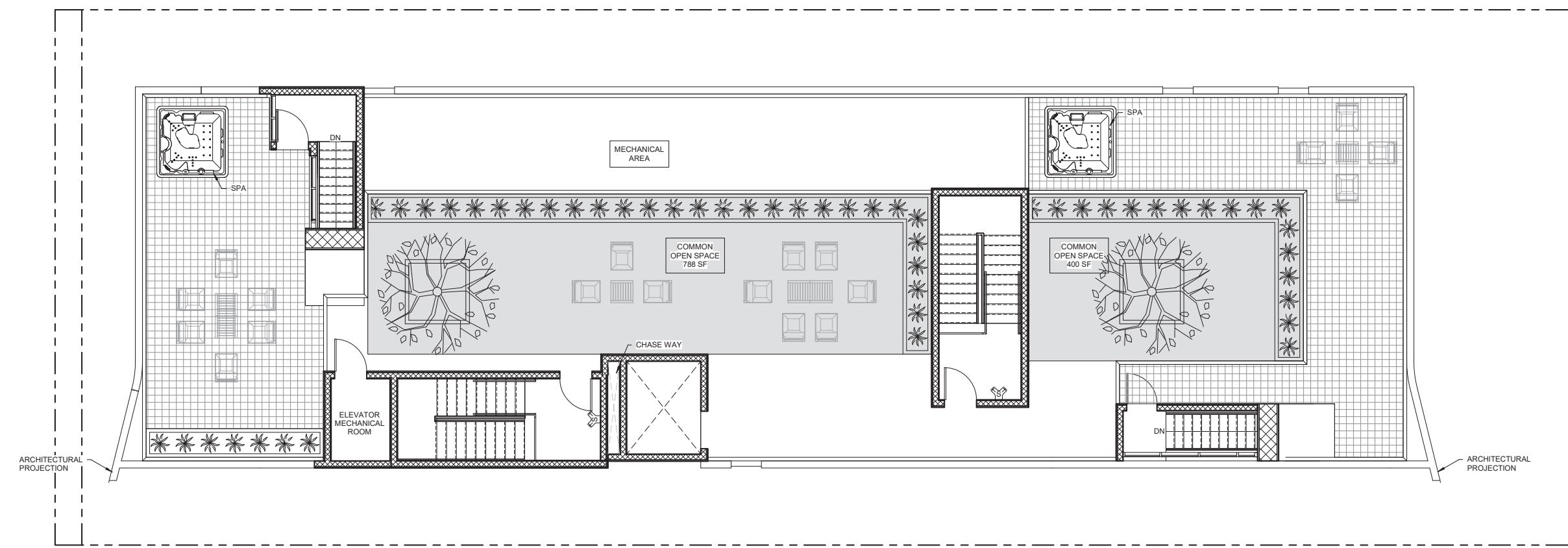
REQUIRED PLANTING AREA: %25 OF THE COMMON OPEN SPACE = 297 SF

PROVIDED PLANTING AREA: 173 SF + 103 SF + 36 SF = 312 SF



LANDSCAPE PLAN DIAGRAM  
3/32" = 1'-0"

2



L1.10  
LANDSCAPE PLAN 2  
3/16" = 1'-0"

1

12737 MITCHELL AVE  
LOS ANGELES  
CA 90066

### REVISIONS

### LANDSCAPE PLAN

DRAWN CR

CHECKED PNK

DATE 4/20/2022 10:24:59 AM

SCALE As indicated

JOB # 20-A015



LICENSED ARCHITECT  
RAMSEY DAHAM  
No. C-34257  
10/31/23  
RENEWAL DATE  
STATE OF CALIFORNIA

109 eucalyptus drive, el segundo, ca 90245  
[o] 310.322.3700

breakformdesign

## **EXHIBIT B**

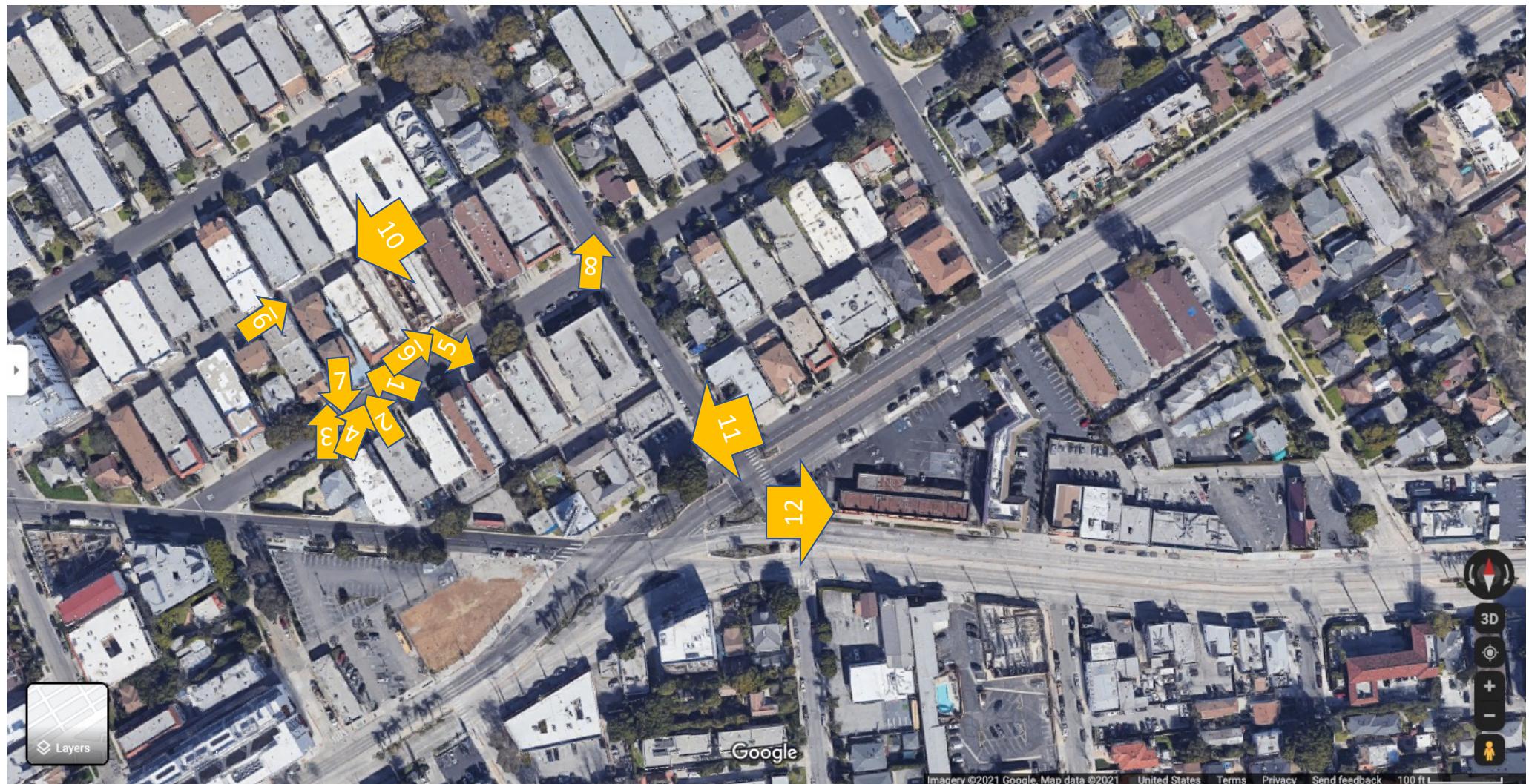
### **MAPS AND PHOTOS**

B1 – Site Photos

B2 – ZIMAS Parcel Profile Report

B3 – Radius Map

B4 – Vicinity Map



Imagery ©2021 Google, Map data ©2021

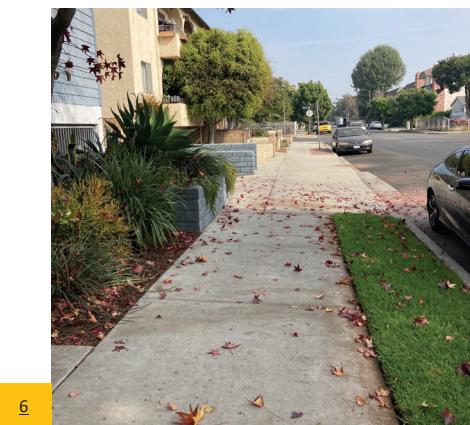
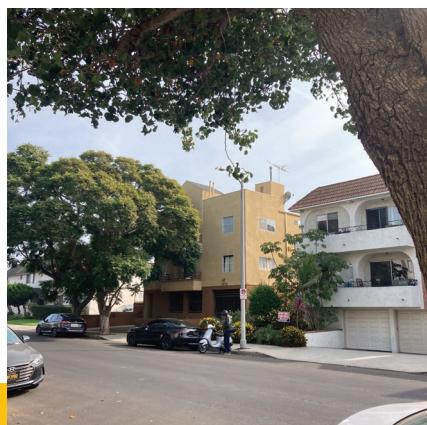
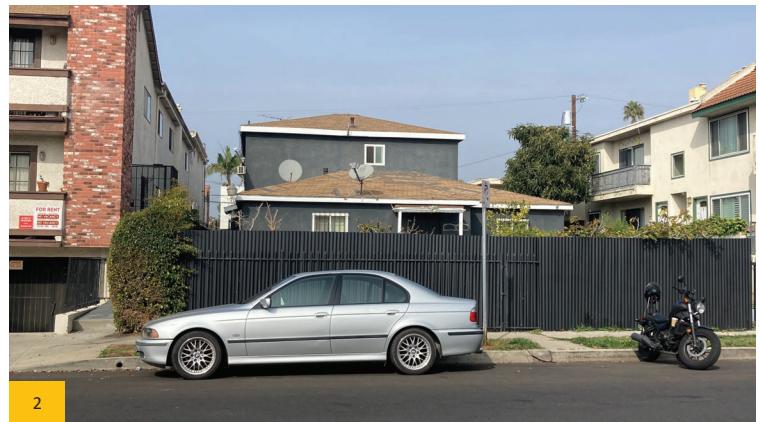
United States

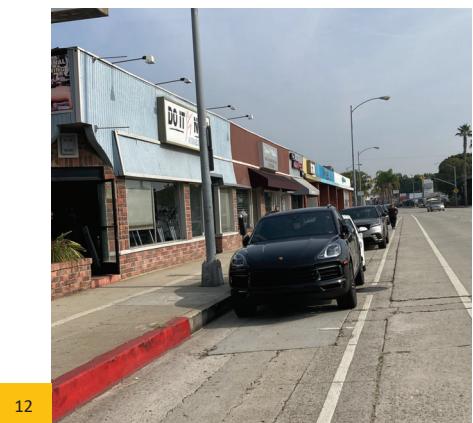
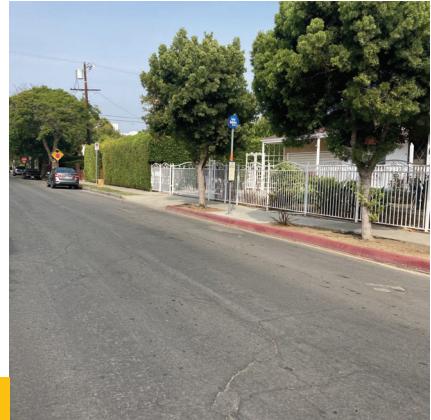
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Privacy

Send feedback

100 ft







# City of Los Angeles

## Department of City Planning

### 12/8/2021

### PARCEL PROFILE REPORT

#### PROPERTY ADDRESSES

12737 W MITCHELL AVE

12735 W MITCHELL AVE

#### ZIP CODES

90066

#### RECENT ACTIVITY

None

#### CASE NUMBERS

CPC-22211-BL

CPC-2018-7547-CPU

CPC-2014-1456-SP

CPC-2005-8252-CA

CPC-1984-226

ORD-186104

ORD-183497

ORD-168999

ORD-138533

ENV-2014-1458-EIR-SE-CE

ENV-2005-8253-ND

#### Address/Legal Information

PIN Number	111B153 1123
Lot/Parcel Area (Calculated)	7,100.8 (sq ft)
Thomas Brothers Grid	PAGE 672 - GRID C4
	PAGE 672 - GRID C5
Assessor Parcel No. (APN)	4236019026
Tract	DEL MAR
Map Reference	M B 10-76
Block	None
Lot	190
Arb (Lot Cut Reference)	None
Map Sheet	111B153

#### Jurisdictional Information

Community Plan Area	Palms - Mar Vista - Del Rey
Area Planning Commission	West Los Angeles
Neighborhood Council	Mar Vista
Council District	CD 11 - Mike Bonin
Census Tract #	2722.02
LADBS District Office	West Los Angeles

#### Planning and Zoning Information

Special Notes	None
Zoning	R3-1
Zoning Information (ZI)	ZI-2452 Transit Priority Area in the City of Los Angeles ZI-1874 Specific Plan: Los Angeles Coastal Transportation Corridor
General Plan Land Use	Medium Residential
General Plan Note(s)	Yes
Hillside Area (Zoning Code)	No
Specific Plan Area	LOS ANGELES COASTAL TRANSPORTATION CORRIDOR
Subarea	None
Special Land Use / Zoning	None
Historic Preservation Review	No
Historic Preservation Overlay Zone	None
Other Historic Designations	None
Other Historic Survey Information	None
Mills Act Contract	None
CDO: Community Design Overlay	None
CPIO: Community Plan Imp. Overlay	None
Subarea	None
CUGU: Clean Up-Green Up	None
HCR: Hillside Construction Regulation	No
NSO: Neighborhood Stabilization Overlay	No
POD: Pedestrian Oriented Districts	None
RFA: Residential Floor Area District	None
RIO: River Implementation Overlay	No
SN: Sign District	No
Streetscape	No
Adaptive Reuse Incentive Area	None

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 (\*) - APN Area is provided "as is" from the Los Angeles County's Public Works, Flood Control, Benefit Assessment.

**Affordable Housing Linkage Fee**

Residential Market Area	High
Non-Residential Market Area	High
Transit Oriented Communities (TOC)	Not Eligible
RPA: Redevelopment Project Area	None
Central City Parking	No
Downtown Parking	No
Building Line	15
500 Ft School Zone	No
500 Ft Park Zone	No

**Assessor Information**

Assessor Parcel No. (APN)	4236019026
APN Area (Co. Public Works)*	0.163 (ac)
Use Code	0400 - Residential - Four Units (Any Combination) - 4 Stories or Less
Assessed Land Val.	\$1,008,710
Assessed Improvement Val.	\$451,256
Last Owner Change	06/22/2021
Last Sale Amount	\$2,200,022
Tax Rate Area	67
Deed Ref No. (City Clerk)	644406 534501 393311 294176 1913492 1683006 1567619-20 1439983 1439966 1205520 1193073 1065568 1001591 0979989

**Building 1**

Year Built	1940
Building Class	D5B
Number of Units	1
Number of Bedrooms	3
Number of Bathrooms	2
Building Square Footage	1,867.0 (sq ft)

**Building 2**

Year Built	1963
Building Class	D6
Number of Units	3
Number of Bedrooms	6
Number of Bathrooms	3
Building Square Footage	2,608.0 (sq ft)

**Building 3**

Building 4	No data for building 3
Building 5	No data for building 4
Rent Stabilization Ordinance (RSO)	No data for building 5

**Additional Information**

Airport Hazard	None
Coastal Zone	None
Farmland	Area Not Mapped

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Urban Agriculture Incentive Zone	YES
Very High Fire Hazard Severity Zone	No
Fire District No. 1	No
Flood Zone	Outside Flood Zone
Watercourse	No
Hazardous Waste / Border Zone Properties	No
Methane Hazard Site	None
High Wind Velocity Areas	No
Special Grading Area (BOE Basic Grid Map A-13372)	No
Wells	None

#### Seismic Hazards

Active Fault Near-Source Zone	
Nearest Fault (Distance in km)	5.4242208
Nearest Fault (Name)	Santa Monica Fault
Region	Transverse Ranges and Los Angeles Basin
Fault Type	B
Slip Rate (mm/year)	1.0000000
Slip Geometry	Left Lateral - Reverse - Oblique
Slip Type	Moderately / Poorly Constrained
Down Dip Width (km)	13.0000000
Rupture Top	0.0000000
Rupture Bottom	13.0000000
Dip Angle (degrees)	-75.0000000
Maximum Magnitude	6.6000000
Alquist-Priolo Fault Zone	No
Landslide	No
Liquefaction	No
Preliminary Fault Rupture Study Area	No
Tsunami Inundation Zone	No

#### Economic Development Areas

Business Improvement District	None
Hubzone	Not Qualified
Opportunity Zone	No
Promise Zone	None
State Enterprise Zone	None

#### Housing

Direct all Inquiries to	Los Angeles Housing Department
Telephone	(866) 557-7368
Website	<a href="https://housing.lacity.org">https://housing.lacity.org</a>
Rent Stabilization Ordinance (RSO)	Yes [APN: 4236019026]
Ellis Act Property	No
AB 1482: Tenant Protection Act	No

#### Public Safety

Police Information	
Bureau	West
Division / Station	Pacific
Reporting District	1445
Fire Information	
Bureau	West
Batallion	4
District / Fire Station	62
Red Flag Restricted Parking	No

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## CASE SUMMARIES

Note: Information for case summaries is retrieved from the Planning Department's Plan Case Tracking System (PCTS) database.

Case Number: CPC-22211-BL

Required Action(s): BL-BUILDING LINE

Project Descriptions(s): Data Not Available

Case Number: CPC-2018-7547-CPU

Required Action(s): CPU-COMMUNITY PLAN UPDATE

Project Descriptions(s): ADOPT COMMUNITY PLAN POLICY DOCUMENT, GENERAL PLAN AMENDMENTS, AND ZONE CHANGES TO APPLY RE-CODE LA ZONING.

Case Number: CPC-2014-1456-SP

Required Action(s): SP-SPECIFIC PLAN (INCLUDING AMENDMENTS)

Project Descriptions(s): SPECIFIC PLAN AMENDMENT

Case Number: CPC-2005-8252-CA

Required Action(s): CA-CODE AMENDMENT

Project Descriptions(s): AN ORDINANCE ESTABLISHING PERMANENT REGULATIONS IMPLEMENTING THE MELLO ACT IN THE COASTAL ZONE.

Case Number: CPC-1984-226

Required Action(s): Data Not Available

Project Descriptions(s): AMENDMENT TO THE COASTAL TRANSPORTATION CORRIDOR SPECIFIC PLAN THE PROPERTY LOCATION IS GENERALLY BOUNDED ON THE EAST BY THE SAN DIEGO FWY; ON THE SOUTH BY THE CITY BOUNDARY OF THE CITY OF EL SEGUNDO; THE NORTH BY THE CITY BOUNDARY OF THE CITY OF SANTA MONICA AND ON THE WEST BY THE PACIFIC OCEAN PROPOSED PROJECT BROAD AMENDMENTS TO THE COASTAL TRANSPORTATION CORRIDOR SPECIFIC PLAN

Case Number: ENV-2014-1458-EIR-SE-CE

Required Action(s): EIR-ENVIRONMENTAL IMPACT REPORT

SE-STATUTORY EXEMPTIONS

CE-CATEGORICAL EXEMPTION

Project Descriptions(s): ENVIRONMENTAL IMPACT REPORT

Case Number: ENV-2005-8253-ND

Required Action(s): ND-NEGATIVE DECLARATION

Project Descriptions(s): AN ORDINANCE ESTABLISHING PERMANENT REGULATIONS IMPLEMENTING THE MELLO ACT IN THE COASTAL ZONE.

## DATA NOT AVAILABLE

ORD-186104

ORD-183497

ORD-168999

ORD-138533

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Address: 12735 W MITCHELL AVE

APN: 4236019026

PIN #: 111B153 1123

Tract: DEL MAR

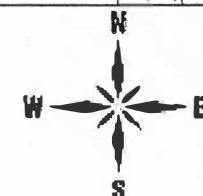
Block: None

Lot: 190

Arb: None

Zoning: R3-1

General Plan: Medium Residential





# **DENSITY BONUS - CONDITIONAL USE PERMIT**

PROJECT ADDRESS:  
12735 - 37 MITCHELL AVE  
LOS ANGELES, CA 90066  
NET ACREAGE: .163

THOMAS BROTHERS:  
PAGE: 672, GRID: C4, C5

SCALE: 1" = 100'

CENTERPOINT RADIUS MAPS  
263 W OLIVE AVE # 193  
BURBANK, CA 91502  
818.220.5401  
centerpointradiusmaps@gmail.com  
[www.centerpointradiusmaps.com](http://www.centerpointradiusmaps.com)  
DRAWN BY: J BOONE  
DATE: 12-10-2021

**LEGAL:**

C.D: 11

**TRACT: D**

C.T: 2722.02

M.B: 10 - 76

P.A: PALMS - MAR VISTA - DEL REY

**BLOCK: NONE**

## USES: FIELD

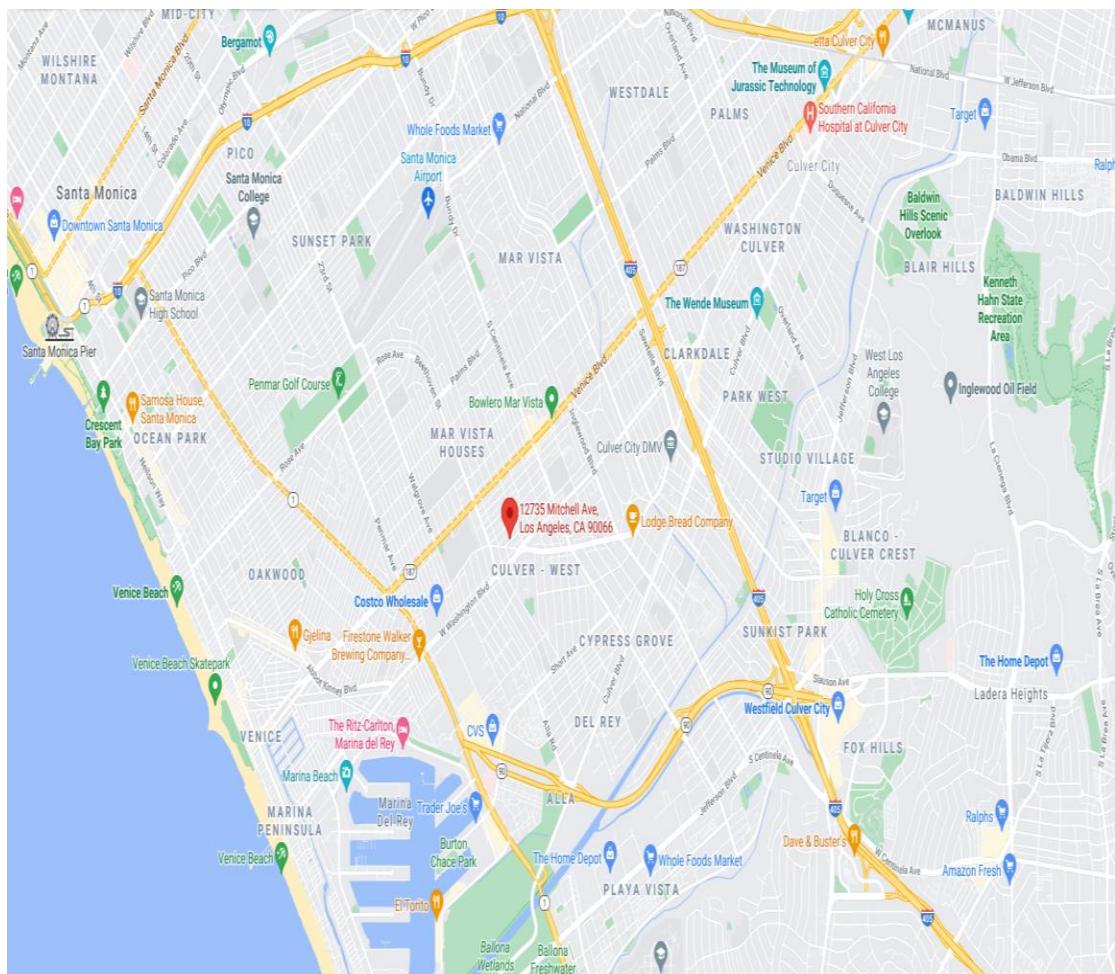
MAP SHEET: 111B153

CASE #:



# Vicinity Map

## 12735-37 Mitchell Ave



## **EXHIBIT C**

### **AGENCY CORRESPONDENCE**

C1 – DCP Housing Services Unit – Affordable Housing Referral Form

C2 – Department of Building and Safety – Preliminary Zoning Assessment

C3 – Los Angeles Housing Department – Replacement Unit Determination

C4 – Bureau of Engineering

C5 – Los Angeles Fire Department

C6 – Urban Forestry Division

C7 – Bureau of Street Lighting

## REFERRAL FORM



### AFFORDABLE HOUSING REFERRAL FORM

This form is to serve as a referral to the Los Angeles City Planning's (LACP) Development Services Center (DSC) for Affordable Housing case filing purposes (in addition to the required Department of City Planning Application and any other necessary documentation); and to the City of Los Angeles Housing Department (LAHD), Department of Building and Safety (LADBS), or other City agency for project status and entitlement need purposes. All Applicants are required to provide a complete set of architectural plans at the time that this form is submitted for review. Any application submitted that is missing any required materials will be considered incomplete and will not be reviewed until all materials are submitted.

This form shall be completed by the Applicant and reviewed and signed by LACP DSC Housing Services Unit (HSU) Staff prior to filing an application for an entitlement, administrative review, or building permit. Any modifications to the content(s) of this form after its authorization by HSU Staff is prohibited. LACP reserves the right to require an updated Referral Form for the project if more than 180 days have transpired since the referral date, or as necessary, to reflect project modifications, policy changes, bus route changes, bus schedule changes, and/or amendments to the Los Angeles Municipal Code (LAMC), local laws, and State laws.

#### THIS SECTION TO BE COMPLETED BY HSU STAFF ONLY

**Planning Staff Name & Title:** Rina Lara, Planning Assistant

**Planning Staff Signature:** \_\_\_\_\_

**Referral Date:** 5/9/2022      **Expiration Date:** 11/5/2022

#### TRANSPORTATION QUALIFIERS (if applicable)

**Major Transit Stop**       **Paratransit / Fixed Bus Route**

**Other:** Does Not Qualify.

**Location of Transit:** N/A

**Qualifier #1:** \_\_\_\_\_

**Service Interval #1:** \_\_\_\_\_ **Service Interval #2:** \_\_\_\_\_

**Qualifier #2:** \_\_\_\_\_

**Service Interval #1:** \_\_\_\_\_ **Service Interval #2:** \_\_\_\_\_

*Service Intervals are calculated by dividing 420 (the total number of minutes during the peak hours of 6 am to 9 am and 3 pm to 7 pm by the number of eligible trips.*

### Referral To:

**Planning DSC - Filing**       **100% Affordable per AB 2345<sup>1</sup>**       **SB 35**

**AB 2162**       **Measure JJJ**

**Other:** Revision: Updated incentives.

## Notes:

PAR-2021-4531-AHRF.

## THIS SECTION TO BE COMPLETED BY THE APPLICANT

## APPLICANT INFORMATION

**Applicant Name:** Jesi Harris, Brian Silveira & Associates

**Phone Number:** 704-277-7332

**Email:** HarrisLandUse@gmail.com

## I. PROPOSED PROJECT

## 1. PROJECT LOCATION/ZONING

**Project Address(es):** 12735-12737 W Mitchell Ave, LA, CA 90066

**Assessor Parcel Number(s):** 4236-019-026

Community Plan: Palms - Mar Vista - Del Rey

### **Existing Zone: R3-1**

**Land Use Designation:** Medium Residential

### Number of Parcels: 1

**Site Size (sf):** 7,100.8 sq ft

**Specific Plan**       **DRB/CDO**       **HPOZ**       **Redevelopment Project Area**

**Enterprise Zone**       **Q Condition/D Limitation (Ordinance No.):** \_\_\_\_\_

**Other Pertinent Zoning Information (specify):** LA Transportation Corridor

<sup>1</sup> AB 1763 incentives were amended by AB 2345.

## 2. DETAILED DESCRIPTION OF PROPOSED PROJECT

Demolition of the 2 existing structures and construction of a 6-story, 15-unit apmt building with 14 parking spaces on the first floor (6 compact, 12 tandem). Proposed project is a 67.67-foot high density bonus project including 22% VLI + 11% LI (2 VL + 1 LI units). with a 62.5% density bonus, incentives and waivers per 12.22 A 25 and 12.24 U 26 and Project Compliance with the LA Transportation Corridor.

## 3. DETAILED DESCRIPTION OF EXISTING SITE AND DEVELOPMENT

two 2-story buildings: 1 single-family dwelling with 3 bedrooms and 1 triplex with three 2-bedroom units

Existing Uses Dwelling Unit (DU) Square Footage (SF)	Existing No. of DUs or Non-Residential SF	Existing No. of DUs or Non-Residential SF to be Demolished	Proposed <sup>2</sup> No. of DUs or Non-Residential SF
Guest Rooms	0	0	0
Studios	0	0	1
One Bedrooms	0	0	0
Two Bedrooms	3	869	14
Three Bedrooms	1	1,867	0
_____ Bedrooms	0	0	0
Non-Residential SF	0	0	0
Other	0	0	0

<sup>2</sup> Per AB 2556, replacement units shall be equivalent to the number of units and number of bedrooms of the existing development.

## 4. APPLICATION TYPE

Density Bonus (per LAMC Section 12.22 A.25 or Government Code Section 65915) with only **Base Incentives** filed in conjunction with another discretionary approval.

Density Bonus with **On-Menu Incentives** (specify):

1) 33% increase in allowable FAR

2) \_\_\_\_\_

3) \_\_\_\_\_

4) \_\_\_\_\_

Density Bonus with **Off-Menu Incentives** (specify):

1) 14 parking spaces in lieu of the 21 parking spaces

2) 12 tandem spaces, in lieu of 1 accessible parking space per unit

3) \_\_\_\_\_

4) \_\_\_\_\_

Density Bonus with **Waivers of Development Standards** (specify):

1) 20% decrease in south side yard

2) 20% decrease in north side yard

3) 22.67-foot increase in height

4) 6 compact stalls + 8 standard stalls in lieu of 1 standard stall per unit

Greater Downtown Housing Incentive Area per LAMC Section 12.22 A.29

Affordable Housing per LAMC Section 11.5.11 (Measure JJJ)

Public Benefit Project per LAMC Section 14.00 A.2

General Plan Amendment per LAMC Section 11.5.6

Request: \_\_\_\_\_

Zone/Height District Change per LAMC Section 12.32

Request: \_\_\_\_\_

Conditional Use per LAMC Section 12.24 U.26

Site Plan Review per LAMC Section 16.05

Specific Plan Project Permit Compliance per LAMC Section 11.5.7 C

Community Design Overlay per LAMC Section 13.08

- Coastal Development Permit per LAMC Section 12.20.2 or 12.20.2.1
- Tract or Parcel Map per LAMC Section 17.00 or 17.50
- Other (specify): LA Transportation Corridor

## 5. ENVIRONMENTAL REVIEW

- Project is Exempt<sup>3</sup>
- Not Yet Filed
- Filed (Case No.): \_\_\_\_\_

## 6. HOUSING DEVELOPMENT PROJECT TYPE

### CHECK ALL THAT APPLY:

- For Rent                     For Sale                     Mixed-Use Project             Residential Hotel
- Extremely Low Income     Very Low Income             Low Income                     Moderate Income
- Market Rate                 Supportive Housing             Senior
- Special Needs (describe): \_\_\_\_\_
- Other Category (describe): \_\_\_\_\_

## 7. DENSITY CALCULATION

### A. Base Density: Maximum density allowable per zoning

Lot size (including any ½ of alleys)<sup>4</sup>    7,475 SF (a)

Density allowed by Zone                    800 SF of lot area per DU (b)

No. of DUs allowed by right (per LAMC) 8 DUs (c) [c = a/b, round down to whole number]

Base Density                                9 DUs (d) [d = a/b, round up to whole number]

### B. Maximum Allowable Density Bonus<sup>5</sup> 13 DUs (e) [e = dx1.35, round up to whole number]

<sup>3</sup> Project may be exempt from CEQA review if it qualifies for a CEQA Exemption or is a Ministerial Project (aka, "By Right").

