

# City of Los Angeles

Department of City Planning • Environmental Review Section  
City Hall • 200 N. Spring Street, Room 763 • Los Angeles, CA 90012



---

## INITIAL STUDY MITIGATED NEGATIVE DECLARATION Northridge Community Plan Area

---

### North Reseda Boulevard Project CPC-2015-3383-ZC-CU-ZAD-SPR ENV-2015-3384-MND VTT-73641

**Project Location:** 8350-8430 N. Reseda Boulevard and 8421-8429 N. Canby Avenue, Los Angeles, CA 91324  
**Council District:** 12 – Mitchell Englander

**Project Description:** The Proposed Project includes the demolition of a car resale center, auto body shop, a commercial office building (27,411 total square feet) and associated surface parking lots for the construction, use and maintenance of a unified development with two mixed-use buildings with 79 apartment units and approximately 10,300 square feet of ground-floor commercial space and a small lot subdivision with 61 small lot homes. The Proposed Project's mixed-use buildings would include 4-stories, approximately 45 feet above grade (3 levels of multi-family residential units over ground-floor retail and parking). The 61 small lots would each include 3-stories with a maximum of 37 feet above grade. Approximately 175 parking spaces would be provided at grade level and in one subterranean level below the mixed-use buildings. Each small lot would provide a two-car parking garage (122 total parking spaces). Vehicular access to the Project Site would be provided via Reseda Boulevard and Canby Avenue. On-site vehicle parking spaces, bicycle parking spaces, private and common open space areas, and trees would be provided to meet the requirements of the LAMC. In total, the Proposed Project would include approximately 202,215 square feet of developed floor area (191,915 square feet of residential space and 10,300 square feet of commercial space) resulting in a floor area ratio (FAR) of 1.43 to 1 averaged across the site.

The Applicant is requesting the following discretionary approvals: (1) Site Plan Review; (2) Zone Change from C2-1VL and P-1VL to C2-1VL; (3) Conditional Use Permit to average density and floor area of unified development; (4) Vesting Tentative Tract Map for 61-units small-lot subdivision and (5) Zoning Administrator's Determination for Transitional Height. The Applicant would also request approvals and permits from the Department of Building and Safety (and other municipal agencies) for project construction activities which may include, but are not limited to, the following: excavation, shoring, grading, foundation, haul route (for the export of approximately 2,000 cubic yards (cy) of asphalt debris and 22,000 cy of soil), removal and replacement of street trees, and building and tenant improvements for the Project Site.

**APPLICANT:**  
Harridge Cadillac Group, LLC

**PREPARED BY:**  
Parker Environmental Consultants

**ON BEHALF OF:**  
The City of Los Angeles  
Department of City Planning  
Environmental Review Section

**August 11, 2016**

---

## TABLE OF CONTENTS

---

Mitigated Negative Declaration Form/CEQA Initial Study Checklist (front insert)

<b>I.</b>	<b>INTRODUCTION</b> .....	I-1
<b>II.</b>	<b>PROJECT DESCRIPTION</b> .....	II-1
	A. PROJECT LOCATION.....	II-1
	B. PROJECT CHARACTERISTICS .....	II-11
	C. ENTITLEMENT REQUESTS.....	II-31
<b>III.</b>	<b>ENVIRONMENTAL IMPACT ANALYSIS</b> .....	III-1
	I. AESTHETICS .....	III-1
	II. AGRICULTURE AND FORESTRY RESOURCES .....	III-6
	III. AIR QUALITY.....	III-8
	IV. BIOLOGICAL RESOURCES .....	III-18
	V. CULTURAL RESOURCES .....	III-22
	VI. GEOLOGY AND SOILS.....	III-28
	VII. GREENHOUSE GAS EMISSIONS.....	III-33
	VIII. HAZARDS AND HAZARDOUS MATERIALS .....	III-41
	IX. HYDROLOGY AND WATER QUALITY .....	III-50
	X. LAND USE AND PLANNING.....	III-57
	XI. MINERAL RESOURCES.....	III-71
	XII. NOISE.....	III-72
	XIII. POPULATION AND HOUSING.....	III-89
	XIV. PUBLIC SERVICES.....	III-95
	XV. RECREATION .....	III-106
	XVI. TRANSPORTATION AND TRAFFIC .....	III-107
	XVII. UTILITIES AND SERVICE SYSTEMS.....	III-128
	XVIII. MANDATORY FINDINGS OF SIGNIFICANCE.....	III-142
<b>IV.</b>	<b>PREPARERS OF THE INITIAL STUDY AND PERSONS CONSULTED</b> .....	IV-1
<b>V.</b>	<b>REFERENCES, ACRONYMS AND ABBREVIATIONS</b> .....	V-1

---

**List of Figures**

Figure II-1: Project Location Map .....	II-2
Figure II-2: Zoning and General Plan Designations .....	II-5
Figure II-3: Aerial Photograph of the Project Site.....	II-7
Figure II-4: Photographs of the Project Site, Views 1-6 .....	II-8
Figure II-5: Photographs of the Surrounding Land Uses, Views 7-12 .....	II-9
Figure II-6: Site Plan .....	II-14
Figure II-7: Typical Multi-Family Residential Floor Plan .....	II-15
Figure II-8: North Mixed-Use Building Elevations.....	II-16
Figure II-9: South Mixed-Use Building Elevations.....	II-17
Figure II-10: Small Lot Home Elevation – Unit A.....	II-18
Figure II-11: Small Lot Home Elevation – Unit B .....	II-19
Figure II-12: Small Lot Home Elevation – Unit C .....	II-20
Figure II-13: Typical Small Lot Home and Mixed-Use Building Section .....	II-21
Figure II-14: Illustrative Renderings .....	II-22
Figure II-15: Location of Related Projects .....	II-30
Figure III-1: Noise Monitoring and Sensitive Receptor Location Map .....	III-76
Figure III-2: Existing (2016) Traffic Volumes – AM Peak Hour .....	III-110
Figure III-3: Existing (2016) Traffic Volumes – PM Peak Hour .....	III-111
Figure III-4: Existing (2016) with Project Traffic Volumes – AM Peak Hour .....	III-115
Figure III-5: Existing (2016) with Project Traffic Volumes – PM Peak Hour.....	III-116
Figure III-6: Future (2019) Without Project Traffic Volumes – AM Peak Hour.....	III-120
Figure III-7: Future (2019) Without Project Traffic Volumes – PM Peak Hour.....	III-121
Figure III-8: Future (2019) with Project Traffic Volumes – AM Peak Hour.....	III-122
Figure III-9: Future (2019) with Project Traffic Volumes – PM Peak Hour.....	III-123

---

**List of Tables**

Table II-1: Summary of Project Site.....	II-1
Table II-2: Proposed Development Program.....	II-11
Table II-3: Summary of Required and Proposed Open Space Areas .....	II-23
Table II-4: Summary of Required and Proposed Vehicle Parking Spaces.....	II-24
Table II-5: Summary of Required and Proposed Bicycle Parking Spaces .....	II-25
Table II-6: Related Project List .....	II-29
Table III-1: Estimated Peak Daily Construction Emissions.....	III-11
Table III-2: Existing Daily Operational Emissions from the Project Site.....	III-12
Table III-3: Proposed Project Estimated Daily Operational Emissions .....	III-13
Table III-4: Localized On-Site Peak Daily Construction Emissions .....	III-15
Table III-5: Climate Change Scoping Plan 2020 Emissions Target.....	III-34
Table III-6: Project Construction-Related Greenhouse Gas Emissions .....	III-37
Table III-7: Existing Project Site Greenhouse Gas Emissions .....	III-37
Table III-8: Project Operational Greenhouse Gas Emissions.....	III-39
Table III-9: Project Consistency with Applicable Objectives and Policies of the Northridge Community Plan Land Use Element for Residential and Commercial Land Uses .....	III-59
Table III-10: Existing Ambient Daytime Noise Levels in Project Site Vicinity.....	III-75
Table III-11: Typical Outdoor Construction Noise Levels.....	III-78
Table III-12: Vibration Damage Potential Threshold Criteria .....	III-82
Table III-13: Vibration Source Levels for Construction Equipment.....	III-83
Table III-14: Community Noise Exposure (CNEL) .....	III-85
Table III-15: Project Noise Impacts at Study Intersections.....	III-87
Table III-16: Cumulative Noise Impacts at Study Intersections .....	III-89
Table III-17: SCAG Population and Housing Projections for the City of Los Angeles and the SCAG Region .....	III-92
Table III-18: Estimated Cumulative Housing Units .....	III-94
Table III-19: Devonshire Area Police Station Crime Statistics .....	III-98
Table III-20: Resident Schools Serving the Project Site .....	III-100
Table III-21: Proposed Project Estimated Student Generation.....	III-101

Table III-22: Estimated Cumulative Student Generation .....	III-102
Table III-23: Recreation and Park Facilities Within the Project Area.....	III-104
Table III-24: Definition of Significant Impact at Intersection.....	III-108
Table III-25: Existing (2016) Condition – Intersection Level of Service .....	III-109
Table III-26: Project Trip Generation Estimates .....	III-113
Table III-27: Existing (2016) Plus Project Condition Level of Service Summary.....	III-114
Table III-28: Future (2019) Condition Level of Service Summary.....	III-118
Table III-29: Proposed Project Estimated Water Demand .....	III-130
Table III-30: Proposed Project Estimated Wastewater Generation.....	III-132
Table III-31: Estimated Cumulative Water Demand.....	III-135
Table III-32: Projected Cumulative Wastewater Generation .....	III-136
Table III-33: Estimated Construction and Demolition Debris .....	III-138
Table III-34: Estimated Operational Solid Waste Generation.....	III-139
Table III-35: Cumulative Operational Solid Waste Generation.....	III-141

**APPENDICES**

APPENDIX A: AIR QUALITY WORKSHEETS

APPENDIX B: HISTORIC RESOURCES REPORT

GPA Consulting, Neill Lehr Cadillac Showroom & Service Buildings, 8400-8430 N. Reseda Boulevard, 8423-8429 N. Canby Avenue, Los Angeles, California, February 9, 2016.

APPENDIX C: GEOTECHNICAL REPORT

Feffer Geological Consulting, Geotechnical Investigation, Proposed New Three-Story On Grade Residences And Three Story Commercial Building with One Subterranean Level, 8350-8430 N. Reseda Boulevard, Northridge, CA 91324, November 18, 2014.

APPENDIX D: GREENHOUSE GAS EMISSIONS WORKSHEETS

APPENDIX E: ENVIRONMENTAL SITE ASSESSMENT

California Environmental Geologists & Engineers, Inc., Environmental Site Assessment – Phase I and Phase II Screening Subsurface Soil Gas Survey, Commercial Property, 8350-8354, 8400, & 8420 Reseda Blvd, APN 2786-021-017 & -18; 2876-022-021, Northridge, CA 91324, November 2014.

APPENDIX F: NOISE MONITORING DATA AND CALCULATION WORKSHEETS

APPENDIX G: TRAFFIC STUDY

Overland Traffic Consultants, Inc., Traffic Impact Study, Mixed-Use Development Located at 8350-8430 Reseda Boulevard in the City of Los Angeles, April 2016.

APPENDIX H: TREE REPORT

Paul A. Lewis, Landscape Architect, Protected Tree Report [PTR] for 8350-8430 Reseda Boulevard, June 23, 2015.

[This page intentionally left blank]

**CITY OF LOS ANGELES**

OFFICE OF THE CITY CLERK  
 ROOM 395, CITY HALL  
 LOS ANGELES, CALIFORNIA 90012

**CALIFORNIA ENVIRONMENTAL QUALITY ACT  
 PROPOSED MITIGATED NEGATIVE DECLARATION**

**LEAD CITY AGENCY:** City of Los Angeles**COUNCIL DISTRICT:** 12 – Mitchell Englander**PROJECT TITLE:**  
North Reseda Boulevard**ENVIRONMENTAL CASE:**  
ENV-2015-3384-MND**CASE NO.**  
CPC-2015-3383-ZC-CU-ZAD-SPR; VTT-73641**PROJECT LOCATION:** 8350-8430 N. Reseda Boulevard and 8421-8429 N. Canby Avenue, Los Angeles, CA 91324

**PROJECT DESCRIPTION:** The Proposed Project includes the demolition of a car resale center, auto body shop, a commercial office building (27,411 total square feet) and associated surface parking lots for the construction, use and maintenance of a unified development with two mixed-use buildings with 79 apartment units and approximately 10,300 square feet of ground-floor commercial space and a small lot subdivision with 61 small lot homes. The Proposed Project's mixed-use buildings would include 4-stories, approximately 45 feet above grade (3 levels of multi-family residential units over ground-floor retail and parking). The 61 small lots would each include 3-stories with a maximum of 37 feet above grade. Approximately 175 parking spaces would be provided at grade level and in one subterranean level below the mixed-use buildings. Each small lot would provide a two-car parking garage (122 total parking spaces). Vehicular access to the Project Site would be provided via Reseda Boulevard and Canby Avenue. On-site vehicle parking spaces, bicycle parking spaces, private and common open space areas, and trees would be provided to meet the requirements of the LAMC. In total, the Proposed Project would include approximately 202,215 square feet of developed floor area (191,915 square feet of residential space and 10,300 square feet of commercial space) resulting in a floor area ratio (FAR) of 1.43 to 1 averaged across the site.

The Applicant is requesting the following discretionary approvals: (1) Site Plan Review; (2) Zone Change from C2-1VL and P-1VL to C2-1VL; (3) Conditional Use Permit to average density and floor area of unified development; (4) Vesting Tentative Tract Map for 61-units small-lot subdivision and (5) Zoning Administrator's Determination for Transitional Height. The Applicant would also request approvals and permits from the Department of Building and Safety (and other municipal agencies) for project construction activities which may include, but are not limited to, the following: excavation, shoring, grading, foundation, haul route (for the export of approximately 2,000 cubic yards (cy) of asphalt debris and 22,000 cy of soil), removal and replacement of street trees, and building and tenant improvements for the Project Site.

**NAME AND ADDRESS OF APPLICANT IF OTHER THAN CITY AGENCY**

Harridge Cadillac Group, LLC  
 6363 Wilshire Boulevard, Suite 600  
 Los Angeles, CA 90048

**FINDING:** The Department of City Planning of the City of Los Angeles has proposed that a Mitigated Negative Declaration be adopted for this Project. The mitigation measures outlined on the attached pages will reduce any potentially significant adverse effects to a level of insignificance.

**SEE ATTACHED SHEET(S) FOR ANY MITIGATION MEASURES IMPOSED**

Any written comment received during the public review period are attached together with the response of the Lead City Agency. The project decision-maker may adopt the mitigated negative declaration, amend it, or require preparation of an EIR. Any changes made should be supported by substantial evidence in the record and appropriate findings made.

**THE INITIAL STUDY PREPARED FOR THIS PROJECT IS ATTACHED.****NAME OF PERSON PREPARING FORM**  
May Sirinopwongsagon**TITLE**  
City Planner**TELEPHONE NUMBER**  
213-978-1372**ADDRESS**  
200 North Spring Street, 7<sup>th</sup> Floor  
Los Angeles, CA 90012**SIGNATURE (Official)**

**DATE**  
SEPT. 7, 2016

**CITY OF LOS ANGELES**

OFFICE OF THE CITY CLERK, ROOM 395, CITY HALL  
LOS ANGELES, CALIFORNIA 90012

**CALIFORNIA ENVIRONMENTAL QUALITY ACT**

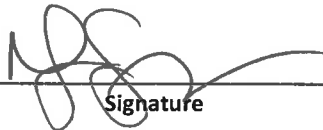
**INITIAL STUDY and CHECKLIST (CEQA Guidelines Section 15063)**

<b>LEAD CITY AGENCY:</b> City of Los Angeles		<b>COUNCIL DISTRICT:</b> 12	<b>DATE:</b> August 11, 2016
<b>RESPONSIBLE AGENCIES:</b> Department of City Planning			
<b>ENVIRONMENTAL CASE:</b> ENV-2015-3384-MND		<b>RELATED CASES:</b> CPC-2015-3383-ZC-CU-ZAD-SPR, VTT-73641	
<b>PREVIOUS ACTIONS CASE NO.</b>		<input type="checkbox"/> DOES have significant changes from previous actions. <input type="checkbox"/> DOES NOT have significant changes from previous actions.	
<p><b>PROJECT DESCRIPTION:</b> The Proposed Project includes the demolition of a car resale center, auto body shop, a commercial office building (27,411 total square feet) and associated surface parking lots for the construction, use and maintenance of a unified development with two mixed-use buildings with 79 apartment units and approximately 10,300 square feet of ground-floor commercial space and a small lot subdivision with 61 small lot homes. The Proposed Project's mixed-use buildings would include 4-stories, approximately 45 feet above grade (3 levels of multi-family residential units over ground-floor retail and parking). The 61 small lots would each include 3-stories with a maximum of 37 feet above grade. Approximately 175 parking spaces would be provided at grade level and in one subterranean level below the mixed-use buildings. Each small lot would provide a two-car parking garage (122 total parking spaces). Vehicular access to the Project Site would be provided via Reseda Boulevard and Canby Avenue. On-site vehicle parking spaces, bicycle parking spaces, private and common open space areas, and trees would be provided to meet the requirements of the LAMC. In total, the Proposed Project would include approximately 202,215 square feet of developed floor area (191,915 square feet of residential space and 10,300 square feet of commercial space) resulting in a floor area ratio (FAR) of 1.43 to 1 averaged across the site. The Applicant is requesting the following discretionary approvals: (1) Site Plan Review; (2) Zone Change from C2-1VL and P-1VL to C2-1VL; (3) Conditional Use Permit to average density and floor area of unified development; (4) Vesting Tentative Tract Map for 61-units small-lot subdivision and (5) Zoning Administrator's Determination for Transitional Height. The Applicant would also request approvals and permits from the Department of Building and Safety (and other municipal agencies) for project construction activities which may include, but are not limited to, the following: excavation, shoring, grading, foundation, haul route (for the export of approximately 2,000 cubic yards (cy) of asphalt debris and 22,000 cy of soil), removal and replacement of street trees, and building and tenant improvements for the Project Site.</p>			
<p><b>ENVIRONMENTAL SETTING:</b> The Project Site includes 143,552 square feet of lot area (3.29 acres) with 140,925 square feet of buildable lot area. The Project Site is currently occupied by one-story buildings (a car resale center, an auto body shop, and an optician office) with street-level parking areas. The surrounding properties are developed with office, commercial land uses, single-family residential, and surface parking lots. Further details are provided in the expanded IS/MND analysis (attached).</p>			
<b>PROJECT LOCATION:</b> 8350-8430 N. Reseda Boulevard and 8421-8429 N. Canby Avenue, Los Angeles, CA 91324			
<b>COMMUNITY PLAN AREA:</b> <input type="checkbox"/> Preliminary <input type="checkbox"/> Proposed <input checked="" type="checkbox"/> Adopted (1998)		<b>STATUS:</b> Northridge <input checked="" type="checkbox"/> Does Conform to Plan <input type="checkbox"/> Does NOT Conform to Plan	<b>AREA PLANNING COMMISSION:</b> North Valley <b>CERTIFIED NEIGHBORHOOD COUNCIL:</b> Northridge South
<b>EXISTING ZONING:</b> C2-1VL ; P-1VL		<b>MAX DENSITY ZONING:</b> 1.5:1	<b>LA River Adjacent:</b> No
<b>GENERAL PLAN LAND USE:</b> General Commercial		<b>MAX. DENSITY PLAN:</b> 1.5:1	<b>PROPOSED PROJECT DENSITY:</b> 1.43:1

**Determination (To be completed by Lead Agency)**

**On the basis of this initial evaluation:**

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions on the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find the proposed project MAY have a “potentially significant impact” or “potentially significant unless mitigated” impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



Signature

CITY PLANNER

Title

(213) 978-1372

Phone

**Evaluation of Environmental Impacts:**

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants based on a project-specific screening analysis).
2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.
4. “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of a mitigation measure has reduced an effect from “Potentially Significant Impact” to “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from “Earlier Analysis,” as described in (5) below, may be cross referenced).