<sup>4</sup> If there is a related subdivision case, the lot area shall be calculated based on the site area after a dedication of land has been provided.

<sup>5</sup> Per AB 2345, 100% affordable housing developments may request an 80% density increase or unlimited density if the project site is within 0.5 miles of a Major Transit Stop.

**C. Proposed Project:** Please indicate total number of DUs requested and break down by levels of affordability set by each category (California Department of Housing and Community Development [HCD] or United States Department of Housing and Urban Development [HUD]). For information on HCD and HUD levels of affordability please contact LAHD at [hcidla.landuse@lacity.org](mailto:hcidla.landuse@lacity.org).

	Total	HCD (State)	HUD (TCAC)
Market Rate	12	N/A	N/A
Managers Unit(s) - Market Rate	0	N/A	N/A
Extremely Low Income (ELI)	0	N/A	N/A
Very Low Income (VLI)	2	2	N/A
Low Income (LI)	1	1	N/A
Moderate Income	0	N/A	N/A
Permanent Supportive Housing — ELI	0	N/A	N/A
Permanent Supportive Housing — VLI	0	N/A	N/A
Permanent Supportive Housing — LI	0	N/A	N/A
Seniors — Market Rate	0	N/A	N/A
Other			
TOTAL No. of DUs Proposed	15	(f)	
TOTAL No. of Affordable Housing DUs	3	(g)	
No. of Density Bonus DUs	6	(h) [If $f > c$ , then $h = f - c$ ; if $f < c$ , then $h = 0$ ]	
Percent of Density Bonus Requested	62.5%	(i) $\{i = 100 \times [(f/d) - 1]\}$ (round down)	
Percent of Affordable Set Aside	22% VLI + 11% LI	(j) [g/d, round down to a whole number]	

## 8. SITE PLAN REVIEW CALCULATION

An application for Site Plan Review (SPR) may be required for projects that meet any of the SPR thresholds as outlined in LAMC Section 16.05 C, unless otherwise exempted per LAMC Section 16.05 D. For Density Bonus projects involving bonus units, please use the formula provided below to determine if the project meets the SPR threshold for unit count. If the project meets the threshold(s) but qualifies under the exemption criteria per Section 16.05 D, please confirm the exemption with LACP's DSC HSU.

9 \_\_\_\_\_ units allowed by right (permitted by LAMC) – 4 \_\_\_\_\_ existing units = 5 \_\_\_\_\_ units

**YES, SPR is required.**

Proposed by-right units minus existing units is equal to or greater than 50<sup>6</sup>

**NO, SPR is not required.**

Base Density units minus existing units is less than 50

**Exempt.**

Specify reason: \_\_\_\_\_

## II. DENSITY BONUS (LAMC SECTION 12.22 A.25, ORDINANCE NO. 179,681)

### 9. PARKING OPTIONS

**CHECK ALL THAT APPLY:**

**Automobile Parking Reductions via Bicycle Parking for Residential Uses<sup>7</sup>. Choose only one of the options, if applicable:**

10%

15% (*Only for residential projects or buildings located within 1,500 feet of a Major Transit Stop*)

30% (*If selecting the 30% parking reduction, the project will be ineligible for any of the Parking Options listed below*)

If selecting the 30% parking reduction, provide the following information:

Required Parking per LAMC: 29 \_\_\_\_\_

Required Parking after the 30% reduction: 21 \_\_\_\_\_

<sup>6</sup> Site Plan Review may also be required if other characteristics of the project exceeds the thresholds listed in LAMC Section 16.05.

<sup>7</sup> Any project utilizing Parking Option 3 may not further reduce automobile parking via bicycle parking.

**Automobile Parking for Residential Uses (choose only one of the following options):**

*Note: Any fractional numbers are rounded up.*

**Parking Option 1.** Based on # of bedrooms, inclusive of Handicapped and Guest parking.

	# of DUs	Spaces/DU	Parking Required	Parking Provided
0-1 Bedroom		1		
2-3 Bedrooms		1.5		
4 or more Bedrooms		2.5		
Stalls Reduced via Bike Parking				Subtract:
<b>TOTALS</b>				

**Parking Option 2.** Reduced only for Restricted Affordable Units and up to 40% of required parking for Restricted Affordable Units may be compact stalls.

	# of DUs	Spaces/DU	Parking Required	Parking Provided
Market Rate (Including Senior Market Rate)		Per Code		
Restricted Affordable		1		
VLI/LI Senior or Disabled		0.5		
Restricted Affordable in Residential Hotel		2.5		
Stalls Reduced via Bike Parking				Subtract:
<b>TOTALS</b>				

**Parking Option 3 [AB 2345 (2020)].** Applies to two types of projects:

- 100% affordable housing developments consisting solely of affordable units, exclusive of a manager's unit(s), with an affordable housing cost to lower income families; or
- Mixed-income developments consisting of 11% VLI or 20% LI units.

**100% Affordable Housing Developments.** There is no minimum parking requirement for any of the following 100% affordable housing developments described below. Check all that apply:

- A housing development located within 0.5 miles of a Major Transit Stop.
- A housing development for individuals who are 62 years of age or older with either paratransit service or unobstructed access, within 0.5 miles to a fixed bus route that operates at least eight times per day.

- Special Needs Housing Development**, as defined in Section 51312 of the Health and Safety Code (H&SC), with either paratransit service or unobstructed access, within 0.5 miles to a fixed bus route that operates at least eight times per day.
- Supportive Housing Development**, as defined in Section 50675.14 of the H&SC.
- Mixed-Income Developments** consisting of 11% VLI or 20% LI units.

	Spaces/Unit	Parking Required	Parking Provided
Located within 0.5 miles of Major Transit Stop with unobstructed access to project	0.5		

**Major Transit Stop** is defined as a site containing an existing rail or bus rapid transit station, a ferry terminal served by either a bus or rail transit service, or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods. It also includes major transit stops that are included in the applicable regional transportation plan.

**Bus Rapid Transit** is defined as public mass transit service provided by a public agency or by a public-private partnership that includes all of the following features:

- 1) Full-time dedicated bus lanes or operation in a separate right-of-way dedicated for public transportation with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods.
- 2) Transit signal priority.
- 3) All-door boarding.
- 4) Fare collection system that promotes efficiency.
- 5) Defined stations.

## 10. INCENTIVES

### A. Qualification for Incentives

Below is the minimum Required Restricted Affordable Housing Units, calculated as a percentage of the base density allowed on the date of the application. Check only one:

Incentives	% Very Low Income	% Low Income	% Moderate Income
One	<input type="checkbox"/> 5% to <10%	<input type="checkbox"/> 10% to <20%	<input type="checkbox"/> 10% to <20%
Two	<input type="checkbox"/> 10% to <15%	<input type="checkbox"/> 20% to <30%	<input type="checkbox"/> 20% to <30%
Three	<input checked="" type="checkbox"/> 15% or greater	<input type="checkbox"/> 30% or greater	<input type="checkbox"/> 30% or greater

- 100% Affordable Housing Developments may request up to four (4) incentives and one (1) Waiver of Development Standard.** Check this box if this applies to the project.

**B. Project Zoning Compliance & Incentives** (Only for projects requesting a Density Bonus with Incentives/Waivers)

	Permitted w/o Incentives	Proposed per Incentives	On-Menu	Off-Menu
<input type="checkbox"/> Yard/Setback (each yard counts as one incentive)				
<input type="checkbox"/> Front (1)	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Front (2)	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Side (1)	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Side (2)	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Rear	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Lot Coverage	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Lot Width	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/> Floor Area Ratio <sup>8</sup>	3:1	3.99:1	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Height/Stories <sup>9</sup>	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Overall Height	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Transitional Height(s)	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Open Space	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Density Calculation	_____	_____	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> Averaging (all count as one incentive — check all that are needed)				
<input type="checkbox"/> FAR				
<input type="checkbox"/> Density				
<input type="checkbox"/> Parking				
<input type="checkbox"/> Open Space				
<input type="checkbox"/> Vehicular Access		12 tandem parking spaces of 14 total spaces in lieu of		
<input checked="" type="checkbox"/> Other Off-Menu Incentives (specify):	1 individually accessible space per unit			
parking reduction from 21 required spaces to 14 spaces				
<input checked="" type="checkbox"/> Waiver of Development Standards (specify):	22.67-ft height increase, 6 compact parking stalls and 8 standard stalls in lieu of the 1 standard parking stall per dwelling unit			
two 20% side yard reductions,				
<input type="checkbox"/> 100% Affordable Housing Development shall receive a height increase of three additional stories up to 33 additional feet. Check the box if this applies to your project.				

**TOTAL No. of Incentives Requested:** **On-Menu** 1 **Off-Menu** 2

**TOTAL No. of Waivers Requested:** 4 (2 on-menu, 2 off-menu)

<sup>8</sup> See LAMC Section 12.22 A.25(f)(4) for additional requirements.

<sup>9</sup> See LAMC Section 12.22 A.25(f)(5) for additional requirements.

## 11. COVENANT

All Density Bonus projects are required to prepare and record an Affordability Covenant to the satisfaction of the LAHD's Occupancy Monitoring Unit before a building permit can be issued. For more information, please contact the LAHD at [hcidla.landuse@lacity.org](mailto:hcidla.landuse@lacity.org).

## III. GREATER DOWNTOWN HOUSING INCENTIVE AREA (LAMC SEC. 12.22 A.29, ORDINANCE NO. 179,076)

## 12. GREATER DOWNTOWN HOUSING INCENTIVE AREA (GDHIA)

### A. Eligibility for Floor Area Bonus

NOTE: The affordability levels required are set by the HUD/TCAC. For information on HCD and HUD levels of affordability please contact the LAHD at [hcidla.landuse@lacity.org](mailto:hcidla.landuse@lacity.org).

- 5% of the total number of DUs provided for VLI households; and
- One of the following shall be provided:
  - 10% of the total number of DUs for LI households; or
  - 15% of the total number of DUs for Moderate Income households; or
  - 20% of the total number of DUs for Workforce Income households, and
- Any DU or Guest Room occupied by a household earning less than 50% of the Area Median Income (AMI) that is demolished or otherwise eliminated shall be replaced on a one-for-one basis within the Community Plan area in which it is located

### B. Incentives

NOTE: Must meet all three (3) eligibility requirements from 12.A above and provide a Covenant & Agreement (See #11).

#### CHECK ALL THAT APPLY:

- A 35% increase in total floor area
- Open Space requirement pursuant to LAMC Section 12.21 G reduced by one-half, provided that a fee equivalent to amount of the relevant park fee, pursuant to LAMC Section 19.17, shall be paid for all dwelling units. See LAMC Section 12.29 A.29(c) for exceptions
- No parking required for units for households earning less than 50% AMI
- No more than one parking space required for each dwelling unit

### C. Additional Incentives to Produce Housing in the GDHIA

- No yard requirements except as required by the Urban Design Standards and Guidelines
- Buildable area shall be the same as the lot area (for the purpose of calculating buildable area for residential and mixed-use)

- Maximum number of dwelling units or guest rooms permitted shall not be limited by the lot area provisions, as long as the total floor area utilized by guest rooms does not exceed the total floor area utilized by dwelling units
- No prescribed percentage of the required open space that must be provided as either common open space or private open space

## IV. MEASURE JJJ<sup>10</sup> (LAMC Sec. 11.5.11, Ordinance No. 184, 745)

### 13. AFFORDABLE REQUIREMENTS

A certain percentage of affordable units is required based on the total number of units in the project.

Fill out either A or B below:

#### A. Rental Projects

- No less than the affordability percentage corresponding to the level of density increase requested or allowed:
  - \_\_\_\_ % VLI    **OR**     \_\_\_\_ % LI
- For projects requesting a General Plan Amendment, Zone Change, and/or Height District Change that results in an increased allowable density greater than 35%:
  - 5% ELI    **AND**     6% VLI    **OR**     15% LI
- For projects requesting a General Plan Amendment, Zone Change, and/or Height District Change that results in an increased allowable density greater than 35%:
  - 5% ELI    **AND**     11% VLI    **OR**     20% LI

#### Required Number of Affordable Units

ELI \_\_\_\_\_ VLI \_\_\_\_\_ LI \_\_\_\_\_

#### B. For Sale Projects

- No less than the affordability percentage corresponding to the level of density increase requested or allowed:
  - \_\_\_\_ % VLI    **OR**     \_\_\_\_ % LI    **OR**     \_\_\_\_ % Moderate Income
- For projects requesting a General Plan Amendment, Zone Change, and/or Height District Change that results in an increased allowable density greater than 35% or allows a residential use where not previously allowed:
  - 11% VLI    **OR**     20% LI    **OR**     40% Moderate Income

#### Required Number of Affordable Units

VLI \_\_\_\_\_ LI \_\_\_\_\_ Moderate Income \_\_\_\_\_

<sup>10</sup> All fractional amounts in Sections 13 and 14 shall be rounded up to the next whole number.

## 14. ALTERNATIVE COMPLIANCE OPTIONS

In lieu of providing the affordable units on site, there are three (3) other options available to comply with Measure JJJ Affordable Requirements. Select one, if applicable; otherwise leave this section blank.

**A. Off-Site Construction** – Construction of affordable units at the following rate:

- Within 0.5 miles of the outer edge of the Project, Affordable Units in Section 13 x 1.0
- Within 2 miles of the outer edge of the Project, Affordable Units in Section 13 x 1.25
- Within 3 miles of the outer edge of the Project, Affordable Units in Section 13 x 1.5

**Updated Required Number of Affordable Units**

ELI \_\_\_\_\_ VLI \_\_\_\_\_ LI \_\_\_\_\_ Moderate Income \_\_\_\_\_

**B. Off-Site Acquisition** – Acquisition of property that will provide affordable units at the following rate:

- Within 0.5 miles of the outer edge of the Project, Affordable Units in Section 13 x 1.0
- Within 1 mile of the outer edge of the Project, Affordable Units in Section 13 x 1.25
- Within 2 miles of the outer edge of the Project, Affordable Units in Section 13 x 1.5

**Updated Required Number of Affordable Units**

ELI \_\_\_\_\_ VLI \_\_\_\_\_ LI \_\_\_\_\_ Moderate Income \_\_\_\_\_

**C. In-Lieu Fee** – From the Affordability Gaps Study published by the Los Angeles City Planning

**Total In-Lieu Fee** \_\_\_\_\_ (Note: Final fee TBD if/when the project is approved)

## 15. DEVELOPER INCENTIVES

Please describe up to a maximum of three (3) incentives:

- 1) \_\_\_\_\_  
\_\_\_\_\_
- 2) \_\_\_\_\_  
\_\_\_\_\_
- 3) \_\_\_\_\_  
\_\_\_\_\_

*Disclaimer: This review is based on the information and plans provided by the applicant at the time of submittal of this form. Applicants are advised to verify any zoning issues such as height, parking, setback, and any other applicable zoning requirements with LADBS.*



## REFERRAL FORMS:

## Preliminary Zoning Assessment Referral

Department of City Planning (DCP) and Department of Building &amp; Safety (DBS)

This form is to serve as an inter-agency referral for City Planning applications associated with a Housing Development Project. As a part of a City Planning application, this completed form shall be accompanied by architectural plans stamped and signed by DBS Plan Check staff following the completion of a zoning Plan Check. Review of the referral form by City staff is intended to identify and determine compliance with City zoning and land use requirements necessary to achieve the proposed project and to ascertain if any zoning issues or necessary approvals are associated with the project and site that need to be resolved through a discretionary City Planning action.

## INSTRUCTIONS: Preliminary Zoning Assessment Referral

1. **Complete the Preliminary Zoning Assessment:**
  - a. **Section I: Project Information:** This section is to be completed by a member of the project team and verified by City staff.
  - b. **Section II: Housing Development Project Determination:** Projects proposing the development of two or more units are screened to determine whether a project is a Housing Development Project and therefore qualifies for completion of Section III of this form and verified plans through a zoning Plan Check with DBS. The determination on Section II will be made by City Planning staff in the PARP unit prior to completion of a zoning Plan Check with DBS. A set of architectural plans, including a site plan and floor plans, are required to complete the determination.
  - c. **Section III: Zoning Plan Check:** Applicants will submit for a zoning Plan Check with DBS to ascertain if any zoning issues or necessary approvals associated with the project and site need to be resolved through a discretionary City Planning action. This completed form shall be accompanied by architectural plans stamped and signed by a DBS Plan Check staff following the completion of a zoning Plan Check. DBS Plan Check staff will sign Section III of the Preliminary Zoning Assessment Form once the zoning plan check verifications are complete.
2. **File application with City Planning:** Following the completion of the Preliminary Zoning Assessment Referral Form and receipt of architectural plans stamped and signed by DBS Plan Check staff, a City Planning application may be filed. Filing appointments may be made online: <https://planning.lacity.org/development-services/appointment/form>.
3. **Contact Information:**

<b>DOWNTOWN OFFICES:</b>	<b>Department of Building and Safety, Affordable Housing Section</b> 201 N. Figueroa St., Ste 830 Los Angeles, CA 90012 Phone: (213) 482-0455 Web: <a href="https://ladbs.org/services/special-assistance/affordable-housing">https://ladbs.org/services/special-assistance/affordable-housing</a> Email: <a href="mailto:LADBS.AHS@lacity.org">LADBS.AHS@lacity.org</a>	<b>Department of City Planning, Preliminary Application Review Program</b> 201 N. Figueroa St., 5 <sup>th</sup> Floor Los Angeles, CA 90012 Web: <a href="https://planning.lacity.org/development-services/preliminary-application-review-program">https://planning.lacity.org/development-services/preliminary-application-review-program</a> Email: <a href="mailto:Planning.PARP@lacity.org">Planning.PARP@lacity.org</a>
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## Section I. Project Information - To be completed by applicant<sup>1</sup>

### 1. PROJECT LOCATION, ZONING & LAND USE JURISDICTION

Project Address: 12737 Mitchell Ave. Los Angeles, CA 90066

Project Name (if applicable): Mitchell Ave

Assessor Parcel Number(s): 4236019026

Legal Description (Lot, Block, Tract): 190, None, Delmar

Community Plan: Palms - Mar Vista - Del Rey Number of Parcels: 1.00 Site Area: 7,100.80 s.f.

Current Zone(s) & Height District(s): R3-1 Land Use Designation: Medium Residential

Alley in rear.....  Yes  No

Coastal Zone.....  Yes  No

Downtown Design Guide Area.....  Yes  No

Enterprise Zone.....  Yes  No

Greater Downtown Housing Incentive Area.....  Yes  No

Hillside Area (Zoning).....  Yes  No

Site contains Historical features.....  Yes  No

Special Grading Area (BOE) Area.....  Yes  No

Very High Fire Hazard Severity Zone .....  Yes  No

Specific Plan: Los Angeles Coastal Transportation Corridor

Historic Preservation Overlay Zone (HPOZ): None

Design Review Board (DRB): None

Redevelopment Project Area: None

Overlay Zone (CPIO/CDO/POD/NSO/RIO/CUGU/etc.):

Q-condition/ D-limitation/ T-classification (ordinance + subarea): None

Legal (Lot Cut Date) \_\_\_\_\_

Related City Planning Cases \_\_\_\_\_

ZIs \_\_\_\_\_

Affidavits \_\_\_\_\_

Easements \_\_\_\_\_

TOC Tier<sup>2</sup> (if applicable to project) \_\_\_\_\_

### 2. PROJECT DESCRIPTION

Project Description/Proposed Use Demo of (e) 2-story 4 unit apartment building for new 5-story 10 unit condo building with parking on ground floor.

No. of Stories: 5 No. of Dwelling Units: 10 Floor Area (Zoning): 15,293 SF

Existing Use/No. of Units: \_\_\_\_\_

### 3. APPLICANT INFORMATION<sup>3</sup>

Name: Carlos Regis

Phone: 310.322.3700

Email: carlos@breakformdesign.com

### 4. REPRESENTATIVE INFORMATION

Name: \_\_\_\_\_

Phone: \_\_\_\_\_

Email: \_\_\_\_\_

<sup>1</sup> All fields in this form must be completed. If an item is not applicable, write N/A.

<sup>2</sup> Must be verified by City Planning, Housing Services Unit

<sup>3</sup> An applicant is a person with a lasting interest in the completed project such as the property owner or a lessee/user of a project. An applicant is not someone filing a case on behalf of a client (i.e. usually not the agent/representative)

## Section II. Housing Development Project determination - To be completed by DCP staff

If a project meets any one (1) of the following categories, then the project is a Housing Development Project. Therefore, completion of Section III of this form and receipt of architectural plans stamped and signed by DBS Plan Check staff would be required for filing a City Planning application. If none of the criteria below applies, then the project is not a Housing Development Project and is not required to continue beyond this section in the Preliminary Zoning Assessment process prior to filing a City Planning application.

Housing Development Project categories (to be determined by DCP staff)	Determination: Yes or No
(a) A residential-only housing development project that creates two units or more	Yes
(b) A mixed-use development consisting of residential and nonresidential uses with at least two-thirds of the Building Area designated for residential use <sup>1</sup>	No
(c) Transitional Housing <sup>2</sup>	No
(d) Supportive Housing <sup>3</sup>	No

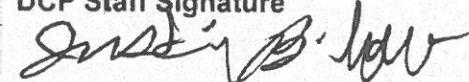
NOTES: 12737 Mitchell Ave.

Plans reviewed sent via email on 5/21/21

DCP Staff Name and Title

Justin Bellow, City Planner

DCP Staff Signature



Date

5/24/21

<sup>1</sup> "Building Area" as defined in California Building Code. Mixed-use projects may be subject to an analysis to determine whether two-thirds of the Building Area is residential.

<sup>2</sup> "Transitional Housing" as defined in California Government Code Section 65582(j)

<sup>3</sup> "Supportive Housing" as defined in California Government Code Section 65582(g)

**Section III. Preliminary Zoning Assessment - To be completed by DBS Plan Check Staff<sup>4</sup>**

Item No.	Zoning Standard	Proposed	Required/Allowed	Standard Met	Applicable Section No. <sup>5</sup>	Comments and Additional Information
1	<b>Use</b>	MULTIFAMILY CONOMINIUM	APARTMENT	<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO	12.10	<input type="checkbox"/> Conditional Use (LAMC Sec. 12.24) for _____
2	<b>Height</b>	67FT - 8 IN	45 FT	<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A	12.21.1 12.22.A.25	<input type="checkbox"/> Transitional Height applies (12.21.1-A.10) <input type="checkbox"/> Commercial Corner Development/Mini-Shopping Center height applies (12.22-A.23(a)(1)) AN OFF-MENU HEIGHT INCREASE IS REQUESTED. 12.22.A.25.F(5)1(B) HEIGHT SETBACK WAIVER IS REQUESTED.
3	<b>No. of Stories</b>	6	N/A	<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A	12.21.1 (if code prevails)	
4	<b>FAR (Floor Area Ratio)</b>	3.98:1	3:1	<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A	12.21.1 12.22.A.25	PER DENSITY BONUS, A 33% INCREASE IS REQUESTED.
5	<b>RFAR (Residential Floor Area Ratio)</b>			<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A		

4 DBS Plan Check staff will sign Section III of the Preliminary Zoning Assessment form and provide stamped and signed architectural plans once the zoning Plan Check verifications are complete.

5 Per the applicable section of the Zoning Code, Specific Plan, Zoning Overlay, Ordinance, Bonus Program, Planning Case Condition.

Item No.	Zoning Standard	Proposed	Required/Allowed	Standard Met	Applicable Section No. <sup>6</sup>	Comments and Additional Information
6	<b>Density</b>	1/473 15 UNITS	1/800 9 UNITS	<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A	12.24.U.26 12.21.A.25 12.10	<p>Density Ratio: 1/800 BY RIGHT</p> <p><input type="checkbox"/> Site Plan Review (16.05) / Major Project CUP (12.24-U.14)</p> <p>20% VERY LOW INCOME UNITS PROVIDED. OFF MENU INCENTIVE IS REQUESTED FOR INCREASE.</p>
7	<b>Setback (Front)</b>	15 FT	15 FT	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO	12.10	<p>Lot Line Location (Street): MITCHELL AVE</p> <p>Lot Line Location (Street):</p> <p>30" FRONT YARD PROJECTION OF UNCOVERED BALCONY.</p>
8	<b>Setback (Side)</b>	7FT - 2 IN	9 FT	<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO	12.10	<p><input type="checkbox"/> Offset/plane break met (if applicable)</p> <p>A 20% DECREASE IN THE REQ'D SIDEYARD SETBACK IS REQUESTED.</p>
9	<b>Setback (Rear)</b>	15 FT	15 FT	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	12.10 12.22.C.10	REAR YARD SETBACK IS MEASURED FROM CENTERLINE OF ALLEY.
10	<b>Building Line</b>	15 FT	15 FT	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	Ordinance No.: 138533	

<sup>6</sup> Per the applicable section of the Zoning Code, Specific Plan, Zoning Overlay, Ordinance, Bonus Program, Planning Case Condition.

Item No.	Zoning Standard	Proposed	Required/Allowed	Standard Met	Applicable Section No. <sup>7</sup>	Comments and Additional Information
11	<b>Parking (automobile)</b>	Residential: 15  Non-Residential: 0	Residential: 21  Non-Residential: 0	<input type="checkbox"/> YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A	12.21.A.4 12.21.A.5.C	Design standards met: <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO PROJECT DOES NOT COMPLY WITH THE FOLLOWING SECTION "In each parking area or garage devoted to parking for dwelling uses all parking stalls in excess of one parking stall per dwelling unit may be designed as compact parking stalls to accommodate compact cars." PER 12.21.A.25, AN OFF-MENUS INCENTIVE IS REQUESTED TO REDUCE THE
12	<b>Parking (bicycle)</b>	Long-term: 33  Short-term: 20	Long-term: 15  Short-term: 2	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	12.21.A.4 12.21.A.16	Facility standards met: <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO Location standards met: <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <b>9 AUTO PARKING SPACES WERE REPLACED WITH BICYCLE PARKING.</b>
13	<b>Open Space</b>	Total (s.f.): 1938  Common (s.f.): 1188  Private (s.f.): 750	Total: 1850  Common:  Private:	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	12.21-G (if code prevails)	Units/Habitable Room <3: 1 =3: 14 >3: 0  Dimensions met: <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO
14	<b>Retaining Walls in Special Grading Areas</b>	Max Height:  Max Quantity:	Max Height:  Max Quantity:	<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A	12.21-C.8 (if code prevails)	

<sup>7</sup> Per the applicable section of the Zoning Code, Specific Plan, Zoning Overlay, Ordinance, Bonus Program, Planning Case Condition.

Item No.	Zoning Standard	Proposed	Required/Allowed	Standard Met	Applicable Section No. <sup>8</sup>	Comments and Additional Information
15	<b>Grading (Zoning &amp; Planning limitations)</b>			<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A		
16	<b>Lot Coverage</b>			<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A		
17	<b>Lot Width</b>			<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A		
18	<b>Space between Buildings</b>			<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A	12.21-C.2(a) (if code prevails)	
19	<b>Passageway</b>	YES	YES	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	12.21-C.2(b) (if code prevails)	LEADS TO HALLWAY WHICH OPENS TO STREET.
20	<b>Location of Accessory Buildings</b>			<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A	12.21-C.5 (if code prevails)	

<sup>8</sup> Per the applicable section of the Zoning Code, Specific Plan, Zoning Overlay, Ordinance, Bonus Program, Planning Case Condition.

Item No.	Zoning Standard	Proposed	Required/Allowed	Standard Met	Applicable Section No. <sup>9</sup>	Comments and Additional Information
21	<b>Loading Area</b>			<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A		
22	<b>Trash &amp; Recycling</b>	YES 30 SF	YES 30 SF	<input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A	12.21.A.19	
23	<b>Landscape</b>	<i>Conformance determined by Los Angeles City Planning</i>				
24	<b>Private Street</b>	<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A		
	<b>Other (e.g. ground floor transparency, lighting, utilities, signage, walls, lot area, minimum frontage, etc.)</b>	<i>See additional sheets, if applicable</i>				Additional Sheet(s) attached: <input type="checkbox"/> YES <input checked="" type="checkbox"/> NO
<b>Plan Check Application No.<sup>10</sup></b> 21010-10001-02704				<b>Notes</b>		
<b>DBS Plan Check Staff Name and Title</b> KEVIN MORALES SEA II			<b>DBS Plan Check Staff Signature<sup>11</sup></b> 		<b>Date</b> 11.09.21	

<sup>9</sup> Per the applicable section of the Zoning Code, Specific Plan, Zoning Overlay, Ordinance, Bonus Program, Planning Case Condition.

<sup>10</sup> This completed form shall be accompanied by plans stamped and signed by a DBS Plan Check staff following the completion of a zoning Plan Check.

<sup>11</sup> LADBS Plan Check staff will sign Section III of the Preliminary Zoning Assessment Form once the zoning plan check verifications are complete.

**ADDITIONAL ZONING AND LAND USE STANDARDS REVIEWED - to be completed by DBS Plan Check Staff**

Item No.	Zoning Standard	Proposed	Required/Allowed	Standard Met	Applicable Section No.	Comments and Additional Information
				<input type="checkbox"/> YES <input type="checkbox"/> NO		
				<input type="checkbox"/> YES <input type="checkbox"/> NO		
				<input type="checkbox"/> YES <input type="checkbox"/> NO		
				<input type="checkbox"/> YES <input type="checkbox"/> NO		
				<input type="checkbox"/> YES <input type="checkbox"/> NO		



Eric Garcetti, Mayor  
Ann Seville, General Manager

DATE: September 1, 2021

TO: 12737 Mitchell Investment LLC, a California limited liability company, Owner  
FROM: Marites Cunanan, Senior Management Analyst II *[Signature]*  
Los Angeles Housing Department

Digitally signed by Marites Cunanan  
Date: 2021.09.01 18:18:10  
-07:00

SUBJECT: **Housing Crisis Act of 2019 (DB)**  
**Amended Replacement Unit Determination**  
**RE: 12735 W. Mitchell Ave., Los Angeles, CA 90066**  
**12737 W. Mitchell Ave., Los Angeles, CA 90066**

Based on the Application for a Replacement Unit Determination (RUD) submitted by 12737 Mitchell Investment LLC, a California Limited Liability Company (Owner) for the above referenced property located at 12735-12737 W. Mitchell Ave., (Property), the Los Angeles Housing Department (LAHD) has determined that four (4) units are subject to replacement pursuant to the requirements of the Housing Crisis Act of 2019 (SB 330).

#### **PROJECT SITE REQUIREMENTS:**

SB 330 prohibits the approval of any proposed housing development project on a site that will require the demolition of existing residential dwelling units or occupied or vacant "Protected Units" unless the proposed housing development project replaces those units as specified below. The replacement requirements below are applicable only to those proposed housing development projects that submit a complete application pursuant to California Government Code Section 65943 to the Department of City Planning on or after January 1, 2020.

#### **Replacement of Existing Residential Dwelling Units.**

The proposed housing development project shall provide at least as many residential dwelling units as the greatest number of residential dwelling units that existed on the project site within the past 5 years.

#### **Replacement of Existing or Demolished Protected Units.**

The proposed housing development project must also replace all existing or demolished "Protected Units." Protected Units are those residential dwelling units that are or were within the 5 years prior to the owner's application for a Replacement Unit Determination: (1) subject to a recorded covenant, ordinance, or law that restricts rents to levels affordable to persons and families of lower or very low income, (2) subject to any form of rent or price control through a public entity's valid exercise of its police power within the 5 past years, (3) **occupied by lower or very low income households (an affordable Protected Unit)**, or (4) that were withdrawn from rent or lease per the Ellis Act, within the past 10 years.

Whether a unit qualifies as an affordable Protected Unit, is primarily measured by the income level of the occupants (i.e. W-2 forms, tax return, pay stubs etc.). In the absence of occupant income documentation, affordability will default to the percentage of extremely low, very low, and low income renters in the jurisdiction as shown in the latest HUD Comprehensive Housing Affordability Strategy (CHAS) database, which at the time this project's application was received, was presently at 30% extremely low income, 19% very low income and 18% low income for Transit Oriented communities (TOC) projects and 49% very low income an 18% low income for Density Bonus (DB) projects. The remaining 33% of the units are presumed above-low income and if subject to the Rent Stabilization Ordinance ("RSO"), must be replaced in accordance with the RSO. All replacement calculations resulting in fractional units shall be rounded up to the next whole number.

### Relocation, Right of Return, Right to Remain for Occupants of Protected Units.

SB 330 also provides the right of first refusal for comparable units (i.e. same bedroom type) in the Owner's proposed new housing development to occupants of Protected Units. Therefore, for occupied units, the replacement units must be of the same bedroom type of the units demolished. The comparable replacement units must be provided at a rent or sales price affordable to the same or lower income category. Occupants of Protected Units also are entitled to receive relocation to state or local law, whichever provides greater assistance and the right to remain in their unit until 6 months before the start of construction.

### **THE PROPOSED HOUSING DEVELOPMENT PROJECT:**

Per the statement received by LAHD on June 29, 2021, the Owner plans to construct a new rental apartment with fifteen (15) residential units using DB.

### **STATUS OF PROJECT SITE/PROPERTY:**

Owner submitted an Application for a RUD for the Property on June 29, 2021. In order to comply with the required 5-year look back period, LAHD must collect and review data from June 2016 to June 2021.

### **Review of Documents:**

Pursuant to the Owner's Grant Deed, the Property was acquired on or around June 22, 2021 by the Owner from David P. Grgis, a single man and Moses J. Grgis, a married man, as his sole and separate property as joint tenants.

Per Department of City Planning (ZIMAS), County Assessor Parcel Information (LUPAMS), DataTree database, Billing Information Management System (BIMS) database, and the Code, Compliance, and Rent Information System (CRIS) database for the Property indicates, the Property commonly known as 12735 W. Mitchell Ave. has a use code of "0400 – Residential – Four Units (Any Combination) – 4 Stories or Less." Google Earth and Google Maps street view shows what appears to be a front single family dwelling and a rear triplex.

The Owner filed for a new building permit, #21010-10000-02704, on June 1, 2021 with the Los Angeles Department of Building and Safety (LADBS). The Owner has not filed a demolition permit with LADBS.

### **REPLACEMENT UNIT DETERMINATION:**

The Existing Residential Units at the Property:

ADDRESS	BEDROOM TYPE	"PROTECTED?"	BASIS OF "PROTECTED" STATUS
12735 #1	2 Bedroom	Yes	RSO
12735 #2	2 Bedroom	Yes	RSO
12735 #3	2 Bedroom	Yes	RSO
12737	3 Bedroom	Yes	RSO
<b>Total: 8 units</b>	<b>9 bedrooms</b>		

No income documents were provided for these unit(s). Pursuant to SB 330, where incomes of existing or former tenants are unknown, the required percentage of affordability is determined by the percentage of extremely low, very low, and low income rents in the jurisdiction as shown in CHAS database. On the date of the original application, the CHAS database shows 49% Very Low (31% to 50% AMI), and 18% Low (51% to 80% AMI).

The balance of these units (i.e. 33%) are presumed to have been occupied by persons and families above-lower income.

<b>Number of Existing Residential Dwelling Units and Protected Units within five (5) years of Owner's application:</b>	4
<b>Number of Protected Units Ellissed within the last (10) years:</b>	0
<b>Number of Affordable Replacement Units required per CHAS:</b>	
4 Units x 67%	3 Units
49% Very Low	2 Units
18% Low	1 Unit
Market Rate RSO Units	1 Units
<b>Number of Unit(s) presumed to be above-lower income subject to replacement:</b>	1

**For Rental:**

Pursuant to CHAS, three (3) unit(s) need to be replaced with two (2) unit(s) restricted to Very Low Income Households and one (1) unit restricted to Low Income Households.

**Vacancy/Occupancy of Units:**

At the time LAHD received this project's application on June 29, 2021, Owner stated that all units were vacant. Based on information received from the Los Angeles Department of Power and Water (LADWP), the level of usage in utilities is consistent with this statement. For the four (4) units that were vacant, per CHAS, three (3) of the units must be replaced with units of equivalent type.

Please note that all the new units may be subject to RSO requirements unless an RSO Exemption is filed and approved by the RSO Section. This determination is provisional and subject to verification by the RSO Section.

This SB 330 determination only applies if the proposed project is a rental DB project and NOT condominiums. In the event the project changes to condominiums, the Owner needs to request a SB 330 amendment to reflect 100% replacement of the units. In addition, if the project is changed from DB to TOC or vice-versa, a SB 330 amendment will also be required and an amendment fee will apply.

**\*\*WARNING\*\***

**LOT TIES AND EXISTING PRE-1978 SINGLE FAMILY DWELLING ON ONE LOT**

<b>ISSUE:</b>	Is a <b>LOT TIE</b> required for the <b>NEW</b> proposed housing development project?
<b>IF NO:</b>	Owner's existing Rent Stabilization (RSO) replacement obligation, if any, remains the <b>SAME</b> as above.
<b>IF YES:</b>	Owner's existing RSO replacement obligation, if any, will <b>INCREASE</b> by one and the proposed housing development project will also be subject to the RSO, unless the existing single family dwelling is demolished before the lot are tied.

If you have any questions about this RUD, please contact Richard Truong at (213) 808-8846, or richard.s.truong@lacity.org.

**NOTE: This determination is provisional and is subject to verification by LAHD's Rent Division.**

cc: Los Angeles Housing Department File  
12737 Mitchell Investment LLC, a California limited liability company, Owner  
Planning.PARP@lacity.org, Department of City Planning

MAC:rt

**CITY OF LOS ANGELES**  
**INTER-DEPARTMENTAL CORRESPONDENCE**

**Date:** March 4, 2022

**To:** Mr. Vincent Bertoni, Director  
Department of City Planning  
Attn: Elizabeth Gallardo (City Planner)

*Then Locket* for  
**From:** Bertram Moklebust, Principal Civil Engineer  
Permit Case Management Division  
Bureau of Engineering

**Subject: Case No. CPC 2021-10394 (CU/DB/HCA): 12735-12737 West  
Michell Avenue**

The following recommendations identifying the infrastructure deficiencies adjacent to the application site are submitted for your use for the approval of a Conditional Use Permit, Density Bonus and Housing Crisis Act adjoining the area involved:

1. Dedication Required:

**Mitchell Avenue** (Local Street) – None.

**Alley** (North of Mitchell Avenue) – A 2.5-foot wide strip of land along alley frontage to complete a 10-foot wide half alley right-of-way.

Improvements Required:

**Michell Avenue** – Remove the existing improvements and reconstruct a new minimum 5-foot wide concrete sidewalk, integral concrete curb and 2-foot gutter along the property frontage. Repair all broken, off-grade asphalt concrete pavement and construct new driveway to comply with ADA requirements.

**Alley** – Reconstruct the 17.5-foot alley with asphalt pavement adjoining the new property line and reconstruct the 2-foot wide longitudinal concrete gutter. These improvements should suitably transition to join the existing improvements.

**Note:** Broken curb and/or gutter includes segments within existing score lines that are depressed or upraised by more than  $\frac{1}{4}$  inch from the surrounding concrete work or are separated from the main body of the concrete piece by a crack through the entire vertical segment and greater than  $\frac{1}{8}$  inch at the surface of the section.

Non- ADA compliant sidewalk shall include any sidewalk that has a cross slope that exceeds 2% and/or is depressed or upraised by more than  $\frac{1}{4}$  inch from the surrounding concrete work or has full concrete depth cracks that have separations greater than  $\frac{1}{8}$  inch at the surface. The sidewalk also includes that portion of the pedestrian path of travel across a driveway.

All new sidewalk curb and gutter shall conform to the Bureau of Engineering Standard Plans S410-2, S440-4, S442-5 and S444-0.

Install tree wells with root barriers and plant street trees satisfactory to the City Engineer and the Urban Forestry Division of the Bureau of Street Services. The applicant should contact the Urban Forestry Division for further information (213) 847-3077.

Notes: Street lighting may be required satisfactory to the Bureau of Street Lighting (213) 847-1551.

Department of Transportation may have additional requirements for dedication and improvements.

Regarding any issue with power poles and gas meters, contact the Department of Water and Power (213) 367-2715.

2. Drain the roof and site to the public right-of-way.
3. Sewer line exist in the Alley. All Sewerage Facilities Charges and Bonded Sewer Fees are to be paid prior to obtaining a building permit.
4. An investigation by the Bureau of Engineering WLA District Office Sewer Counter may be necessary to determine the capacity of the existing public sewers to accommodate the proposed development. Submit a request to the West Los Angeles District Office of the Bureau of Engineering at (310) 575-8384.
5. Submit parking area and driveway plans to the WLA District Office of the Bureau of Engineering and the Department of Transportation for review and approval.

Any questions regarding this report may be directed to Quyen M. Phan of my staff at (213) 808-8604.

cc: West Los Angeles District Office

**CITY OF LOS ANGELES**  
**INTER-DEPARTMENTAL CORRESPONDENCE**

February 4, 2022

TO: Vincent Bertoni, AICP, Director of Planning  
Department of City Planning  
Attention: Elizabeth Gallardo

FROM: Los Angeles Fire Department

SUBJECT: **CPC-2021-10394-CU-DB-HCA.:12735 Mitchell**

Submit plot plans for Fire Department approval and review prior to recordation of City Planning Case.

**RECOMMENDATIONS:**

Access for Fire Department apparatus and personnel to and into all structures shall be required.

Address identification. New and existing buildings shall have approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property.

One or more Knox Boxes will be required to be installed for LAFD access to project. Location and number to be determined by LAFD Field Inspector. (Refer to FPB Req # 75).

The entrance or exit of all ground dwelling units shall not be more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.

The Fire Department may require additional vehicular access where buildings exceed 28 feet in height.

Smoke Vents may be required where roof access is not possible; location and number of vents to be determined at Plan Review.

Where above ground floors are used for residential purposes, the access requirement shall be interpreted as being the horizontal travel distance from the street, driveway, alley, or designated fire lane to the main entrance of individual units.

No building or portion of a building shall be constructed more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.

The following recommendations of the Fire Department relative to fire safety shall be incorporated into the building plans, which includes the submittal of a plot plan for approval by the Fire Department either prior to the recordation of a final map or the approval of a building permit. The plot plan shall include the following minimum design features: fire lanes, where required, shall be a minimum of 20 feet in width; all structures must be within 300 feet of an approved fire hydrant, and entrances to any dwelling unit or guest room shall not be more than

150 feet in distance in horizontal travel from the edge of the roadway of an improved street or approved fire lane.

2014 CITY OF LOS ANGELES FIRE CODE, SECTION 503.1.4 (EXCEPTION)

- a. When this exception is applied to a fully fire sprinklered residential building equipped with a wet standpipe outlet inside an exit stairway with at least a 2 hour rating the distance from the wet standpipe outlet in the stairway to the entry door of any dwelling unit or guest room shall not exceed 150 feet of horizontal travel AND the distance from the edge of the roadway of an improved street or approved fire lane to the door into the same exit stairway directly from outside the building shall not exceed 150 feet of horizontal travel.
- b. It is the intent of this policy that in no case will the maximum travel distance exceed 150 feet inside the structure and 150 feet outside the structure. The term "horizontal travel" refers to the actual path of travel to be taken by a person responding to an emergency in the building.
- c. This policy does not apply to single-family dwellings or to non-residential buildings.

Site plans shall include all overhead utility lines adjacent to the site.

Where access for a given development requires accommodation of Fire Department apparatus, overhead clearance shall not be less than 14 feet.

No proposed development utilizing cluster, group, or condominium design of one or two family dwellings shall be more than 150 feet from the edge of the roadway of an improved street, access road, or designated fire lane.

**FPB #105**

5101.1 Emergency responder radio coverage in new buildings. All new buildings shall have approved radio coverage for emergency responders within the building based upon the existing coverage levels of the public safety communication systems of the jurisdiction at the exterior of the building. This section shall not require improvement of the existing public safety communication systems.

That in order to provide assurance that the proposed common fire lane and fire protection facilities, for the project, not maintained by the City, are properly and adequately maintained, the sub-divider shall record with the County Recorder, prior to the recordation of the final map, a covenant and agreement (Planning Department General Form CP-6770) to assure the following:

- A. The establishment of a property owners association, which shall cause a yearly inspection to be, made by a registered civil engineer of all common fire lanes and fire protection facilities. The association will undertake any necessary maintenance and corrective measures. Each

future property owner shall automatically become a member of the association or organization required above and is automatically subject to a proportionate share of the cost.

B. The future owners of affected lots with common fire lanes and fire protection facilities shall be informed of their responsibility for the maintenance of the devices on their lots. The future owner and all successors will be presented with a copy of the maintenance program for their lot. Any amendment or modification that would defeat the obligation of said association as the Advisory Agency must approve required hereinabove in writing after consultation with the Fire Department.

C. In the event that the property owners association fails to maintain the common property and easements as required by the CC and R's, the individual property owners shall be responsible for their proportional share of the maintenance.

D. Prior to any building permits being issued, the applicant shall improve, to the satisfaction of the Fire Department, all common fire lanes and install all private fire hydrants to be required.

E. That the Common Fire Lanes and Fire Protection facilities be shown on the Final Map.

The plot plans shall be approved by the Fire Department showing fire hydrants and access for each phase of the project prior to the recording of the final map for that phase. Each phase shall comply independently with code requirements.

Any roof elevation changes in excess of 3 feet may require the installation of ships ladders.

Provide Fire Department pathway front to rear with access to each roof deck via gate or pony wall less than 36 inches.

Building designs for multi-storied residential buildings shall incorporate at least one access stairwell off the main lobby of the building; But, in no case greater than 150ft horizontal travel distance from the edge of the public street, Private Street or Fire Lane. This stairwell shall extend onto the roof.

Entrance to the main lobby shall be located off the address side of the building.

Any required Fire Annunciator panel or Fire Control Room shall be located within 20ft visual line of site of the main entrance stairwell or to the satisfaction of the Fire Department.

Where rescue window access is required, provide conditions and improvements necessary to meet accessibility standards as determined by the Los Angeles Fire Department.

Adequate off-site public and on-site private fire hydrants may be required. Their number and location to be determined after the Fire Department's review of the plot plan.

Any required fire hydrants to be installed shall be fully operational and accepted by the Fire Department prior to any building construction.

Elizabeth.gallardo@lacity.org

February 4, 2022

CPC-2021-10394-CU-DB-HCA.:12735 Mitchell

Page 4

The applicant is further advised that all subsequent contact regarding these conditions must be with the Hydrant and Access Unit. This would include clarification, verification of condition compliance and plans or building permit applications, etc., and shall be accomplished **BY** **APPOINTMENT ONLY**, in order to assure that you receive service with a minimum amount of waiting please call **(213) 482-6543**. You should advise any consultant representing you of this requirement as well.

RALPH M. TERRAZAS  
Fire Chief

Kristin Crowley, Fire Marshal  
Bureau of Fire Prevention and Public Safety

KC:MRC:mrc

CPC-2021-10394-CU-DB-HCA.:12735 Mitchell

**CITY OF LOS ANGELES**  
INTER-DEPARTMENTAL CORRESPONDENCE

**DATE:** February 18, 2022

**TO:** Sergio Ibarra, City Planner  
Department of City Planning

**FROM:**   
Bryah Ramirez, Street Tree Superintendent I  
Bureau of Street Services, Urban Forestry Division

**SUBJECT:** CPC-2021-10394-CU-DB-HCA – 12735 W. MITCHELL AVE

In regard to your request for review of this case regarding Urban Forestry requirements, it is our recommendation that:

**1. STREET TREES**

- a. Project shall preserve all healthy mature street trees whenever possible. All feasible alternatives in project design should be considered and implemented to retain healthy mature street trees. A permit is required for the removal of any street tree and shall be replaced 2:1 as approved by the Board of Public Works and Urban Forestry Division.
- b. Plant street trees at all feasible planting locations within dedicated streets as directed and required by the Bureau of Street Services, Urban Forestry Division. All tree plantings shall be installed to current tree planting standards when the City has previously been paid for tree plantings. The subdivider or contractor shall notify the Urban Forestry Division at: (213) 847-3077 upon completion of construction for tree planting direction and instructions.

**Note:** Removal of street trees requires approval from the Board of Public Works. All projects must have environmental (CEQA) documents that appropriately address any removal and replacement of street trees. Contact Urban Forestry Division at: (213) 847-3077 for tree removal permit information.

BR:djm

**CITY OF LOS ANGELES**  
INTER-DEPARTMENTAL CORRESPONDENCE

**Date:** 1/24/2022

**To:** Charlie Rausch, Senior City Planner  
Department of City Planning  
200 N. Spring St., 6th Floor MS-395

**From:**   
Gil De La Cruz, P.E.  
Case Management Supervisor  
Private Development Division  
Bureau of Street Lighting

**SUBJECT: STREET LIGHTING REQUIREMENTS FOR DISCRETIONARY ACTIONS**

**CITY PLANNING CASE No.:** CPC 2021-10394 CU DB HCA  
12735 W MITCHELL AVE

The Bureau of Street Lighting's recommended condition of approval for the subject city planning case is as follows: (Improvement condition added to S-3 (c) where applicable.)

**IMPROVEMENT CONDITION:** No street lighting requirements.

**NOTES:**

The quantity of street lights identified may be modified slightly during the plan check process based on illumination calculations and equipment selection.

Conditions set: 1) in compliance with a Specific Plan, 2) by LADOT, or 3) by other legal instrument excluding the Bureau of Engineering conditions, requiring an improvement that will change the geometrics of the public roadway or driveway apron may require additional or the reconstruction of street lighting improvements as part of that condition.

## **EXHIBIT D**

### **ENVIRONMENTAL CLEARANCE**

**ENV-2021-10395-CE**

D1 – Notice of Exemption & Justification for  
Categorical Exemption

D2 – Tree Letter

D3 – DOT Referral Form & VMT Calculator

D4 – LADBS Soils Report Approval Letter &  
Geotechnical Reports

## CITY OF LOS ANGELES

OFFICE OF THE CITY CLERK

200 NORTH SPRING STREET, ROOM 395  
LOS ANGELES, CALIFORNIA 90012

## CALIFORNIA ENVIRONMENTAL QUALITY ACT

## NOTICE OF EXEMPTION

(PRC Section 21152; CEQA Guidelines Section 15062)

Pursuant to Public Resources Code § 21152(b) and CEQA Guidelines § 15062, the notice should be posted with the County Clerk by mailing the form and posting fee payment to the following address: Los Angeles County Clerk/Recorder, Environmental Notices, P.O. Box 1208, Norwalk, CA 90650. Pursuant to Public Resources Code § 21167 (d), the posting of this notice starts a 35-day statute of limitations on court challenges to reliance on an exemption for the project. Failure to file this notice as provided above, results in the statute of limitations being extended to 180 days.

## PARENT CASE NUMBER(S) / REQUESTED ENTITLEMENTS

CPC-2021-10394-CU-DB-HCA-PHP

LEAD CITY AGENCY <b>City of Los Angeles (Department of City Planning)</b>	CASE NUMBER ENV-2021-10395-CE
PROJECT TITLE 12735-12737 West Mitchell Avenue	COUNCIL DISTRICT 11 - Bonin
PROJECT LOCATION (Street Address and Cross Streets and/or Attached Map) <b>12735-12737 West Mitchell Avenue, Los Angeles, CA 90066</b>	<input type="checkbox"/> Map attached.
PROJECT DESCRIPTION: The project is the construction of a six-story, 67-foot 8-inch tall residential building comprised of 15 dwelling units (including 2 Very Low Income and 1 Low Income units). The project will be approximately 19,047 square feet in floor area with a Floor Area Ratio ("FAR") of 3.99:1. The project will provide 14 parking spaces at-grade. The site is currently improved with a two-story single-family dwelling and triplex which will be demolished for the project. No existing significant trees will be removed. The project will also involve minimal grading of less than 500 cubic yards of soil.	<input type="checkbox"/> Additional page(s) attached.
NAME OF APPLICANT / OWNER: <b>12737 Mitchell Investment, LLC / Brian Silveira &amp; Associates</b>	
CONTACT PERSON (If different from Applicant/Owner above) <b>Connie Chauv</b>	(AREA CODE) TELEPHONE NUMBER 213 978 0016   EXT.
EXEMPT STATUS: (Check all boxes, and include all exemptions, that apply and provide relevant citations.)	
STATE CEQA STATUTE & GUIDELINES	
<input type="checkbox"/> STATUTORY EXEMPTION(S) Public Resources Code Section(s) _____	
<input checked="" type="checkbox"/> CATEGORICAL EXEMPTION(S) (State CEQA Guidelines Sec. 15301-15333 / Class 1-Class 33) CEQA Guideline Section(s) / Class(es) <u>Section 15332 (Class 32)</u>	
<input type="checkbox"/> OTHER BASIS FOR EXEMPTION (E.g., CEQA Guidelines Section 15061(b)(3) or (b)(4) or Section 15378(b) )	

JUSTIFICATION FOR PROJECT EXEMPTION: Class 32: In-fill development meeting the conditions described in CEQA Guidelines 15332: (a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with the applicable zoning designation and regulations. (b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. (c) The project site has no value as habitat for endangered, rare or threatened species. (d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. (e) The site can be adequately served by all required utilities and public services.	<input type="checkbox"/> Additional page(s) attached
<input type="checkbox"/> None of the exceptions in CEQA Guidelines Section 15300.2 to the categorical exemption(s) apply to the Project. <input type="checkbox"/> The project is identified in one or more of the list of activities in the City of Los Angeles CEQA Guidelines as cited in the justification.	
IF FILED BY APPLICANT, ATTACH CERTIFIED DOCUMENT ISSUED BY THE CITY PLANNING DEPARTMENT STATING THAT THE DEPARTMENT HAS FOUND THE PROJECT TO BE EXEMPT.	
If different from the applicant, the identity of the person undertaking the project.	
CITY STAFF USE ONLY:	
CITY STAFF NAME AND SIGNATURE Connie Chauv	STAFF TITLE City Planner
ENTITLEMENTS APPROVED Density Bonus, Conditional Use	

DISTRIBUTION: County Clerk, Agency Record

Rev. 6-22-2021



ERIC GARCETTI  
MAYOR

## JUSTIFICATION FOR PROJECT EXEMPTION ENV-2021-10395-CE

The Planning Department determined that the City of Los Angeles Guidelines for the implementation of the California Environmental Quality Act of 1970 and the CEQA Guidelines designate the subject project as Categorically Exempt under CEQA Guidelines, Article 19, Section 15332 (Class 32), Case No. ENV-2021-10395-CE.

The project is the construction of a six-story, 67-foot 8-inch tall residential building comprised of 15 dwelling units (including 2 Very Low Income and 1 Low Income units). The project will be approximately 19,047 square feet in floor area with a Floor Area Ratio ("FAR") of 3.99:1. The project will provide 14 parking spaces at-grade. The site is currently improved with a two-story single-family dwelling and triplex which will be demolished for the project. No existing significant trees will be removed. The project will also involve minimal grading of less than 500 cubic yards of soil.

As a residential building, and a project which is characterized as in-fill development, the project qualifies for the Class 32 Categorical Exemption.

### **CEQA Determination – Class 32 Categorical Exemption Applies**

A project qualifies for a Class 32 Categorical Exemption if it is developed on an infill site and meets the following criteria:

(a) **The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with the applicable zoning designation and regulations.**

The project site is in the Palms – Mar Vista – Del Rey Community Plan, and is designated for Medium Residential land uses, with corresponding zones of R3 and R3(PV). The site is zoned R3-1 and is therefore consistent with the land use designation. The project site is in the Los Angeles Coastal Transportation Corridor Specific Plan ("LACTC", Ordinance Nos. 186,104 and 186,105), and the Project is subject to Department of Transportation clearance of the LACTC. Height District No. 1 limits the Floor Area Ratio ("FAR") to 3:1 and building height to 45 feet with no limit on the number of stories; however, the proposed project will have a FAR of 3.99:1 and a height of 67 feet 8 inches as permitted by State Density Bonus Law in exchange for providing two (2) units for Very Low Income Households and one (1) unit for Low Income Households for 55 years. There is a 15-foot Building Line along Mitchell Avenue established under Ordinance No. 138533. As demonstrated in the case file, the project is consistent with the General Plan, the applicable Palms – Mar Vista – Del Rey Community Plan designation and policies, and all applicable zoning designations and regulations as permitted by Density Bonus law.

**(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.**

The subject site is wholly within the City of Los Angeles, on a site that is approximately 0.163 acres (7,100 square feet) and is surrounded primarily by multi-family residential uses and some single-family dwellings. Properties immediately adjacent to the site are developed primarily with two- and three-story multi-family residential uses and are similarly zoned R3-1, and surrounding properties are developed or proposed for multi-family buildings up to five stories tall. Other parcels further south fronting Washington Boulevard are zoned C2-1 and developed with commercial uses including a liquor store, laundromat, as well as a schoolhouse; further southwest is the city boundary for Culver City. The subject site is within 500 feet of several bus stops served by the Santa Monica Big Blue Bus 16 line, and Culver CityBus 1 and 2 lines.

**(c) The project site has no value as habitat for endangered, rare or threatened species.**

The site is previously disturbed and surrounded by development and therefore is not, and has no value as, a habitat for endangered, rare or threatened species. The site is currently improved with a single-family dwelling and triplex; all existing structures will be demolished. There are no protected trees or shrubs on the subject site or in the adjacent public right of way that would be removed as verified in the Tree Letter prepared by The Urban Lumberjack dated December 7, 2021.

**(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.**

Regulatory Compliance Measures – The project will be subject to Regulatory Compliance Measures (RCMs), which require compliance with the City of Los Angeles Noise Ordinance, pollutant discharge, dewatering, stormwater mitigations; and Best Management Practices for stormwater runoff. More specifically, RCMs include but are not limited to the following, to ensure the project will not have significant impacts:

- **Regulatory Compliance Measure RC-AQ-1 (Demolition, Grading and Construction Activities): Compliance with provisions of the SCAQMD District Rule 403.** The project shall comply with all applicable standards of the Southern California Air Quality Management District, including the following provisions of District Rule 403:
  - All unpaved demolition and construction areas shall be wetted at least twice daily during excavation and construction, and temporary dust covers shall be used to reduce dust emissions and meet SCAQMD District Rule 403. Wetting could reduce fugitive dust by as much as 50 percent.
  - The construction area shall be kept sufficiently dampened to control dust caused by grading and hauling, and at all times provide reasonable control of dust caused by wind.
  - All clearing, earth moving, or excavation activities shall be discontinued during periods of high winds (i.e., greater than 15 mph), so as to prevent excessive amounts of dust.
  - All dirt/soil loads shall be secured by trimming, watering or other appropriate means to prevent spillage and dust.
  - All dirt/soil materials transported off-site shall be either sufficiently watered or securely covered to prevent excessive amount of dust.

- General contractors shall maintain and operate construction equipment so as to minimize exhaust emissions.
- Trucks having no current hauling activity shall not idle but be turned off.
- **Regulatory Compliance Measure RC-AQ-2:** In accordance with Sections 2485 in Title 13 of the California Code of Regulations, the idling of all diesel-fueled commercial vehicles (weighing over 10,000 pounds) during construction shall be limited to five minutes at any location.
- **Regulatory Compliance Measure RC-AQ-3:** In accordance with Section 93115 in Title 17 of the California Code of Regulations, operation of any stationary, diesel-fueled, compression-ignition engines shall meet specified fuel and fuel additive requirements and emission standards.
- **Regulatory Compliance Measure RC-AQ-4:** The Project shall comply with South Coast Air Quality Management District Rule 1113 limiting the volatile organic compound content of architectural coatings.
- **Regulatory Compliance Measure RC-AQ-5:** The Project shall install odor-reducing equipment in accordance with South Coast Air Quality Management District Rule 1138.
- **Regulatory Compliance Measure RC-AQ-6:** New on-site facility nitrogen oxide emissions shall be minimized through the use of emission control measures (e.g., use of best available control technology for new combustion sources such as boilers and water heaters) as required by South Coast Air Quality Management District Regulation XIII, New Source Review.
- **Regulatory Compliance Measure RC-GEO-1 (Seismic):** The design and construction of the project shall conform to the California Building Code seismic standards as approved by the Department of Building and Safety.
- **Regulatory Compliance Measure RC-NO-1 (Demolition, Grading, and Construction Activities):** The project shall comply with the City of Los Angeles Noise Ordinance and any subsequent ordinances, which prohibit the emission or creation of noise beyond certain levels at adjacent uses unless technically infeasible.

These RCMs will ensure the project will not have significant impacts on noise and water.

Geotechnical - The applicant has submitted a Geotechnical Engineering Exploration Report prepared by Irvine Geotechnical, Inc. dated February 24, 2021, and a Supplemental Geotechnical Engineering Exploration Report dated April 18, 2022. RCMs also include the submittal of the Geology and Soils Report to the Department of Building and Safety (“DBS”), and compliance with a Soils Report Approval Letter (Log No. 116492, dated March 10, 2021) which details conditions of approval that must be followed. In addition, the RCMs require that design and construction of the building must conform to the California Building Code, and grading on site shall comply with the City’s Landform Grading Manual, as approved by the Department of Building and Safety Grading Division.