5. Earlier analysis must be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR, or negative declaration. Section 15063 (c)(3)(D). In this case, a brief discussion should identify the following:
  - a. Earlier Analysis Used. Identify and state where they are available for review.
  - b. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
  - c. Mitigation Measures. For effects that are “Less Than Significant With Mitigation Measures Incorporated,” describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated
7. Supporting Information Sources: A sources list should be attached, and other sources used or individuals contacted should be cited in the discussion.
8. This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project’s environmental effects in whichever format is selected.
9. The explanation of each issue should identify:
  - a. The significance criteria or threshold, if any, used to evaluate each question; and
  - b. The mitigation measure identified, if any, to reduce the impact to less than significant.

**Environmental Factors Potentially Affected:**

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a “Potentially Significant Impact” as indicated by the checklist on the following pages.

<input checked="" type="checkbox"/> <b>AESTHETICS</b> <input type="checkbox"/> <b>AGRICULTURE AND FOREST RESOURCES</b> <input type="checkbox"/> <b>AIR QUALITY</b> <input checked="" type="checkbox"/> <b>BIOLOGICAL RESOURCES</b> <input type="checkbox"/> <b>CULTURAL RESOURCES</b> <input type="checkbox"/> <b>GEOLOGY AND SOILS</b>	<input type="checkbox"/> <b>GREENHOUSE GAS EMISSIONS</b> <input checked="" type="checkbox"/> <b>HAZARDS AND HAZARDOUS MATERIALS</b> <input type="checkbox"/> <b>HYDROLOGY AND WATER QUALITY</b> <input type="checkbox"/> <b>LAND USE AND PLANNING</b> <input type="checkbox"/> <b>MINERAL RESOURCES</b> <input checked="" type="checkbox"/> <b>NOISE</b>	<input type="checkbox"/> <b>POPULATION AND HOUSING</b> <input checked="" type="checkbox"/> <b>PUBLIC SERVICES</b> <input type="checkbox"/> <b>RECREATION</b> <input checked="" type="checkbox"/> <b>TRANSPORTATION AND TRAFFIC</b> <input type="checkbox"/> <b>UTILITIES</b> <input checked="" type="checkbox"/> <b>MANDATORY FINDINGS OF SIGNIFICANCE</b>
--	---	---

**INITIAL STUDY CHECKLIST** (To be completed by the Lead City Agency)

**PROponent NAME:** Harridge Cadillac Group, LLC      **PHONE NUMBER:** (323) 658-1511

**APPLICANTS ADDRESSES:**  
 Harridge Cadillac Group, LLC  
 6363 Wilshire Boulevard, Suite 600  
 Los Angeles, CA 90048

**AGENCY REQUIRING CHECKLIST:** City of Los Angeles      **DATE SUBMITTED:** August 11, 2016  
 Department of City Planning

**PROPOSAL NAME (If Applicable):** North Reseda Boulevard Project

		Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
<p><b>PLEASE NOTE THAT EACH AND EVERY RESPONSE IN THE CITY OF LOS ANGELES INITIAL STUDY AND CHECKLIST IS SUMMARIZED FROM AND BASED UPON THE ENVIRONMENTAL ANALYSIS CONTAINED IN ATTACHEMENT B, EXPLANATION OF CHECKLIST DETERMINATIONS. PLEASE REFER TO THE APPLICABLE RESPONSE IN ATTACHMENT B FOR A DETAILED DISCUSSION OF CHECKLIST DETERMINATIONS.</b></p>					
<b>I. AESTHETICS</b>					
a.	HAVE A SUBSTANTIAL ADVERSE EFFECT ON A SCENIC VISTA?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	SUBSTANTIALLY DAMAGE SCENIC RESOURCES, INCLUDING, BUT NOT LIMITED TO, TREES, ROCK OUTCROPPINGS, AND HISTORIC BUILDINGS, OR OTHER LOCALLY RECOGNIZED DESIRABLE AESTHETIC NATURAL FEATURE WITHIN A CITY-DESIGNATED SCENIC HIGHWAY?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c.	SUBSTANTIALLY DEGRADE THE EXISTING VISUAL CHARACTER OR QUALITY OF THE SITE AND ITS SURROUNDINGS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d.	CREATE A NEW SOURCE OF SUBSTANTIAL LIGHT OR GLARE WHICH WOULD ADVERSELY AFFECT DAY OR NIGHTTIME VIEWS IN THE AREA?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>II. AGRICULTURE AND FOREST RESOURCES</b>					
a.	CONVERT PRIME FARMLAND, UNIQUE FARMLAND, OR FARMLAND OF STATEWIDE IMPORTANCE, AS SHOWN ON THE MAPS PREPARED PURSUANT TO THE FARMLAND MAPPING AND MONITORING PROGRAM OF THE CALIFORNIA RESOURCES AGENCY, TO NON-AGRICULTURAL USE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	CONFLICT WITH EXISTING ZONING FOR AGRICULTURAL USE, OR A WILLIAMSON ACT CONTRACT?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	CONFLICT WITH EXISTING ZONING FOR, OR CAUSE REZONING OF, FOREST LAND (AS DEFINED IN PUBLIC RESOURCES CODE SECTION 1220(G)), TIMBERLAND (AS DEFINED BY PUBLIC RESOURCES CODE SECTION 4526), OR TIMBERLAND ZONED TIMBERLAND PRODUCTION (AS DEFINED BY GOVERNMENT CODE SECTION 51104(G))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	RESULT IN THE LOSS OF FOREST LAND OR CONVERSION OF FOREST LAND TO NON-FOREST USE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	INVOLVE OTHER CHANGES IN THE EXISTING ENVIRONMENT WHICH, DUE TO THEIR LOCATION OR NATURE, COULD RESULT IN CONVERSION OF FARMLAND, TO NON-AGRICULTURAL USE OR CONVERSION OF FOREST LAND TO NON-FOREST USE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>III. AIR QUALITY</b>					
a.	CONFLICT WITH OR OBSTRUCT IMPLEMENTATION OF THE SCAQMD AIR QUALITY MANAGEMENT PLAN OR CONGESTION MANAGEMENT PLAN?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	VIOLATE ANY AIR QUALITY STANDARD OR CONTRIBUTE SUBSTANTIALLY TO AN EXISTING OR PROJECTED AIR QUALITY VIOLATION?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c.	RESULT IN A CUMULATIVELY CONSIDERABLE NET INCREASE OF ANY CRITERIA POLLUTANT FOR WHICH THE AIR BASIN IS NON-ATTAINMENT (OZONE, CARBON MONOXIDE, & PM 10) UNDER AN APPLICABLE FEDERAL OR STATE AMBIENT AIR QUALITY STANDARD?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d.	EXPOSE SENSITIVE RECEPTORS TO SUBSTANTIAL POLLUTANT CONCENTRATIONS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e.	CREATE OBJECTIONABLE ODORS AFFECTING A SUBSTANTIAL NUMBER OF PEOPLE?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

		Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
<b>IV. BIOLOGICAL RESOURCES</b>					
a.	HAVE A SUBSTANTIAL ADVERSE EFFECT, EITHER DIRECTLY OR THROUGH HABITAT MODIFICATION, ON ANY SPECIES IDENTIFIED AS A CANDIDATE, SENSITIVE, OR SPECIAL STATUS SPECIES IN LOCAL OR REGIONAL PLANS, POLICIES, OR REGULATIONS BY THE CALIFORNIA DEPARTMENT OF FISH AND GAME OR U.S. FISH AND WILDLIFE SERVICE ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b.	HAVE A SUBSTANTIAL ADVERSE EFFECT ON ANY RIPARIAN HABITAT OR OTHER SENSITIVE NATURAL COMMUNITY IDENTIFIED IN THE CITY OR REGIONAL PLANS, POLICIES, REGULATIONS BY THE CALIFORNIA DEPARTMENT OF FISH AND GAME OR U.S. FISH AND WILDLIFE SERVICE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	HAVE A SUBSTANTIAL ADVERSE EFFECT ON FEDERALLY PROTECTED WETLANDS AS DEFINED BY SECTION 404 OF THE CLEAN WATER ACT (INCLUDING, BUT NOT LIMITED TO, MARSH VERNAL POOL, COASTAL, ETC.) THROUGH DIRECT REMOVAL, FILLING, HYDROLOGICAL INTERRUPTION, OR OTHER MEANS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	INTERFERE SUBSTANTIALLY WITH THE MOVEMENT OF ANY NATIVE RESIDENT OR MIGRATORY FISH OR WILDLIFE SPECIES OR WITH ESTABLISHED NATIVE RESIDENT OR MIGRATORY WILDLIFE CORRIDORS, OR IMPEDE THE USE OF NATIVE WILDLIFE NURSERY SITES?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	CONFLICT WITH ANY LOCAL POLICIES OR ORDINANCES PROTECTING BIOLOGICAL RESOURCES, SUCH AS TREE PRESERVATION POLICY OR ORDINANCE (E.G., OAK TREES OR CALIFORNIA WALNUT WOODLANDS)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f.	CONFLICT WITH THE PROVISIONS OF AN ADOPTED HABITAT CONSERVATION PLAN, NATURAL COMMUNITY CONSERVATION PLAN, OR OTHER APPROVED LOCAL, REGIONAL, OR STATE HABITAT CONSERVATION PLAN?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>V. CULTURAL RESOURCES</b>					
a.	CAUSE A SUBSTANTIAL ADVERSE CHANGE IN SIGNIFICANCE OF A HISTORICAL RESOURCE AS DEFINED IN STATE CEQA SECTION 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	CAUSE A SUBSTANTIAL ADVERSE CHANGE IN SIGNIFICANCE OF AN ARCHAEOLOGICAL RESOURCE PURSUANT TO STATE CEQA SECTION 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c.	DIRECTLY OR INDIRECTLY DESTROY A UNIQUE PALEONTOLOGICAL RESOURCE OR SITE OR UNIQUE GEOLOGIC FEATURE?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d.	DISTURB ANY HUMAN REMAINS, INCLUDING THOSE INTERRED OUTSIDE OF FORMAL CEMETERIES?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e.	CAUSE A SUBSTANTIAL ADVERSE CHANGE IN THE SIGNIFICANCE OF A SITE, FEATURE, PLACE, CULTURAL LANDSCAPE, SACRED PLACE, OR OBJECT WITH CULTURAL VALUE TO A CALIFORNIA NATIVE AMERICAN TRIBE THAT IS LISTED OR DETERMINED ELIGIBLE FOR LISTING ON THE CALIFORNIA REGISTER OF HISTORICAL RESOURCES, LISTED ON A LOCAL HISTORICAL REGISTER, OR OTHERWISE DETERMINED BY THE LEAD AGENCY TO BE A TRIBAL CULTURAL RESOURCE?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>VI. GEOLOGY AND SOILS</b>					
a.	EXPOSURE OF PEOPLE OR STRUCTURES TO POTENTIAL SUBSTANTIAL ADVERSE EFFECTS, INCLUDING THE RISK OF LOSS, INJURY OR DEATH	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

		Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
	INVOLVING: RUPTURE OF A KNOWN EARTHQUAKE FAULT, AS DELINEATED ON THE MOST RECENT ALQUIST-PRIOLO EARTHQUAKE FAULT ZONING MAP ISSUED BY THE STATE GEOLOGIST FOR THE AREA OR BASED ON OTHER SUBSTANTIAL EVIDENCE OF A KNOWN FAULT? REFER TO DIVISION OF MINES AND GEOLOGY SPECIAL PUBLICATION 42.				
b.	STRONG SEISMIC GROUND SHAKING?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c.	SEISMIC-RELATED GROUND FAILURE, INCLUDING LIQUEFACTION?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d.	LANDSLIDES?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	RESULT IN SUBSTANTIAL SOIL EROSION OR THE LOSS OF TOPSOIL?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f.	BE LOCATED ON A GEOLOGIC UNIT OR SOIL THAT IS UNSTABLE, OR THAT WOULD BECOME UNSTABLE AS A RESULT OF THE PROJECT, AND POTENTIAL RESULT IN ON- OR OFF-SITE LANDSLIDE, LATERAL SPREADING, SUBSIDENCE, LIQUEFACTION, OR COLLAPSE?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g.	BE LOCATED ON EXPANSIVE SOIL, AS DEFINED IN TABLE 18-1-B OF THE UNIFORM BUILDING CODE (1994), CREATING SUBSTANTIAL RISKS TO LIFE OR PROPERTY?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h.	HAVE SOILS INCAPABLE OF ADEQUATELY SUPPORTING THE USE OF SEPTIC TANKS OR ALTERNATIVE WASTE WATER DISPOSAL SYSTEMS WHERE SEWERS ARE NOT AVAILABLE FOR THE DISPOSAL OF WASTE WATER?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>VII. GREENHOUSE GAS EMISSIONS</b>					
a.	GENERATE GREENHOUSE GAS EMISSIONS, EITHER DIRECTLY OR INDIRECTLY, THAT MAY HAVE A SIGNIFICANT IMPACT ON THE ENVIRONMENT?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	CONFLICT WITH AN APPLICABLE PLAN, POLICY OR REGULATION ADOPTED FOR THE PURPOSE OF REDUCING THE EMISSIONS OF GREENHOUSE GASES?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>III. HAZARDS AND HAZARDOUS MATERIALS</b>					
a.	CREATE A SIGNIFICANT HAZARD TO THE PUBLIC OR THE ENVIRONMENT THROUGH THE ROUTINE TRANSPORT, USE, OR DISPOSAL OF HAZARDOUS MATERIALS	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	CREATE A SIGNIFICANT HAZARD TO THE PUBLIC OR THE ENVIRONMENT THROUGH REASONABLY FORESEEABLE UPSET AND ACCIDENT CONDITIONS INVOLVING THE RELEASE OF HAZARDOUS MATERIALS INTO THE ENVIRONMENT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c.	EMIT HAZARDOUS EMISSIONS OR HANDLE HAZARDOUS OR ACUTELY HAZARDOUS MATERIALS, SUBSTANCES, OR WASTE WITHIN ONE-QUARTER MILE OF AN EXISTING OR PROPOSED SCHOOL?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d.	BE LOCATED ON A SITE WHICH IS INCLUDED ON A LIST OF HAZARDOUS MATERIALS SITES COMPILED PURSUANT TO GOVERNMENT CODE SECTION 65962.5 AND, AS A RESULT, WOULD IT CREATE A SIGNIFICANT HAZARD TO THE PUBLIC OR THE ENVIRONMENT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e.	FOR A PROJECT LOCATED WITHIN AN AIRPORT LAND USE PLAN OR, WHERE SUCH A PLAN HAS NOT BEEN ADOPTED, WITHIN TWO MILES OF A PUBLIC AIRPORT OR PUBLIC USE AIRPORT, WOULD THE PROJECT RESULT IN A SAFETY HAZARD FOR PEOPLE RESIDING OR WORKING IN THE PROJECT AREA?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

		Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
f.	FOR A PROJECT WITHIN THE VICINITY OF A PRIVATE AIRSTRIP, WOULD THE PROJECT RESULT IN A SAFETY HAZARD FOR THE PEOPLE RESIDING OR WORKING IN THE AREA?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g.	IMPAIR IMPLEMENTATION OF OR PHYSICALLY INTERFERE WITH AN ADOPTED EMERGENCY RESPONSE PLAN OR EMERGENCY EVACUATION PLAN?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h.	EXPOSE PEOPLE OR STRUCTURES TO A SIGNIFICANT RISK OF LOSS, INJURY OR DEATH INVOLVING WILDLAND FIRES, INCLUDING WHERE WILDLANDS ARE ADJACENT TO URBANIZED AREAS OR WHERE RESIDENCES ARE INTERMIXED WITH WILDLANDS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>IX. HYDROLOGY AND WATER QUALITY</b>					
a.	VIOLATE ANY WATER QUALITY STANDARDS OR WASTE DISCHARGE REQUIREMENTS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	SUBSTANTIALLY DEplete GROUNDWATER SUPPLIES OR INTERFERE WITH GROUNDWATER RECHARGE SUCH THAT THERE WOULD BE A NET DEFICIT IN AQUIFER VOLUME OR A LOWERING OF THE LOCAL GROUNDWATER TABLE LEVEL (E.G., THE PRODUCTION RATE OF PRE-EXISTING NEARBY WELLS WOULD DROP TO A LEVEL WHICH WOULD NOT SUPPORT EXISTING LAND USES OR PLANNED LAND USES FOR WHICH PERMITS HAVE BEEN GRANTED)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	SUBSTANTIALLY ALTER THE EXISTING DRAINAGE PATTERN OF THE SITE OR AREA, INCLUDING THROUGH THE ALTERATION OF THE COURSE OF A STREAM OR RIVER, IN A MANNER WHICH WOULD RESULT IN SUBSTANTIAL EROSION OR SILTATION ON- OR OFF-SITE?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d.	SUBSTANTIALLY ALTER THE EXISTING DRAINAGE PATTERN OF THE SITE OR AREA, INCLUDING THROUGH THE ALTERATION OF THE COURSE OF A STREAM OR RIVER, OR SUBSTANTIALLY INCREASE THE RATE OR AMOUNT OF SURFACE RUNOFF IN AN MANNER WHICH WOULD RESULT IN FLOODING ON- OR OFF SITE?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e.	CREATE OR CONTRIBUTE RUNOFF WATER WHICH WOULD EXCEED THE CAPACITY OF EXISTING OR PLANNED STORMWATER DRAINAGE SYSTEMS OR PROVIDE SUBSTANTIAL ADDITIONAL SOURCES OF POLLUTED RUNOFF?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f.	OTHERWISE SUBSTANTIALLY DEGRADE WATER QUALITY?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g.	PLACE HOUSING WITHIN A 100-YEAR FLOOD PLAIN AS MAPPED ON FEDERAL FLOOD HAZARD BOUNDARY OR FLOOD INSURANCE RATE MAP OR OTHER FLOOD HAZARD DELINEATION MAP?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h.	PLACE WITHIN A 100-YEAR FLOOD PLAIN STRUCTURES WHICH WOULD IMPEDE OR REDIRECT FLOOD FLOWS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i.	EXPOSE PEOPLE OR STRUCTURES TO A SIGNIFICANT RISK OF LOSS, INQUIRY OR DEATH INVOLVING FLOODING, INCLUDING FLOODING AS A RESULT OF THE FAILURE OF A LEVEE OR DAM?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j.	INUNDATION BY SEICHE, TSUNAMI, OR MUDFLOW?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>X. LAND USE AND PLANNING</b>					
a.	PHYSICALLY DIVIDE AN ESTABLISHED COMMUNITY?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	CONFLICT WITH APPLICABLE LAND USE PLAN, POLICY OR REGULATION OF AN AGENCY WITH JURISDICTION OVER THE PROJECT (INCLUDING BUT NOT LIMITED TO THE GENERAL PLAN, SPECIFIC PLAN, COASTAL PROGRAM, OR ZONING ORDINANCE) ADOPTED FOR THE PURPOSE OF AVOIDING OR MITIGATING AN	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

		Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
	ENVIRONMENTAL EFFECT?				
c.	CONFLICT WITH ANY APPLICABLE HABITAT CONSERVATION PLAN OR NATURAL COMMUNITY CONSERVATION PLAN?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>XI. MINERAL RESOURCES</b>					
a.	RESULT IN THE LOSS OF AVAILABILITY OF A KNOWN MINERAL RESOURCE THAT WOULD BE OF VALUE TO THE REGION AND THE RESIDENTS OF THE STATE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	RESULT IN THE LOSS OF AVAILABILITY OF A LOCALLY-IMPORTANT MINERAL RESOURCE RECOVERY SITE DELINEATED ON A LOCAL GENERAL PLAN, SPECIFIC PLAN, OR OTHER LAND USE PLAN?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>XII. NOISE</b>					
a.	EXPOSURE OF PERSONS TO OR GENERATION OF NOISE IN LEVEL IN EXCESS OF STANDARDS ESTABLISHED IN THE LOCAL GENERAL PLAN OR NOISE ORDINANCE, OR APPLICABLE STANDARDS OF OTHER AGENCIES?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b.	EXPOSURE OF PEOPLE TO OR GENERATION OF EXCESSIVE GROUNDBORNE VIBRATION OR GROUNDBORNE NOISE LEVELS?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c.	A SUBSTANTIAL PERMANENT INCREASE IN AMBIENT NOISE LEVELS IN THE PROJECT VICINITY ABOVE LEVELS EXISTING WITHOUT THE PROJECT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d.	A SUBSTANTIAL TEMPORARY OR PERIODIC INCREASE IN AMBIENT NOISE LEVELS IN THE PROJECT VICINITY ABOVE LEVELS EXISTING WITHOUT THE PROJECT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e.	FOR A PROJECT LOCATED WITHIN AN AIRPORT LAND USE PLAN OR, WHERE SUCH A PLAN HAS NOT BEEN ADOPTED, WITHIN TWO MILES OF A PUBLIC AIRPORT OR PUBLIC USE AIRPORT, WOULD THE PROJECT EXPOSE PEOPLE RESIDING OR WORKING IN THE PROJECT AREA TO EXCESSIVE NOISE LEVELS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f.	FOR A PROJECT WITHIN THE VICINITY OF A PRIVATE AIRSTRIP, WOULD THE PROJECT EXPOSE PEOPLE RESIDING OR WORKING IN THE PROJECT AREA TO EXCESSIVE NOISE LEVELS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>XIII. POPULATION AND HOUSING</b>					
a.	INDUCE SUBSTANTIAL POPULATION GROWTH IN AN AREA EITHER DIRECTLY (FOR EXAMPLE, BY PROPOSING NEW HOMES AND BUSINESSES) OR INDIRECTLY (FOR EXAMPLE, THROUGH EXTENSION OF ROADS OR OTHER INFRASTRUCTURE)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	DISPLACE SUBSTANTIAL NUMBERS OF EXISTING HOUSING NECESSITATING THE CONSTRUCTION OF REPLACEMENT HOUSING ELSEWHERE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c.	DISPLACE SUBSTANTIAL NUMBERS OF PEOPLE NECESSITATING THE CONSTRUCTION OF REPLACEMENT HOUSING ELSEWHERE?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>XIV. PUBLIC SERVICES</b>					
a.	FIRE PROTECTION?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	POLICE PROTECTION?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c.	SCHOOLS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d.	PARKS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

		Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
e.	OTHER PUBLIC FACILITIES?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>XV. RECREATION</b>					
a.	WOULD THE PROJECT INCREASE THE USE OF EXISTING NEIGHBORHOOD AND REGIONAL PARKS OR OTHER RECREATIONAL FACILITIES SUCH THAT SUBSTANTIAL PHYSICAL DETERIORATION OF THE FACILITY WOULD OCCUR OR BE ACCELERATED?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	DOES THE PROJECT INCLUDE RECREATIONAL FACILITIES OR REQUIRE THE CONSTRUCTION OR EXPANSION OF RECREATIONAL FACILITIES WHICH MIGHT HAVE AN ADVERSE PHYSICAL EFFECT ON THE ENVIRONMENT?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>XVI. TRANSPORTATION AND TRAFFIC</b>					
a.	CONFLICT WITH AN APPLICABLE PLAN, ORDINANCE OR POLICY ESTABLISHING MEASURES OF EFFECTIVENESS FOR THE PERFORMANCE OF THE CIRCULATION SYSTEM, TAKING INTO ACCOUNT ALL MODES OF TRANSPORTATION INCLUDING MASS TRANSIT AND NON-MOTORIZED TRAVEL AND RELEVANT COMPONENTS OF THE CIRCULATION SYSTEM, INCLUDING BUT NOT LIMITED TO INTERSECTIONS, STREETS, HIGHWAYS AND FREEWAYS, PEDESTRIAN AND BICYCLE PATHS AND MASS TRANSIT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b.	CONFLICT WITH AN APPLICABLE CONGESTION MANAGEMENT PROGRAM, INCLUDING BUT NOT LIMITED TO LEVEL OF SERVICE STANDARDS AND TRAVEL DEMAND MEASURES, OR OTHER STANDARDS ESTABLISHED BY THE COUNTY CONGESTION MANAGEMENT AGENCY FOR DESIGNATED ROADS OR HIGHWAYS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c.	RESULT IN A CHANGE IN AIR TRAFFIC PATTERNS, INCLUDING EITHER AN INCREASE IN TRAFFIC LEVELS OR A CHANGE IN LOCATION THAT RESULTS IN SUBSTANTIAL SAFETY RISKS?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d.	SUBSTANTIALLY INCREASE HAZARDS TO A DESIGN FEATURE (E.G., SHARP CURVES OR DANGEROUS INTERSECTIONS) OR INCOMPATIBLE USES (E.G., FARM EQUIPMENT)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e.	RESULT IN INADEQUATE EMERGENCY ACCESS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f.	CONFLICT WITH ADOPTED POLICIES, PLANS OR PROGRAMS REGARDING PUBLIC TRANSIT, BICYCLE, OR PEDESTRIAN FACILITIES, OR OTHERWISE DECREASE THE PERFORMANCE OR SAFETY OF SUCH FACILITIES?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>XVII. UTILITIES</b>					
a.	EXCEED WASTEWATER TREATMENT REQUIREMENTS OF THE APPLICABLE REGIONAL WATER QUALITY CONTROL BOARD?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b.	REQUIRE OR RESULT IN THE CONSTRUCTION OF NEW WATER OR WASTEWATER TREATMENT FACILITIES OR EXPANSION OF EXISTING FACILITIES, THE CONSTRUCTION OF WHICH COULD CAUSE SIGNIFICANT ENVIRONMENTAL EFFECTS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c.	REQUIRE OR RESULT IN THE CONSTRUCTION OF NEW STORMWATER DRAINAGE FACILITIES OR EXPANSION OF EXISTING FACILITIES, THE CONSTRUCTION OF WHICH COULD CAUSE SIGNIFICANT ENVIRONMENTAL EFFECTS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d.	HAVE SUFFICIENT WATER SUPPLIES AVAILABLE TO SERVE THE PROJECT FROM EXISTING ENTITLEMENTS AND RESOURCE, OR ARE NEW OR EXPANDED ENTITLEMENTS NEEDED?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

		Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
e.	RESULT IN A DETERMINATION BY THE WASTEWATER TREATMENT PROVIDER WHICH SERVES OR MAY SERVE THE PROJECT THAT IT HAS ADEQUATE CAPACITY TO SERVE THE PROJECT'S PROJECTED DEMAND IN ADDITION TO THE PROVIDER'S EXISTING COMMITMENTS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f.	BE SERVED BY A LANDFILL WITH SUFFICIENT PERMITTED CAPACITY TO ACCOMMODATE THE PROJECT'S SOLID WASTE DISPOSAL NEEDS?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g.	COMPLY WITH FEDERAL, STATE, AND LOCAL STATUTES AND REGULATIONS RELATED TO SOLID WASTE?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>XVIII. MANDATORY FINDINGS OF SIGNIFICANCE</b>					
a.	DOES THE PROJECT HAVE THE POTENTIAL TO DEGRADE THE QUALITY OF THE ENVIRONMENT, SUBSTANTIALLY REDUCE THE HABITAT OF FISH OR WILDLIFE SPECIES, CAUSE A FISH OR WILDLIFE POPULATION TO DROP BELOW SELF-SUSTAINING LEVELS, THREATEN TO ELIMINATE A PLANT OR ANIMAL COMMUNITY, REDUCE THE NUMBER OR RESTRICT THE RANGE OF A RARE OR ENDANGERED PLANT OR ANIMAL OR ELIMINATE IMPORTANT EXAMPLES OF THE MAJOR PERIODS OF CALIFORNIA HISTORY OR PREHISTORY?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b.	DOES THE PROJECT HAVE IMPACTS WHICH ARE INDIVIDUALLY LIMITED, BUT CUMULATIVELY CONSIDERABLE? ("CUMULATIVELY CONSIDERABLE" MEANS THAT THE INCREMENTAL EFFECTS OF AN INDIVIDUAL PROJECT ARE CONSIDERABLE WHEN VIEWED IN CONNECTION WITH THE EFFECTS OF PAST PROJECTS, THE EFFECTS OF OTHER CURRENT PROJECTS, AND THE EFFECTS OF PROBABLE FUTURE PROJECTS).	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c.	DOES THE PROJECT HAVE ENVIRONMENTAL EFFECTS WHICH CAUSE SUBSTANTIAL ADVERSE EFFECTS ON HUMAN BEINGS, EITHER DIRECTLY OR INDIRECTLY?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**DISCUSSION OF THE ENVIRONMENTAL EVALUATION (Attach additional sheets if necessary)**

The Environmental Impact Assessment includes the use of official City of Los Angeles and other government source reference materials related to various environmental impact categories (e.g., Hydrology, Air Quality, Biology, Cultural Resources, etc.). The State of California, Department of Conservation, Division of Mines and Geology – Seismic Hazard Maps and reports, are used to identify potential future significant seismic events; including probable magnitudes, liquefaction, and landslide hazards. Based on Applicant information provided in the Master Land Use Application and Environmental Assessment Form, impact evaluations were based on stated facts contained therein, including but not limited to, reference materials indicated above, field investigation of the project site, and other reliable reference materials known at the time.

Project specific impacts were evaluated based on all relevant facts indicated in the Environmental Assessment Form and expressed through the Applicant’s project description and supportive materials. Both the Initial Study Checklist and Checklist Explanations, in conjunction with the City of Los Angeles’s Adopted Thresholds Guide and CEQA Guidelines, were used to reach reasonable conclusions on environmental impacts as mandated under the California Environmental Quality Act (CEQA).

The project as identified in the project description may cause potentially significant impacts on the environment without mitigation. Therefore, this environmental analysis concludes that a Mitigated Negative Declaration shall be issued to avoid and mitigate all potential adverse impacts on the environment by the imposition of mitigation measures and/or conditions contained and expressed in this document; the environmental case file known as **ENV-2015-3384-MND** and the associated case(s), **CPC-2015-3383-ZC-CU-ZAD-SPR and VTT-73641**. Finally, based on the fact that these impacts can be feasibly mitigated to less than significant, and based on the findings and thresholds for Mandatory Findings of Significance as described in the California Environmental Quality Act, section 15065, the overall project impacts(s) on the environment (after mitigation) **will not:**

- Substantially degrade environmental quality.
- Substantially reduce fish or wildlife habitat.
- Cause a fish or wildlife habitat to drop below self sustaining levels.
- Threaten to eliminate a plant or animal community.
- Reduce number, or restrict range of a rare, threatened, or endangered species.
- Eliminate important examples of major periods of California history or prehistory.
- Achieve short-term goals to the disadvantage of long-term goals.
- Result in environmental effects that are individually limited but cumulatively considerable.
- Result in environmental effects that will cause substantial adverse effects on human beings.

**ADDITIONAL INFORMATION:**

All supporting documents and references are contained in the Environmental Case File referenced above and may be viewed in the EIR Unit, Room 763, City Hall.

For City information, addresses, and phone numbers: visit the City’s website at <http://www.lacity.org>; City Planning- and Zoning Information Mapping Automated System (ZIMAS) [cityplanning.lacity.org/](http://cityplanning.lacity.org/) or EIR Unit, City Hall, 200 N Spring Street, Room 763. Seismic Hazard Maps – <http://gmw.consrv.ca.gov/shmp/> Engineering/Infrastructure/Topographic Maps/Parcel Information – <http://boemaps.eng.ci.la.ca.us/index0.1htm> or City’s main website under the heading “Navigate LA.”

<b>PREPARED BY:</b> May Sirinopwongsagon	<b>TITLE:</b> City Planner	<b>TELEPHONE NO.:</b> 213-978-1372	<b>DATE:</b> August 11, 2016
---	-------------------------------	---------------------------------------	---------------------------------

## APPENDIX A: ENVIRONMENTAL IMPACTS EXPLANATION TABLE

	Impact	Explanation	Mitigation Measures
<b>I. AESTHETICS</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	AES-1, AES-2
<b>II. AGRICULTURAL RESOURCES</b>			
a.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
e.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>III. AIR QUALITY</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
e.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>IV. BIOLOGICAL RESOURCES</b>			
a.	Potential Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	BIO-1
b.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
e.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
f.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>V. CULTURAL RESOURCES</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>VI. GEOLOGY AND SOILS</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
e.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
f.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
g.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
h.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>VII. GREENHOUSE GAS EMISSIONS</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	GHG-1
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	GHG-1
<b>VIII. HAZARDS AND HAZARDOUS MATERIALS</b>			

	<b>Impact</b>	<b>Explanation</b>	<b>Mitigation Measures</b>
a.	Less than Significant impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	HAZ-1
c.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	HAZ-2
d.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	HAZ-1, HAZ-2
e.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
f.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
g.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
h.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>IX. HYDROLOGY AND WATER QUALITY</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
e.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
f.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
g.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
h.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
i.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
j.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>X. LAND USE AND PLANNING</b>			
a.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>XI. MINERAL RESOURCES</b>			
a.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>XII. NOISE</b>			
a.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	N-1, N-2
b.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	N-1
c.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	N-3
d.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	N-1, N-2
e.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
f.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>XIII. POPULATION AND HOUSING</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>XIV. PUBLIC SERVICES</b>			
a.i	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
a.ii	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	PS-1

	<b>Impact</b>	<b>Explanation</b>	<b>Mitigation Measures</b>
a.iii	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
a.iv.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
a.v.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>XV. RECREATION</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>XVI. TRANSPORTATION AND TRAFFIC</b>			
a.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	TRAFFIC-1, TRAFFIC-2
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
e.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
f.	Potentially Significant unless Mitigation Incorporated.	See expanded environmental analysis (attached).	TRAFFIC-1.
<b>XVII. UTILITIES AND SERVICE SYSTEMS</b>			
a.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
d.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
e.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
f.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
g.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
<b>XVIII. MANDATORY FINDINGS OF SIGNIFICANCE</b>			
a.	No Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
b.	Less Than Significant Impact.	See expanded environmental analysis (attached).	No mitigation measures are required.
c.	Potentially Significant Unless Mitigation Incorporated.	See expanded environmental analysis (attached).	See mitigation measures below.

## **MITIGATION MEASURES**

### **AESTHETICS**

#### **AES-1 Aesthetics (Light)**

- Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties, or the public right-of-way, nor from above.

#### **AES-2 Aesthetics (Glare)**

- The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat.

### **AGRICULTURE AND FORESTRY RESOURCES**

No mitigation measures are required.

### **AIR QUALITY**

No mitigation measures are required.

### **BIOLOGICAL RESOURCES**

#### **BIO-1 (Tree Removal (Non-Protected Trees)):**

- Prior to the issuance of any permit, a plot plan shall be prepared indicating the location, size, type, and general condition of all existing trees on the site and within the adjacent public right(s)-of-way.
- All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net, new trees, located within the parkway of the adjacent public right(s)-of-way, may be counted toward replacement tree requirements.
- Removal or planting of any tree in the public right-of-way requires approval of the Board of Public Works. Contact Urban Forestry Division at: 213-847-3077. All trees in the public right-of-way shall be provided per the current standards of the Urban Forestry Division the Department of Public Works, Bureau of Street Services.

### **CULTURAL RESOURCES**

No mitigation measures are required.

### **GEOLOGY AND SOILS**

No mitigation measures are required.

### **GREENHOUSE GAS EMISSIONS**

#### **GHG-1 Greenhouse Gas Emissions**

- Low- and non-VOC containing paints, sealants, adhesives, solvents, asphalt primer, and architectural coatings (where used), or pre-fabricated architectural panels shall be used in the construction of the Project to reduce VOC emissions to the maximum extent practicable.

**HAZARDS AND HAZARDOUS MATERIALS**

**HAZ-1** To verify whether residual areas of impacted soil maybe present under buildings, a Soil Management Plan (SMP) shall be developed for the future grading work, especially for the identification and segregation of the petroleum hydrocarbons (TPH) impacted soil. It is recommended that a post-grading soil gas survey be conducted to confirm that a vapor intrusion hazard does not exist for future onsite structures. The SMP and results of the post-grading soil gas survey shall be submitted to the LARWQCB File #913240316 for their review and incorporation into the agency file.

**HAZ-2 Construction Activity Near Schools**

- The Applicant and contractors shall maintain ongoing contact with administrator of Magnolia Science Academy 7. The administrative offices shall be contacted when demolition, grading and construction activity begin on the project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323)342-1400 and guarantee that safe and convenient pedestrian and bus routes to the school be maintained.
- The Applicant shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety.
- There shall be no staging or parking of construction vehicles, including vehicles to transport workers on Roscoe Boulevard, adjacent to the school.
- Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on Roscoe Boulevard, adjacent to the school, during school hours.

**HYDROLOGY AND WATER QUALITY**

No mitigation measures are required.

**LAND USE AND PLANNING**

No mitigation measures are required.

**MINERAL RESOURCES**

No mitigation measures are required.

**NOISE****N-1 Demolition, Grading, and Construction Activities**

- Construction and demolition shall be restricted to the hours of 7:00 am to 6:00 pm Monday through Friday, and 8:00 am to 6:00 pm on Saturday.
- To the maximum extent practical, demolition and construction activities shall be scheduled so as to avoid operating several pieces of equipment simultaneously, which causes high noise levels.
- The project contractor shall use power construction equipment with state-of-the-art noise shielding and muffling devices.