Traffic - The Project does not exceed the threshold criteria established by LADOT for preparing a traffic study. The Department of Transportation (LADOT) Referral Form dated June 2, 2022 and the Vehicle Miles Traveled (VMT) calculator indicated that the number of daily vehicle trips will be 72 which is under the threshold of 250 or more daily vehicles trips to require VMT analysis. Therefore, the project does not exceed the threshold criteria established by LADOT for preparing a traffic study and will not have any significant impacts related to traffic.

Noise – The Project must comply with the adopted City of Los Angeles Noise Ordinances No. 144,331 and 161,574 and LAMC Section 41.40 as indicated above in RC-NO-1, LAMC

Section 112.05, as well as any subsequent Ordinances, which prohibit the emission or creation of noise beyond certain levels. These Ordinances cover both operational noise levels (i.e., post-construction), and any construction noise impacts. The Project does not exceed the threshold criteria for preparing a noise study. As a result of this mandatory compliance, the proposed Project will not result in any significant noise impacts.

Air Quality – There are several Regulatory Compliance Measures listed above (RC-AQ-1 through RC-AQ-6) which regulate air quality-related impacts for projects citywide. The Project does not exceed the threshold criteria for preparing an air quality study; at 15 dwelling units, the Project is well under the screening criteria of 80 units for air quality studies. As a result of this mandatory compliance, the proposed Project will not result in any significant air quality impacts.

**(e) The site can be adequately served by all required utilities and public services.**

The project site will be adequately served by all public utilities and services given that the construction of a multi-family building will be on a site which has been previously developed and is consistent with the General Plan.

Therefore, the project meets all of the Criteria for the Class 32 Categorical Exemption.

**CEQA Section 15300.2: Exceptions to the Use of Categorical Exemptions**

There are five (5) Exceptions which must be considered in order to find a project exempt under Class 32:

**(a) Cumulative Impacts.** *All exemptions for these classes are inapplicable when the cumulative impact of successive projects of the same type in the same place, over time is significant.*

There are other projects proposed, approved, or under construction within proximity to the site, including but not limited to:

- 12759 West Caswell Avenue – construction of a new 5-story 14-unit apartment building
- 3984 South Meier Street – construction of a 5-story, 40-unit apartment building

While there could potentially be a succession of known projects of the same type and in the same place as the subject project, all projects are subject to the citywide Regulatory Compliance measures as noted above, which regulate impacts related to air quality, noise, and geology to a less than significant level. There is no evidence to conclude that significant impacts will occur based on past project approvals or that the proposed Project's impacts are cumulatively considerable when evaluating any cumulative impacts associates with construction noise and transportation/traffic in the surrounding area.

Therefore, in conjunction with citywide RCMs and compliance with other applicable regulations, no foreseeable cumulative impacts are expected.

**(b) Significant Effect Due to Unusual Circumstances.** *A categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances.*

The project proposes a residential building in an area zoned and designated for such development. All adjacent lots are developed with multi-family and single-family residential uses, and the subject site is of a similar size and slope to nearby properties. The project proposes a FAR of 3.99:1 on a site that is permitted to have an FAR of 3.0:1 by the site's zoning. The project is eligible for the FAR 3.99:1 through an On-Menu Density Bonus Incentive. The project size and height is not unusual for the vicinity of the subject site, and is similar in scope to other existing multi-family dwellings and proposed future projects in the area. Furthermore, there is no substantial evidence in the administrative record that this project will cause a significant impact. Thus, there are no unusual circumstances which may lead to a significant effect on the environment, and this exception does not apply.

(c) **Scenic Highways.** *A categorical exemption shall not be used for a project which may result in damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway.*

The only State Scenic Highway within the City of Los Angeles is the Topanga Canyon State Scenic Highway, State Route 27, which travels through a portion of Topanga State Park. State Route 27 is located approximately 8.7 miles northwest of the subject site. Therefore, the subject site will not create any impacts within a designated state scenic highway, and this exception does not apply.

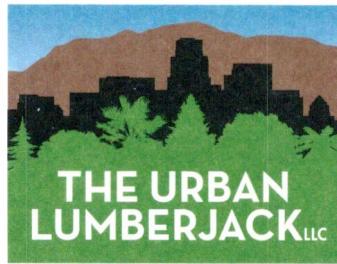
(d) **Hazardous Waste Sites.** *A categorical exemption shall not be used for a project located on a site which is included on any list complied pursuant to Section 65962.5 of the Government Code*

According to Envirostor, the State of California's database of Hazardous Waste Sites, neither the subject site, nor any site in the vicinity, is identified as a hazardous waste site. Therefore, the project is not identified as a hazardous waste site, or in the vicinity of a hazardous waste site, and this exception does not apply.

(e) **Historical Resources.** *A categorical exemption shall not be used for a project which may cause a substantial adverse change in the significance of a historical resource.*

The project site is not listed in the National Register of Historic Places, California Register of Historical Resources, the Los Angeles Historic-Cultural Monuments Register, and/or any local register, and was not found to be a potential historic resource based on the City's HistoricPlacesLA website or SurveyLA, the citywide survey of Los Angeles. As such, the Project would have no impact on historical resources. Based on this, the project will not result in a substantial adverse change to the significance of a historic resource and this exception does not apply.

12-7-2021



Tree Inventory Report  
12735 and 12737 Mitchell Avenue  
Los Angeles, CA 90066  
Lot 190, Del Mar Tract  
Book 10, page 76

To Whom It May Concern:

On Saturday, January 4<sup>th</sup> 2021 I visited the above-referenced with the intention of creating an inventory of the existing tree stock and to determine if there were any trees on or adjacent to the property which qualify as Protected under the guidelines of City Ordinance 186873 as well as to determine if there were City-owned trees in front of the property which might be impacted by the proposed development.

The 4 tree groups or species protected under City Ordinance 186873 are : All California Native Oaks (*Quercus agrifolia*, *Q. engelmannii*, *Q. lobata* and etc.), the Western Sycamore (*Platanus racemosa*), the Southern California Black Walnut (*Juglans californica californica*) and the California Bay (*Umbellularia californica*).

The 2 Species of shrubs protected by City Ordinance 186873 are: the Mexican Elderberry (*Sambucus Mexicana*) and the Toyon (*Heteromeles arbutifolia*)

None of the Protected native trees or shrubs mentioned above are to be found on or adjacent to the property in question. Furthermore, there are no City trees in front of this property.

Therefore, in the absence of municipally owned trees and from the perspective of City Ordinance 177404, there are no arboreal impediments to developing this property.

Please feel free if I may provide any additional information and thanks for your time and attention to this matter.

Sincerely yours,

Steve Marshall  
ISA Certified Arborist  
Member, American Society of Consulting Arborists  
The Urban Lumberjack LLC  
CA LIC 740167



The Urban Lumberjack Tree Care • Reports • Consultations  
5937 Great Oak Circle, Los Angeles, CA 90042 • (323) 477-3961  
[theurbanlumberjack.com](http://theurbanlumberjack.com) • [theurbanlumberjack@SBGlobal.net](mailto:theurbanlumberjack@SBGlobal.net)  
Insured • Bonded • License #740167

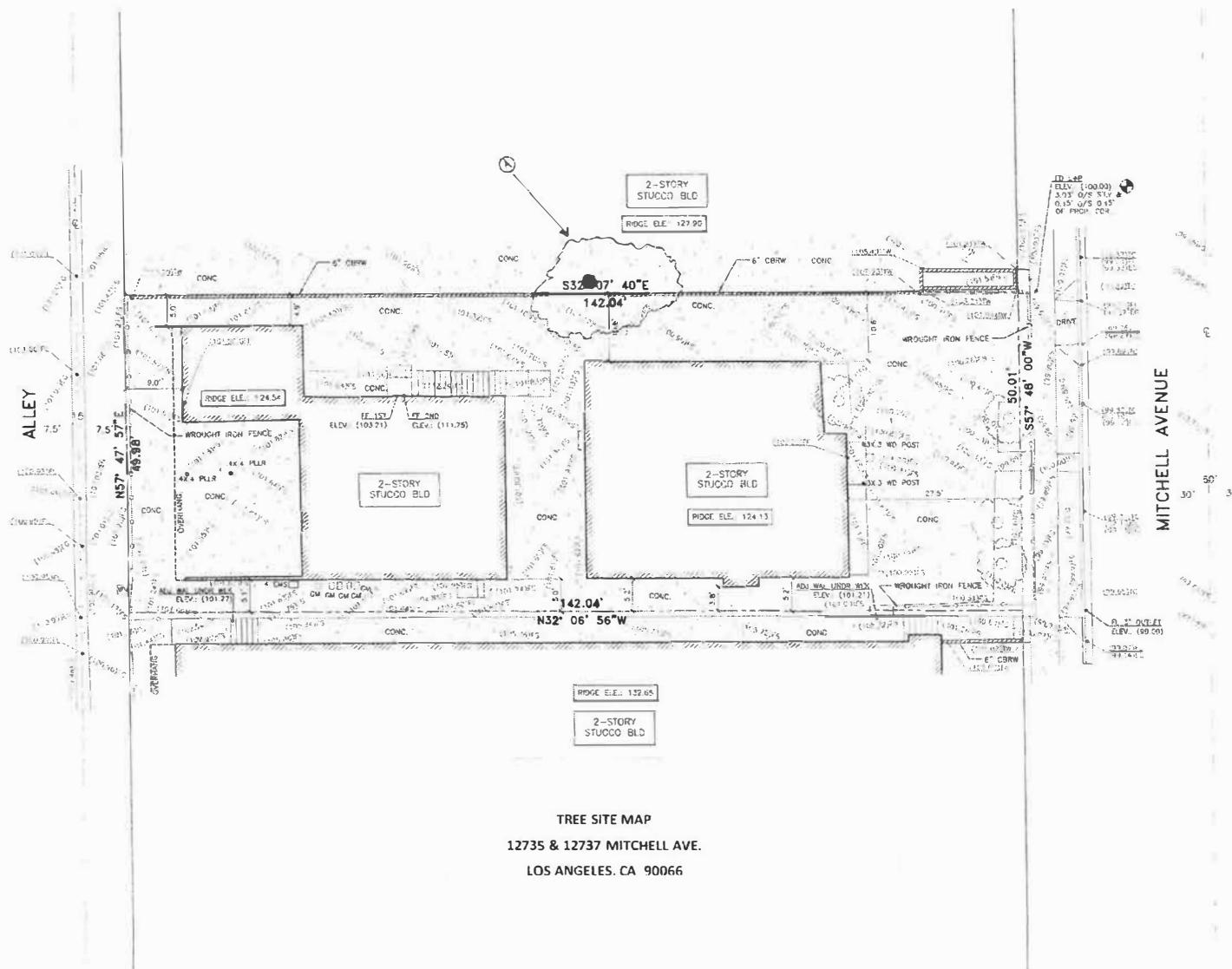


## TREE INVENTORY INDEX

12735 & 12737 MITCHELL  
LOS ANGELES, CA 90066

There are no trees of sufficient DSH to merit reporting onsite at the above-referenced address. The only tree large enough to require detailing is to be found on the lot immediately to the east of the subject property but close enough to overhang the subject property. Access to this neighboring property was not available so the DSH and the spread of canopy are necessarily estimates. Furthermore, the north/south and east/west canopy widths are estimated from the widest diameter of the living crown and are only approximately in the directions listed.

Letter	Species	DSH	Height	Diameter North/ South	Diameter East West	Notes
A	Loquat <i>Eriobotrya japonica</i>	8.0"	23'	22'	18'	Thriving



PLEASE NOTE

IF THIS DRAWING IS PROVIDED IN AN ELECTRONIC FORMAT (MAILED OR ON COMPUTER DISC) AS A COURTESY TO OUR CLIENT, THE ELECTRONIC FILE DOES NOT CONSTITUTE THE OFFICIAL AND LEGAL DELIVERY OF OUR PROFESSIONAL WORK PRODUCT. THAT IS, THE ELECTRONIC FILE IS ALTERED, THE PDF MUST BE REFERRED TO FOR THE ORIGINAL AND CORRECT SURVEY INFORMATION. PACIFIC LAND CONSULTANTS, INC. SHALL NOT BE RESPONSIBLE FOR ANY MODIFICATIONS MADE TO THE ELECTRONIC FILE OR FOR ANY PRODUCTS DERIVED FROM THE ELECTRONIC FILE WHICH ARE NOT REVIEWED, SIGNED AND SEALED BY PACIFIC LAND CONSULTANTS, INC.

## UNDERGROUND UTILITIES

ALL INFORMATION SHOWN HEREON REGARDING UNDERGROUND UTILITIES WAS TAKEN FROM VISIBLE SURFACE EVIDENCE OR SOURCES NOT CONNECTED WITH THIS COMPANY AND WHILE SAID INFORMATION IS BELIEVED CORRECT, NO LIABILITY IS ASSUMED FOR THE ACCURACY OR COMPLETENESS OF SAID DATA.

## BENCHMARK

FD L+P  
ASSUMED EL: 100' 00"

**LEGAL DESCRIPTION**

LOT 190 IN DEL MAR TRACT, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 10, PAGE 76 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

4708 ± 2.199.37.59.67

Page #235-012-026



PACIFIC LAND CONSULTANTS, INC.  
28441 HICHRIDGE RD. SUITE 230  
ROLLING HILLS ESTATES. CA 90274  
(310) 544-8689

FILE NAME: 2011BLS.DWG  
DATE: 12-16-2020  
SCALE: 1/8"=1'-0"

DRAWN  
BY: A.P. PROJECT SITE:  
12737 MITCHELL AVENUE  
LOS ANGELES, CA

JOB NO. 20118

CHRISTOPHER W. VASSALLO D.L.S. #4741  
REGISTRATION EXPIRY: 12-31-2022



SCALE 0 4 8 FEET

INC.	FILE NAME: 2011BIS.DWG
	DATE: 12-16-2020
	SCALE: 1/8"=1'-0"
	JOB NO. 2011B
	SHEET 1 OF 1



## REFERRAL FORMS:

### TRANSPORTATION STUDY ASSESSMENT

#### DEPARTMENT OF TRANSPORTATION - REFERRAL FORM

**RELATED CODE SECTION:** Los Angeles Municipal Code Section 16.05 and various code sections.

**PURPOSE:** The Department of Transportation (LADOT) Referral Form serves as an initial assessment to determine whether a project requires a Transportation Assessment.

#### GENERAL INFORMATION

- Administrative: Prior to the submittal of a referral form with LADOT, a Planning case must have been filed with the Department of City Planning.
- All new school projects, including by-right projects, must contact LADOT for an assessment of the school's proposed drop-off/pick-up scheme and to determine if any traffic controls, school warning and speed limit signs, school crosswalk and pavement markings, passenger loading zones and school bus loading zones are needed.
- Unless exempted, projects located within a transportation specific plan area may be required to pay a traffic impact assessment fee regardless of the need to prepare a transportation assessment.
- Pursuant to LAMC Section 19.15, a review fee payable to LADOT may be required to process this form. The applicant should contact the appropriate LADOT Development Services Office to arrange payment.
- LADOT's Transportation Assessment Guidelines, VMT Calculator, and VMT Calculator User Guide can be found at <http://ladot.lacity.org>.
- A transportation study is not needed for the following project applications:
  - Ministerial / by-right projects
  - Discretionary projects limited to a request for change in hours of operation
  - Tenant improvement within an existing shopping center for change of tenants
  - Any project only installing a parking lot or parking structure
  - Time extension
  - Single family home (unless part of a subdivision)
- This Referral Form is not intended to address the project's site access plan, driveway dimensions and location, internal circulation elements, dedication and widening, etc. These items require separate review and approval by LADOT.

#### SPECIAL REQUIREMENTS

When submitting this referral form to LADOT, include the completed documents listed below.

- Copy of Department of City Planning Application (CP-7771.1).
- Copy of a fully dimensioned site plan showing all existing and proposed structures, parking and loading areas, driveways, as well as on-site and off-site circulation.
- If filing for purposes of Site Plan Review, a copy of the Site Plan Review Supplemental Application.
- Copy of project-specific VMT Calculator<sup>1</sup> analysis results.

**TO BE VERIFIED BY PLANNING STAFF PRIOR TO LADOT REVIEW**

**LADOT DEVELOPMENT SERVICES DIVISION OFFICES:** Please route this form for processing to the appropriate LADOT Office as follows:

<b>Metro</b> 213-972-8482 100 S. Main St, 9 <sup>th</sup> Floor Los Angeles, CA 90012	<b>West LA</b> 213-485-1062 7166 W. Manchester Blvd Los Angeles, CA 90045	<b>Valley</b> 818-374-4699 6262 Van Nuys Blvd, 3 <sup>rd</sup> Floor Van Nuys, CA 91401
--	--	--

**1. PROJECT INFORMATION**

Case Number: \_\_\_\_\_

Address: \_\_\_\_\_

Project Description: \_\_\_\_\_ CC 2 VLI and 1 LI units

Seeking Existing Use Credit (will be calculated by LADOT): Yes \_\_\_\_\_ No \_\_\_\_\_ Not sure \_\_\_\_\_

Applicant Name: \_\_\_\_\_

Applicant E-mail: \_\_\_\_\_ Applicant Phone: \_\_\_\_\_

Planning Staff Initials: \_\_\_\_\_ Date: \_\_\_\_\_

**2. PROJECT REFERRAL TABLE**

	Land Use (list all)	Size / Unit	Daily Trips <sup>1</sup>
Proposed <sup>1</sup>	-----	---	---
	<i>Total trips<sup>1</sup>:</i>		
a. Does the proposed project involve a discretionary action? b. Would the proposed project generate 250 or more daily vehicle trips <sup>2</sup> ? c. If the project is replacing an existing number of residential units with a smaller number of residential units, is the proposed project located within one-half mile of a heavy rail, light rail, or bus rapid transit station <sup>3</sup> ?		Yes <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/>	
If <b>YES</b> to a. and b. or c., or to <b>all</b> of the above, the Project <u>must</u> be referred to LADOT for further assessment.			
Verified by: Planning Staff Name: _____ Phone: _____			
Signature: <u>Connie Chauw</u>		Date: <u>6/2/22</u>	

<sup>1</sup> Qualifying Existing Use to be determined by LADOT staff on following page, per LADOT's Transportation Assessment Guidelines.

<sup>2</sup>To calculate the project's total daily trips, use the VMT Calculator. Under 'Project Information', enter the project address, land use type, and intensity of all proposed land uses. Select the '+' icon to enter each land use. After you enter the information, copy the 'Daily Vehicle Trips' number into the total trips in this table. Do not consider any existing use information for screening purposes. For additional questions, consult LADOT's [VMT Calculator User Guide](#) and the LADOT Transportation Assessment Guidelines (available on the LADOT website).

<sup>3</sup> Relevant transit lines include: Metro Red, Purple, Blue, Green, Gold, Expo, Orange, and Silver line stations; and Metrolink stations.

**TO BE COMPLETED BY LADOT**

**3. PROJECT INFORMATION**

	<b>Land Use (list all)</b>	<b>Size / Unit</b>	<b>Daily Trips</b>
Proposed			
		<i>Total new trips:</i>	
Existing			
		<i>Total existing trips:</i>	
		<i>Net Increase / Decrease (+ or -)</i>	

- a. Is the project a single retail use that is less than 50,000 square feet? **Yes**  **No**
- b. Would the project generate a net increase of 250 or more daily vehicle trips? **Yes**  **No**
- c. Would the project result in a net increase in daily VMT? **Yes**  **No**
- d. If the project is replacing an existing number of residential units with a smaller number of residential units, is the proposed project located within one-half mile of a heavy rail, light rail, or bus rapid transit station? **Yes**  **No**
- e. Does the project trigger Site Plan Review (LAMC 16.05)? **Yes**  **No**
- f. Project size:
  - i. Would the project generate a net increase of 1,000 or more daily vehicle trips? **Yes**  **No**
  - ii. Is the project's frontage 250 linear feet or more along a street classified as an Avenue or Boulevard per the City's General Plan? **Yes**  **No**
  - iii. Is the project's building frontage encompassing an entire block along a street classified as an Avenue or Boulevard per the City's General Plan? **Yes**  **No**

**VMT Analysis (CEQA Review)**

If **YES** to a. and **NO** to d. a VMT analysis is **NOT** required.

If **YES** to both b. and c.; or to d. a VMT analysis **is** required.

**Access, Safety, and Circulation Assessment (Corrective Conditions)**

If **YES** to b., a project access, safety, and circulation evaluation may be required.

If **YES** to e. and either f.i., f.ii., or f.iii., an access assessment may be required.

LADOT Comments:

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*Please note that this form is not intended to address the project's site access plan, driveway dimensions and location, internal circulation elements, dedication and widening, etc. These items require separate review and approval by LADOT. Qualifying Existing Use to be determined per LADOT's Transportation Assessment Guidelines.*

4. Specific Plan with Trip Fee or TDM Requirements: **Yes**  **No**

Fee Calculation Estimate: \_\_\_\_\_

VMT Analysis Required (Question b. satisfied): **Yes**  **No**

Access, Safety, and Circulation Evaluation Required (Question b. satisfied): **Yes**  **No**

Access Assessment Required (Question b., e., and either f.i., f.ii. or f.iii satisfied): **Yes**  **No**

Prepared by DOT Staff Name: \_\_\_\_\_ Phone: \_\_\_\_\_

Signature: \_\_\_\_\_ Date: \_\_\_\_\_

# CITY OF LOS ANGELES VMT CALCULATOR Version 1.3



*Project Screening Criteria: Is this project required to conduct a vehicle miles traveled analysis?*

## Project Information

Project: 12735 MITCHELL

Scenario: 15 DU (3 AFFORDABLE)

Address: 12735 W MITCHELL AVE, 90066



**Is the project replacing an existing number of residential units with a smaller number of residential units AND is located within one-half mile of a fixed-rail or fixed-guideway transit station?**

Yes     No

## Existing Land Use

Land Use Type	Value	Unit	DU	<a href="#">+</a>
Housing   Single Family				
<a href="#">Click here to add a single custom land use type (will be included in the above list)</a>				

## Proposed Project Land Use

Land Use Type	Value	Unit	DU	<a href="#">+</a>
Housing   Affordable Housing - Family	3	DU		
Housing   Multi-Family	12	DU		
Housing   Affordable Housing - Family	3	DU		
<a href="#">Click here to add a single custom land use type (will be included in the above list)</a>				

## Project Screening Summary

Existing Land Use	Proposed Project
0 Daily Vehicle Trips	72 Daily Vehicle Trips
0 Daily VMT	477 Daily VMT

### Tier 1 Screening Criteria

Project will have less residential units compared to existing residential units & is within one-half  mile of a fixed-rail station.

### Tier 2 Screening Criteria

The net increase in daily trips < 250 trips **72**  
Net Daily Trips

The net increase in daily VMT ≤ 0 **477**  
Net Daily VMT

The proposed project consists of only retail land uses ≤ 50,000 square feet total. **0.000**  
ksf

**The proposed project is not required to perform VMT analysis.**





## SOILS REPORT APPROVAL LETTER

March 10, 2021

LOG # 116492  
SOILS/GEOLOGY FILE - 2

12737 Mitchell Investment, LLC  
12737 W. Mitchell Ave.  
Los Angeles, CA 90066

TRACT: DEL MAR (M P 10-76)  
LOT(S): 190  
LOCATION: 12737 W. Mitchell Ave. (aka 12735 W. Mitchell Ave.)

CURRENT REFERENCE <u>REPORT/LETTER(S)</u>	REPORT <u>No.</u>	DATE OF <u>DOCUMENT</u>	<u>PREPARED BY</u>
Soils Report	IC 20168-J	02/24/2021	Irvine Geotechnical
Laboratory Test Report	SL21.3542	02/04/2021	Soil Labworks, LLC

The Grading Division of the Department of Building and Safety has reviewed the referenced reports that provide recommendations for the proposed 4-story apartment building at grade, as shown on the plan and cross sections, at the subject site. No basement or retaining walls are proposed. The earth materials at the subsurface exploration locations consist of up to 1 feet of uncertified fill underlain by native soils. No groundwater was encountered during exploration to a depth of 11 feet below the existing grade. The consultants recommend to support the proposed structure(s) on conventional foundations bearing on native undisturbed soils or a blanket of properly placed compacted fill a minimum of 3 feet thick.

The referenced reports are acceptable, provided the following conditions are complied with during site development:

(Note: Numbers in parenthesis ( ) refer to applicable sections of the 2020 City of LA Building Code. P/BC numbers refer to the applicable Information Bulletin. Information Bulletins can be accessed on the internet at LADBS.ORG.)

1. The soils engineer shall review and approve the detailed plans prior to issuance of any permit. This approval shall be by signature on the plans that clearly indicates the soils engineer has reviewed the plans prepared by the design engineer; and, that the plans included the recommendations contained in their reports (7006.1).
2. All recommendations of the report(s) that are in addition to or more restrictive than the conditions contained herein shall be incorporated into the plans.

3. A copy of the subject and appropriate referenced reports and this approval letter shall be attached to the District Office and field set of plans (7006.1). Submit one copy of the above reports to the Building Department Plan Checker prior to issuance of the permit.
4. A grading permit shall be obtained for all structural fill (106.1.2).
5. All man-made fill shall be compacted to a minimum 90 percent of the maximum dry density of the fill material per the latest version of ASTM D 1557. Where cohesionless soil having less than 15 percent finer than 0.005 millimeters is used for fill, it shall be compacted to a minimum of 95 percent relative compaction based on maximum dry density. Placement of gravel in lieu of compacted fill is only allowed if complying with LAMC Section 91.7011.3.
6. If import soils are used, no footings shall be poured until the soils engineer has submitted a compaction report containing in-place shear test data and settlement data to the Grading Division of the Department; and, obtained approval (7008.2).
7. Compacted fill shall extend beyond the footings a minimum of five feet, as recommended (7011.3).
8. Existing uncertified fill shall not be used for support of footings, concrete slabs or new fill (1809.2, 7011.3).
9. Drainage in conformance with the provisions of the Code shall be maintained during and subsequent to construction (7013.12).
10. Grading shall be scheduled for completion prior to the start of the rainy season, or detailed temporary erosion control plans shall be filed in a manner satisfactory to the Grading Division of the Department and the Department of Public Works, Bureau of Engineering, B-Permit Section, for any grading work in excess of 200 cubic yards (7007.1).

1828 Sawtelle Blvd., 3rd Floor, West LA (310) 575-8388

11. All loose foundation excavation material shall be removed prior to commencement of framing (7005.3).
12. The applicant is advised that the approval of this report does not waive the requirements for excavations contained in the General Safety Orders of the California Department of Industrial Relations (3301.1).
13. Temporary excavations that remove lateral support to the public way, adjacent property, or adjacent structures shall be constructed using ABC slot cuts, as recommended. Note: Lateral support shall be considered to be removed when the excavation extends below a plane projected downward at an angle of 45 degrees from the bottom of a footing of an existing structure, from the edge of the public way or an adjacent property. (3307.3.1)
14. Where any excavation, not addressed in the approved reports, would remove lateral support (as defined in 3307.3.1) from a public way, adjacent property or structures, a supplemental report shall be submitted to the Grading Division of the Department containing recommendations for shoring, underpinning, and sequence of construction. Shoring recommendations shall include the maximum allowable lateral deflection of shoring system to prevent damage to adjacent structures, properties and/or public ways. Report

shall include a plot plan and cross-section(s) showing the construction type, number of stories, and location of adjacent structures, and analysis incorporating all surcharge loads that demonstrate an acceptable factor of safety against failure. (7006.2 & 3307.3.2)

15. Prior to the issuance of any permit that authorizes an excavation where the excavation is to be of a greater depth than are the walls or foundation of any adjoining building or structure and located closer to the property line than the depth of the excavation, the owner of the subject site shall provide the Department with evidence that the adjacent property owner has been given a 30-day written notice of such intent to make an excavation (3307.1).
16. Unsurcharged temporary excavations over 5 feet exposing soil shall be trimmed back at a gradient not exceeding 1:1, as recommended.
17. ABC slot-cut method may be used for unsurcharged temporary excavations with each slot not exceeding 5 feet in height and not exceeding 8 feet in width, as recommended. The soils engineer shall verify in the field if the existing earth materials are stable in the slot-cut excavation. Each slot shall be inspected by the soils engineer and approved in writing prior to any worker access.
18. All foundations shall derive entire support from native undisturbed soils, or a blanket of properly placed compacted fill a minimum of 3 feet thick, as recommended and approved by the soils engineer by inspection.
19. Footings supported on approved compacted fill or expansive soil shall be reinforced with a minimum of four (4),  $\frac{1}{2}$ -inch diameter (#4) deformed reinforcing bars. Two (2) bars shall be placed near the bottom and two (2) bars placed near the top of the footing.
20. The foundation/slab design shall satisfy all requirements of the Information Bulletin P/BC 2017-116 "Foundation Design for Expansive Soils" (1803.5.3).
21. Slabs placed on approved compacted fill shall be at least  $3\frac{1}{2}$  inches thick and shall be reinforced with  $\frac{1}{2}$ -inch diameter (#4) reinforcing bars spaced a maximum of 16 inches on center each way.
22. Concrete floor slabs placed on expansive soil shall be placed on a 4-inch fill of coarse aggregate or on a moisture barrier membrane. The slabs shall be at least  $3\frac{1}{2}$  inches thick and shall be reinforced with  $\frac{1}{2}$ -inch diameter (#4) reinforcing bars spaced a maximum of 16 inches on center each way.
23. The seismic design shall be based on a Site Class D, as recommended. All other seismic design parameters shall be reviewed by LADBS building plan check. According to ASCE 7-16 Section 11.4.8, the long period coefficient ( $F_v$ ) may be selected per Table 11.4-2 in ASCE 7-16, provided that the value of the Seismic Response Coefficient ( $C_s$ ) is determined by Equation 12.8-2 for values of the fundamental period of the building ( $T$ ) less than or equal to  $1.5T_s$ , and taken as 1.5 times the value computed in accordance with either Equation 12.8-3 for  $T$  greater than  $1.5T_s$  and less than or equal to  $TL$  or Equation 12.8-4 for  $T$  greater than  $TL$ . Alternatively, a supplemental report containing a site-specific ground motion hazard analysis in accordance with ASCE 7-16 Section 21.2 shall be submitted for review and approval.
24. The structure shall be connected to the public sewer system per P/BC 2020-027.

25. All roof, pad and deck drainage shall be conducted to the street in an acceptable manner in non-erosive devices or other approved location in a manner that is acceptable to the LADBS and the Department of Public Works (7013.10).
26. All concentrated drainage shall be conducted in an approved device and disposed of in a manner approved by the LADBS (7013.10).
27. The soils engineer shall inspect all excavations to determine that conditions anticipated in the report have been encountered and to provide recommendations for the correction of hazards found during grading (7008, 1705.6 & 1705.8).
28. Prior to pouring concrete, a representative of the consulting soils engineer shall inspect and approve the footing excavations. The representative shall post a notice on the job site for the LADBS Inspector and the Contractor stating that the work inspected meets the conditions of the report. No concrete shall be poured until the LADBS Inspector has also inspected and approved the footing excavations. A written certification to this effect shall be filed with the Grading Division of the Department upon completion of the work. (108.9 & 7008.2)
29. Prior to excavation an initial inspection shall be called with the LADBS Inspector. During the initial inspection, the sequence of construction; ABC slot cuts; protection fences; and, dust and traffic control will be scheduled (108.9.1).
30. Slot cutting shall be performed under the inspection and approval of the soils engineer and deputy grading inspector (1705.6, 1705.8).
31. Prior to the placing of compacted fill, a representative of the soils engineer shall inspect and approve the bottom excavations. The representative shall post a notice on the job site for the LADBS Inspector and the Contractor stating that the soil inspected meets the conditions of the report. No fill shall be placed until the LADBS Inspector has also inspected and approved the bottom excavations. A written certification to this effect shall be included in the final compaction report filed with the Grading Division of the Department. All fill shall be placed under the inspection and approval of the soils engineer. A compaction report together with the approved soil report and Department approval letter shall be submitted to the Grading Division of the Department upon completion of the compaction. In addition, an Engineer's Certificate of Compliance with the legal description as indicated in the grading permit and the permit number shall be included (7011.3).
32. No footing/slab shall be poured until the compaction report is submitted and approved by the Grading Division of the Department.

LEILA ETAAT  
Structural Engineering Associate II

Log No. 116492  
213-482-0480

cc: Irvine Geotechnical, Project Consultant  
WL District Office



April 18, 2022  
IC 20168-I

12737 Mitchell Investment, LLC  
12737 West Mitchell Avenue  
Los Angeles, California 90066

**Subject**

Supplemental Geotechnical Engineering Exploration  
Proposed 15-Unit Apartment Building  
Lot 190, Del Mar Tract  
12735 & 12737 West Mitchell Avenue  
Los Angeles, California

**References: Report by Irvine Geotechnical, Inc.:**

*Geotechnical Engineering Exploration, Proposed Multi-Family Residential Structure  
Lot 190, Del Mar Tract, 12735 & 12737 West Mitchell Avenue, Los Angeles, California,  
dated February 24, 2021*

**City of Los Angeles Department of Building and Safety, Grading Division:**

*Soils Report Approval Letter, Log #116492, dated March 10, 2021*

Dear Gentle Persons,

Irvine Geotechnical has prepared this supplemental report to provide additional information and clarification to the Grading Division for the scope of the proposed project. This supplemental report follows consultation with the architect and review of the latest plans prepared by Breakform Design.

The scope of the project is similar to that previously presented to and approved by the Department. However, the number of stories for the structure has increased from 4 to 6. The

**145 N. Sierra Madre Blvd., Suite #1 • Pasadena • California • 91107 • Phone: 626-844-6641/Fax: 626-604-0394**

April 18, 2022  
IC 20168-I  
Page 2

building will contain 15 apartments located in 5 levels of on top of a concrete podium. The lower level will contain on-grade parking. The roof level will also include a deck and terrace. The Planning Department is requiring the geotechnical report and Grading Division letter match the current project description.

The recommended bearing material remains approved compacted fill or alluvium. Recommendations for design of foundations, slabs, site grading, and drainage improvements contained in the preliminary report remain valid and applicable. Seismic design values and earthwork parameters contained in the preliminary report remain valid and applicable.

Irvine Geotechnical appreciates the opportunity to provide our service on this project. Any questions concerning the data or interpretation of this, or the referenced report should be directed to the undersigned.

Respectfully submitted,  
Irvine Geotechnical, Inc.

Jon A. Irvine  
E.G. 1691/G.E. 2891

\icprojects\2020 projects\ic20168 breakform mitchell\ic20168 breakform supplemental.docx



Enc: Soils Report Approval Letter, Log #116492, dated March 10, 2021

xc: (3) Addressee



## SOILS REPORT APPROVAL LETTER

March 10, 2021

LOG # 116492  
SOILS/GEOLOGY FILE - 2

12737 Mitchell Investment, LLC  
12737 W. Mitchell Ave.  
Los Angeles, CA 90066

TRACT: DEL MAR (M P 10-76)  
LOT(S): 190  
LOCATION: 12737 W. Mitchell Ave. (aka 12735 W. Mitchell Ave.)

CURRENT REFERENCE <u>REPORT/LETTER(S)</u>	REPORT <u>No.</u>	DATE OF <u>DOCUMENT</u>	<u>PREPARED BY</u>
Soils Report	IC 20168-J	02/24/2021	Irvine Geotechnical
Laboratory Test Report	SL21.3542	02/04/2021	Soil Labworks, LLC

The Grading Division of the Department of Building and Safety has reviewed the referenced reports that provide recommendations for the proposed 4-story apartment building at grade, as shown on the plan and cross sections, at the subject site. No basement or retaining walls are proposed. The earth materials at the subsurface exploration locations consist of up to 1 feet of uncertified fill underlain by native soils. No groundwater was encountered during exploration to a depth of 11 feet below the existing grade. The consultants recommend to support the proposed structure(s) on conventional foundations bearing on native undisturbed soils or a blanket of properly placed compacted fill a minimum of 3 feet thick.

The referenced reports are acceptable, provided the following conditions are complied with during site development:

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3. A copy of the subject and appropriate referenced reports and this approval letter shall be attached to the District Office and field set of plans (7006.1). Submit one copy of the above reports to the Building Department Plan Checker prior to issuance of the permit.
4. A grading permit shall be obtained for all structural fill (106.1.2).
5. All man-made fill shall be compacted to a minimum 90 percent of the maximum dry density of the fill material per the latest version of ASTM D 1557. Where cohesionless soil having less than 15 percent finer than 0.005 millimeters is used for fill, it shall be compacted to a minimum of 95 percent relative compaction based on maximum dry density. Placement of gravel in lieu of compacted fill is only allowed if complying with LAMC Section 91.7011.3.
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24. The structure shall be connected to the public sewer system per P/BC 2020-027.

25. All roof, pad and deck drainage shall be conducted to the street in an acceptable manner in non-erosive devices or other approved location in a manner that is acceptable to the LADBS and the Department of Public Works (7013.10).
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32. No footing/slab shall be poured until the compaction report is submitted and approved by the Grading Division of the Department.

LEILA ETAAT  
Structural Engineering Associate II

Log No. 116492  
213-482-0480

cc: Irvine Geotechnical, Project Consultant  
WL District Office



GEOTECHNICAL ENGINEERING EXPLORATION  
PROPOSED MULTI-FAMILY RESIDENTIAL STRUCTURE  
LOT 190, DEL MAR TRACT  
12735 & 12737 WEST MITCHELL AVENUE  
LOS ANGELES, CALIFORNIA

FOR BREAKFORM DESIGN  
IRVINE GEOTECHNICAL, INC. PROJECT NUMBER IC 20168-J  
FEBRUARY 24, 2021

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## INTRODUCTION

This report has been prepared per our agreement and summarizes findings of Irvine Geotechnical's geotechnical engineering exploration performed on the site. The purpose of this study is to evaluate the nature, distribution, and engineering properties of the earth materials underlying the site with respect to the design and construction of the proposed project.

## INTENT

It is the intent of this report to assist in the design and completion of the proposed project. The recommendations are intended to reduce geotechnical risks affecting the project. The professional opinions and advice presented in this report are based upon commonly accepted standards and are subject to the general conditions described in the **NOTICE** section of this report.

## EXPLORATION

The scope of the field exploration was determined from our consultation with the architect. The preliminary plans prepared by Breakform Design were considered prior to beginning work on this project. Exploration was conducted using techniques normally applied to this type of project in this setting. This report is limited to the area of the exploration and the proposed project as shown on the enclosed Site Plan and cross sections. Conditions affecting portions of the property outside the area explored are beyond the scope of this report.

Exploration was conducted on January 18, 2021, with the aid of hand labor. It included excavating four test pits to a maximum depth of 11 feet. Samples of the earth materials were obtained and delivered to the soils engineering laboratory of Soil Labworks, LLC, for testing and analysis. Downhole observation of the earth materials was performed by the engineering geologist.

Office tasks included laboratory testing of selected soil samples, researching records on file at the City of Los Angeles, reviewing historical topographic maps and aerial photographs, preparing the Site Plan and cross sections, and performing engineering analysis. Earth materials exposed in the test pits are described on the enclosed Log of Test Pits. Appendix I contains a discussion of the laboratory testing procedures and results.

The proposed project, surface geologic conditions, and the location of the test pits are shown on the Site Plan. Subsurface distribution of the earth materials and the proposed project are shown on Sections A and B.

## **RESEARCH**

The building and grading records of the City of Los Angeles Department of Building and Safety (LADBS) were researched prior to preparing this report. Geotechnical reports pertaining to the subject property were not located.

## **PROPOSED PROJECT**

Information concerning the proposed project was provided by the architect. The preliminary plans prepared by Breakform Design were a guide for exploring the site and preparing this report. It is proposed to redevelop the site with a 4-level apartment building. The upper 3 levels will consist of residential units and the lower level will be at-grade parking. No retaining walls are planned at this time. Formal plans have not been prepared and await the conclusions and recommendations of this report.

## **SITE DESCRIPTION**

The subject property consists of a partially graded and developed lot in the southwest portion of the Santa Monica Basin in the Mar Vista neighborhood of the City of Los Angeles,

California. It is located on the northwest side of Mitchell Avenue, about  $\frac{1}{4}$  mile southeast of Venice Boulevard (State Route 187), and about 1.5 miles southwest of the San Diego Freeway (Interstate 405). The site is developed with two residential buildings with a parking area adjacent to Mitchell Avenue at the front (southeast) portion of the lot and a covered carport at the rear (northwest) portion of the lot, adjacent to the alley. The surrounding area is developed with similar multi-family residential buildings.

Topographically, the site is located on a level alluvial deposit derived from nearby upland sources. Physical relief within the property limits is roughly 2 feet. Past grading consisted of minor cut and fill operations to construct the residential buildings, landscaping, and hardscape. It should be noted that the finished-floor elevation shown on the Site Plan is relative to an arbitrary datum of 100 feet established at the southeast corner of the property. The relative finished-floor elevation is about 101 feet. The building pad and finished floor elevation is roughly 44 feet (MSL).

Vegetation on the site consists of a few shrubs and planters. Surface drainage over the lot is by sheet flow runoff down the contours of the land generally toward the southeast. Roof drainage consists of typical peaked roofing that directs drainage to gutters and downspouts that outlet to grade.

## GROUNDWATER

Groundwater was not encountered during exploration. Historically highest groundwater in this area of Mar Vista is estimated to be about 10 to 20 feet below the ground surface (Plate 1.2, *Historically Highest Groundwater Contours and Borehole Log Data Locations, Venice 7½ Minute Quadrangle*, in Seismic Hazard Zone Report for the Venice Quadrangle, SHZR-036).

Seasonal fluctuations in groundwater levels may occur due to variations in climate, irrigation, and other factors not evident at the time of the exploration. Fluctuations in groundwater levels may also occur across the site.

## **EARTH MATERIALS**

### **Fill**

Fill, associated with previous site grading, blankets portions of the site to a maximum observed thickness of 12 inches in the vicinity of Test Pits 1 and 4. The fill may be thicker elsewhere onsite in areas not explored. The fill consists of varying admixtures of clay, silt and sand that are brown, dark brown, gray-brown, very slightly moist to saturated, slightly porous, and firm/loose to stiff/dense. The fill contains roots and occasional gravel to 2 inches in diameter.

### **Alluvium**

Natural alluvial deposits were encountered in all four test pits. The alluvium consists of varying admixtures of clay, silt, sand, and gravel that are brown, dark brown, gray, gray-brown, yellow-brown, slightly porous, very slightly moist to saturated, and firm/medium dense to stiff/very dense. The alluvium contains gravel and slate fragments to 2 inches in diameter.

## **GENERAL SEISMIC CONSIDERATIONS**

Southern California is located in an active seismic region and numerous known and undiscovered earthquake faults are present in the region. Hazards associated with fault rupture and earthquakes include direct affects such as strong ground shaking and ground rupture, as well as secondary effects such as liquefaction, landsliding, and lurching. The United States Geological Survey (USGS), California Geologic Survey (CGS), Southern California Earthquake Center (SCEC), private consultants, and universities have been studying earthquakes in southern California for several decades. Early studies were directed toward earthquake prediction and early warning of strong ground shaking. Research and practice have shown that earthquake prediction is not practical or sufficiently accurate to benefit the general public. Also, several recent and damaging

earthquakes have occurred on faults that were unknown prior to rupture. Current standards and the California Building Code call for earthquake-resistant design of structures as opposed to prediction.

## Building Code Seismic Coefficients

Seismic design parameters within the Building Code include amplification of the seismic forces on the structure depending on the soil type, distance to seismic source, and intensity of shaking. The purpose of the Code seismic design parameters is to prevent collapse of structures and loss of life during strong ground shaking. Cosmetic damage should be expected.

The following table lists the applicable seismic coefficients for the 2020 Los Angeles Building Code.

SEISMIC COEFFICIENTS (2020 Los Angeles Building Code)		
Latitude = 33.9977°N Longitude = -118.4366°W	Short Period (0.2s)	One-Second Period
Earth Materials and Site Class Chapter 20 - ASCE 7	Alluvium – D	
Seismic Design Category from Table 1613.2.5(1) and 1613.2.5(2)	D	
Spectral Accelerations from Figures 1613.2.1 (1) through 1613.2.1(8)	$S_S = 1.879g$	$S_1 = 0.665g$
Site Coefficients from Tables 1613.2.3 (1) and 1613.2.3 (2)	$F_A = 1.2$	$F_V = 1.7$
Spectral Response Accelerations from Equations 16-36 and 16-37	$S_{MS} = 2.254g$	$S_{M1} = 1.131g$
Design Accelerations from Equations 16-38 and 16-39	$S_{DS} = 1.503g$	$S_{D1} = 0.754g$

## Seismic Hazards

The principal seismic hazard to the subject property and proposed project is strong ground shaking from earthquakes produced by local faults. Modern, well-constructed buildings are designed to resist ground shaking through the use of shear panels, moment-resisting frames, and reinforcement. Additional precautions may be taken to protect personal property and reduce the chance of injury, including strapping down water heaters and securing furniture and appliances. It is likely that the subject property will be shaken by future earthquakes produced in southern California. However, secondary effects, such as surface rupture, lurching, liquefaction, consolidation, ridge shattering, and landsliding, should not occur at the subject property.

## Alquist-Priolo Fault Rupture Hazard Study Zone

California faults are classified as active, potentially active, or inactive. Faults from past geologic periods of mountain building that do not display any evidence of recent offset are considered “inactive” or “potentially active.” Faults that have historically produced earthquakes or show evidence of movement within the Holocene (past 11,000 years) are considered “active faults.” Active faults that are capable of causing large earthquakes may also cause ground rupture. The Alquist-Priolo Special Studies Zone Act of 1972 was enacted to protect structures from hazards associated with fault ground rupture. No known active faults cross the subject property and the site is not located within an Alquist-Priolo Fault Rupture Hazard Study Zone. The ground rupture hazard at the site is considered low to nil.

## Seismic Hazard Zones

The California State Legislature enacted the Seismic Hazards Mapping Act of 1990, which was prompted by damaging earthquakes in California, and was intended to protect public safety from the effects of strong ground shaking, liquefaction, landslides, and other

earthquake-related hazards. The Seismic Hazards Mapping Act requires that the State Geologist delineate various “seismic hazards zones.” The maps depicting the zones are released by the California Geological Survey.

The Seismic Hazards Mapping Act requires a site investigation by a certified engineering geologist and/or civil engineer with expertise in geotechnical engineering for projects sited within a hazard zone. The investigation is to include recommendations for a “minimum level of mitigation” that should reduce the risk of ground failure during an earthquake to a level that does not cause the collapse of buildings for human occupancy. The Seismic Hazards Mapping Act does not require mitigation to a level of no ground failure and/or no structural damage.

Seismic Hazard Zone delineations are based on correlation of a combination of factors, including: surface distribution of soil deposits; physical relief; depth to historic high groundwater; shear strength of the soils; and occurrence of past seismic deformation. The subject property is located within the United States Geologic Survey, Venice Quadrangle. Seismic hazards within the Venice Quadrangle were evaluated by the CGS in their report, *“Seismic Hazard Zone Report for the Venice 7.5-minute Quadrangle, Los Angeles County, California,”* Seismic Hazard Zone Report 036. According to the Seismic Hazard Zones Map, the site is not mapped within a zone of required investigation for earthquake-induced landsliding or liquefaction.

## Ground Motion

Spectral accelerations and peak ground accelerations at the site were determined for the Risk-Targeted Maximum Considered Earthquake ( $MCE_R$ ) and Geometric Mean Peak Ground Acceleration ( $MCE_G$ ) following the procedures in ASCE 7-16 and the 2019 Building Code. The computed  $PGA_M$  for this site is 0.961g. According to the USGS deaggregation website (<https://earthquake.usgs.gov/hazards/interactive/>) and using a ground motion with a 10 percent probability of exceedance in 50 years, the modal deaggregated earthquake PGA

and moment magnitude are 0.497g and 6.34, respectively. The modal distance to the ground motion source is 7.87 km. For a ground motion with a 2 percent probability of exceedance in 50 years, the mean deaggregated earthquake PGA and moment magnitude are 0.837g and 6.72, respectively. The mean distance to the ground motion source is 9.32 km.

## **CONCLUSIONS AND RECOMMENDATIONS**

### **General Findings**

The conclusions and recommendations of this exploration are based upon four test pits, research of available records, consultation, years of experience observing similar properties in similar settings, and review of the development plans. It is the finding of Irvine Geotechnical that construction of the proposed project is feasible from a geotechnical engineering standpoint, provided the advice and recommendations contained in this report are included in the plans and are implemented during construction.

The recommended bearing material for the proposed improvements is the undisturbed alluvium, encountered within 12 inches of the existing grade, or a certified compacted fill pad. The existing fill is not considered suitable for foundation or slab support. Conventional foundations are considered appropriate to support the proposed improvements.

### **Geotechnical Issues**

Geotechnical issues affecting the site include disturbance of near-surface soils, expansive soils, removing lateral support from existing foundations or property lines to construct the compacted fill pads.

Demolition of existing structures may significantly disturb the near-surface natural soils. If the natural soils are elected as the bearing material for the proposed improvements, then

care should be taken to reduce disturbance of near-surface soils during demolition activities, particularly in areas of anticipated new foundations. New foundations may need to be locally deepened through the disturbed soils to fully embed into the dense undisturbed soils.

The onsite soils are in the medium-expansion range. Maintaining the moisture content of the onsite soils during construction of compacted fill pads should be performed to ensure the soils do not dry out and cause settlement. The placing of new compacted fill with onsite soils for support of foundations and slabs should be performed with moisture content in the over-optimum range as described in the **SITE PREPARATION** section.

Vertical excavations removing lateral or vertical support from existing foundations or property lines will require the use of temporary shoring or slot cuts. Recommendations for slot cuts are provided in the **TEMPORARY EXCAVATIONS** section of this report.

## **SITE PREPARATION**

Surficial materials consisting of fill are present on the site. Remedial grading is recommended to improve site conditions for the support of foundations and slabs.

### **General Grading Specifications**

The following guidelines may be used in preparation of the grading plan and job specifications. Irvine Geotechnical would appreciate the opportunity of reviewing the plans to ensure that these recommendations are included. The grading contractor should be provided with a copy of this report.

- A. The site should be prepared to receive compacted fill by removing all vegetation, debris, and existing fill. The exposed excavated area should be observed by the soils engineer or geologist prior to placing compacted fill. The exposed grade should be scarified to a depth of six inches, moistened to optimum moisture content, and recompacted to 90 percent of the maximum density.

- B. If the fill is intended for structural support, the proposed building site shall be excavated to a minimum depth of 3 feet below the bottom of all footings. The lateral extent of the removal should extend a minimum of 5 feet beyond the building footprint or equal to the thickness of the fill pad. If the fill is intended for support of slabs and pavements, the depth of removal may be limited to the thickness of the existing fill. The excavated areas shall be observed by the soils engineer or geologist prior to placing compacted fill.
- C. Fill, consisting of soil approved by the soils engineer, shall be placed in horizontal lifts and compacted in six-inch layers with suitable compaction equipment. The excavated onsite materials are considered satisfactory for reuse in the controlled fills. Any imported fill shall be observed by the soils engineer prior to use in fill areas. Rocks larger than six inches in diameter shall not be used in the fill.
- D. The fill shall be compacted to at least 90 percent of the maximum laboratory density for the material used. Where cohesionless soil (less than 15 percent finer than 0.005 millimeters) is used for fill, it shall be compacted to a minimum of 95 percent relative compaction. The fill should be placed at a moisture content that is at or within 3 percent over optimum. The maximum density and optimum moisture content shall be determined by ASTM D 1557-12 or equivalent.
- E. Field observation and testing shall be performed by the soils engineer during grading to assist the contractor in obtaining the required degree of compaction and the proper moisture content. Where compaction is less than required, additional compactive effort shall be made with adjustment of the moisture content, as necessary, until 90 percent compaction is obtained. One compaction test is required for each 500 cubic yards or two vertical feet of fill placed.
- F. At one time, the site may have been serviced by a private sewerage. Private sewage disposal systems generally consist of a septic tank and one or more cesspool or seepage pits. Any seepage pits or cesspools found during grading should be properly abandoned in conformance with the city's guidelines. As a minimum, the liner and debris should be removed to expose the bearing material. The void may then be filled with compacted fill or another approved material.

## Shrinking/Bulking

The following table contains the estimated bulking and shrinking factors to be used in determining earthwork volumes. The factors were determined by ratios of in-situ density or compacted density or loose density.

EARTHWORK FACTORS					
Earth Material	In-Situ Density (pcf)	Loose Density (pcf)	Compacted Density (pcf)	Bulking Factor (%)	Shrinking Factor (%)
Fill & Alluvium	112.3	98.0	112.3	14.6	0.0

## Excavation Characteristics

The test pits did not encounter hard, cemented earth materials. Difficult excavation conditions are not anticipated for the proposed development.

## FOUNDATION DESIGN

### General Conditions

The following foundation recommendations are minimum requirements. The structural engineer may require footings that are deeper, wider, or larger in diameter, depending on the final loads.

### Spread Footings

Continuous and/or pad footings may be used to support the proposed structures provided they are founded in alluvium or certified compacted fill. Continuous footings should be a

minimum of 12 inches in width. Pad footings should be a minimum of 24 inches square. The following chart contains the recommended allowable design parameters.

Bearing Material	Minimum Embedment Depth of Footing (Inches)	Vertical Bearing (psf)	Coefficient of Friction	Passive Earth Pressure (pcf)	Maximum Earth Pressure (psf)
Certified Compacted Fill	24	2,000	0.30	250	3,000
Alluvium	24	2,000	0.30	250	3,000

Increases in the bearing value are allowable at a rate of 400 pounds per square foot for each additional foot of footing width or depth to a maximum of 3,000 pounds per square foot. For bearing calculations, the weight of the concrete in the footing may be neglected.

The bearing value shown above is for the total of dead and frequently applied live loads and may be increased by one third for short duration loading, which includes the effects of wind or seismic forces.

The on-site soils are medium-expansive. Footings should be reinforced following the recommendations of the structural engineer. It is recommended that continuous footings be reinforced with a minimum of four #4 steel bars; two placed near the top and two near the bottom of the footings. Footings should be cleaned of all loose soil, moistened, free of shrinkage cracks and approved by the geotechnical engineer prior to placing forms, steel or concrete.

## Foundation Settlement

Settlement of the foundation system is expected to occur on initial application of loading. A settlement of  $\frac{1}{4}$  to  $\frac{1}{2}$  inch may be anticipated. Differential settlement should not exceed  $\frac{1}{2}$  inch.

## RETAINING WALLS

Retaining walls are not planned for the current project. Retaining wall recommendations can be provided under separate cover upon request.

## TEMPORARY EXCAVATIONS

Temporary excavations will be required to construct the recommended compacted fill pad. The excavations could be up to 5 feet in height and will expose fill over alluvium. Where not surcharged by existing footings or structures, soils are capable of maintaining vertical excavations up to 5 feet. Where vertical excavations in soil exceed 5 feet in height, the upper portion should be trimmed to 1:1 (45 degrees).

It should be noted that regardless of stability, excavations that remove lateral support from property lines or existing structures are not allowed by the Code. The following section from Chapter 33 of the Building Code governs temporary excavations:

### *3307.3 Temporary excavations and shoring*

*3307.3.1 General. Excavations shall not remove the lateral support from a public way, from an adjacent property or from an existing structure. For the purpose of this section, the lateral support shall be considered to have been removed when any of the following conditions exist:*

- 1. The excavation exposes any adverse geological formations, which would affect the lateral support of a public way or an adjacent structure.*
- 2. The excavation extends below a plane extending downward at an angle of 45 degrees from the edge of the public way or an adjacent property.*

*Exception: Normal footing excavations not exceeding two feet in depth will not be construed as removing lateral support.*

- 3. The excavation extends below a plane extending downward at an angle of 45 degrees from the bottom of an existing structure.*

Vertical excavations removing lateral or vertical support from existing foundations or property lines will require the use of slot cuts.

## Slot Cutting

Vertical excavations removing support from property lines will require the use of temporary shoring or slot cutting (ABC method). The slot cutting method uses the earth as a buttress and allows the excavation to proceed in phases. The initial excavation is made at a slope of 1:1. Alternate slots of 8 feet in width and 5 feet in height may be worked. The remaining earth buttresses should be 16 feet in width. The footings should be completed and backfilled in the slots before the "B" earth buttresses are excavated. The "C" earth buttresses may be excavated upon completion of the footings and backfilling of the "B" areas.

A representative of the geotechnical engineer should be present during grading to see temporary slopes. All excavations should be stabilized within 30 days of initial excavation. Water should not be allowed to pond on top of the excavations nor to flow toward them. No vehicular surcharge should be allowed within three feet of the top of the cut.

## CORROSION

The pH of the soils is near neutral and not a factor in corrosion. The chloride content is low and not a factor in design. The sulfate content is negligible and not a factor in concrete design. The resistivity indicates that the soils are corrosive to ferrous metals.

## FLOOR SLABS & CONCRETE DECKING

Floor slabs and concrete decking should be cast over an approved compacted fill cap. In areas of existing fill, the ground should be prepared and the fill placed in conformance with the **SITE PREPARATION** section of this report.

Slabs should be at least 4 inches thick and reinforced with a minimum of #4 bars on 16-inch centers, each way. Care should be taken to cast the reinforcement near the center of the slab. For interior slabs and slabs with a floor covering, a moisture barrier is recommended. For performance and concrete curing, it is recommended that the vapor barrier be 10-mil thick and placed over at least two inches of clean sand and then covered by at least two inches of clean sand. The topping sand is intended to prevent punctures during placement of the reinforcing steel and to aid in the concrete cure.

Slabs which will be provided with a moisture-sensitive floor covering should be designed to resist moisture in conformance with ACI 302.2R-06 (Guide for Concrete Slabs that Receive Moisture-Sensitive Flooring Material). Specifications for under-slab vapor retarder/barrier are typically the responsibility of the architect or flooring specialist. We would be happy to assist the architect and/or flooring specialist on their specifications for moisture protection of slabs that are to receive moisture sensitive coverings.

Many agencies require floor slabs be constructed in conformance with the Green Building Code that requires slabs be poured directly on top of the vapor barrier, which is to be underlain by four inches of gravel. Since the vapor barrier is to be placed on the gravel, it is important to exercise care to prevent damaging the moisture barrier during construction. From a geotechnical engineering standpoint, a vapor barrier may be placed over 4 inches of gravel, provided that the vapor barrier is of sufficient strength to resist punctures and tearing. If plastic sheeting is used, this may require a greater than 10 mil thickness. Bentonitic barriers such as Miraclay or Volclay may also be used as long as they conform to the minimum requirements of durability, strength and waterproofing. Vapor barriers should conform to ASTM E 1745 and ACI 302.2R-06 (Guide for Concrete Slabs that Receive Moisture-Sensitive Flooring Materials).

It should be noted that cracking of concrete floor slabs is very common during curing. The cracking occurs because concrete shrinks as it dries. Crack control joints which are commonly used in exterior decking to control such cracking are normally not used in

interior slabs. The reinforcement recommended above is intended to reduce cracking and its proper placement is critical to the slab's performance. The minor shrinkage cracks which often form in interior slabs generally do not present a problem when carpeting, linoleum, or wood floor coverings are used. The slab cracks can, however, lead to surface cracks in brittle floor coverings such as ceramic tile. A mortar bed or slip sheet is recommended between the slab and tile to limit the potential for cracking.

Slabs should be protected with a polyethylene plastic vapor barrier placed beneath the slab. This barrier is intended to prevent the upward migration of moisture from the subgrade soils through the porous concrete slab. It should be noted that vapor barriers are penetrated by any number of elements including water lines, drain lines, and footings. These barriers are therefore not completely watertight. It is recommended that a surface seal be placed on slabs which will receive a wood floor. The floor installer should be consulted regarding an adequate product.

## **DRAINAGE**

Control of site drainage is important for the performance of the proposed project. Roof gutters are recommended for the proposed structures. Pad and roof drainage should be collected and transferred to the street or approved location in non-erosive drainage devices. Drainage should not be allowed to pond on the pad or against any foundation. The 2019 California Building Code specifies that the grade within 10 feet of the foundation be sloped to drain at a 5 percent gradient away from the building. Drainage control devices require periodic cleaning, testing, and maintenance to remain effective.

## **Infiltration**

A LID plan has not been provided to Irvine Geotechnical for our review and onsite infiltration is currently not proposed. Infiltration testing was performed to determine the feasibility. If feasible, a supplemental report will be prepared to present recommendations to the Department of the design of the LID system.

Infiltration testing was performed in the alluvial soils in Test Pit 1. The tested saturated infiltration rate ( $K_{sat}$ ) range from is 116 to 425 in/hr. This rate is within the range of published hydraulic conductivities for native gravelly sand deposits. A safety factor of 3 is recommended for design of the infiltration system. The recommended design  $K_{sat}$  is 30 in/hr.

## PLAN REVIEW

Formal plans ready for submittal to the Building Department should be reviewed by Irvine Geotechnical. Any change in scope of the project may require additional work.

## SITE OBSERVATIONS DURING CONSTRUCTION

Please advise Irvine Geotechnical at least 24 hours prior to any required site visit. The agency approved plans and permits should be at the jobsite and available to our representative. The project consultant will perform the observation and post a notice at the jobsite of his visit and findings. This notice should be given to the agency inspector.

During construction, a number of reviews by this office are recommended to verify site geotechnical conditions and conformance with the intent of the recommendations for construction. Although not all possible geotechnical observation and testing services are required by the reviewing agency, the more site reviews requested, the lower the risk of future problems. It is recommended that all grading, foundation, and drainage excavations be seen by a representative of the geotechnical engineer PRIOR to placing fill, forms, pipe, concrete, or steel. Any fill which is placed should be approved, tested, and verified if used for engineering purposes. Temporary excavations should be observed by a representative of the Geotechnical Engineer.

The following site reviews are advised or required. Should the observations reveal any unforeseen hazards, the geologist/engineer will recommend treatment.

Pre-construction meeting	Advised
Temporary excavations	Required
Bottom excavation for removals	Required
Compaction of fill	Required
Foundation excavations	Required
Slab subgrade pre-saturation	Required
Slab subgrade moisture barrier membrane	Advised
Slab subgrade rock placement	Advised
Slab steel placement	Advised
Subdrain and rock placement behind retaining walls	Required
Compaction of utility trench backfill	Advised

Irvine Geotechnical requires at least a 24-hour notice prior to any required site visits. The approved plans and building/grading permits should be on the job and available to the project consultant.