**N-2 Demolition, Grading, and Construction Activities**

- The project contractor shall erect a temporary noise-attenuating sound barrier along the perimeter of the Project Site's easterly property line to block the line of sight between the construction equipment and the adjacent residential land uses. The sound wall shall be a minimum of 8 feet in height and shall be comprised of either ¾-inch plywood or an alternative sound absorbing material capable of achieving a 10 dBA reduction in sound level.

**N-3 Parking Structure Ramps**

- Concrete, not metal, shall be used for construction of parking ramps.
- The interior ramps shall be textured to prevent tire squeal at turning areas.

**POPULATION AND HOUSING**

No mitigation measures are required.

**PUBLIC SERVICES****PS-1 Police**

The plans shall incorporate the Design Guidelines (defined in the following sentence) relative to security, semi-public and private spaces, which may include but not be limited to access control to building, secured parking facilities, walls/fences with key systems, well-illuminated public and semi-public space designed with a minimum of dead space to eliminate areas of concealment, location of toilet facilities or building entrances in high-foot traffic areas, and provision of security guard patrol throughout the project site if needed. Please refer to "Design Out Crime Guidelines: Crime Prevention Through Environmental Design", published by the Los Angeles Police Department. Contact the Community Relations Division, located at 100 W. 1st Street, #250, Los Angeles, CA 90012; (213) 486-6000. These measures shall be approved by the Police Department prior to the issuance of building permits.

**RECREATION**

No mitigation measures are required.

**TRANSPORTATION AND TRAFFIC**

**TRAFFIC-1** A Construction Work Site Traffic Control Plan shall be submitted to DOT for review and approval in accordance with the LAMC prior to the start of any construction work. The Construction Work Site Traffic Control Plan shall show the location of any roadway or sidewalk closures, traffic detours, haul routes, hours of operation, protective devices, warning signs and access to abutting properties. All construction related traffic shall be restricted to off-peak hours.

**TRAFFIC-2** All delivery truck loading and unloading shall take place on site or within the boundaries of an approved traffic control plan.

**UTILITIES AND SERVICE SYSTEMS**

No mitigation measures are required.

**MANDATORY FINDINGS OF SIGNIFICANCE**

See above mitigation measures.

**END**

---

# I. INTRODUCTION

---

## PROJECT INFORMATION

Project Title: North Reseda Boulevard Project

Project Location: 8350-8430 N. Reseda Boulevard and 8421-8429 N. Canby Avenue  
Los Angeles, CA 91324

Project Applicant: Harridge Cadillac Group, LLC  
6363 Wilshire Boulevard, Suite 600  
Los Angeles, CA 90048

Lead Agency: City of Los Angeles  
Department of City Planning  
200 N. Spring Street, Room 763  
Los Angeles, CA 90012

## PROJECT SUMMARY

The Project Site includes 143,552 square feet of lot area (3.29 acres). The Project Site currently includes three one-story buildings (a car resale center, an auto body shop, and a commercial office building) totaling 27,411 square feet of building area along with surface parking areas. The surrounding properties are developed with single-family housing, office, commercial land uses, and surface parking lots.

The Proposed Project includes the demolition of a car resale center, auto body shop, a commercial office building (27,411 total square feet) and associated surface parking lots for the construction, use and maintenance of a unified development with two mixed-use buildings with 79 apartment units and approximately 10,300 square feet of ground-floor commercial space and a small lot subdivision with 61 small lot homes. The Proposed Project's mixed-use buildings would include 4-stories, approximately 45 feet above grade (3 levels of multi-family residential units over ground-floor retail and parking). The 61 small lots would each include 3-stories with a maximum of 37 feet above grade. Approximately 175 parking spaces would be provided at grade level and in one subterranean level below the mixed-use buildings. Each small lot would provide a two-car parking garage (122 total parking spaces). Vehicular access to the Project Site would be provided via Reseda Boulevard and Canby Avenue. On-site vehicle parking spaces, bicycle parking spaces, private and common open space areas, and trees would be provided to meet the requirements of the LAMC. In total, the Proposed Project would include approximately 202,215 square feet of developed floor area (191,915 square feet of residential space and 10,300 square feet of commercial space) resulting in a floor area ratio (FAR) of 1.43 to 1 averaged across the site.

The Applicant is requesting the following discretionary approvals: (1) Site Plan Review; (2) Zone Change from C2-1VL and P-1VL to C2-1VL; (3) Conditional Use Permit to average density and floor area of unified development; (4) Vesting Tentative Tract Map for 61-units small-lot subdivision and (5)

Zoning Administrator's Determination for Transitional Height. The Applicant would also request approvals and permits from the Department of Building and Safety (and other municipal agencies) for project construction activities which may include, but are not limited to, the following: excavation, shoring, grading, foundation, haul route (for the export of approximately 2,000 cubic yards (cy) of asphalt debris and 22,000 cy of soil), removal and replacement of street trees, and building and tenant improvements for the Project Site.

## **ORGANIZATION OF THE INITIAL STUDY**

This expanded IS/MND is organized into six sections as follows:

**Initial Study Checklist:** This Section contains the completed IS Checklist showing the significance level under each environmental impact category.

**Introduction:** This Section provides introductory information such as the Proposed Project title, the Project Applicant, and the lead agency for the Proposed Project.

**Project Description:** This Section provides a detailed description of the Proposed Project including the environmental setting, project characteristics, related project information, and environmental clearance requirements.

**Environmental Impact Analysis:** This Section contains an assessment and discussion of impacts for each environmental issue identified in the Initial Study Checklist. Where the evaluation identifies potentially significant effects, mitigation measures are provided to reduce such impacts to less-than-significant levels.

**Preparers of the Initial Study and Persons Consulted:** This Section provides a list of consultant team members and governmental agencies that participated in the preparation of the IS.

**References, Acronyms and Abbreviations:** This Section includes various documents and information used and referenced during the preparation of the IS, along with a list of commonly used acronyms.

## II. PROJECT DESCRIPTION

### A. PROJECT LOCATION

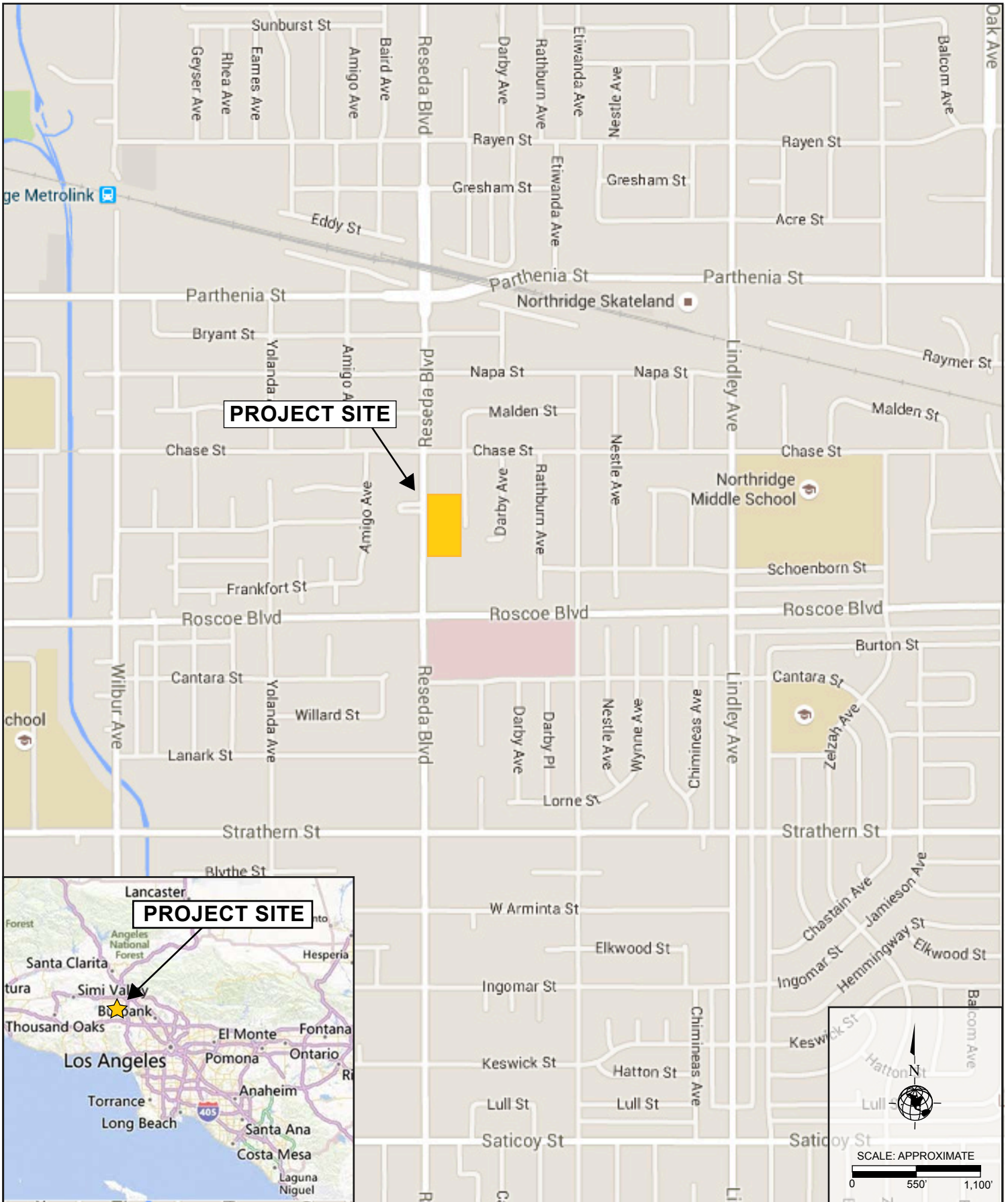
#### PROJECT LOCATION

The Project Site is located within the boundaries of the Northridge Community Plan area within the City of Los Angeles. As shown in Figure II-1, Project Location Map, the Project Site is located within the community of Northridge in the west San Fernando Valley area of the City of Los Angeles. The Project Site encompasses five parcels equating to approximately 143,552 square feet of gross lot area (3.29 acres) and 140,925 square feet of net lot area (3.24 acres). The Project Site's property addresses, Assessor's Parcel Numbers (APN), land use and lot area are summarized in Table II-1, Summary of the Project Site, below.

**Table II-1  
Summary of Project Site**

Address	APN	Existing Land Use	Lot Area (square feet)
8350 N. Reseda Boulevard 8352 N. Reseda Boulevard	2786022021	Commercial Office Building	143,552 (140,925 net square feet)
8366 N. Reseda Boulevard 8370 N. Reseda Boulevard 8354 N. Reseda Boulevard	2786021018	Auto Body Shop / Car Showroom	
8408 N. Reseda Boulevard 8404 N. Reseda Boulevard 8400 N. Reseda Boulevard 8420 N. Reseda Boulevard 8416 N. Reseda Boulevard 8422 N. Reseda Boulevard 8421 N. Canby Avenue 8423 N. Canby Avenue 8424 N. Reseda Boulevard 8425 N. Canby Avenue 8426 N. Reseda Boulevard 8427 N. Canby Avenue 8428 N. Reseda Boulevard 8429 N. Canby Avenue 8430 N. Reseda Boulevard	2786021017	Auto Body Shop / Car Showroom	
<i>Sources: City of Los Angeles Department of Planning, Zone Information and Map Access System, website: http://zimas.lacity.org/, accessed October 2015.</i>			

The Project Site is generally bounded by Reseda Boulevard to the west, where one story commercial, retail, and office buildings are located across from the Project Site. One to two-story commercial, retail, and office buildings border the Project Site to the north and south. Canby Avenue and single-family homes bound the Project Site to the east. Single-family homes are located along Canby Avenue across from the Project Site.



Source: Google Map, 2015.



Figure II-1  
Project Location Map

---

## REGIONAL AND LOCAL ACCESS

Primary vehicular access to the Project Site is provided by the Ventura Freeway (US-101) located approximately 3 miles to the south, the Ronald Reagan Freeway (CA-118) located approximately 3.6 miles to the north, and the San Diego Freeway (I-405) located approximately 3.5 miles to the west of the Project Site. All of these freeways provide regional access to and from the San Fernando Valley and the Los Angeles area. Project access to the Ventura Freeway (US-101) is provided from Reseda Boulevard / Ventura Boulevard. Project access to and from the Ronald Reagan Freeway (CA-118) is provided from Reseda Boulevard. Project access to the San Diego Freeway (I-405) is provided from Roscoe Boulevard.

Pursuant to the Mobility Element, arterial roadways are designated Boulevards and Avenues. Local street access is provided by the grid roadway system surrounding the Project Site. Reseda Boulevard, which borders the Project Site to the west, is a north-south Boulevard II roadway providing two travel lanes in each direction. Roscoe Boulevard, located approximately 0.1 mile south of the Project Site, is an east-west Boulevard II roadway providing two to three lanes in each direction, depending on the street parking restrictions. Canby Avenue, which borders the east side of the Project Site, is a north-south non-continuous local street, designated as a Local Street - Standard. This roadway provides one lane in each direction and on-street parking for the adjacent residential neighborhood. Chase Street, an east-west collector street, runs perpendicular to Canby Avenue and Reseda Boulevard, north of the Project Site. Chase Street provides one lane in each direction and on-street parking.

Public transportation in the Project area is provided by the Metropolitan Transportation Authority (Metro) and the LADOT DASH. Metro provides transit routes 150 and 240 along Reseda Boulevard and Ventura Boulevard with major stops at Westfield Topanga and Promenade Malls, Warner Center Transit Hub, Pierce College, Sherman Oaks Galleria, Universal Studios City Walk and CSUN. Bus stops for Metro bus line 240 are located along Reseda Boulevard where it intersects with Chase Street and another stop where it intersects with Roscoe Boulevard, within walking distance of the Project Site. Metro routes 152 and 353 limited provide service from Woodland Hills via Fallbrook Avenue and Roscoe Boulevard to Sun Valley with stops at Van Nuys and Burbank Airports and ultimately to the North Hollywood Red and Orange Line Stations. The Metro bus stop for bus lines 152 and 353 is located at the intersection of Yolanda Avenue and Roscoe Boulevard, approximately 0.3 mile southwest of the Project Site. Rapid Line 744 runs along Reseda Boulevard, Ventura Boulevard and Van Nuys Boulevard with major stops at CSUN, Panorama Mall, Van Nuys Civic Center, Metrolink Stations at Van Nuys and Northridge and the Hansen Dam Recreation area. The Metro Rapid 744 bus stop is located at the intersection of Roscoe Boulevard and Reseda Boulevard, approximately 0.1 mile south of the Project Site. The DASH Northridge route runs through major points of interest in Northridge and Reseda. The closest DASH Northridge bus stops in close proximity to the Project Site are located on the Roscoe Boulevard and Reseda Boulevard intersection and Chase Street and Reseda Boulevard intersection.

## **ZONING AND LAND USE DESIGNATIONS**

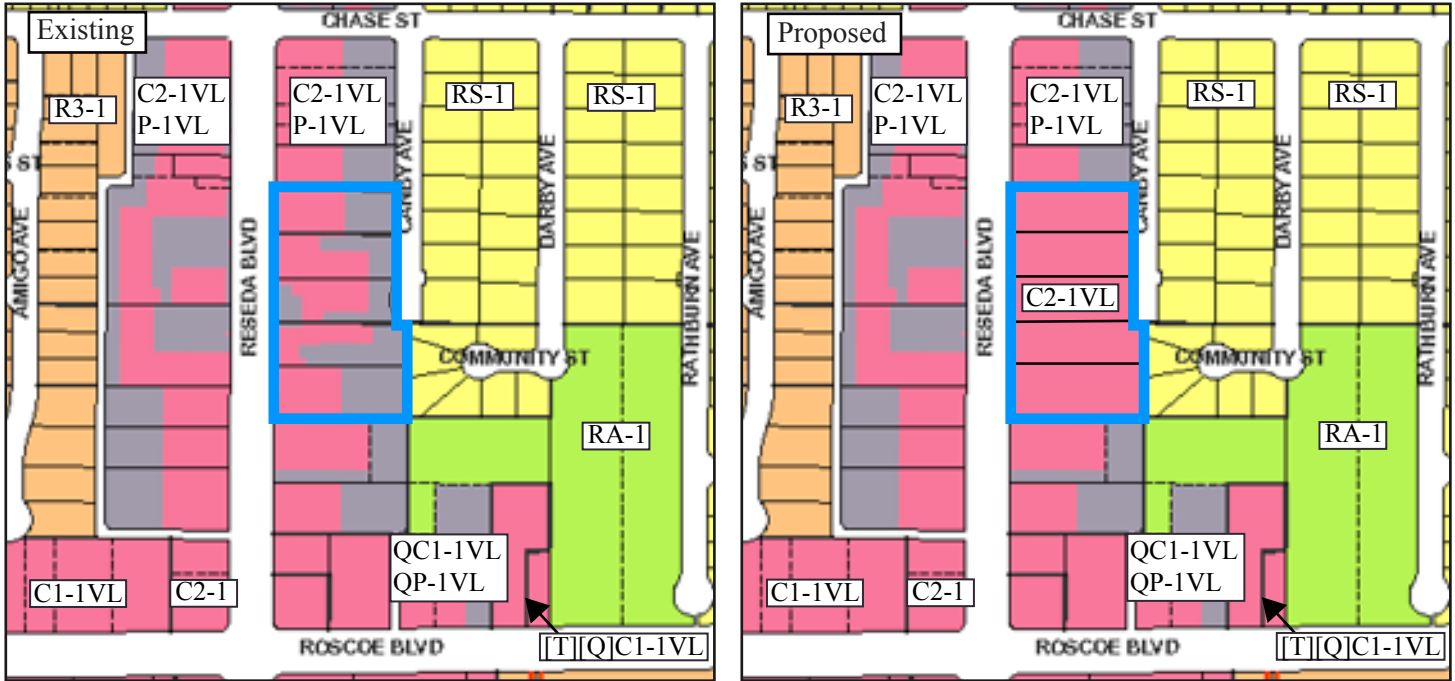
The Project Site is zoned C2-1VL and P-1VL and the General Plan land use designation is “General Commercial.” The partial areas zoned P-1VL are Automobile Parking Zones that contain surface parking areas for the commercial and office buildings on-site. Figure II-2, Zoning and General Plan Designations, shows the existing zonings and land use designations on the Project Site and in the surrounding area. As shown in Figure II-2, the Project Site is bordered by C2, P, and RS zoned properties. The properties to the north, south and west of the Project Site are zoned C2-1VL and P-1VL with General Plan land use designations of General Commercial, and the properties to the east are zoned RS-1 with General Plan land use designation of Low I Residential.

### **Northridge Community Plan**

All development activity on-site is subject to the land use regulations of the Northridge Community Plan (Community Plan).

The Northridge Community Plan designates the Project Site as General Commercial land use designation. The General Commercial land use designation lists the following corresponding zones: C1.5, C2, C4, and RAS3. The Project Site is currently zoned C2-1VL and P-1VL within Height District No. 1VL, which is Very Limited Height District. Height District 1VL limits buildings heights to three stories, not exceed 45 feet above grade, and an FAR of 1.5:1.