## FINAL INSPECTION

Many projects are required by the agency to have final geologic and soils engineering reports upon completion of the grading.

## CONSTRUCTION SITE MAINTENANCE

It is the responsibility of the contractor to maintain a safe construction site. When excavations exist on a site, the area should be fenced and warning signs posted. All pile excavations must be properly covered and secured. Soil generated by foundation and subgrade excavations should be either removed from the site or properly placed as a certified compacted fill. Soil must not be spilled over any descending slope. Workers should not be allowed to enter any unshored trench excavations over five feet deep.

## GENERAL CONDITIONS

This report and the exploration are subject to the following NOTICE. Please read the NOTICE carefully, it limits our liability.

### NOTICE

In the event of any changes in the design or location of any structure, as outlined in this report, the conclusions and recommendations contained herein may not be considered valid unless the changes are reviewed by us and the conclusions and recommendations are modified or reaffirmed after such review.

The subsurface conditions, excavation characteristics, and geologic structure described herein and shown on the enclosed cross sections have been projected from excavations on the site as indicated and should in no way be construed to reflect any variations that may occur between these excavations or that may result from changes in subsurface conditions.

Fluctuations in the level of groundwater may occur due to variations in rainfall, temperature, irrigation, and other factors not evident at the time of the measurements reported herein. Fluctuations also may occur across the site. High groundwater levels can be extremely hazardous. Saturation of earth materials can cause subsidence or slippage of the site.

If conditions encountered during construction appear to differ from those disclosed herein, notify us immediately so we may consider the need for modifications. Compliance with the design concepts, specifications or recommendations during construction requires the review of the engineering geologist and geotechnical engineer during the course of construction.

**THE EXPLORATION WAS PERFORMED ONLY ON A PORTION OF THE SITE AND CANNOT BE  
CONSIDERED AS INDICATIVE OF THE PORTIONS OF THE SITE NOT EXPLORED.**

This report is issued and made for the sole use and benefit of the client, is not transferable and is as of the exploration date. Any liability in connection herewith shall not exceed the fee for the exploration. No warranty, expressed or implied, is made or intended in connection with the above exploration or by the furnishing of this report or by any other oral or written statement.

**THIS REPORT WAS PREPARED ON THE BASIS OF THE PRELIMINARY DEVELOPMENT PLAN  
OR CONCEPT FURNISHED. FINAL PLANS SHOULD BE REVIEWED BY THIS OFFICE AS  
ADDITIONAL GEOTECHNICAL WORK MAY BE REQUIRED.**

February 24, 2021

IC 20168-J

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Irvine Geotechnical appreciates the opportunity to provide our service on this project. Any questions concerning the data or interpretation of this report should be directed to the undersigned.

Respectfully submitted,  
Irvine Geotechnical, Inc.

Jon A. Irvine  
E.G. 1691/G.E. 2891

[https://irvinegeotech-my.sharepoint.com/personal/jirvine\\_irvinegeotech\\_com/Documents/Standard%20Form%202021A.docx](https://irvinegeotech-my.sharepoint.com/personal/jirvine_irvinegeotech_com/Documents/Standard%20Form%202021A.docx)



Enc: Appendix I - Laboratory Testing by Soil Labworks  
Moisture-Density Relationship (Plate A-1)  
Shear Test Diagrams (Plates B-1 through B-4)  
Consolidation Diagrams (Plates C-1 through C-3)

Vicinity Map  
Regional Geologic Map  
Log of Test Pits (4 Pages)  
Calculation Sheet  
Sections A and B  
Site Plan

xc: (3) Addressee

## STATEMENT OF RESPONSIBILITY - SOIL TESTING BY SOIL LABWORKS, LLC

Laboratory testing by Soil Labworks, LLC, was performed under the supervision of the undersigned engineer. Irvine Geotechnical and Jon A. Irvine has reviewed referenced laboratory testing report dated February 4, 2021, and the results appear to be reasonable for this area of the Santa Monica Basin. Irvine Geotechnical and the undersigned engineer concurs with the findings of Soil Labworks, LLC, and accepts professional responsibility for utilizing the data.



SL21.3542  
February 4, 2021

Irvine Geotechnical  
145 N. Sierra Madre Boulevard  
Suite 1  
Pasadena, California 91107

**Subject:** Laboratory Testing

**Site:** 12737 Mitchell Avenue  
Los Angeles, California

**Job:** IRVINE/BREAKFORM/MITCHELL- IC20165

Laboratory testing for the subject property was performed by Soil Labworks, LLC., under the supervision of the undersigned Engineer. Samples of the earth materials were obtained from the subject property by personnel of Irvine Geotechnical and transported to the laboratory of Soil Labworks for testing and analysis. The laboratory tests performed are described and results are attached.

Services performed by this facility for the subject property were conducted in a manner consistent with that level of care and skill ordinarily exercised by members of the profession currently practicing in the same locality under similar conditions.

Respectfully Submitted:

SOIL LABWORKS, LLC

JON A. IRVINE  
G.E. 2891  
Enc: Appendix



## APPENDIX

### Laboratory Testing

#### Sample Retrieval - Hand Labor

Samples of earth materials were obtained by driving a thin-walled steel sampler with successive blows of a drop hammer. The earth material was retained in brass rings of 2.416 inches inside diameter and 1.00 inch height. The samples were stored in closefitting, water-tight containers for transportation to the laboratory.

#### Moisture Density

The field moisture content and dry density were determined for each of the soil samples. The dry density was determined in pounds per cubic foot following ASTM 2937-17e2. The moisture content was determined as a percentage of the dry soil weight conforming to ASTM 2216-19. The results are presented below in the following table. The percent saturation was calculated on the basis of an estimated specific gravity. Description of earth materials used in this report and shown on the attached Plates were provided by the client.

Test Pit/Boring No.	Sample Depth (Feet)	Soil Type	Dry Density (pcf)	Moisture Content (percent)	Percent Saturation (G <sub>s</sub> =2.65)
TP1	2	Alluvium	109.5	11.9	62
TP1	5	Alluvium	106.5	10.6	51
TP1	7	Alluvium	114.1	6.2	36
TP1	10	Alluvium	127.8	3.3	30
TP2	½	Fill	92.2	5.1	17
TP2	2	Alluvium	110.2	17.9	95
TP2	5	Alluvium	114.8	15.0	90
TP2	7	Alluvium	117.7	7.7	50
TP2	10	Alluvium	117.5	3.0	19
TP3	2	Alluvium	106.9	19.1	92
TP3	5	Alluvium	98.6	21.6	85
TP3	7	Alluvium	113.1	16.6	95
TP3	10	Alluvium	124.8	2.6	21
TP4	2	Alluvium	114.1	17.1	100
TP4	5	Alluvium	95.9	27.3	100
TP4	7	Alluvium	117.5	14.9	97
TP4	10	Alluvium	127.6	3.9	35

## Compaction Character

Compaction tests were performed on bulk samples of the earth materials in accordance with ASTM D1557-12ei. The results of the tests are provided on the table below and on the "Moisture-Density Relationship", A-Plates. The specific gravity of the fill was estimated from the compaction curves.

Test Pit/Boring No.	Sample Depth (Feet)	Soil Type	Maximum Dry Density (pcf)	Optimum Moisture Content (Percent)
TP2	0-10.5	Fill	124.4	10.6

## Shear Strength

The peak and ultimate shear strengths of the fill and alluvium were determined by performing consolidated and drained direct shear tests in conformance with ASTM D3080/D3080M-11. The tests were performed in a strain-controlled machine manufactured by GeoMatic. The rate of deformation was 0.01 inches per minute. Samples were sheared under varying confining pressures, as shown on the "Shear Test Diagrams," B-Plates. Remolded samples were prepared at 90 percent of the maximum density for shear tests. The remolding procedure consists of selecting a representative sample from a bulk bag and sieving it through a No. 4 sieve. The moisture content of the material is then determined. A formula is then used to calculate the weight of the material that must fit in a ring when compacted to 90 percent of the maximum density. This calculated amount of material is then weighed out and pounded into a ring until all the material is used and the ring is full. The moisture conditions during testing are shown on the following table and on the B-Plates. The samples indicated as saturated were artificially saturated in the laboratory. All saturated samples were sheared under submerged conditions.

Test Pit/Boring No.	Sample Depth (Feet)	Dry Density (pcf)	As-Tested Moisture Content (percent)
TP2	1/2	92.2	30.0
TP3	5	98.6	31.2
TP4	5	95.9	31.9
TP2*	0-10.5	112.0	20.6

\* Sample remolded to 90 % of the laboratory maximum density.

## Consolidation

One-dimensional consolidation tests were performed on samples of the alluvium in a consolidometer manufactured by GeoMatic in conformance with ASTM D2435/D2435M-11. The tests were performed on 1-inch high samples retained in brass rings. The samples were initially loaded to approximately ½ of the field over-burden pressure and then unloaded to compensate for the effects of possible disturbance during sampling. Loads were then applied in a geometric progression and resulting deformation recorded. Water was added at a specific load to determine the effect of saturation. The results are plotted on the "Consolidation Test," C-Plates.

## Expansion Index

The expansive character of the fill was determined by performing Expansion Index Tests in accordance with UBC 18.2 and ASTM 4829-11. A bulk sample of earth material was compacted at a specific moisture content using one fifth the compacted energy for the modified proctor test. The sample was then saturated and the expansion measured. The results of the tests are provided on the following table.

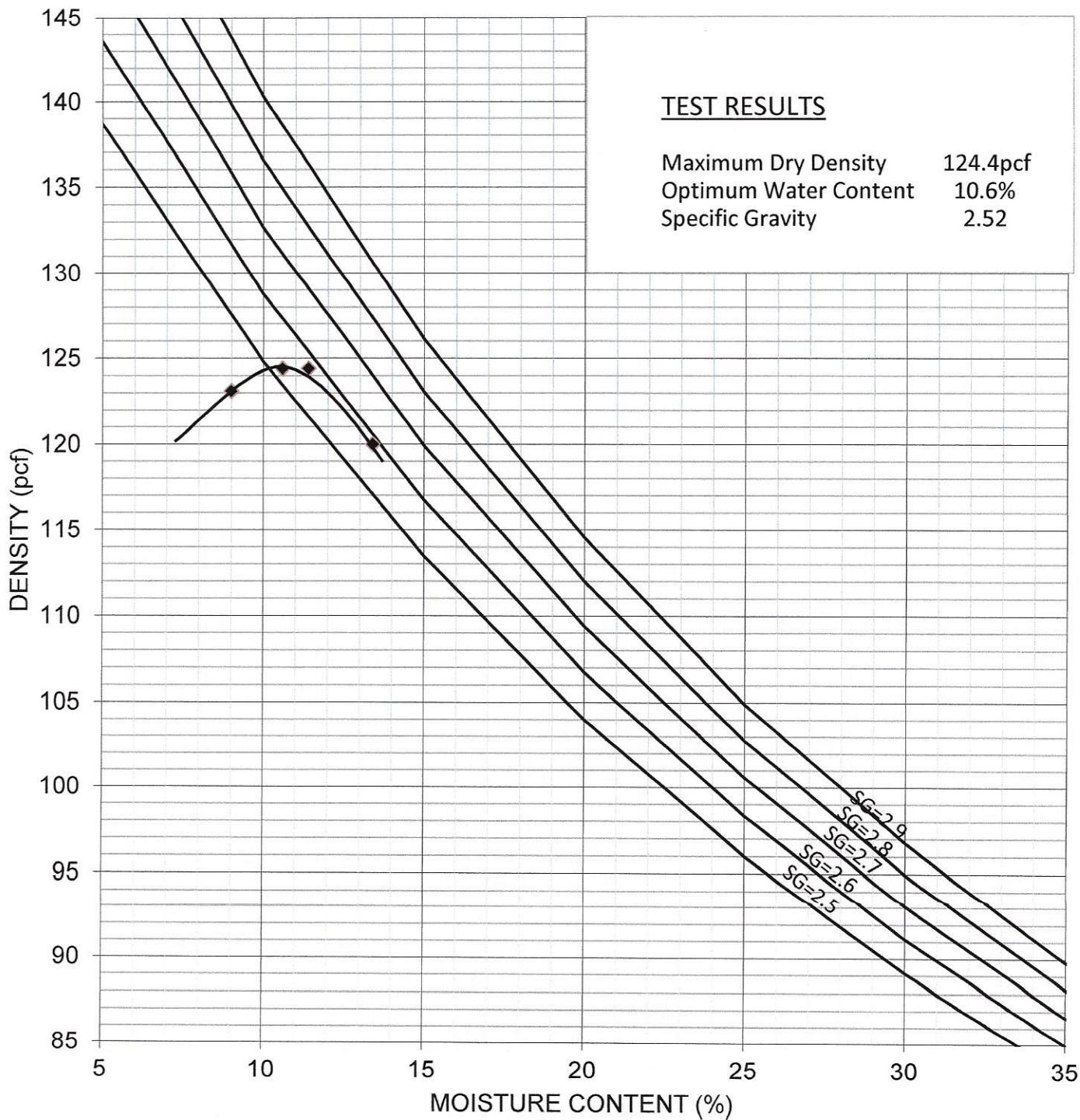
<b>Test Pit No.</b>	<b>Sample Depth (Feet)</b>	<b>Soil Type</b>	<b>Expansion Index</b>
TP2	0-10.5	Fill	59



## MOISTURE-DENSITY RELATIONSHIP A-1

JN: SL21.3542 CONSULTANT: JAI  
CLIENT: IRVINE/Breakform-12737 Mitchell Ave  
TP2 @ 0-10.5'  
EARTH MATERIAL: FILL

NOTE: ASTM Test Method D-1557-12





## SHEAR DIAGRAM B-1

JN: SL21.3542 CONSULTANT JAI

CLIENT: Irvine/Breakform-12737 Mitchell Avenue

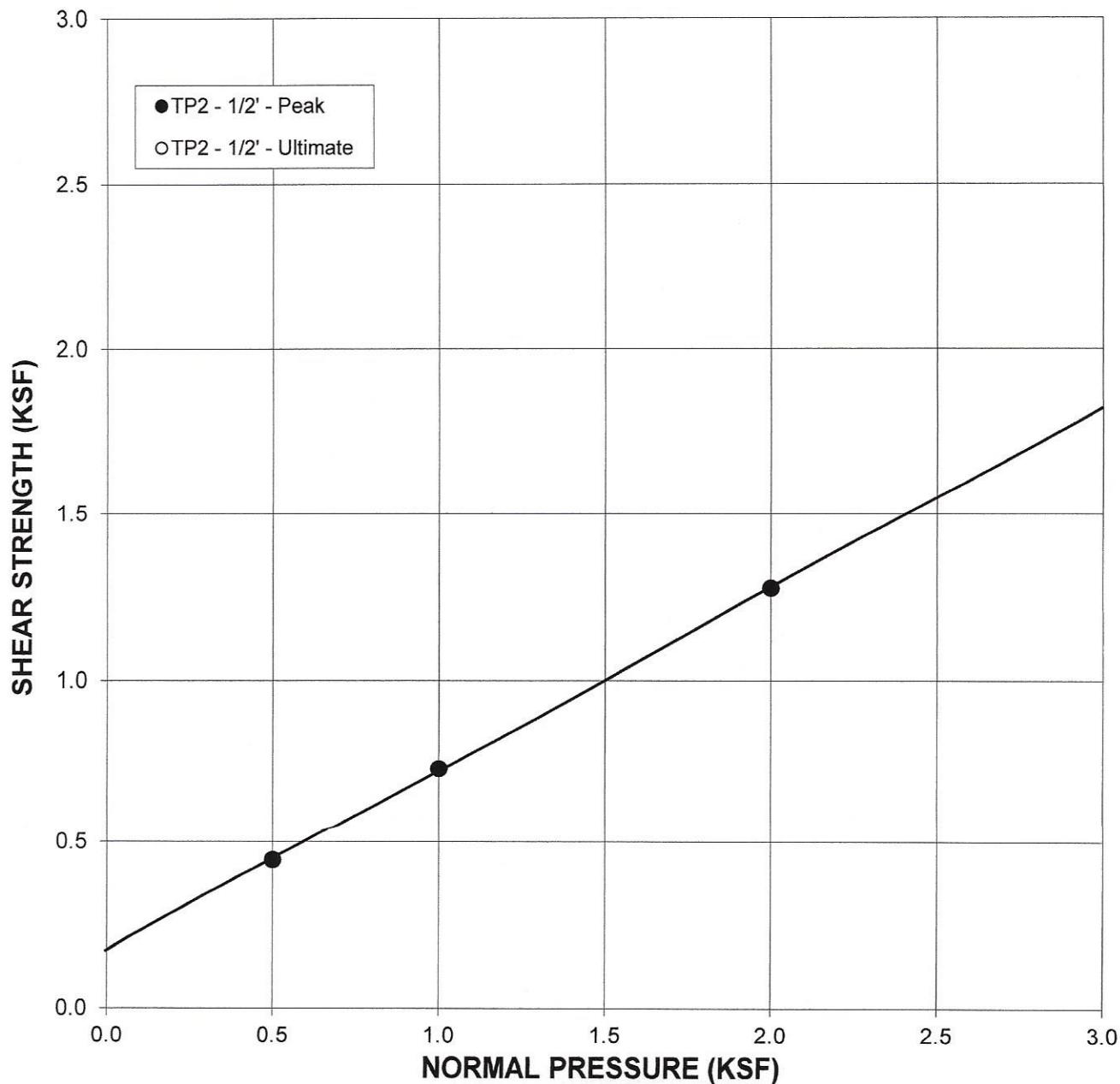
EARTH MATERIAL:

FILL

Phi Angle      PEAK      ULTIMATE  
Cohesion      28      28      degrees  
                    180      180      psf

Average Moisture Content      30.0%  
Average Dry Density (pcf)      92.2  
Percent Saturation      100.0%

### DIRECT SHEAR TEST - ASTM D-3080





**SOIL  
LABWORKS** LLC

## SHEAR DIAGRAM B-2

JN: SL21.3542 CONSULTANT JAI

CLIENT: Irvine/Breakform-12737 Mitchell Avenue

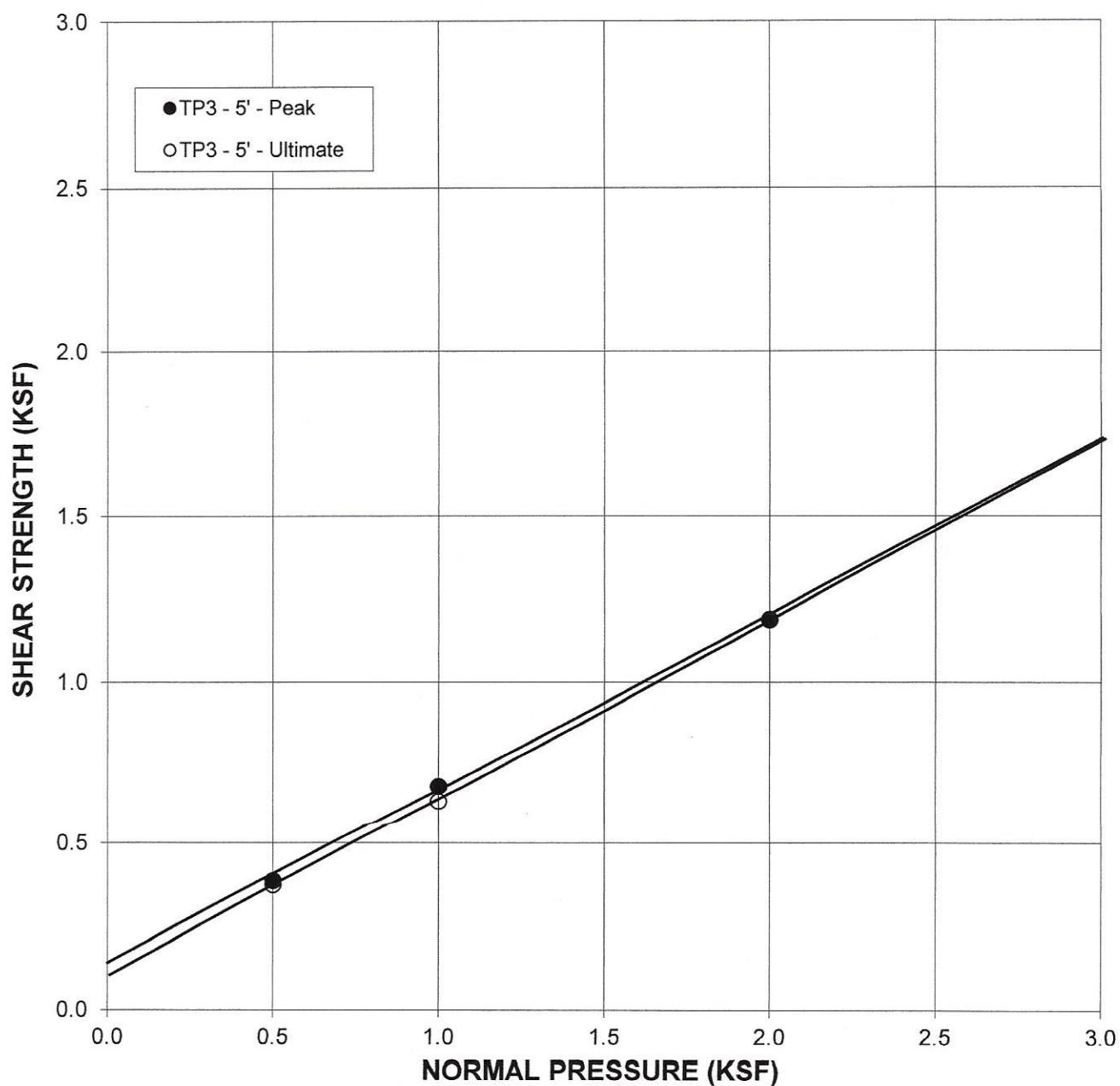
EARTH MATERIAL: ALLUVIUM

	PEAK	ULTIMATE	Average Moisture Content	31.2%
Phi Angle	27.5	28	degrees	
Cohesion	140	100	psf	98.6

Average Dry Density (pcf)  
Percent Saturation

100.0%

### DIRECT SHEAR TEST - ASTM D-3080





**SOIL  
LABWORKS** LLC

## SHEAR DIAGRAM B-3

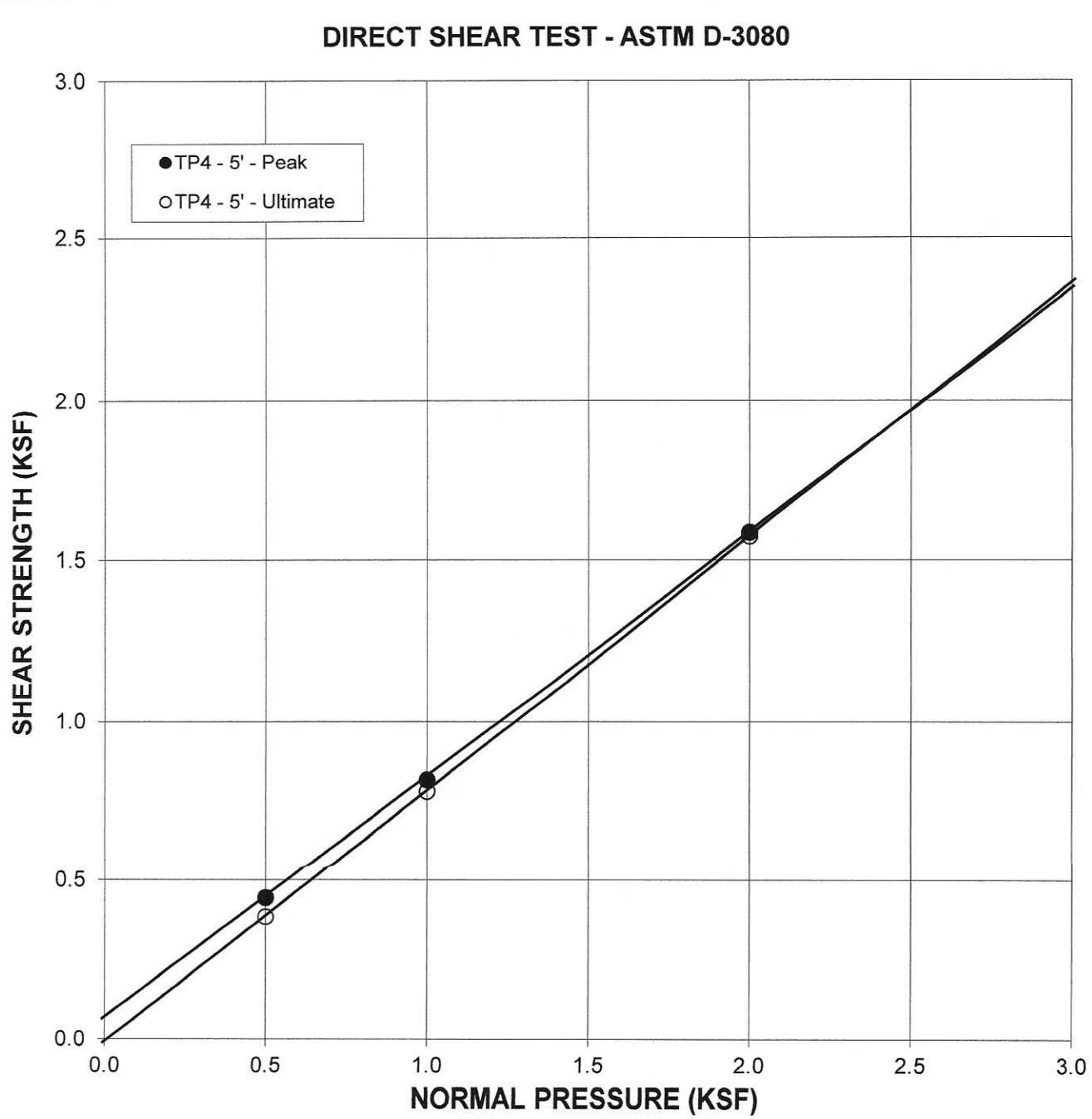
JN: SL21.3542 CONSULTANT JAI

CLIENT: Irvine/Breakform-12737 Mitchell Avenue

EARTH MATERIAL: ALLUVIUM

Phi Angle	PEAK	ULTIMATE
Cohesion	37	38
	65	0
		degrees
		psf

Average Moisture Content	31.9%
Average Dry Density (pcf)	95.9
Percent Saturation	100.0%





**SOIL  
LABWORKS**  
LLC

## SHEAR DIAGRAM B-4

JN: SL21.3542 CONSULTANT JAI

CLIENT: Irvine/Breakform-12737 Mitchell Avenue

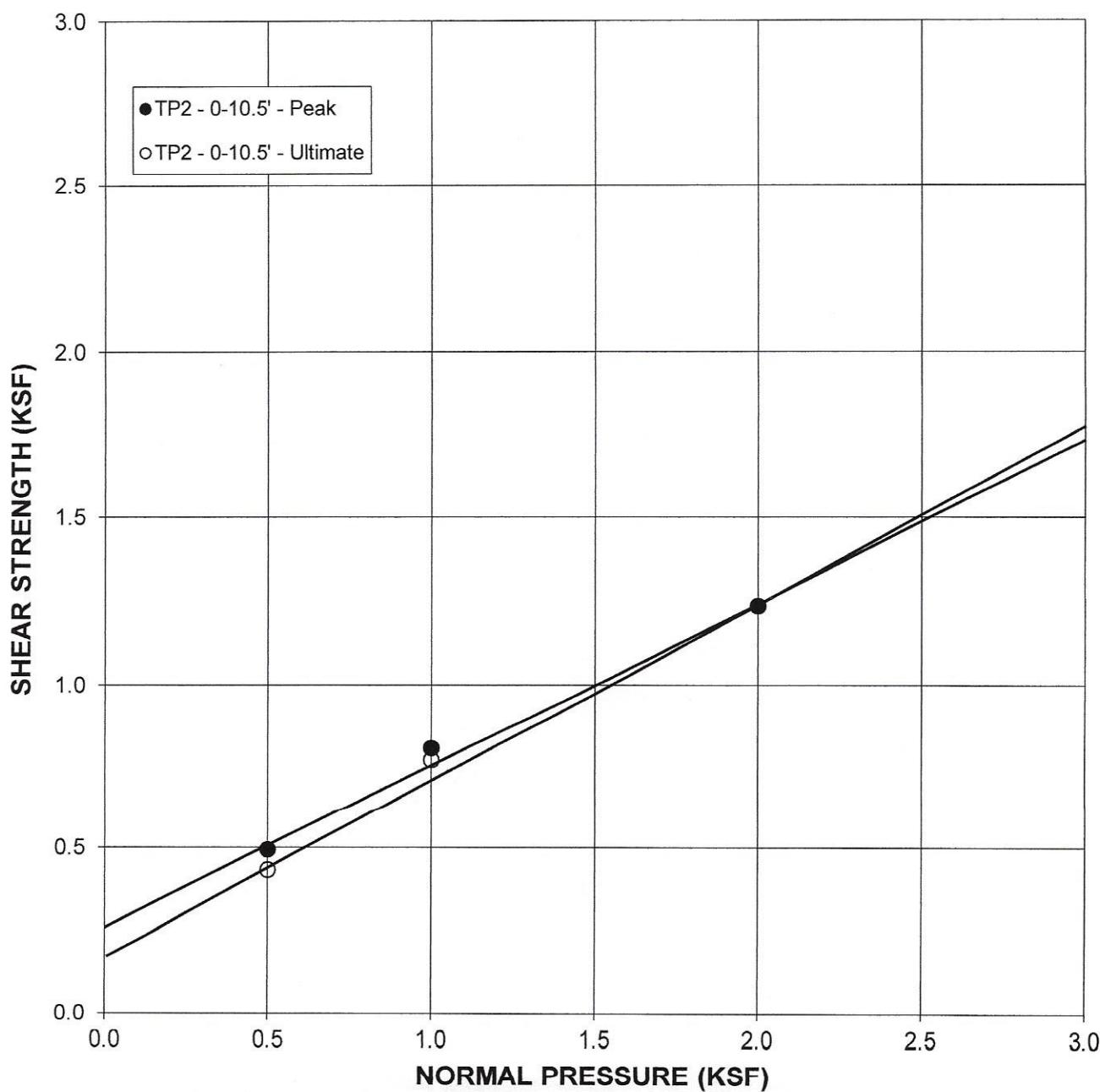
EARTH MATERIAL: FILL

Sample remolded to 90 % of the laboratory maximum density

	PEAK	ULTIMATE
Phi Angle	26.5	28.5
Cohesion	265	180
	degrees	psf

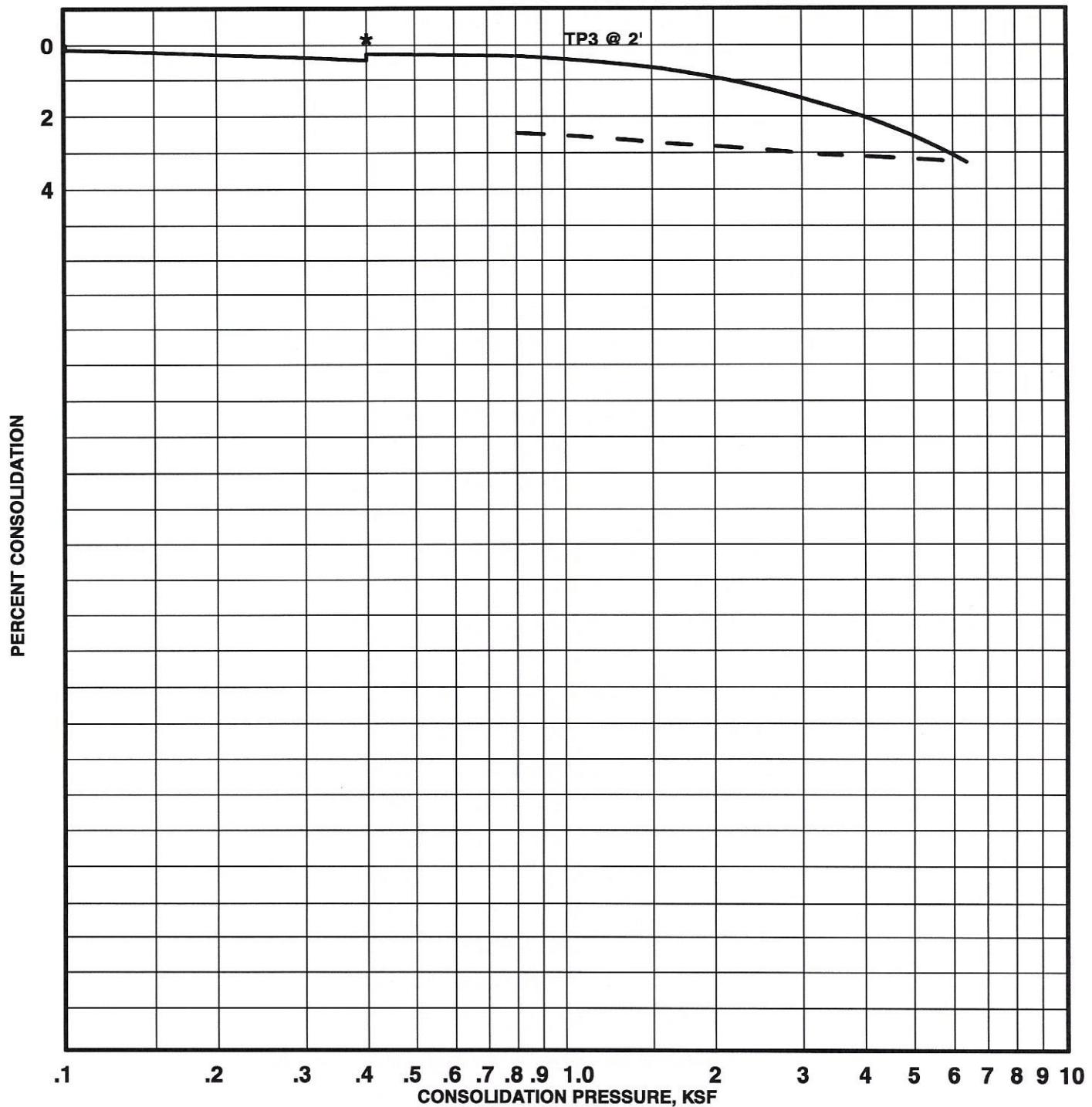
Average Moisture Content	20.6%
Average Dry Density (pcf)	112.0
Percent Saturation	100.0%