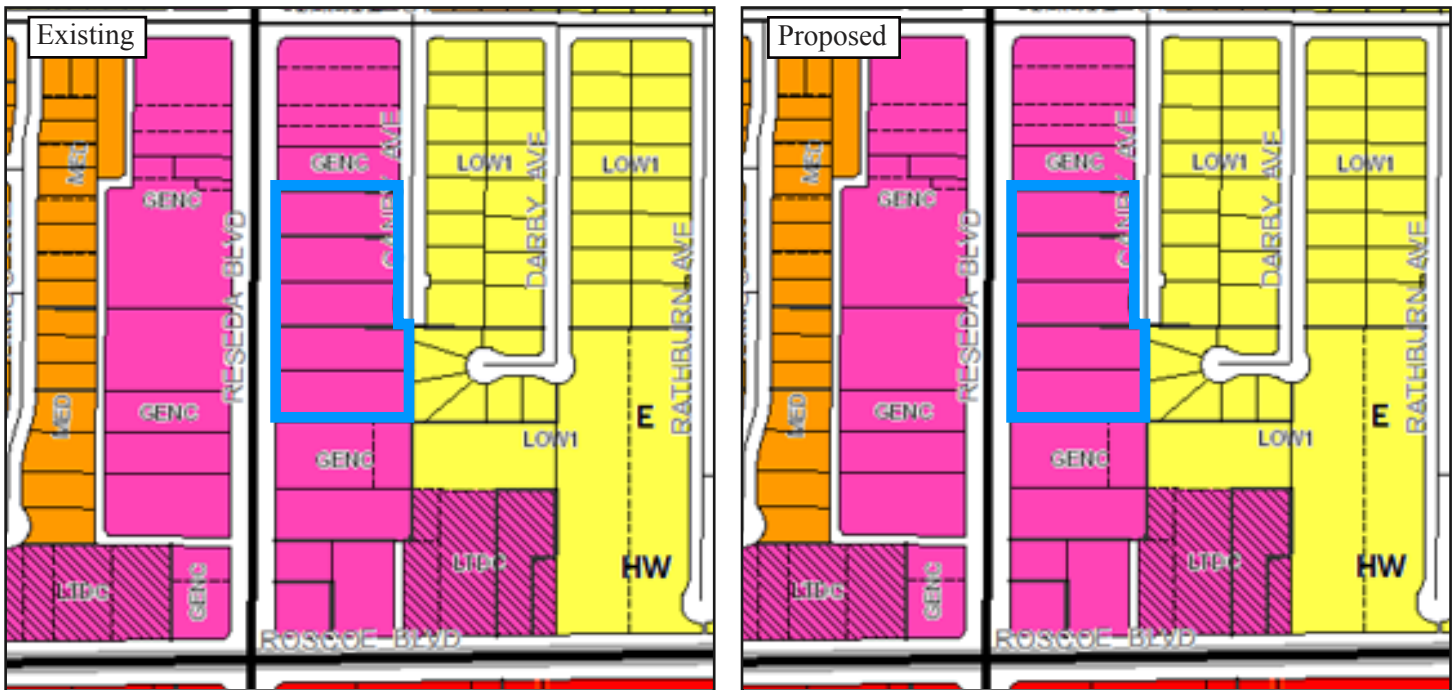
### Zoning Designations



#### LEGEND

- Zoning: C1 and C2
- Zoning: R3
- Zoning: RA
- Zoning: P
- Zoning: RS
- Project Site

### General Plan Land Use Designations



#### LEGEND

- General Commercial
- Community Commercial
- Low Density Residential
- HW** House of Worship
- Limited Commercial
- Medium Residential
- E** Private Elementary School
- Project Site

Source: ZIMAS, City of Los Angeles, Department of City Planning, 2015; and Parker Environmental Consultants, 2015.



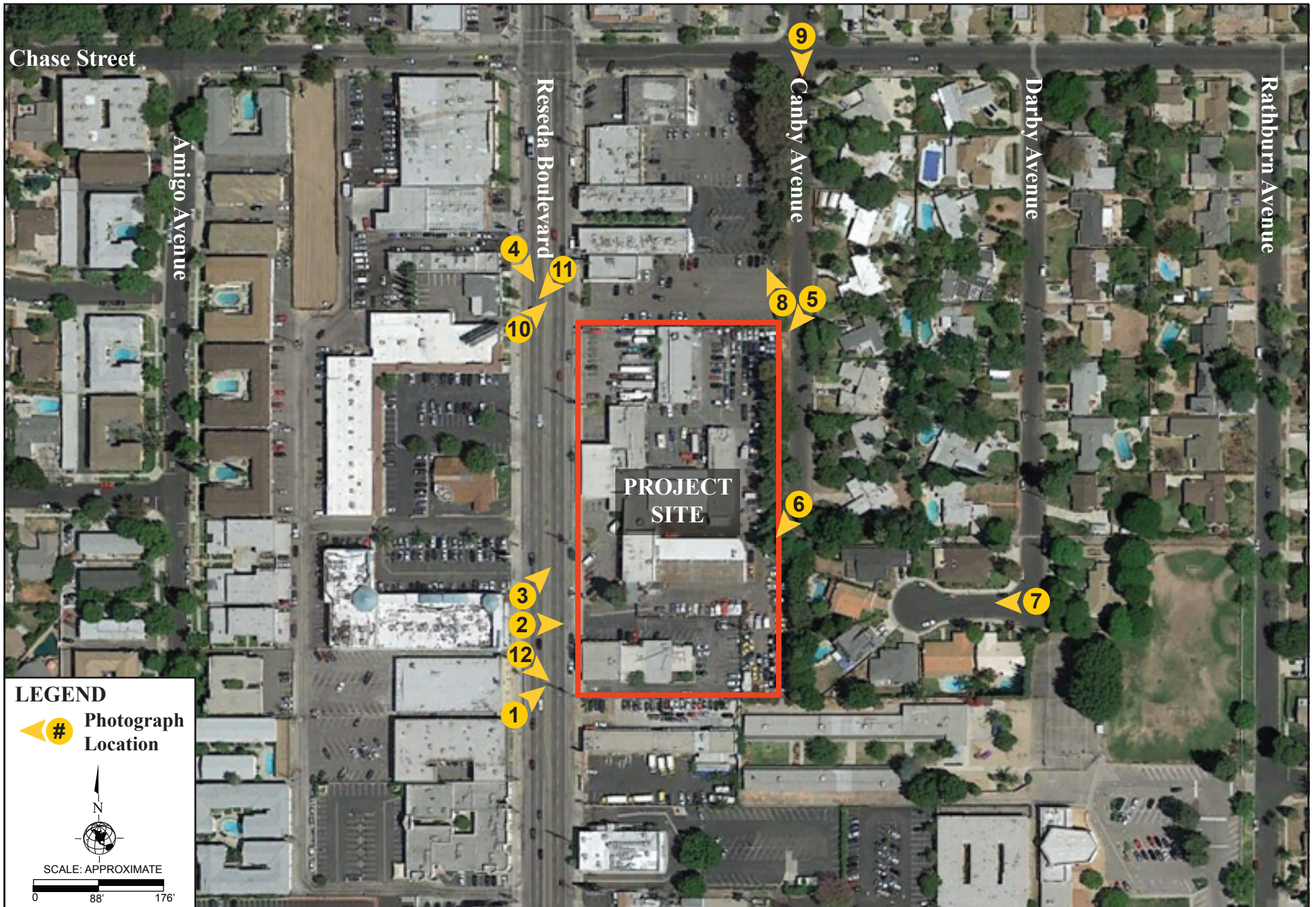
Figure II-2  
Existing and Proposed  
Zoning and General Plan Land Use Designations

## EXISTING CONDITIONS

Figure II-3, Aerial Photograph and Photograph Location Map, shows an aerial view of the Project Site and identifies the location points for the site photographs and surrounding land use photographs shown in Figure II-4, Photographs of the Project Site, and Figure II-5, Photographs of the Surrounding Land Uses, respectively. The Project Site is currently improved with buildings utilized as a car resale center, an auto body shop, and a commercial office building. Each property has asphalt-paved surface parking spaces. The buildings altogether contain approximately 27,411 square feet of building area including 21,986 square feet of automobile associated uses and 5,425 square feet of a commercial office building. The Neill Lehr Cadillac Showroom is located on the northern portion of the Project Site at 8400 N. Reseda Boulevard. The commercial office contains an optician office and a mental health counseling center. The commercial office's parking area contains sliding gates on the primary driveway to limit access on-site during non-operational hours. The auto body shops contain parking in the front and some gated parking areas on the east side of the Project Site. The east side of the Project Site is surrounded by fencing along Canby Avenue and the single-family homes.

The Project Site is entirely devoid of any California or Los Angeles protected trees. There are four street trees on the public right-of-way along Reseda Boulevard adjacent to the Project Site, and approximately 22 trees on-site, including 13 trees along the west side of Canby Avenue. Of these 22 trees, there are 18 trees with a diameter over 12 inches.

Vehicular access to the Project Site and designated surface parking areas are currently provided by driveways along Reseda Boulevard. There is some restricted street parking surrounding the Project Site along Reseda Boulevard. Pursuant to LAMC, a solid masonry wall must divide the Project Site from the abutting residential zone to the east of the Project Site. Therefore, there is no current vehicular access to the east side of the Project Site from Canby Avenue due to the surrounding walls and fencing. Photographs depicting the current conditions of the Project Site are provided in Figure II-4, Photographs of the Project Site.



Source: Parker Environmental Consultants, October 22, 2015.



View 1: From the west side of Reseda Boulevard, looking northeast at the Project Site.



View 2: From the west side of Reseda Boulevard, looking east at the Project Site.



View 3: From the west side of Reseda Boulevard, looking northeast at the Project Site.



View 4: From the west side of Reseda Boulevard, looking southeast at the Project Site.



View 5: From Canby Avenue, looking southwest at the Project Site.



View 6: From south end of Canby Avenue, looking southwest at the Project Site.

Source: Parker Environmental Consultants, October 22, 2015.



Figure II-4  
Photographs of the Project Site  
Views 1-6



View 7: From the south end of Darby Avenue, looking west at the properties that border the Project Site to the east.



View 8: From Canby Avenue, looking northwest at the properties to the north of the Project Site.



View 9: From the intersection of Chase Street and Canby Avenue, looking south down Canby Avenue.



View 10: From the west side of Reseda Boulevard, looking northeast at the properties to the north of the Project Site.



View 11: From the east side of Reseda Boulevard, looking southwest at the properties west of the Project Site.



View 12: From the west side of Reseda Boulevard, looking southeast at the properties south of the Project Site.

Source: Parker Environmental Consultants, October 22, 2015.



Figure II-5  
Photographs of Surrounding Land Uses  
Views 7-12

## **SURROUNDING LAND USES**

The properties surrounding the Project Site include commercial/commercial, offices, and single-family residential land uses. Photographs of the land uses immediately surrounding the Project Site are provided in Figure II-5, Photographs of the Surrounding Land Uses.

East: The properties and land uses to the east of the Project Site include single-family homes. The northeast portion of the Project Site is bordered by Canby Avenue, where the street reaches a dead end. Single-family homes run along Canby Avenue across from the Project Site. Single-family residences directly border the southeast portion of the Project Site to the east. The properties to the east of the Project Site are zoned RS-1 and have a General Plan land use designation of Low I Residential. See Figure II-5, Views 7 and 9.

West: Reseda Boulevard bounds the Project Site to the west. The properties and land uses to the west of the Project Site, across Reseda Boulevard, include one-story commercial, retail, medical, and office land uses. Properties to the west are zoned C2-1VL and P-1VL and have General Plan land use designations of General Commercial. See Figure II-5, View 11.

North: To the immediate north of the Project Site is a car rental business, along with designated surface parking spaces for its rental cars. This property has a short cement wall dividing its property and the Project Site. North of the car rental property are one to two-story commercial and retail land uses. Properties to the north are zoned C2-1VL and P-1VL and have General Plan land use designations of General Commercial. See Figure II-5, Views 8 and 10.

South: To the immediate south of the Project Site is an auto repair shop. Other properties south of the Project Site include medical and dental offices, general offices, a restaurant, and commercial/retail uses. Properties to the south are zoned C2-1VL and P-1VL and have General Plan land use designations of General Commercial. See Figure II-5, View 12.

---

## II. PROJECT DESCRIPTION

### B. PROJECT CHARACTERISTICS

---

#### PROPOSED DEVELOPMENT

The Proposed Project includes the demolition of the existing on-site buildings and the construction of a unified development with two four-story mixed-use residential and commercial buildings and a small lot subdivision with 61 three-story small lot homes. The two mixed-use buildings would be fronting Reseda Boulevard and would include a maximum of 79 residential apartments and up to 10,300 square feet of ground-floor retail and restaurant uses. The mixed-use buildings would include an at-grade parking level and one subterranean parking level. The mixed-use buildings would be a maximum height of 45 feet above grade. The small lot subdivision with 61 small lot homes would be constructed on the eastern portion of the Project Site. The small lot homes would each include three stories and provide a two-vehicle garage. The small lot homes would reach a maximum height of 37 feet above grade. A summary of the Proposed Project is provided in Table II-2, Proposed Development Program, below. The plan layout of the Proposed Project is depicted in Figure II-6, Site Plan. The floor plans of the mixed-use buildings is depicted in Figure II-15.

**Table II-2  
Proposed Development Program**

Land Uses	Proposed Dwelling Units Mix	Proposed Floor Area (Square Feet)
<b>Proposed Project:</b>		
<b>Residential</b>		
Multi-family Units	79	72,193
Small Lot Units	61	119,722
<b>Subtotal Residential</b>	<b>140</b>	<b>191,915</b>
<b>Commercial</b>		
Ground-floor Commercial	--	10,300
<b>Subtotal Retail</b>	--	<b>10,300</b>
<b>TOTAL:</b>	<b>140</b>	<b>202,215 sf</b>
<b>Floor Area Ratio (1.5:1 allowed):</b>		<b>1.43:1</b>
<i>Source: Van Tilburg, Banvard &amp; Soderbergh, AIA, February 16, 2016.</i>		

#### Residential Uses

As shown in Table II-2, above, the Proposed Project would include a maximum of 79 multi-family residential units and 61 small lots totaling approximately 191,915 square feet of residential floor area. The multi-family unit mix is diverse and may include, 26 studio units, 34 one-bedroom units, and 19 two-bedroom units of varying sizes and configurations. The small lot unit mix is diverse and may include 20 two-bedroom units and 41 three-bedroom units of varying size and configuration.

The mixed-use buildings would include a residential lobby located on the ground floor. Additional residential amenity space and common open space would be located on the ground floor. Private open space would be provided as private balconies.

### **Commercial Uses**

The two mixed-use buildings would include neighborhood serving ground-floor retail and restaurant uses which totals up to approximately 10,300 square feet of floor area. The commercial uses would include retail and restaurant uses that front Reseda Boulevard.

### **FLOOR AREA**

The Project Site is zoned C2-1VL and P-1VL. The Height District No. 1VL permits an FAR of 1.5 times the total area of the lot. The Project Site consists of 143,552 square feet of total lot area allowing for 211,387 square feet of floor area. The Applicant requests a conditional use permit to average the floor area ratio of the unified development. The Proposed Project would provide approximately 202,215 square feet of floor area for an approximate 1.43:1 FAR averaged across the site.

### **DENSITY**

The Height District No. 1VL also limits the total floor area from exceeding 1.5 times the total area of the lot. The Project Site consists of approximately 140,925 square feet (3.23 acres) of total lot area with a buildable area of 140,925. The allowable floor area on-site is 211,387 square feet. The Applicant requests a conditional use permit to average the floor area of the Unified Development. The Proposed Project would provide a total of approximately 202,215 square feet of floor area for an approximate 1.43:1 FAR averaged across the site. Pursuant to LAMC Section 12.14.C, residential land uses are permitted in a C2 zone with compliance to all the regulations of an R4 Zone. Therefore, the residential uses of the Proposed Project must follow the R4 zone lot area requirements for all portions of buildings used for residential purposes. As such, the minimum lot area per dwelling unit is 400 square feet. The Applicant requests a conditional use permit to average the density of the unified development.

### **BUILDING HEIGHT**

In Height District 1VL, mixed-use buildings are permitted a maximum height of 45 feet above grade and a maximum of three stores. Buildings which are used entirely for residential purposes are limited to number of feet in height, but are not limited to the number of stories. The Proposed Project includes two four-story mixed-used buildings and 61 small lot homes. The two four-story mixed-use buildings will have a maximum roof height of approximately 45 feet above grade and roof structures that extend to a maximum height of 53 feet above grade. The small lot homes would have a maximum roof height of approximately 37 feet above grade. However, due to the close proximity properties zoned RS-1 and RA-1 to the east, the Proposed Project is subject to the requirements of Transitional Height, pursuant to LAMC Section 12.21.1-A,10. The Applicant has requested a determination for the number of stories for the mixed-use buildings and a Zoning Administrator's Determination for transitional building height to allow for a greater building height than required.

The Proposed Project's buildings elevations are depicted in Figure II-8 through II-12.

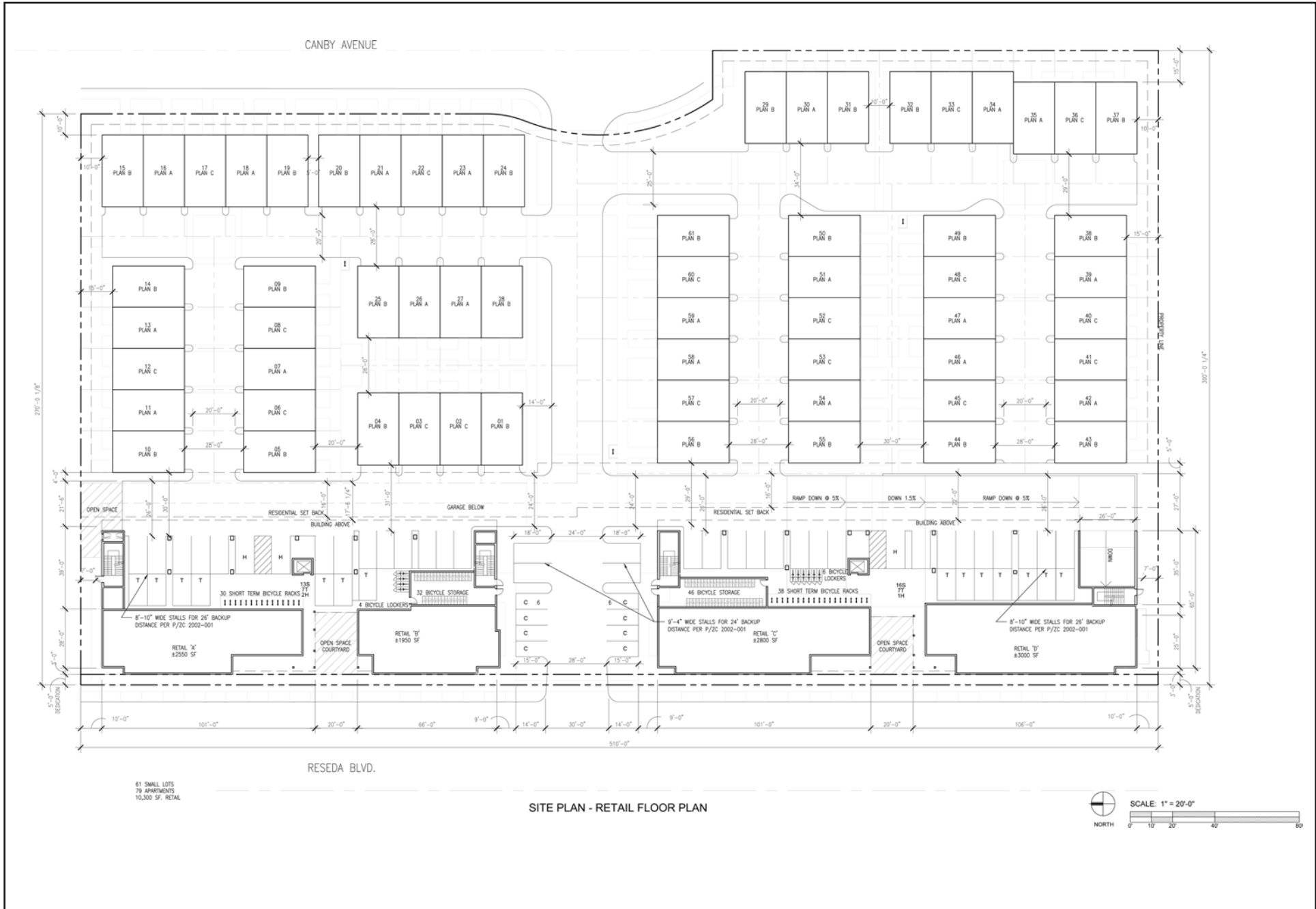
### **SETBACKS**

No front setbacks are required for the Proposed Project. The side and rear setbacks for residential uses in a C2 zone must follow the same setbacks required for a R4 zone at the lowest residential story. Rear yards must be 16 feet plus one foot for each story over the 3<sup>rd</sup> floor. Side yards must be 5 feet plus one foot for every story over the 2<sup>nd</sup> floor. Therefore, the required side and rear yards for the Proposed Project are 7 feet and 16 feet, respectively. The Proposed Project would provide the following: a zero-foot front setback, a 7-foot side setback, and a 16-foot rear setback.

Pursuant to the LAMC Section 12.22.C 27(e) and Small Lot Ordinance (Ordinance No. 176,354), no front, side or rear yard shall be required between lots within an approved small lot subdivision. However, a five-foot setback shall be provided where a lot abuts a lot that is not created pursuant to this subdivision. The Proposed Project would be consistent with this requirement.

### **ARCHITECTURAL FEATURES**

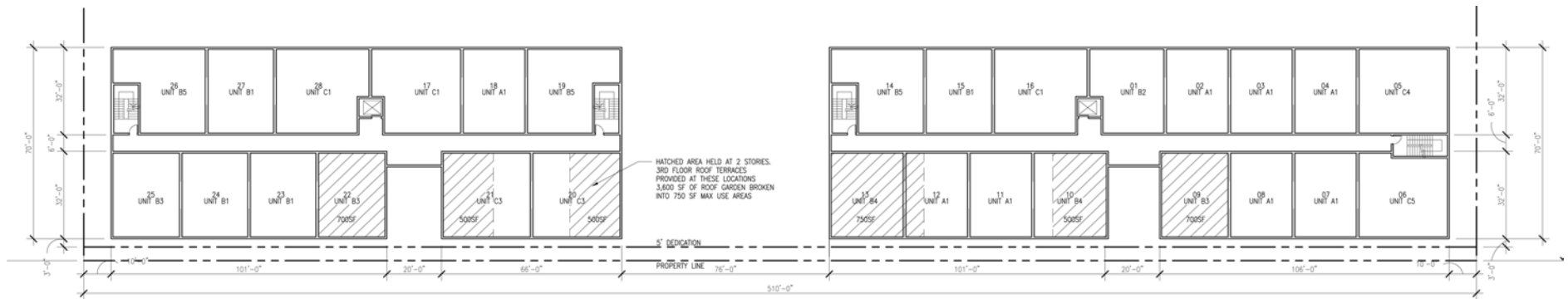
The Proposed Project consists of a unified development with two four-story mixed-use buildings with 79 multi-family dwelling units and 10,300 square feet of ground-floor commercial space and a small lot subdivision with 61 small lot homes. The proposed building would provide one level of subterranean parking and at-grade parking in the mixed-use buildings. Architectural materials and elements for the proposed mixed-use development include cement stucco, prefinished metal panel, cement board siding, aluminum storefront systems, aluminum windows, concrete masonry, and metal railings. The architectural materials and elements for the 61 small lot units include concrete roof tiles, metal gutters and downspouts, cement fascia and stucco, aluminum windows, aluminum stucco reveal, fiberglass doors, shutters, and planter boxes. Building elevations and sections depicting the scale and massing of the proposed structures are shown in Figures II-8 through II-13. Illustrative renderings depicting the scale and massing of the proposed structures are depicted in Figure II-14.



Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



Figure II-6  
Site Plan



TYPICAL RESIDENTIAL FLOOR PLAN

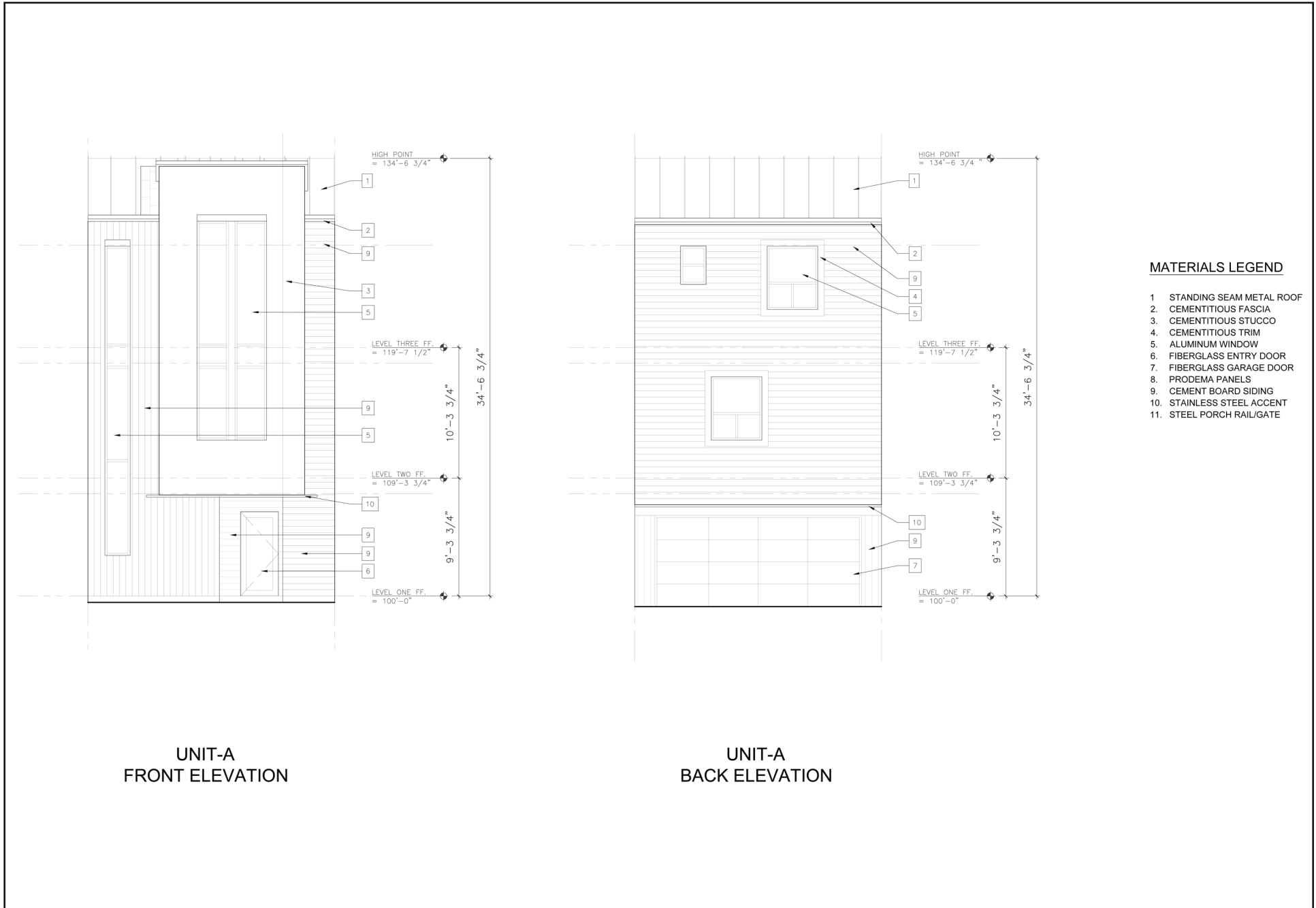
Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



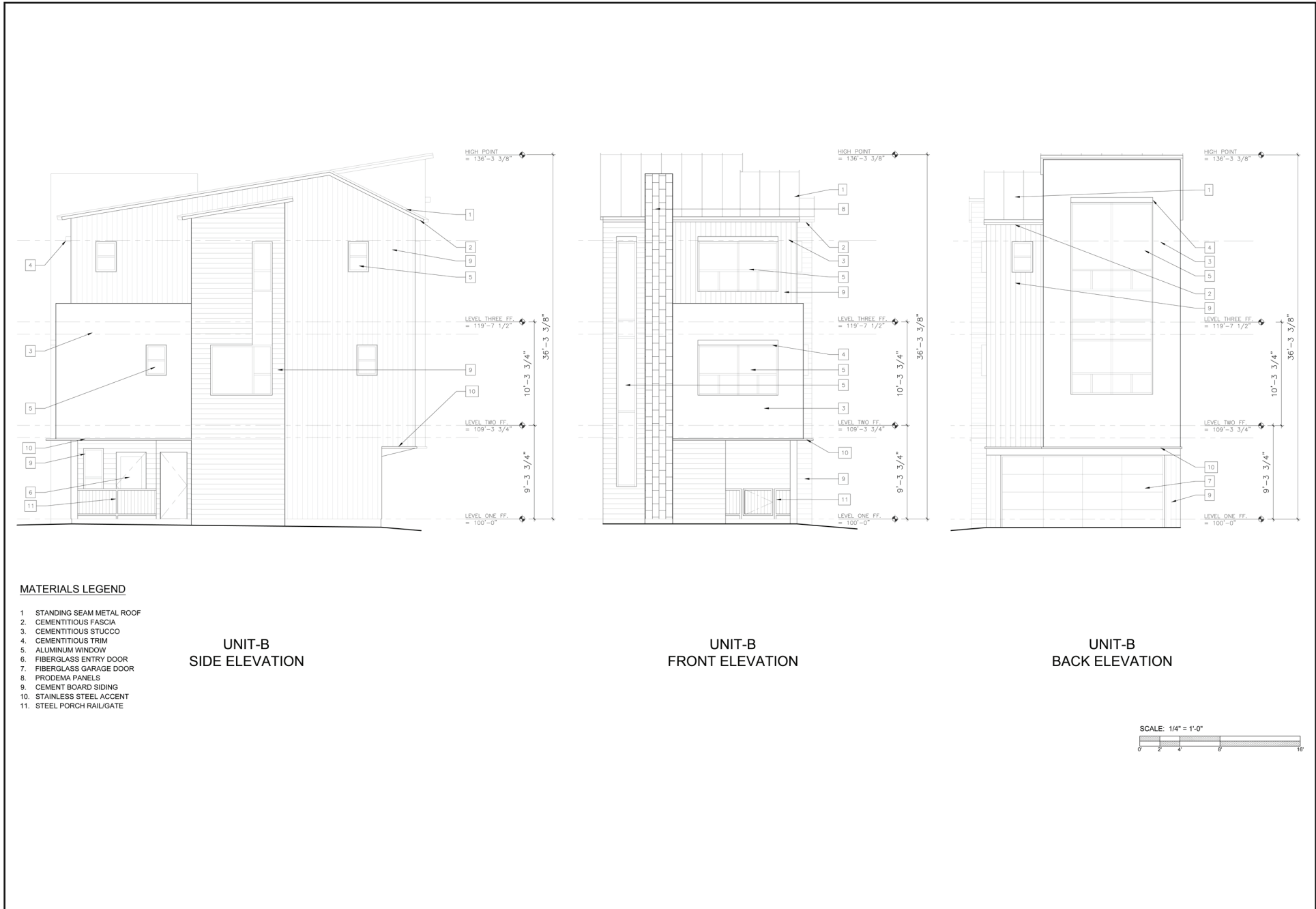
Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



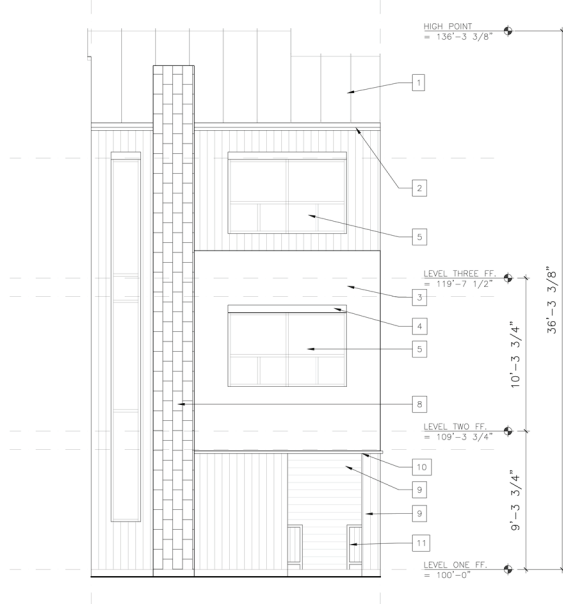
Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



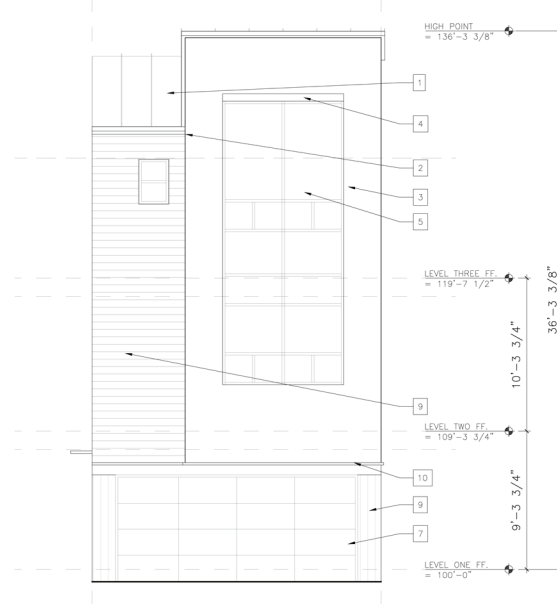
Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



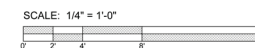
UNIT-C  
FRONT ELEVATION



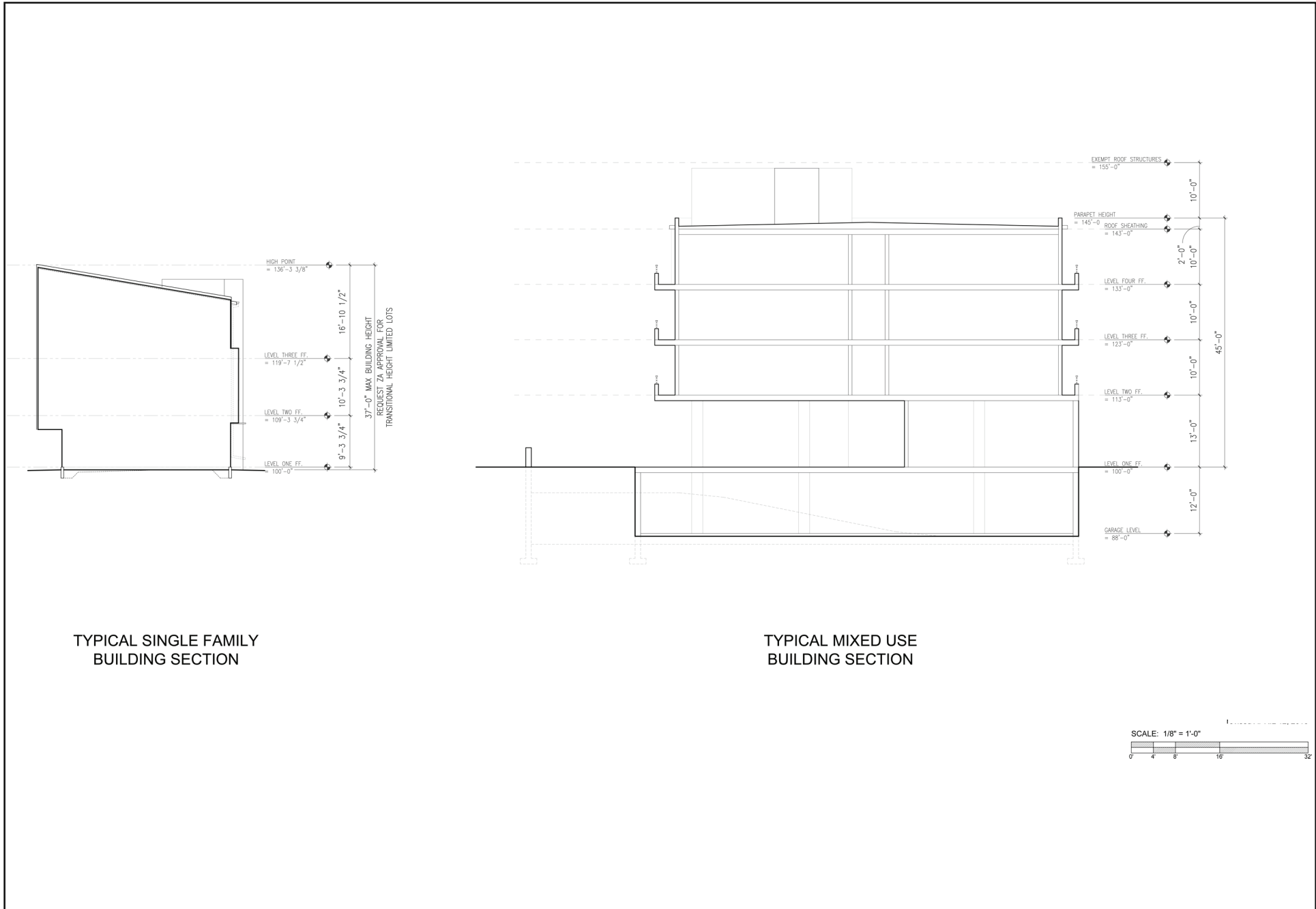
UNIT-C  
BACK ELEVATION

**MATERIALS LEGEND**

- 1. STANDING SEAM METAL ROOF
- 2. CEMENTITIOUS FASCIA
- 3. CEMENTITIOUS STUCCO
- 4. CEMENTITIOUS TRIM
- 5. ALUMINUM WINDOW
- 6. FIBERGLASS ENTRY DOOR
- 7. FIBERGLASS GARAGE DOOR
- 8. PRODEMA PANELS
- 9. CEMENT BOARD SIDING
- 10. STAINLESS STEEL ACCENT
- 11. STEEL PORCH RAILGATE



Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.