### DIRECT SHEAR TEST - ASTM D-3080



**CONSOLIDATION TEST**  
PROJECT: 3542 IRVINE/BREAKFORM-12737 MITCHELL AVE  
SAMPLE: TP3 @ 2'

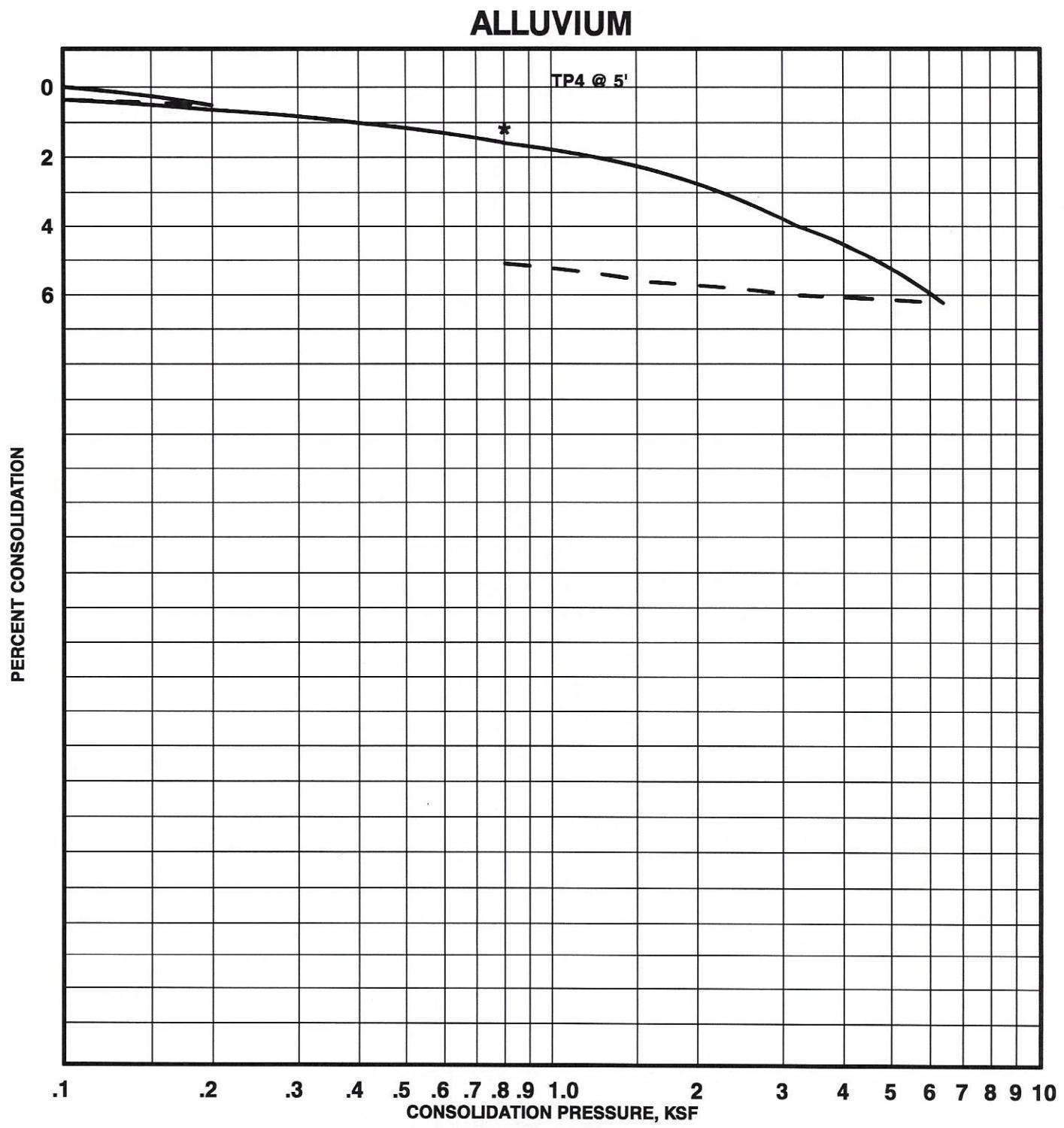
**ALLUVIUM**



\* Water Added

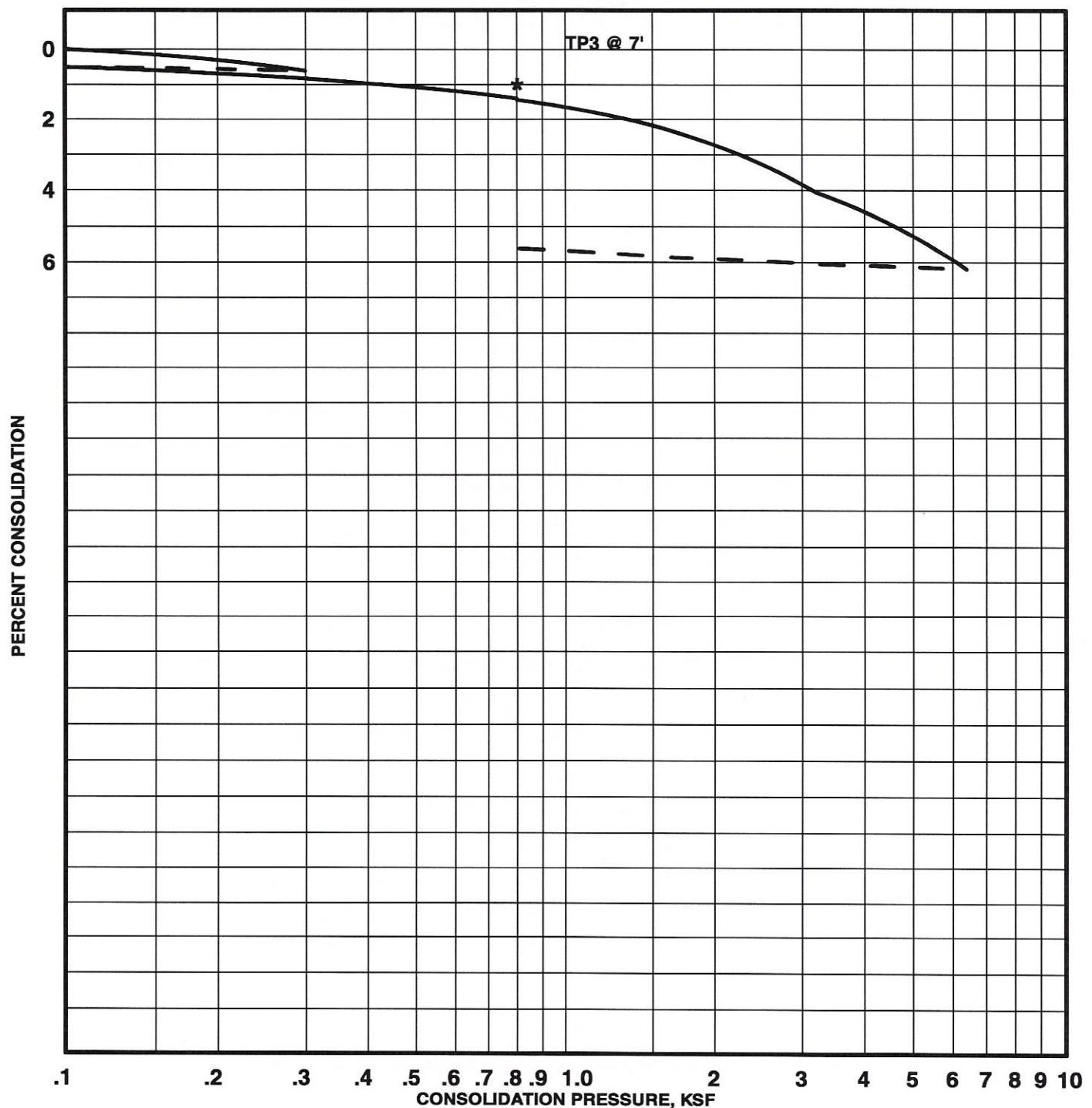
PLATE:

**CONSOLIDATION TEST**  
PROJECT: 3542 IRVINE/BREAKFORM-12737 MITCHELL AVE  
SAMPLE: TP4 @ 5'



**CONSOLIDATION TEST**  
PROJECT: 3542 IRVINE/BREAKFORM-12737 MITCHELL AVE  
SAMPLE: TP3 @ 7'

**ALLUVIUM**



\* Water Added

PLATE:



## SLOT CUT ANALYSIS

IC: 20168 CONSULT: KLJ  
CLIENT: BREAKFORM MITCHELL

### CALCULATION SHEET #

CALCULATE THE FACTOR OF SAFETY OF SLOT CUT EXCAVATIONS. ASSUME COHESIVE AND FRICTIONAL RESISTANCE ALONG THE SIDES OF SLOTS AS WELL AS THE FAILURE SURFACE. THE HORIZONTAL PRESSURE ON THE SIDES OF THE SLOTS IS THE AT-REST PRESSURE (1-SIN(phi)).

### CALCULATION PARAMETERS

EARTH MATERIAL:	ALLUVIUM	EXCAVATION HEIGHT:	5 feet
SHEAR DIAGRAM:	B-2	BACKSLOPE ANGLE:	0 degrees
COHESION:	140 psf	SURCHARGE:	0 pounds
PHI ANGLE:	27.5 degrees	SURCHARGE TYPE:	P Point
DENSITY:	129pcf	INITIAL FAILURE ANGLE:	20 degrees
SLOT BOUNDARY CONDITIONS		FINAL FAILURE ANGLE:	70 degrees
SLOT CUT WIDTH:	8 feet	INITIAL TENSION CRACK:	2 feet
COHESION:	140 psf	FINAL TENSION CRACK:	15 feet
PHI ANGLE:	27.5 degrees		

### CALCULATED RESULTS

CRITICAL FAILURE ANGLE	55 degrees
HORIZONTAL DISTANCE TO UPSLOPE TENSION CRACK	2.0 feet
DEPTH OF TENSION CRACK	2.1 feet
TOTAL EXTERNAL SURCHARGE	0.0 pounds
VOLUME OF FAILURE WEDGE	57.1 ft <sup>3</sup>
WEIGHT OF FAILURE WEDGE	7372.3 pounds
LENGTH OF FAILURE PLANE	3.5 feet
SURFACE AREA OF FAILURE PLANE	28 ft <sup>2</sup>
SURFACE AREA OF SIDES OF SLOTS	7.1 ft <sup>2</sup>
NUMBER OF TRIAL WEDGES ANALYZED	3345 trials
TOTAL RESISTING FORCE ALONG WEDGE BASE (FrB)	2871.6 pounds
TOTAL RESISTING FORCE ALONG WEDGE SIDES (FrS)	1584.1 pounds
RESULTANT HORIZONTAL COMPONENT OF FORCE	-39.7 pounds
CALCULATED FACTOR OF SAFETY	1.36

### CONCLUSIONS:

THE CALCULATION INDICATES THAT SLOTS CUTS UP TO 8 FEET WIDE AND 5 FEET HIGH IN THE ALLUVIUM HAVE A SAFETY FACTOR GREATER THAN 1.25 AND ARE TEMPORARILY STABLE.

118°28.000' W

118°27.000' W

118°26.000' W

WGS84 118°25.000' W

## VICINITY MAP

34°01.000' N

118°27.000' W

118°26.000' W

WGS84 118°25.000' W

34°01.000' N

34°00.000' N

118°27.000' W

118°26.000' W

WGS84 118°25.000' W

34°00.000' N

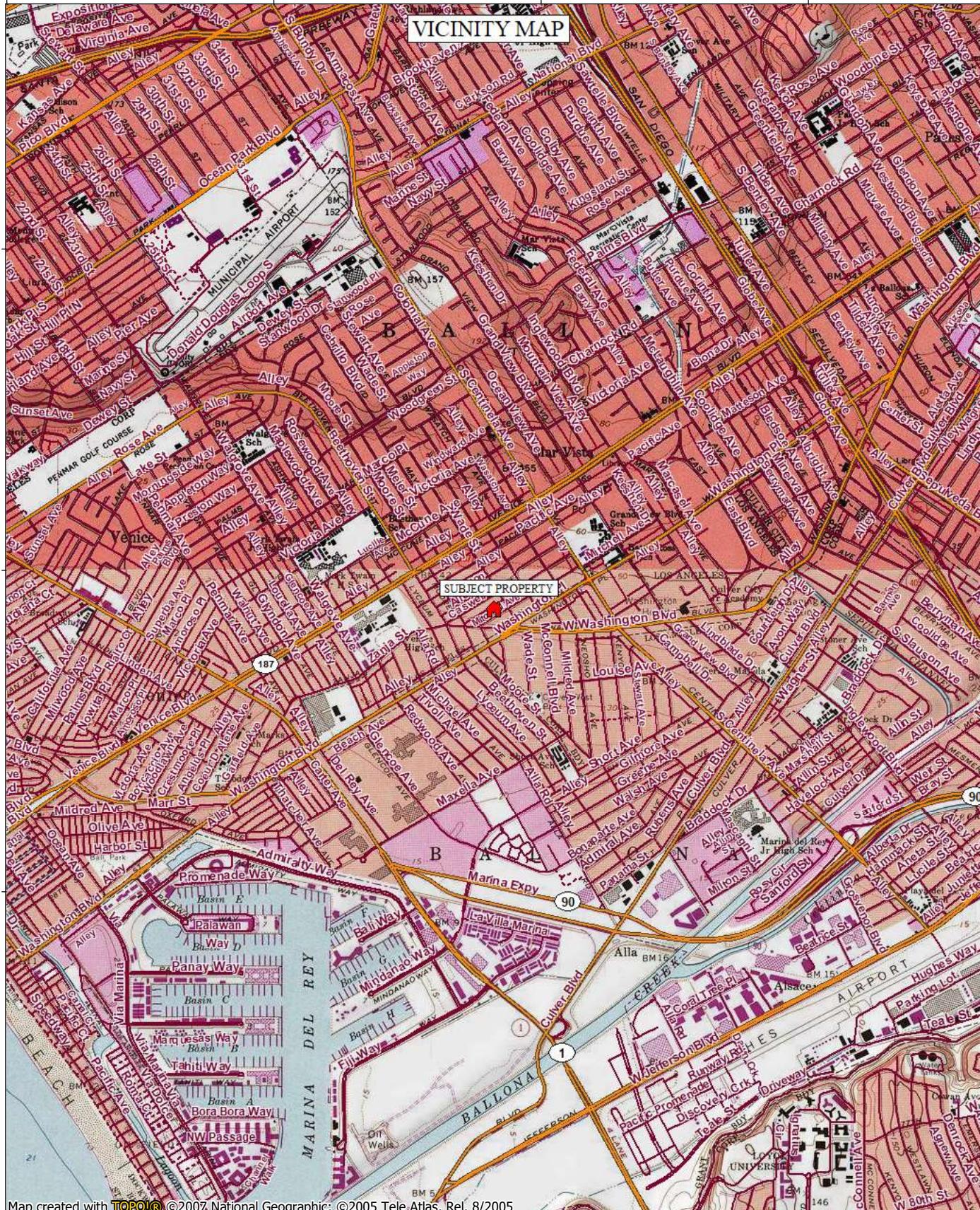
33°59.000' N

118°27.000' W

118°26.000' W

WGS84 118°25.000' W

33°59.000' N



118°28.000' W

118°27.000' W

118°26.000' W

WGS84 118°25.000' W

TN\* MN

12°

02/10/21

# IRVINE

## GEOTECHNICAL Inc

## REGIONAL GEOLOGIC MAP

1C:

20168

## CONSULT:

KJ

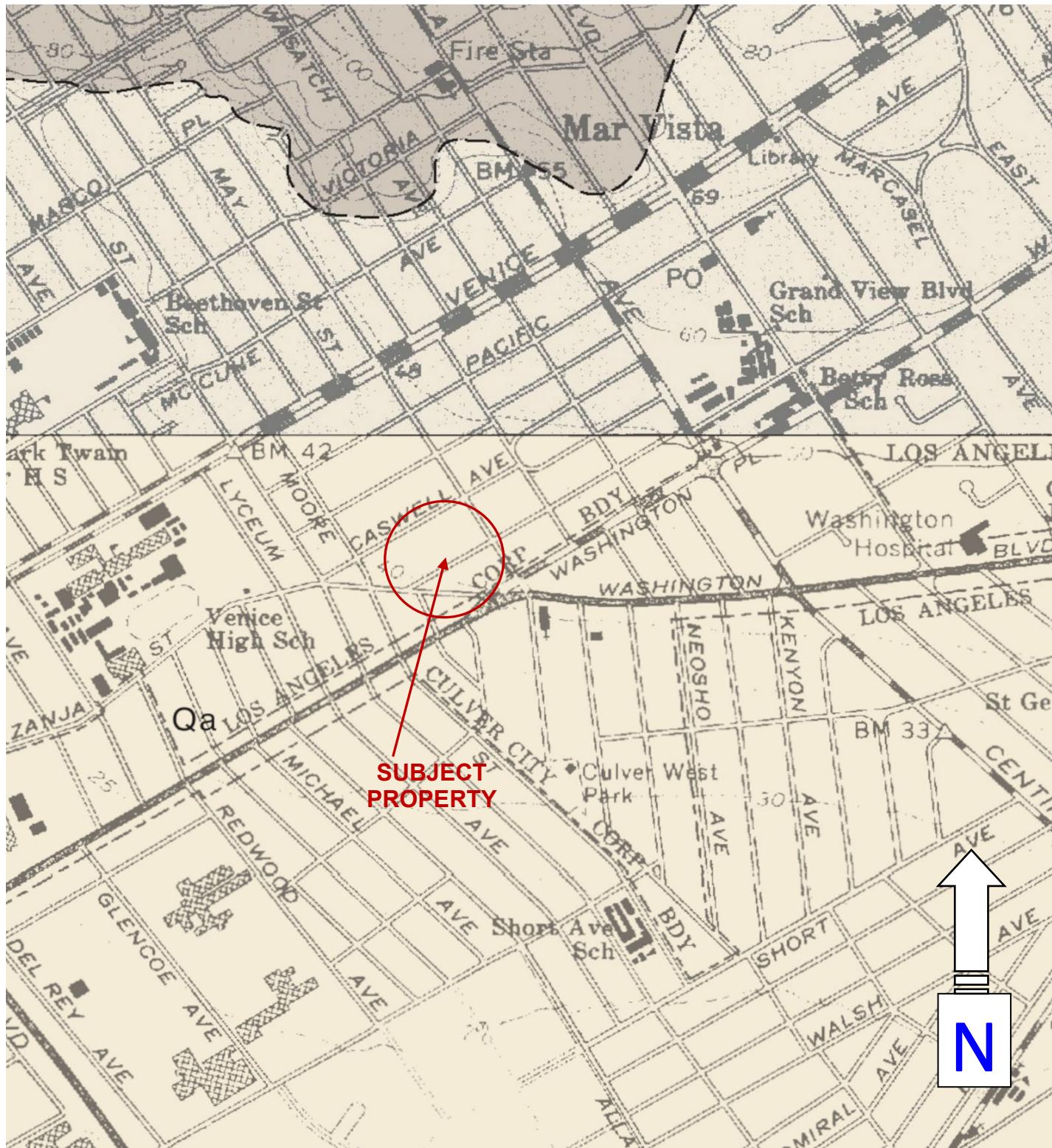
## CLIENT

## BREAKFORM

**SCALE:**

1" = 1,000'

REFERENCE: Geologic Maps of the Santa Monica Mountains and Vicinity, CD Compilation T.W. Dibblee, 2001





## LOG OF TEST PITS

PROJECT IC20168 BREAKFORM  
 DRILL DATE 1/18/21  
 LOG DATE 1/18/21  
 LOGGED BY KJONES  
 DRILL TYPE Hand Labor/Hand Auger  
 DIAMETER 6 Inches

SURFACE ELEVATION 100 feet

DRILLING CONTRACTOR Mike's Excavating Service

SURFACE CONDITIONS Planter at southeast portion of property

### TEST PIT 1

Sample Type	Sample Depth (feet)	Blows per foot	Moisture (%)	Dry Unit Weight (pcf)	Saturation (%)	USCS Code	Elevation (feet)	Depth (feet)	Lithologic Description
R	2	N/A	11.9	109.5	62	ML	100.0	0	<b>FILL:</b> Sandy Silt with minor Clay binder, dark brown, brown, moist, medium dense, roots, occasional gravel to 1/2" in diameter
						ML	99.0	1	<b>ALLUVIUM:</b> Sandy Silt with minor Clay binder, dark brown, brown, slightly porous, moist, medium dense, occasional gravel and slate fragments to 1/4" in diameter
R	5	N/A	10.6	106.5	51		98.0	2	
R	7	N/A	6.2	114.1	36	SM	97.0	3	
R	10	N/A	3.3	127.8	30	GP	96.0	4	
							95.0	5	
							94.0	6	
							93.0	7	Silty Sand, brown, slightly moist, dense, occasional gravel and slate fragments to 1/4" in diameter
							92.0	8	
							91.0	9	Gravelly Sand, gray, slightly moist, dense to very dense, gravel and slate fragments to 2" in diameter
							90.0	10	
							89.0	11	<b>END TP1 @ 11':</b> No Water; Caving @ 10'; Fill to 1' Hand Augered: 0' - 11'



## LOG OF TEST PITS

PROJECT IC20168 BREAKFORM  
 DRILL DATE 1/18/21  
 LOG DATE 1/18/21  
 LOGGED BY KJONES  
 DRILL TYPE Hand Labor/Hand Auger  
 DIAMETER 30 Inches

SURFACE ELEVATION 100.5 feet

DRILLING CONTRACTOR Mike's Excavating Service

SURFACE CONDITIONS Planter at southeast corner of building to the south

### TEST PIT 2

Sample Type	Sample Depth (feet)	Blows per foot	Moisture (%)	Dry Unit Weight (pcf)	Saturation (%)	USCS Code	Elevation (feet)	Depth (feet)	Lithologic Description
R	1/2	N/A	5.1	92.2	17	ML	100.5	0	<b>FILL:</b> Sandy Silt with minor Clay binder, brown, very slightly moist, slightly porous, loose to medium dense, occasional roots, gravel to 2" in diameter
R	2	N/A	17.9	110.2	95	CL	99.5	1	<b>ALLUVIUM:</b> Silty Clay, dark brown, brown, wet, firm to stiff, occasional gravel and slate fragments to 1/2" in diameter
R	5	N/A	15.0	114.8	90	CL	98.5	2	
R	7	N/A	7.7	117.7	50	SM	97.5	3	
R	10	N/A	3.0	117.5	19	GP	96.5	4	
							95.5	5	Sandy Clay, gray-brown, wet, firm to stiff, occasional gravel and slate fragments to 1" in diameter
							94.5	6	
							93.5	7	Silty Sand, brown, slightly moist, dense, occasional gravel and slate fragments to 1/4" in diameter
							92.5	8	
							91.5	9	
							90.5	10	Gravelly Sand, gray, brown, very slightly moist, dense, gravel and slate fragments to 2" in diameter
									<b>END TP2 @ 10.5':</b> No Water; Caving @ 10.5'; Fill to 1' Hand Augered: 5' - 10.5'



## LOG OF TEST PITS

PROJECT IC20168 BREAKFORM  
DRILL DATE 1/18/21  
LOG DATE 1/18/21  
LOGGED BY KJONES  
DRILL TYPE Hand Labor/Hand Auger  
DIAMETER 6 Inches

SURFACE ELEVATION 101 feet

DRILLING CONTRACTOR Mike's Excavating Service

**SURFACE CONDITIONS** Concrete surface at north corner of existing building to the south

# TEST PIT 3

Sample Type	Sample Depth (feet)	Blows per foot	Moisture (%)	Dry Unit Weight (pcf)	Saturation (%)	USCS Code	Elevation (feet)	Depth (feet)	Lithologic Description
R	2	N/A	19.1	106.9	92	ML	101.0	0	<b>FILL:</b> Clayey Silt, gray-brown, brown, wet, medium dense, occasional gravel to 1/4" in diameter
R	5	N/A	21.6	98.6	85	ML	100.0	1	<b>ALLUVIUM:</b> Clayey Silt, gray-brown, brown, wet, medium dense, occasional gravel and slate fragments to 1/4" in diameter
R	7	N/A	16.6	113.1	95	CL	99.0	2	
R	10	N/A	2.6	124.8	21	GP	98.0	3	
							97.0	4	
							96.0	5	
							95.0	6	
							94.0	7	Sandy Clay, gray-brown, wet, stiff, occasional gravel and slate fragments to 1/4" in diameter
							93.0	8	
							92.0	9	Gravelly Sand, gray, very slightly moist, dense to very dense, gravel and slate fragments to 1" in diameter
							91.0	10	
									<b>END TP3 @ 10.5':</b> No Water; No Caving; Fill to 6"
									Hand Augered: 0' - 10.5



## LOG OF TEST PITS

PROJECT IC20168 BREAKFORM  
 DRILL DATE 1/18/21  
 LOG DATE 1/18/21  
 LOGGED BY KJONES  
 DRILL TYPE Hand Labor/Hand Auger  
 DIAMETER 6 Inches

SURFACE ELEVATION 101.5 feet

DRILLING CONTRACTOR Mike's Excavating Service

SURFACE CONDITIONS Concrete surface at north corner of property

### TEST PIT 4

Sample Type	Sample Depth (feet)	Blows per foot	Moisture (%)	Dry Unit Weight (pcf)	Saturation (%)	USCS Code	Elevation (feet)	Depth (feet)	Lithologic Description
R	2	N/A	17.1	114.1	100	CL/ML	101.5	0	<b>FILL:</b> Silty Clay/Clayey Silt, gray-brown, saturated, firm to stiff/medium dense to dense, occasional gravel to 1/4" in diameter
							100.5	1	
							99.5	2	
							98.5	3	
							97.5	4	
	5	N/A	27.3	95.9	100	CL/ML	96.5	5	<b>ALLUVIUM:</b> Silty Clay/Clayey Silt, gray-brown, saturated, firm to stiff/medium dense to dense, occasional gravel and slate fragments to 1/4" in diameter
							95.5	6	
	7	N/A	14.9	117.5	97	CL	94.5	7	
							93.5	8	Sandy Clay, gray-brown, wet, stiff, occasional gravel and slate fragments to 1/4" in diameter
							92.5	9	
	10	N/A	3.9	127.6	35	GP	91.5	10	Gravelly Sand, gray, yellow-brown, slightly moist, dense to very dense, gravel and slate fragments to 2" in diameter
							<b>END TP4 @ 10.5':</b> No Water; Caving @ 9.5'; Fill to 1' Hand Augered: 0' - 10.5'		