View of the North Mixed-Use Building from Reseda Boulevard



View of the South Mixed-Use Building from Reseda Boulevard



View of the Small Lot Single-Family homes from Canby Avenue

Source: Van Tilburg, Banvard and Soderbergh, April 12, 2016.

**OPEN SPACE AND LANDSCAPING**

The open space requirements and amount of open space proposed for the Proposed Project are summarized in Table II-3, Summary of Required and Proposed Open Space Areas, below. The Proposed Project would be required to provide 10,175 square feet of open space for the multi-family units, and the Project Site would provide 10,230 square feet of open space for the multi-family units. The Project’s small lot units do not require common open space. Nevertheless, the Proposed Project would include approximately 17,589 square feet of common open space at grade to serve the residents of the small lot homes. Further, the small lot homes would provide adequate yard setbacks pursuant to the LAMC Section 12.22.C 27(e) and Small Lot Ordinance (Ordinance No. 176,354), which provides that a five-foot setback shall be located on a small lot subdivision lot where it abuts a lot that is not created pursuant to the subdivision. No front, side, or rear yard are required between lots within an approved small lot subdivision. As such, the Proposed Project includes a total of approximately 27,819 square feet of open space to serve Project Site residents and their guests. The Project would also provide one tree per every four units for a total of 20 trees on-site. There are a total of four existing street trees adjacent to the Project Site along Reseda Boulevard. A total of 22 trees currently exist on the Project Site. Of these on-site trees, 13 trees are located along Canby Avenue outside of the property fencing. All of these trees are expected to be removed during construction and replaced in consultation with the City of Los Angeles Division of Urban Forestry and approved by the Board of Public Works. Eighteen of the on-site trees have a diameter over 12 inches and must be replaced on a 1:1 basis with new 24-inch box sized trees.

**Table II-3  
Summary of Required and Proposed Open Space Areas**

<b>LAMC Open Space Requirements</b>	<b>Dwelling Units</b>	<b>Open Space (square feet) <sup>[a]</sup></b>
Small Lot – No Open Space Required	61	0
Less than 3 Habitable Rooms (100 sf/du)	26	2,600
3 Habitable Rooms (125 sf/du)	34	4,250
More than 3 Habitable Rooms (175 sf/du)	19	3,325
<b>Total</b>	<b>79</b>	<b>10,175 sf</b>
<b>Proposed Open Space</b>	<b>Open Space (square feet)</b>	
Small Lot Subdivision	17,589	
Ground-Floor Common Open Space	1,730	
Common Roof Terraces/Garden	3,600	
Common Amenity Room	1,500	
Private Open Space Decks	3,400	
<b>Total</b>	<b>27,819 sf</b>	
<i>Notes: du = dwelling unit; sq = square feet</i> <sup>a</sup> LAMC 12.21 G.2. Open Space Requirement for Six or More Residential Units. For buildings containing 6 or more du per lot: 100 sf per due with less than 3 habitable rooms, 125 sf per du having 3 habitable rooms, and 175 sf per du having more than 3 habitable rooms. Source: Van Tilburg, Banvard & Soderbergh, AIA, March 24, 2016.		

**PARKING AND VEHICLE ACCESS**

Parking for the proposed commercial and residential uses of the mixed-use building would be provided in the one level of subterranean parking and at-grade parking. Each small lot unit in the small lot subdivision would have two parking spaces in each lot. Vehicular access to the Project Site would be provided via a primary driveway on Reseda Boulevard and a secondary driveway along Canby Avenue.

Pursuant to LAMC Section 12.21.A.4(a), for small lot subdivision units, there shall be at least two (2) spaces provided within the boundaries of the parcel or tract map. For other dwelling units, there shall be at least one (1) space for each dwelling unit of less than three habitable rooms, one and one-half (1.5) spaces for each dwelling unit of three habitable rooms, and two (2) spaces for each dwelling unit of more than three habitable rooms. As summarized in Table II-4, and discussed in further detail below, the Proposed Project would require a total of 295 parking spaces, which includes: 115 multi-family residential spaces, 122 small lot residential spaces, and 58 commercial spaces. The Proposed Project would provide 297 spaces which is consistent with the applicable parking requirements of the LAMC.

**Table II-4  
Summary of Required and Proposed Vehicle Parking Spaces**

Description	Quantity	Parking Required <sup>a, b</sup>		Parking Provided
		Rate	Spaces	
<b>Multi-Family Residential</b>				
Units with less than 3 Habitable Rooms	26	1 per du	26	--
Units with 3 Habitable Rooms	34	1.5 per du	51	--
Units with more than 3 Habitable Rooms	19	2 per du	38	--
<i>Subtotal Multi-Family</i>	<b>79 du</b>		<b>115</b>	<b>117</b>
<b>Small Lot Residential</b>				
Small lot homes	61 du	2 per du	122	122
<i>Subtotal Small Lot</i>	<b>61 du</b>		<b>122</b>	<b>122</b>
<b>Commercial</b>				
Retail	5,150 sf	4 per 1,000 sf	21	--
Restaurant	5,150 sf	1 per 100 sf	51	--
Bicycle Reduction <sup>c</sup>	--	20%	-14	--
<i>Subtotal Retail</i>	<b>10,300 sf</b>		<b>58</b>	<b>58</b>
<b>TOTAL</b>			<b>295</b>	<b>297</b>
<i>Notes:</i> du = dwelling unit, sf = square feet <sup>a</sup> LAMC 12.21 A.4.(a). Off-Street Automobile Parking Requirements. 2 spaces on the same lot with one-family du, 1 space per du of less than 3 habitable rooms, 1.5 spaces per du of 3 habitable rooms, and 2 spaces per du of more than 3 habitable rooms. <sup>b</sup> LAMC 12.21 A.4. Off-Street Automobile Parking Requirements. 4 space per 1,000 sf for general retail and 1 per 100 sf for restaurant uses. <sup>c</sup> LAMC 12.24.Y. Bicycle Reduction for providing parking within 1,500 feet of transit facility. Source: Van Tilburg, Banvard & Soderbergh, AIA, March 24, 2016.				

The Proposed Project provides on-site bicycle parking for short-term and long-term bike storage. All bicycle parking for the mixed-use buildings would be provided on the subterranean and at-grade parking levels. As summarized in Table II-5, below, the Proposed Project would be consistent with the applicable parking requirements of the LAMC for bicycle parking spaces.

**Table II-5  
Summary of Required and Proposed Bicycle Parking Spaces**

Mixed-Use Building	Quantity	Parking Required <sup>a</sup>		Total Spaces Required	Total Spaces Provided
		Short Term	Long Term		
<b>Residential</b>		<b>(1 per 10 DUs)</b>	<b>(1 per DU)</b>		
Small Lot Units <sup>b</sup>	61 du	6	--	6	6
Multi-Family Units	79 du	8	79	87	87
<b>Commercial</b>		<b>(1 per 2,000 sf)</b>	<b>(1 per 2,000 sf)</b>		
Commercial	10,300 sf	5	5	10	10
Additional Spaces Required for 20% Parking Reduction		--	--	56	60
<b>TOTAL</b>		<b>19</b>	<b>84</b>	<b>159</b>	<b>163</b>
<i>Notes: du = dwelling unit, sf = square feet</i> <sup>a</sup> LAMC 12.21 A.16. Bicycle Parking and Shower Facilities. <sup>b</sup> Resident long-term for small lot homes not required. Small lot homes require one short-term visitor bicycle space per 10 dwelling units. Source: Van Tilburg, Banvard & Soderbergh, AIA, March 24, 2016.					

## CONSTRUCTION

### *Construction Schedule/Phasing*

For purposes of analyzing impacts associated with air quality, this analysis assumes a Project construction schedule of approximately 30 months, with final buildout occurring in 2019. Construction activities associated with the Project would be undertaken in five main steps: (1) demolition/site clearing, (2) excavation, grading and foundations and (3) vertical building construction (4) paving, and (5) architectural coatings / finishing. The vertical building construction phase includes the construction of the proposed buildings, connection of utilities to the buildings, laying irrigation for landscaping, architectural coatings, paving, and landscaping the Project Site.

#### *Demolition/Site Clearing Phase*

This phase would include the demolition of approximately 27,411 square feet of existing building area and asphalt covered surface parking lots (approximately 2,000 cubic yards of asphalt). In addition, this phase would include the removal of street trees, walls, fences, and associated debris. The demolition/site clearing would be completed in approximately 2 months.

#### *Excavation, Grading and Foundation Phase*

After the completion of demolition/site clearing, the excavation phase for the Proposed Project would occur for approximately 3 months and would involve the cut and fill of land to ensure the proper base and slope for the building foundations. The Proposed Project would require approximately 22,000 cubic yards of soil to be hauled off-site in order to build the subterranean parking garage. Haul trips would occur outside of the peak hours and during the permissible hauling hours identified in the haul route to be approved by the Department of Building and Safety.

### *Building Construction Phase*

The building construction phase consists of below grade and above grade structures and is expected to occur for approximately 20 months. The building construction phase consists of construction of the building foundations, basement walls, parking structure and residential/commercial.

### *Architectural Coating, Finishing, and Paving*

Upon completion of the structures, architectural coating, finishing, and paving would occur. The architectural finishing phase would involve installation of windows, doors, cabinetry, appliances, and would also involve the application of interior and exterior paint and finish-coating materials. It is estimated that the paving would occur for approximately one month, and architectural coatings would occur over the final four months of the construction phase.

### *Temporary Right-of-Way Encroachment*

Construction activities may necessitate temporary lane closures on streets adjacent to the Project Site on an intermittent basis for utility relocations/hook-ups, delivery of materials, and other construction activities as may be required. However, site deliveries and the staging of all equipment and materials would be organized in the most efficient manner possible on-site to mitigate any temporary impacts to the neighborhood and surrounding traffic. Construction equipment would be staged on-site for the duration of construction activities. Traffic lane and right-of-way closures, if required, will be properly permitted by the City agencies and will conform to City standards.

Unless stated otherwise, all construction activities would be performed in accordance with all applicable state and federal laws and City Codes and policies with respect to building construction and activities. As provided in Section 41.40 of LAMC, the permissible hours of construction within the City are 7:00 a.m. to 9:00 p.m. Monday through Friday, and between 8:00 a.m. and 6:00 p.m. on any Saturday or national holiday. No construction activities are permitted on Sundays. The Proposed Project would comply with these restrictions.

### ***Haul Route***

All construction and demolition debris would be recycled to the maximum extent feasible. Demolition debris and soil materials from the Site that cannot be recycled or diverted would be hauled to the Sunshine Canyon or Chiquita Canyon landfills, which accept construction and demolition debris and inert waste from areas within the City of Los Angeles. The Sunshine Canyon Landfill is approximately 10 miles north of the Project Site (approx. 20 miles round trip). The Chiquita Canyon Landfill is approximately 23 miles to the north of the Project Site (approx. 46 miles round trip). For recycling efforts, the Recology Los Angeles Facility and East Valley Diversion Facility both accept construction

and demolition waste for recycling and are both located in Sun Valley approximately 13 and 15 miles east of the Project Site, respectively.<sup>1</sup>

For purposes of analyzing the construction-related impacts, it is anticipated that the excavation and soil export would involve 18-wheel bottom-dump trucks with a 20 cubic yard hauling capacity (i.e., 30 tons maximum gross weight). All truck staging would either occur on-site or at designated off-site locations and radioed into the site to be filled. Traveling from the Project Site, the proposed haul route can utilize either the Ronald Reagan (CA-118) Freeway. Traveling to and from the 118 Freeway, the haul route would utilize the Reseda Boulevard on- and off-ramp. The haul routes specified above may be modified in compliance with City policies, provided DOT and/or Street Services approves any such modification.

## RELATED PROJECTS

In accordance with CEQA Guidelines Section 15064(h), this IS/MND includes an evaluation of the Project's cumulative impacts. The guidance provided under CEQA Guidelines Section 15064 (h) is as follows:

*“(1) When assessing whether a cumulative effect requires an EIR, the lead agency shall consider whether the cumulative impact is significant and whether the effects of the project are cumulatively considerable. An EIR must be prepared if the cumulative impact may be significant and the project's incremental effect, though individually limited, is cumulatively considerable. “Cumulatively considerable” means that the incremental effects of an individual project are significant when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.*

*(2) A lead agency may determine in an initial study that a project's contribution to a significant cumulative impact will be rendered less than cumulatively considerable and thus is not significant. When a project might contribute to a significant cumulative impact, but the contribution will be rendered less than cumulatively considerable through mitigation measures set forth in a mitigated negative declaration, the initial study shall briefly indicate and explain how the contribution has been rendered less than cumulatively considerable.*

*(3) A lead agency may determine that a project's incremental contribution to a cumulative effect is not cumulatively considerable if the project will comply with the requirements in a previously approved plan or mitigation program (including, but not limited to, water quality control plan, air quality attainment or maintenance plan, integrated waste management plan, habitat conservation plan, natural community conservation plan, plans or regulations for the reduction of greenhouse gas emissions) that provides specific requirements that will avoid or substantially lessen the cumulative problem within the geographic area in which the project is located. Such plans or programs must be specified in law or adopted by the public agency with jurisdiction over the affected resources through a public review process to implement, interpret, or make*

---

<sup>1</sup> Los Angeles Department of Public Works, Construction and Demolition Debris Recycling Facilities in Los Angeles County, website: [http://ladpw.org/epd/CD/cd\\_attachments/Recycling\\_Facilities.pdf](http://ladpw.org/epd/CD/cd_attachments/Recycling_Facilities.pdf), accessed October 2015.

---

*specific the law enforced or administered by the public agency. When relying on a plan, regulation or program, the lead agency should explain how implementing the particular requirements in the plan, regulation or program ensure that the project's incremental contribution to the cumulative effect is not cumulatively considerable. If there is substantial evidence that the possible effects of a particular project are still cumulatively considerable notwithstanding that the project complies with the specified plan or mitigation program addressing the cumulative problem, an EIR must be prepared for the project.*

*(4) The mere existence of significant cumulative impacts caused by other projects alone shall not constitute substantial evidence that the proposed project's incremental effects are cumulatively considerable.”*

In light of the guidance summarized above, an adequate discussion of a project's significant cumulative impact, in combination with other closely related projects, can be based on either: (1) a list of past, present, and probable future producing related impacts; or (2) a summary of projections contained in an adopted local, regional, statewide plan, or related planning document that describes conditions contributing to the cumulative effect. (CEQA Guidelines Section 15130(b)(1)(A)-(B)). The lead agency may also blend the “list” and “plan” approaches to analyze the severity of impacts and their likelihood of occurrence. Accordingly, all proposed, recently approved, under construction, or reasonably foreseeable projects that could produce a related or cumulative impact on the local environment, when considered in conjunction with the Project, were identified for evaluation.

The related projects identified are included in Table II-6, Related Projects List, below. A total of nine related projects were identified within the affected Project area. An analysis of the cumulative impacts associated with these related projects and the Project are provided under each individual environmental impact category in Section III of this IS/MND. The locations of the related projects are shown in Figure II-15, Location of Related Projects.

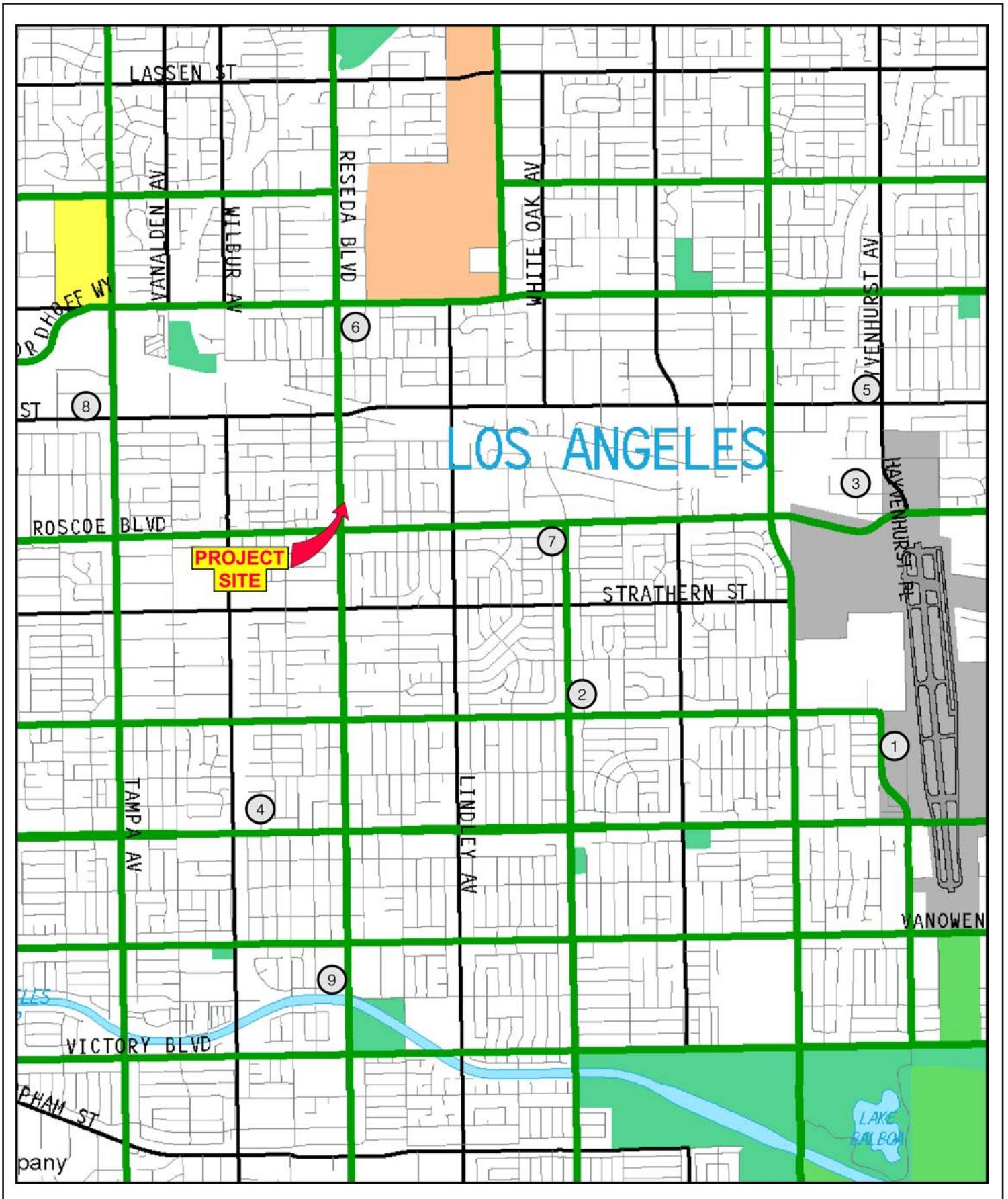
**Table II-6  
Related Projects List**

<b>Project Number</b>	<b>Location/Address</b>	<b>Project Description</b>	<b>Size</b>	<b>Units</b>
1	7501 Hayvenhurst Avenue	Van Nuys LAWA Master Plan	--	--
2	9750 White Oak Avenue	Condominiums	173	du
3	16719 Schoenborn Street	Industrial	80,133	sf
4	7251 Amigo Avenue	Apartments Senior Housing	154 56	du du
5	16617 Parthenia Street	Private School	265	stu
6	9010 Reseda Boulevard	Retail	7,800	sf
7	17710 Roscoe Boulevard	McDonalds	4,587	sf
8	19401 Parthenia Street	Apartments Retail Storage	312 35,694 7,470	du sf sf
9	6611 Reseda Boulevard	Apartments Retail	254 8,256	du sf

*Notes: du = dwelling unit, sf = square feet, emp = employee, stu = student*

*All Related Project information comes from the Traffic Study unless otherwise stated.*

*Source: Overland Traffic Consultants, Inc., Traffic Impact Analysis for a Mixed-Use Development Located at 8350-8430 Reseda Boulevard, City of Los Angeles, March 2016.*



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.