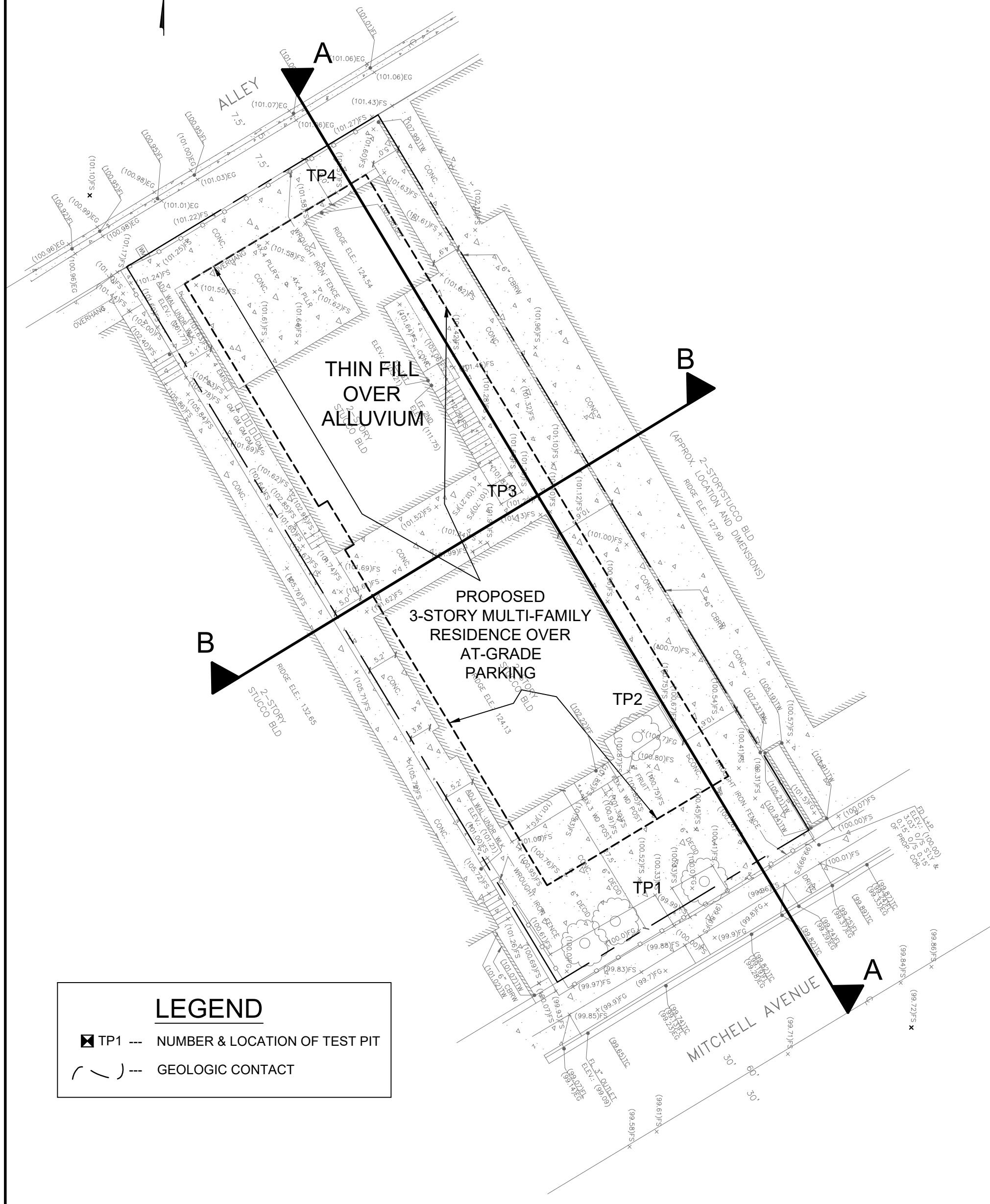


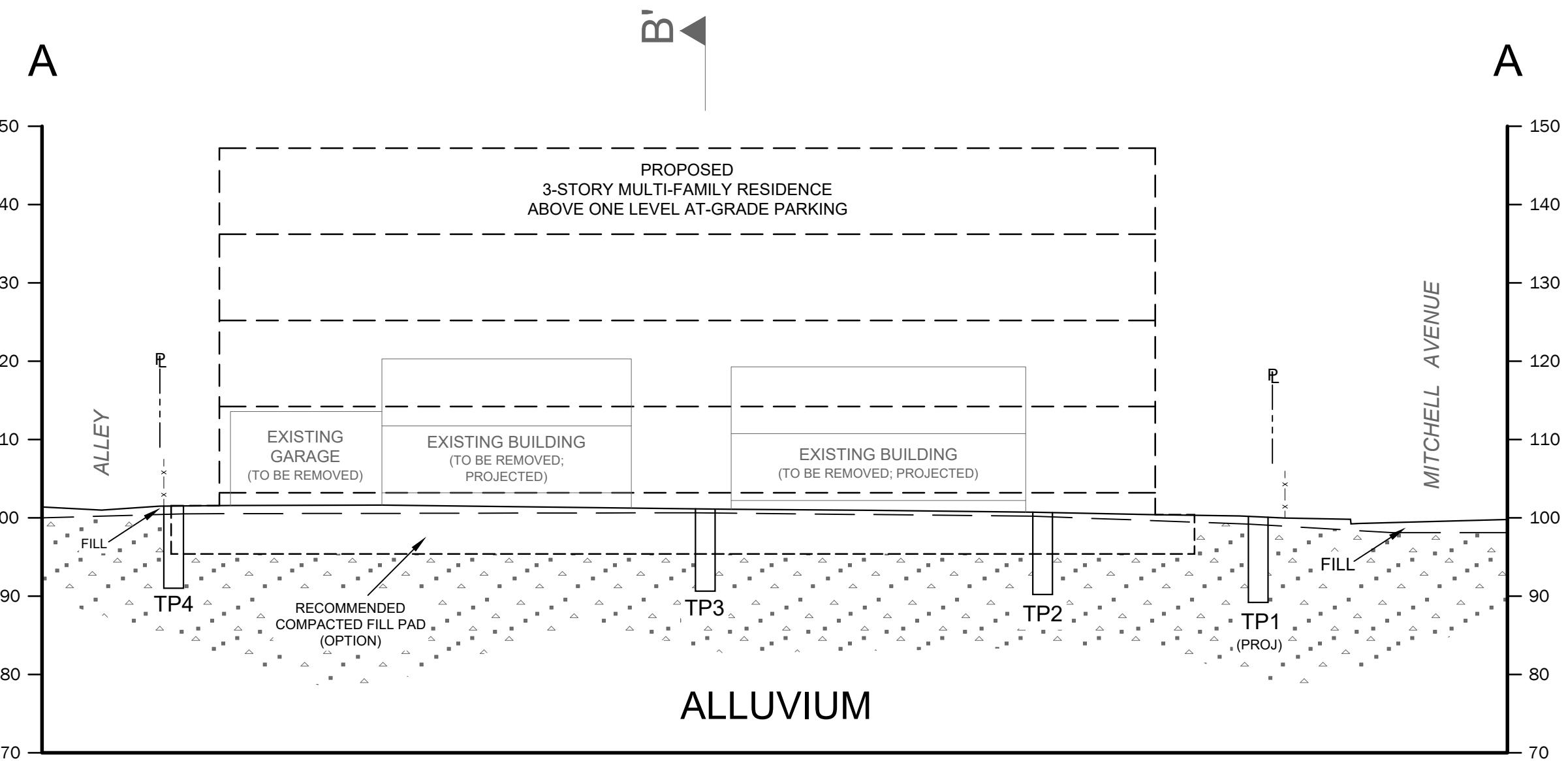
# SITE PLAN

PROJECT: IC20168 BREAKFORM

CONSULTANT: KJ

SCALE: 1" = 16'





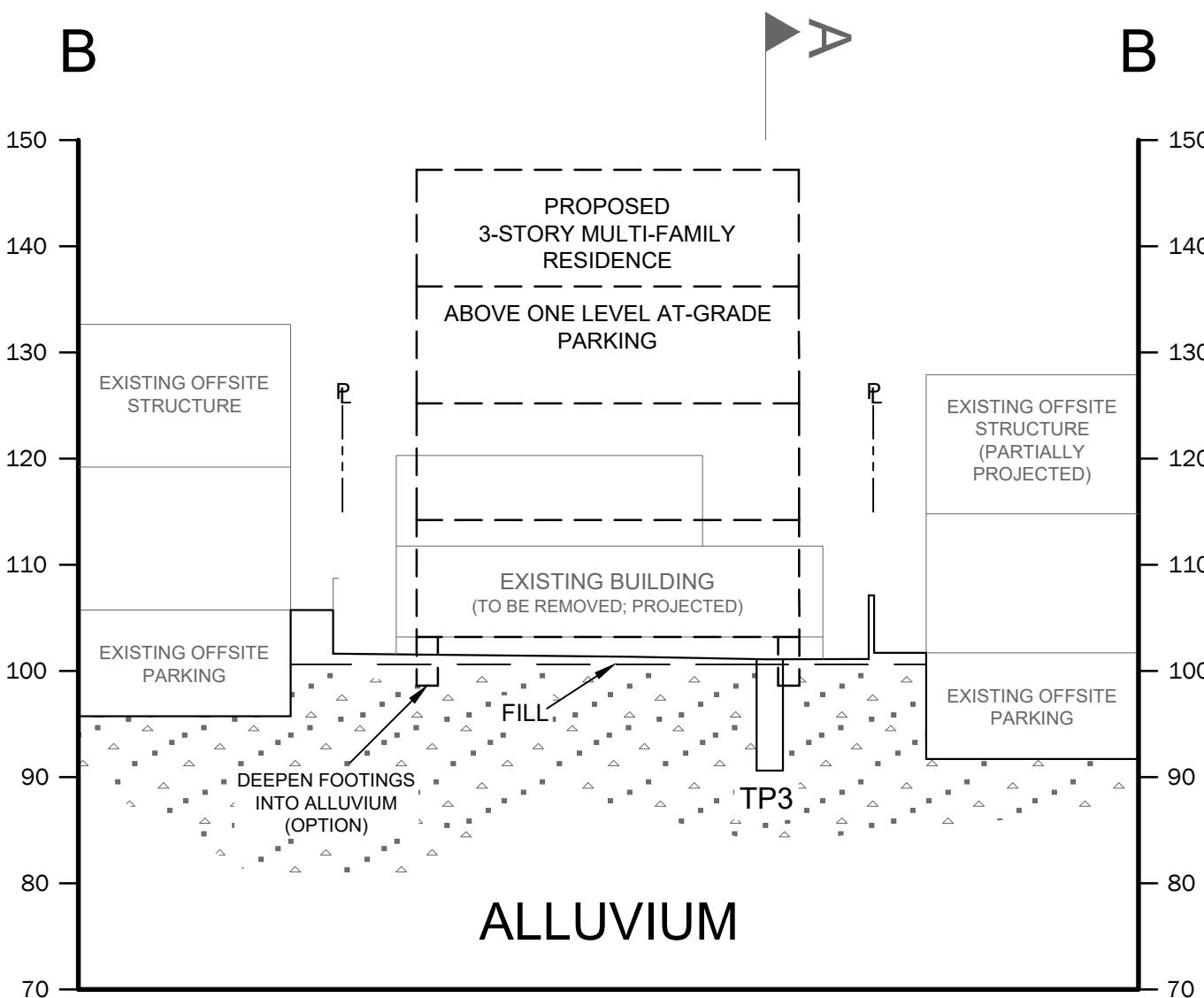


## SECTION B - B

PROJECT: IC20168 BREAKFORM

CONSULTANT: KJ

SCALE: 1" = 16'



**EXHIBIT E**

**PUBLIC CORRESPONDENCE**



Connie Chauv &lt;connie.chauv@lacity.org&gt;

## Opposition to CPC-2021-10394

chair@marvista.org <chair@marvista.org>  
To: Connie.Chauv@lacity.org

Wed, Jun 8, 2022 at 7:03 PM

Dear Ms. Chauv,

Attached is a letter in opposition to CPC-2021-10394 at 12735-7 W. Mitchell Avenue.

Please, let me know if any additional information is needed.

Thank you! Have a great week!

Best,

Kathryn



Kathryn Wheeler  
Community Director  
MVCC Chair

Kathryn.Wheeler@MarVista.org

310-929-0512  
MarVista.org  
@MarVistaCC



PO Box 66871  
LA CA 90066



Certified  
August 13, 2002

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MVCC Letter - Opposition to 12735-7 Mitchell.pdf  
854K



## MarVista.org

P.O. Box 66871  
Mar Vista, CA 90066  
424-256-3633  
[info@marvista.org](mailto:info@marvista.org)

### Officers 2021-2022

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Kathryn Wheeler  
[Kathryn.Wheeler@MarVista.org](mailto:Kathryn.Wheeler@MarVista.org)

#### 1<sup>st</sup> Vice Chair

Andrew Marton  
[Andrew.Marton@MarVista.org](mailto:Andrew.Marton@MarVista.org)

#### 2<sup>nd</sup> Vice Chair

Jordan Paul  
[Jordan.Paul@MarVista.org](mailto:Jordan.Paul@MarVista.org)

#### Secretary

Charlene Samiley  
[Charlene.Samiley@MarVista.org](mailto:Charlene.Samiley@MarVista.org)

#### Treasurer

Drew Ruesch  
[Drew.Ruesch@MarVista.org](mailto:Drew.Ruesch@MarVista.org)

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#### Zone 2

Martin Rubin

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Holly Tilson

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Kathy Rodriguez  
Bitta Jansma Sharma  
Mary Beth Blakey

### Community Director

Kathryn Wheeler



Certified Neighborhood Council  
August 13, 2002

## VIA EMAIL

**TO:** Connie Chauv Department of City Planning  
City of Los Angeles, [Connie.Chauv@LACity.org](mailto:Connie.Chauv@LACity.org)

**CC:** The Honorable Mike Bonin, Councilmember, 11th District  
City of Los Angeles, [Mike.Bonin@LACity.org](mailto:Mike.Bonin@LACity.org)

Len Nguyen, Senior Planning Deputy CD11  
City of Los Angeles, [Len.Nguyen@LACity.org](mailto:Len.Nguyen@LACity.org)

Jesi Harris, Brian Silveira & Associates  
Venice, CA, [HarrisLandUse@gmail.com](mailto:HarrisLandUse@gmail.com)

### RE: **OPPOSITION TO CPC-2021-10394-CU-DB-HCA-PHP**

Dear Ms. Chauv:

On December 20, 2021, 12737 Mitchell Development, LLC, represented by Brian Silveira & Associates, submitted plans subject to administrative approval to the Los Angeles Department of City Planning to affect the demolition of a single-family home and a triplex to build a six-story, 15-unit apartment building with a height increase of 22.5 feet, front/back setback reductions, FAR increase 33%, 15 parking spaces instead of 21 required, with 12 in tandem, at 12735 and 12737 W. Mitchell Ave. in Zone 7 of the Mar Vista Community Council.

At the regularly scheduled meeting held February 16, 2022, the Mar Vista Community Council again discussed the project and took action to oppose CPC-2021-10394 with the following policy motion:

The MVCC is Opposed to Department of City Planning file CPC-2021-10394-CU-DB-HCA-PHP for a new development at 12735 Mitchell.

Our action is based on the outreach to and input from the Mar Vista Community.

Thank you for your thoughtful consideration of the matter,

Kathryn Wheeler, Chair  
Mar Vista Community Council  
[Chair@MarVista.org](mailto:Chair@MarVista.org)



Dylan Sittig <dylan.sittig@lacity.org>

---

## Opposition to Construction on 12725-7 Mitchell Ave.

1 message

---

**Taylor Walsh** <taylor.s.tracy@gmail.com>  
To: dylan.sittig@lacity.org

Tue, Feb 15, 2022 at 7:21 PM

Hi Dylan,

My name is Taylor Walsh, I'm a resident of Zone 7, and I live adjacent to the proposed development of the buildings involving 12725 through 12727 Mitchell Avenue. I'm opposed to this demolition and construction because of the high-rise structural plan, the ultimate increase in local traffic, and the creation of an additional building site on a street already saturated with construction.

As a neighbor of the involved buildings, I value the quiet streets and low rising residential structures surrounding this area. As the public plans state, this new building would be over 22.5 feet higher than the current height limit. At this moment, there are two construction site within only 50 feet of the proposed project. If approved, the developmental process will create a noticeable rise in noise, debris, and traffic.

I urge you to consider these concerns going into the Board of Directors meeting tomorrow evening.

Thank you,  
Taylor Walsh  
Resident of Zone 7



Connie Chauv <connie.chauv@lacity.org>

---

## case number CPC-2021-10394-CU-DB-HCA-PHP

---

Daren Black <daren@practicalayurveda.com>  
To: "connie.chauv@lacity.org" <connie.chauv@lacity.org>

Fri, Aug 12, 2022 at 10:38 AM

Dear Connie:

This is regarding the proposed development for [12737 Mitchell Ave., 90066](#).

There are several points which are crucial to note that make the plans for 12737 Mitchell seem poorly considered for everyone living in the surrounding neighborhoods:

1. Parking in the surrounding neighborhoods is very competitive, even during work hours.
2. Parking at night is already difficult when residents sometimes have to drive around for up to ten minutes and search for parking.
3. Parking at night sometimes already requires parking 2 or more blocks away from one's own residence.
4. Three new buildings are halfway through construction within one block of 12737 Mitchell. These 3 buildings will add serious parking stress to the neighborhood.
5. 2 other buildings are being demolished right now for additional new rental properties. Making a total of at least 5 new buildings within a block!
6. There is not enough street parking available for residents and guests of the current buildings, especially not including the nearly completed buildings.
7. Most units, even most one bedroom units require 2 parking spaces to accommodate roommates and families with 2 vehicles. This is the reality given the high rents and people needing roommates to split rent plus likelihood of couples.
8. Any building that plans to provide only one parking space per unit is an example of bad planning, poor taste and arrogance in the face of reality.
9. As an arrogant slap in the face of all residents and all city employees, 12737 Mitchell plans 15 units with only 14 parking spaces. There not only are not enough spaces for their own residents, this is planned overflow, congestion and chaos. 12737 Mitchell will put at least 15 more cars on the street when the building is filled with tenants. 15 cars will fill up all of one side of the street and part of the other.
10. Many of the new units say they will have some low income units. Really? Will there be a unit for less than \$1000 per month? Who will make sure this happens???
11. All these points add up to stupidity compounded by greed and arrogance. These types of developments need to be stopped. Every new building should have at least 2 parking spaces per unit, plus spaces for guest parking. There should be units available for under \$1000 per month and under \$2000 per month.

Thank you for helping to make Los Angeles a better city,

Daren Black

[12760 Caswell](#)

Los Angeles, CA 90066

310 430 2209



Connie Chauv <connie.chauv@lacity.org>

---

## Re: Bad project being rushed through. case number CPC-2021-10394-CU-DB-HCA-PHP

2 messages

**Housing Element** <housingelement@lacity.org>

To: Daren Black <daren@practicalayurveda.com>, Connie Chauv <connie.chauv@lacity.org>

Wed, Aug 17, 2022 at 8:56 AM

+Connie Chauv

Hi Daren, I've added Connie Chauv the project planner for this case to the thread. [Density Bonus projects](#) such as [this](#), are eligible for density increases, parking reductions and other incentives in exchange for on site affordable units. This is a requirement of state law. According to the project description for the case, the applicant is proposing 33% of the units be affordable to very low income households. Depending on the number of bedrooms proposed, rents could range between \$768-\$987 (I'm not familiar with the unit types in the project but that range assumes studios to 2 bedrooms), rents and income limits are shown [here](#). Affordable units are enforced annually through recorded land covenants maintained by the Los Angeles Housing Department (LAHD).

If you have specific questions about the project Connie may be able to provide more information.

**Housing Element Staff**  
**Los Angeles City Planning**



200 N. Spring St., Room 750  
Los Angeles, CA. 90012  
[Planning4LA.org](#)  
T: (213) 978-1302



For more information on the Plan to HouseLA visit our [website](#), and/or join our [listserv](#) (if you have not already), in order to stay abreast of any Plan updates.

On Mon, Aug 15, 2022 at 10:41 AM Daren Black <[daren@practicalayurveda.com](mailto:daren@practicalayurveda.com)> wrote:

Hello:

This is regarding the proposed development for [12737 Mitchell Ave., 90066](#). Hoping you can block or alter said plans.

There are several points which are crucial to note that make the plans for 12737 Mitchell seem very poorly considered for everyone living in the surrounding neighborhoods:

1. Parking in the surrounding neighborhoods is very competitive, even during work hours.
2. Parking at night is already difficult when residents sometimes have to drive around for up to ten minutes and search for parking.
3. Parking at night sometimes already requires parking 2 or more blocks away from one's own residence.
4. Three new buildings are halfway through construction within one block of 12737 Mitchell. These 3 buildings will add serious parking stress to the neighborhood.
5. 3 other buildings are being demolished right now for additional new rental properties. Making a total of at least 5 new buildings within a block!

6. There is not enough street parking available for residents and guests of the current buildings, especially not including the nearly completed buildings.
7. Most units, even most one bedroom units require 2 parking spaces to accommodate roommates and families with 2 vehicles. This is the reality given the high rents and people needing roommates to split rent plus likelihood of couples.
8. Any building that plans to provide only one parking space per unit is an example of bad planning, poor taste and arrogance in the face of reality.
9. As an arrogant slap in the face of all residents and all city employees, 12737 Mitchell plans 15 units with only 14 parking spaces. There not only are not enough spaces for their own residents, this is planned overflow, congestion and chaos. 12737 Mitchell will put at least 15 more cars on the street when the building is filled with tenants. 15 cars will fill up all of one side of the street and part of the other.
10. Many of the new units say they will have some low income units. Really? Will there be units for less than \$1000 per month? Who will make sure this happens???
11. All these points add up to stupidity compounded by greed and arrogance. These types of developments need to be stopped. Every new building should have at least 2 parking spaces per unit, plus spaces for guest parking. There should be units available for under \$1000 per month and under \$1500 per month.

Thank you for helping to make Los Angeles a better city,

Sincerely,  
Daren Black  
12760 Caswell  
Los Angeles, CA 90066

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**Daren Black** <daren@practicalayurveda.com>

Wed, Aug 17, 2022 at 5:25 PM

To: Housing Element <HousingElement@lacity.org>, Connie Chauv <connie.chauv@lacity.org>

Thank you for responding.

33% of 15 units is 5 units

You are telling me that they are telling you they will have 5 units affordable to very low income but that is not what the project description flyer said. Which looks suspicious.

The parking issue is a very serious concern and you need to include that in the formula. Otherwise we are just getting screwed by developers who will live elsewhere while they save on building costs by shortchanging parking spaces. This has dramatic effects on a lot of people.

Daren Black  
Los Angeles, CA 90066

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**From:** [blair.smith@lacity.org](mailto:blair.smith@lacity.org) <blair.smith@lacity.org> on behalf of Housing Element  
<housingelement@lacity.org>

**Sent:** Wednesday, August 17, 2022 8:56 AM

**To:** Daren Black <[daren@practicalayurveda.com](mailto:daren@practicalayurveda.com)>; Connie Chauv <[connie.chauv@lacity.org](mailto:connie.chauv@lacity.org)>

**Subject:** Re: Bad project being rushed through. case number CPC-2021-10394-CU-DB-HCA-PHP

[Quoted text hidden]



Connie Chauv <connie.chauv@lacity.org>

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## CPC-2021-10394-CU-DB-HCA-PHP

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**Lynne Ledford** <lynnecp243@aol.com>  
Reply-To: Lynne Ledford <lynnecp243@aol.com>  
To: "Connie.Chauv@lacity.org" <Connie.Chauv@lacity.org>

Wed, Aug 17, 2022 at 7:45 AM

Good Morning,

Unable to attend meeting due to work commitments.

Questions relating to the proposed project:

[12737 Mitchell Ave., Los Angeles CA 90066](#)  
Case Number: CPC-2021-10394-CU-DB-HCA-PHP

Waivers - How were these waivers approved?  
Once the waivers are approved is there any option for rebuttal?  
I see that Mike Bonin is listed as council district. Were they discussed  
and approved by him? Not sure how this process works.

Height restriction waiver - adding density - at what cost to current  
residents in this community? Impacting quality of life of others.  
Who decides the low rent applicants?

Parking - Compact Parking?  
Extremely congested area now - adding more people and  
hoping they won't have cars for transportation does not seem  
logical - definitely not possible in the near future. So many projects  
are currently going on in this immediate area.

Trees - 2 Fig Trees at least 1 lemon tree

Overall Concerns - Too tall and big overall with not enough parking.  
This neighborhood community has been decimated over the last few  
years. It seems hopeless but have to ask the questions.

Understanding the need for progress but also the need to keep  
our communities healthy - otherwise - what's the point?

I appreciate the opportunity for the open forum.

Best regards,

Lynne Ledford  
[12731 Mitchell Ave.](#)  
Los Angeles, CA 90066



Connie Chauv <connie.chauv@lacity.org>

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## Opposition to 12735 W Mitchell Ave

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**Taylor Walsh** <taylor.s.tracy@gmail.com>  
To: connie.chauv@lacity.org

Wed, Aug 17, 2022 at 8:44 AM

Hello,

I'm writing to voice my concerns and opposition to today's Hearing Officer hearing regarding the potential new development at 12735 W Mitchell Ave. I'm unable to join the hearing today at 10am. Please forward this to the respective parties.

As a neighbor of the proposed building, I strongly oppose new development. Currently, there are 4 new construction sites on the same block with 2 more in the works (the new triangle business park on Zanja/Washington and the demolition of a single-family home on Caswell Ave).

It is unsightly for the applicant to be planning a building that is over 20 ft taller than what is permitted in the zone. I'm concerned about the lack of parking due to the applicant's request to minimize building parking from 21 spaces to merely 14.

Most importantly, I'm concerned about the safety of the neighborhood during the time before demolition. I've witnessed a spike in homelessness, hazardous waste, trash, and crime around the current construction sites on the block. I have written and called law enforcement about trespassers and erratic homeless dwelling in the abandoned structures on the block. There is absolutely no need to contribute to this issue by starting demolition and development on a perfectly fit building.

Attached is the current situation which produces constant noise and shaking from the surrounding area. The proposed development at 12735 W Mitchell is three buildings to the left of the photo. I strongly urge the Hearing Officer to consider these concerns and oppose the new development. Thank you.

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Taylor Walsh



**Construction on W Mitchell Ave.jpeg**  
3528K



Connie Chauv <connie.chauv@lacity.org>

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## Development at 12737 Mitchell

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Jeannie Schwulst <jeanniemkc@gmail.com>  
To: Connie.Chauv@lacity.org

Sat, Aug 20, 2022 at 1:42 PM

Ms Chauv,

I had listened to the Zoom meeting on Wednesday August 17th regarding the proposal to build a six story building at 12735-37 Mitchell Ave. I had commented at the time via phone. I live in the area of this property. On the call I had said that the amount of parking in this area is already a problem. To put up a building that does not provide adequate parking for the residents I believe is going to highly increase the parking issues. To think that not providing parking will encourage people to use public transportation is a mistake. Also charging a separate rental fee may cause people to decide to park on the street. There are three buildings on the corner of Zanja St. and Mitchell Ave that are in the process of being built, plus one on the corner of Frances Ave. and Mitchell. Once they are completed that will bring additional density and cars to the area. Where are guests to park? When I have more than one guest they may have to park as far as two or three blocks away. I have lived in this area for over 30 years and the number of buildings that have gone up in the past five years is disturbing. Then there is the idea of the height. If this 6 story building goes up on a residential street, with nothing else near that height, will cause obstructed views and in cases of buildings on either side, blocked sunlight. The building on the corner of Wade and Matteson has already totally blocked my view of snow capped mountains to the east. This building would be obstructing views to the south. So as you may have guessed I am totally against the construction of this building. As a representative of our HOA I do not feel it should be approved as presented.

--  
Jeannie Schwulst President HOA  
12720 Matteson Ave. #3  
310-993-7824



Connie Chauv <connie.chauv@lacity.org>

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## Comments about the Project at 12735-12737 Mitchell Ave, LA 90066

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**George Noutsios** <gnoutsios@yahoo.com>  
To: Connie Chauv <connie.chauv@lacity.org>  
Cc: "gnoutsios@yahoo.com" <gnoutsios@yahoo.com>

Tue, Aug 23, 2022 at 12:51 PM

Dear Connie,

Thank you for leading the hearing for the LA Department of City Planning. My name is George Noutsios and I am the owner of the 9 Unit apartment building property at [12731 Mitchell Ave, LA 90066](#), located adjacent to the proposed development.

The comments and observations stated here were mostly verbally presented last Wednesday August 17, 2022 during your meeting with the community. This message is an attempt to put them in writing for your consideration.

1- This 15 Unit 6 story development is completely out of place with the rest of the properties in the area. All of the existing buildings in the neighborhood are no more than three story high, even buildings erected very recently and on a larger lots are no more than four story high.

2- I am opposed to all the waivers requested by this developer. Using the justification of two-three low income apartments these developers want to violate all the building department requirements for setbacks and parking for this property. My residents also have certain rights to live in a peaceful and pleasant environment as the new residents of this new building want. Furthermore, I want to keep the privacy of my residents and the privacy of the new residents and proper setbacks somehow provide this essential requirement for close quarter living.

Also light and air breeze is an essential need for someone who lives in close quarters, setbacks supposed to do that but this developer wants to ignore this because he wants to cram as many units as he can in this small lot. I hope the City thinks differently and do what is right for everyone..

3- The 12 tandem parking spaces provided are a major issue for this neighborhood. You cannot allocate tandem parking spaces to two different residents living in different units. The representative of the developer mentioned a parking fee of around \$300/month for each space, how can a low income person can afford this fee? More likely even the regular tenants will be parking in the already crowded public streets.

4- Hypothetical advocacy for public transportation and bike use is not working in real life. All of my young and lean tenants have bikes but they seldom use them and only for leisurely drive every few weeks. No one goes shopping with the bike. During regular working days I always check how often the bike lanes on Venice Blvd are being utilized and I only see one or two bikers every three to four minutes.

5- Wanted to know the location of the HVAC machinery for all of the units in this next door development and are they planned to go to the roof and what kind of noise abatement it is planned for the peace and tranquility of my residents.

6- Since I have not been provided the plans for this development I do not know how many trash cans and recyclable containers they plan to have and where they plan to locate them. At this time I have problems with people from the neighborhood using my recyclable containers.

7- Worrying about compacting from this development and how all this structure when completed will impact my foundation and the subterranean parking of my building.

Sincerely

George Noutsios



Connie Chauv <connie.chauv@lacity.org>

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## proposal of construction Case #CPC-2021-103-CU-DB-HCA-PHP

Jeannie Schwulst <jeanniemkc@gmail.com>  
To: Connie.Chauv@lacity.org

Tue, Sep 27, 2022 at 3:57 PM

To Whom It May Concern:

I am writing this letter regarding the proposal of construction Case #CPC-2021-103-CU-DB-HCA-PHP. I want to express my disapproval strongly of the construction of a new six story building at 12735-12737 Mitchell Ave. This building will be taller by over 22 ft. of anything else bordered by Beethoven to the west, Frances to the east, Washington Blvd. to the south and Venice Blvd. to the north. This section of Mar Vista is mostly two story multi-family apartments and condos. Outside of this square is probably 90% single family houses. The density is already more than street parking can handle for residents. This proposed structure does not even have enough planned parking for the residents let alone any guests. To expect this to encourage people to use public transportation, bike, uber or use scooters is a mistake. Now let's add the fact that there are 5 if not 6 (I can't tell if one is to be two separate or one large complex) currently under construction within one block of this proposed complex and one on Caswell due for demolition. I have lived in my building at [12720 Matteson Ave.](#) for 34 years. When I entertain, I have to ask my guests to carpool, Uber, take a cab or be prepared to walk blocks to find parking. My building was built in 1980 with only one guest spot. That was a mistake at the time and it can lead to arguments among the owners as to who gets to use it. I don't think ANYTHING should be built without adequate parking for guests. To build one that does not even offer enough parking for the possible number of residents is a huge mistake. People currently park in the alleys, impinge on driveways and even have parked on or blocking sidewalks.

I highly disapprove for these reasons!!

--  
Jeannie Schwulst  
President HOA [12720 Matteson Ave.](#)  
310-993-7824