---

## II. PROJECT DESCRIPTION

### C. ENTITLEMENT REQUESTS

---

Harridge Cadillac Group, LLC (“Applicant”) is requesting approval of the following discretionary actions:

- (1) **Vesting Tentative Tract Map for Small Lot Subdivision:** The Applicant is requesting a vesting tentative tract map for a small lot subdivision to allow 61 small lot single family dwellings and one mixed-use lot.
- (2) **Zone Change from C2-1VL and P-1VL to C2-1VL:** The Applicant is requesting a zone change from “footprint zoning” of C2-1VL and P-1VL to C2-1VL.
- (3) **Site Plan Review to Permit a Total of 140 Residential Units:** The Project will create, or result in an increase of, 50 or more dwelling units. As such, it must obtain Site Plan Review before a grading permit, foundation permit, building permit, or use of the land permit is issued pursuant to LAMC Section 16.05.
- (4) **Conditional Use Permit to Permit Floor Area and Density Averaging in a Unified Development:** The Applicant is requesting a Conditional Use to permit floor area and density averaging in this unified development pursuant to LAMC Section 12.24.W.19. In connection with this request, the decision maker also has authority to grant deviations from area and height regulations under 12.24F.
- (5) **Zoning Administrator’s Determination for Transitional Height:** The Applicant is requesting for relief under LAMC §12.24-X.22 to allow buildings of a height greater than the limits set forth in LAMC §12.21.1-A.10.

The Applicant will also request approvals and permits from the Department of Building and Safety (and other municipal agencies) for project construction activities which may include, but are not limited to, the following: excavation, shoring, grading, foundation, haul route (for the export of approximately 2,000 cy of asphalt debris and 22,000 cy of soil), removal of street trees, and building and tenant improvements for the Project Site.

---

## III. ENVIRONMENTAL IMPACT ANALYSIS

---

### INTRODUCTION

This section of the Initial Study contains an assessment and discussion of impacts associated with the environmental issues and subject areas identified in the Initial Study Checklist (Appendix G to the State CEQA Guidelines, C.C.R. Title 14, Chapter 3, 15000-15387). The analytical methodology and thresholds of significance are based on the *L.A. CEQA Thresholds Guide (2006)* unless otherwise noted.

As discussed in Section II, Project Description, the Proposed Project includes the construction of a unified development with two, four-story mixed-use buildings with up to 79 dwelling units (26 studio units, 34 one-bedroom units, and 19 two-bedroom units) and approximately 10,300 square feet of commercial floor area, and the development of a small lot subdivision improved with 61 three-story small lot residential units. The intent of the Proposed Project is to comply with the LAMC with respect to vehicle and bicycle parking, on-site open space, landscaping (trees), and all applicable school and recreation fees associated with residential units.

### ENVIRONMENTAL IMPACT ANALYSIS

#### I. AESTHETICS

##### a) Would the project have a substantial adverse effect on a scenic vista?

**Less Than Significant Impact.** The City's CEQA thresholds provide that a significant impact may occur if the Proposed Project were to allow development to occur in an existing natural open space area or has the potential to introduce features that would block or detract from the existing valued aesthetic quality of a scenic vista. Scenic vistas are generally described in two ways: panoramic views (visual access to a large geographic area, for which the field of view can be wide and extend into the distance) and focal views (visual access to a particular object, scene, or feature of interest). The Project Site is currently developed with 21,986 square feet of auto service/repair shops, a 5,425 square foot commercial office building, and surface parking areas.<sup>1</sup> There are four street trees along Reseda Boulevard, and 22 trees on-site (13 of which are along Canby Avenue). There are no protected trees located on the Project Site.

---

<sup>1</sup> *Overland Traffic Consultants, Inc. Traffic Impact Analysis for a Mixed-Use Development, Located at 8350-8430 Reseda Boulevard, City of Los Angeles, March 2016.*

The Proposed Project would not block or detract from the existing valued aesthetic quality of a scenic vista. The Project Site is not bordered by or within the viewshed of any designated scenic highway.<sup>2</sup> As discussed above, the Project Site is currently developed with approximately 21,986 square feet of automobile services, 5,425 square feet of commercial office land uses, and street-level parking areas. The Project includes the demolition of the existing buildings on-site and the construction of a unified development with two mixed-use commercial and residential buildings and a small lot subdivision with 61 small lot residential units. The heights for the buildings would reach a maximum of 45 feet above grade for the mixed-use buildings and 37 feet above grade for the 61 small lot homes. The Project would not block or detract from the existing valued aesthetic quality of a scenic vista. Impacts of the Proposed Project on a scenic vista would be less than significant.

**b) Would the project substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a city-designated scenic highway?**

**Less Than Significant Impact.** The City's CEQA thresholds provide that a significant impact may occur if scenic resources would be damaged and/or removed by development of a project. The Project Site is currently developed with 21,986 square feet of automobile services, a 5,425 square foot commercial office, and surface parking areas. According to SurveyLA, the northern property, the Neill Lehr Cadillac Showroom located at 8400 N. Reseda Boulevard, appears to be eligible for historic status as a National Register, California Register and for local listing through survey evaluation. The property contains a car showroom that proves as an example of a 1960s automobile showroom in Northridge, exhibiting the essential characteristics of the property type.<sup>3</sup> However, as described in the Historic Resources Report, GPA Consulting determined that while the showroom section of the property displays a late example of its type and retains integrity, large sections to the rear and sides of the showroom have been altered. As a whole, the property is not a significant and intact example of an automobile showroom. GPA Consulting determined that the car showroom on-site is not a significant cultural or historic resource. (See also, Response to Checklist Question V(a), below.) As such, a less than significant impact would occur to historic buildings.

Additionally, the Project Site is entirely developed, and no unique vegetation or unique geologic features exist on-site. Therefore, the Proposed Project would not damage and/or remove any scenic resources within a State or City designated scenic highway, and a less than significant impact would occur.

---

<sup>2</sup> *City of Los Angeles, Department of City Planning, Mobility Element 2035, An Element of the General Plan, Appendix B, January 20, 2016.*

<sup>3</sup> *SurveyLA, Northridge Report Individual Resources, July 13, 2015. website: <http://preservation.lacity.org/sites/default/files/Northridge%20Individual.pdf>, accessed October 2015.*

**c) Would the project substantially degrade the existing visual character or quality of the site and its surroundings?**

**Less Than Significant Impact.** The City’s CEQA thresholds provide that a significant impact may occur if the Proposed Project were to introduce features that would detract from the existing valued aesthetic quality of a neighborhood, community, or localized area by conflicting with important aesthetic elements or the quality of the area (such as theme, style, setbacks, density, massing, etc.) or by being inconsistent with applicable design guidelines. The Proposed Project would be required to comply with all applicable building code requirements, some of which are listed below. Thus, with adherence to the following regulatory code compliance measures, impacts related to the general aesthetic appearance, maintenance, and character of the Project Site would be less than significant.

- **Regulatory Compliance Measure RC-AES-1 (Vandalism):** The project shall comply with all applicable building code requirements, including the following:
  - Every building, structure, or portion thereof, shall be maintained in a safe and sanitary condition and good repair, and free from, debris, rubbish, garbage, trash, overgrown vegetation or other similar material, pursuant to Municipal Code Section 91.8104.
  - The exterior of all buildings and fences shall be free from graffiti when such graffiti is visible from a street or alley, pursuant to Municipal Code Section 91.8104.15.
- **Regulatory Compliance Measure RC-AES-2 (Signage):** Compliance with provisions of the Los Angeles Building Code. The project shall comply with the Los Angeles Municipal Code Section 91.6205, including on-site signage maximums and multiple temporary sign restrictions, as applicable.
- **Regulatory Compliance Measure RC-AES-3 (Signage on Construction Barriers):** The project shall comply with the Los Angeles Municipal Code Section 91.6205, including but not limited to the following provisions:
  - The applicant shall affix or paint a plainly visible sign, on publically accessible portions of the construction barriers, with the following language: “POST NO BILLS.”
  - Such language shall appear at intervals of no less than 25 feet along the length of the publicly accessible portions of the barrier.
  - The applicant shall be responsible for maintaining the visibility of the required signage and for maintaining the construction barrier free and clear of any unauthorized signs within 48 hours of occurrence.

*Building Heights and Massing*

The Project is currently zoned C2-1VL and P-1VL within Height District No. 1VL, which is defined as a Very Limited Height District. The Height District 1VL limits the proposed buildings to three stories, and it shall not exceed 45 feet above grade. However, a building in Height District 1VL designed and used entirely for residential purposes shall be limited to the number of feet in height, but not the number of stories. Further, pursuant to LAMC Section 12.21.1 A.10, the buildings in a C zone within 50 to 99 feet from a RW1 Zone or a more restrictive zone is limited to 33 feet above grade. As it pertains to the Proposed Project, the land uses to the east of the Project Site are zoned RS-1 and RA-1, which are more

restrictive than the RW1 zone. As such, the small lot homes along the eastern property line are limited to 33 feet above grade. The remaining small lot homes would be limited to 45 feet above grade. The small lot homes would include three-stories and reach a maximum height of 37 feet above grade, which is only a few feet taller than allowed by the LAMC. The mixed-use buildings along Reseda Boulevard would include a maximum of 45 feet above grade. The Applicant requests for a zoning administrator's determination for transitional building heights to allow for a greater building height than required. With approval of the zoning administrator's determination for transitional height, the Proposed Project would be consistent with the height requirements for the Project Site.

#### *Shade/Shadow*

Shadows from buildings and natural vegetation or landscaping features are a general condition of the urbanized environment, and is considered an aesthetic issue by the City of Los Angeles to be evaluated during the CEQA review. In accordance with the *L.A. CEQA Thresholds Guide*, "facilities and operations sensitive to the effects of shading include: routinely useable outdoor spaces associated with residential, recreational, or institutional (e.g., schools, convalescent homes) land uses; commercial uses such as pedestrian oriented outdoor spaces or restaurants with outdoor eating areas; nurseries; and existing solar collectors." These land uses are termed "shadow-sensitive" because sunlight is important to function, physical comfort of commerce. Pursuant to the *L.A. CEQA Thresholds Guide*, a shading impact would normally be considered significant if the Proposed Project's structures cast shadows on a shadow sensitive land use for more than three hours each day between the hours of 9:00 a.m. and 3:00 p.m. Pacific Standard Time between late October and early April, or for more than four hours between the hours of 9:00 a.m. and 5:00 p.m. Pacific Daylight Time between early April and late October.

The proposed buildings would range from three stories for the small lot residential units with a maximum height of 37 feet above grade, and four stories for the mixed-use buildings with a maximum height of 45 feet above grade. Pursuant to the *L.A. CEQA Thresholds Guide*, detailed shade and shadow analysis is warranted if a proposed structure is over 60 feet in height and is located within the vicinity of shadow sensitive land uses. Because the height of the proposed structures are well below 60 feet, the shade and shadow impacts of the Proposed Project would be minimal. As a result of the relatively short shadow lengths, and the orientation and setbacks of structure and yards within each developed lot, no shade sensitive land uses would be adversely impacted by the Proposed Project's projected shadow patterns to the immediate north, east, south, or west of the Project Site. Therefore, with respect to shade/shadow, a less than significant impact would occur.

#### **d) Would the project create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?**

**Potentially Significant Unless Mitigation Incorporated.** The City's CEQA thresholds provide that a significant impact may occur if a project introduces new sources of light or glare on or from the project site which would be incompatible with the areas surrounding the project site, or which pose a safety hazard to motorists utilizing adjacent streets or freeways. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether the Proposed Project results in a significant nighttime illumination impact shall be

made considering the following factors: (a) the change in ambient illumination levels as a result of proposed project sources; and (b) the extent to which proposed project lighting would spill off the Project Site and affect adjacent light-sensitive areas.

### *Light*

Lighting for the Proposed Project would be provided in order to illuminate the building entrances, common open space areas, and parking areas, largely to provide adequate night visibility for retailers, patrons, and residents and to provide a measure of security. All outdoor lighting would be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties or the public right-of-way. A moderate degree of illumination already exists in the Project vicinity in the form of streetlights, building lighting, and car headlights along Reseda Boulevard. The Proposed Project would not generate a substantial increase in ambient lighting as the majority of lighting would be directed towards the interior of the Project Site and away from any nearby land uses. The Proposed Project may include thematic up-lighting to accentuate the architectural features of the proposed mixed-use buildings and individual small lot homes. A new vehicular driveway would be utilized along Canby Avenue to provide secondary access to the Project Site. The Project's lighting fixtures would be installed and operated in accordance with 99.05.106.8 (Light Pollution Reduction) of the City of Los Angeles Green Building Code, which requires outdoor lighting systems to be designed and installed to comply with the minimum requirements in the California Energy Code, or comply with a local ordinance, whichever is more stringent. To ensure the lighting sources are not directly visible by adjacent properties, Mitigation Measure AES-1 would require all lighting fixtures to include directional shields and to be positioned in a manner that the direct light source is not visible from adjacent land uses. Vehicular access to and from the Project Site would be provided from driveways off Reseda Boulevard and Canby Avenue. The Proposed Project would not introduce any new sources of substantial light that are incompatible with the surrounding areas. With implementation of the Mitigation Measure AES-1, any Proposed Project's lighting impacts would be reduced to less than significant.

### *Glare*

Potential reflective surfaces in the Project vicinity include automobiles traveling and parked on streets, exterior building windows, and surfaces of brightly painted buildings. Excessive glare not only restricts visibility, but also increases the ambient heat reflectivity in a given area. The Proposed Project would not introduce any new substantial sources of glare that are incompatible with the surrounding areas. The exterior of the Proposed Project's buildings would be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat. With the incorporation of Mitigation Measure AES-2, below, the Project's potential impacts related to glare would be reduced to a less than significant level.

**Mitigation Measures:****AES-1 Aesthetics (Light)**

- Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties, the public right-of-way, nor from above.

**AES-2 Aesthetics (Glare)**

- The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat.

**Cumulative Impacts**

**Less Than Significant Impact.** The Proposed Project would have a less than significant impact on Aesthetics, as discussed above. Development of the Proposed Project in conjunction with the nine related projects would result in an intensification of existing prevailing land uses in an already urbanized area within the City of Los Angeles. Development of the related projects is expected to occur in accordance with adopted plans and regulations. With respect to the overall visual quality of the surrounding neighborhood, each of the related projects would be subject to site plan review by the Los Angeles Department of City Planning for review and approval. The site plan review process would ensure each project is designed and constructed in a manner that is consistent with and compatible with the existing urban form and character of the surrounding environment. Therefore, cumulative aesthetic impacts would be less than significant.

**II. AGRICULTURE AND FORESTRY RESOURCES****a) Would the project convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?**

**No Impact.** The Project Site is located in a developed area within the Northridge area of the City of Los Angeles. No farmland or agricultural activity exists on the Project Site, nor are there any farmland or agricultural activities in the vicinity of the Project Site. According to the “Los Angeles County Important Farmland 2012” map, which was prepared by the California Department of Conservation, Division of Land Resource Protection, the soils at the Project Site are not candidate for listing as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance.<sup>4</sup> Therefore, no impact to agricultural lands would occur.

---

<sup>4</sup> *State of California Department of Conservation, Division of Land Resource Protection, Farmland Mapping and Monitoring Program, Los Angeles County Important Farmland 2012 Map, January 2015.*

**b) Would the project conflict with existing zoning for agricultural use, or a Williamson Act Contract?**

**No Impact.** The Project Site is located within the jurisdiction of the City of Los Angeles and is, therefore, subject to the applicable land use and zoning requirements in the Los Angeles Municipal Code (LAMC). The Project Site is currently zoned C2-1VL and P-1VL with a General Plan land use designation of General Commercial and is not zoned for agricultural production, and no farmland activities exist on-site. In addition, no Williamson Act Contracts are in effect for the Project Site.<sup>5</sup> Therefore, no impact would occur.

**c) Would the project conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?**

**No Impact.** The Project Site is zoned C2-1VL and P-1VL, which has a land use designation of General Commercial in the Northridge Community Plan Area. The Project Site is not zoned as forestland or timberland, and there is no timberland production at the Project Site. Therefore, no impact would occur.

**d) Would the project result in the loss of forest land or conversion of forest land to non-forest use?**

**No Impact.** The Project Site is fully developed and currently contains auto service/repair shops, a commercial office building and associated street-level parking areas. The Project Site is located in a developed area within the Northridge community in the City of Los Angeles. There is no significant vegetation on-site. No forested lands or protected vegetation exist on or in the vicinity of the Project Site. Therefore, no impact would occur.

**e) Would the project involve other changes in the existing environment, which due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?**

**No Impact.** Neither the Project Site, nor nearby properties, are currently utilized for agricultural or forestry uses. As discussed above, the Project Site is not classified in any "Farmland" category designated by the State of California. According to the "Los Angeles County Important Farmland 2012" map, which was prepared by the California Department of Conservation, Division of Land Resource Protection, the soils at

---

<sup>5</sup> *Department of Conservation, Land Conservation Act Maps, 2012 State of California Williamson Act Contract Land*, website <http://www.conservation.ca.gov/dlrp/lca/Pages/Index.aspx>, accessed October 2015.

the Project Site is not candidates for listing as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance.<sup>6</sup> Therefore, no impact would occur.

### **Cumulative Impacts**

**No Impact.** Development of the Proposed Project in combination with the nine related projects would not result in the conversion of State-designated agricultural land from agricultural use to a non-agricultural use, nor result in the loss of any forest land or conversion of forest land to non-forest use. The Los Angeles County Important Farmland 2012 Map maintained by the California Division of Land Resource Protection indicates that the Project Site and the surrounding area are not included in the Important Farmland category.<sup>7</sup> The Project Site is located in an urbanized area in the Northridge Community Plan area within the City of Los Angeles and does not include any State-designated agricultural lands or forest uses. Therefore, no cumulative impact would occur.

### **III. AIR QUALITY**

#### **a) Would the project conflict with or obstruct implementation of the applicable air quality plan?**

**Less Than Significant Impact.** A significant air quality impact could occur if the Proposed Project is not consistent with the applicable Air Quality Management Plan (AQMP) or would in some way represent a substantial hindrance to employing the policies or obtaining the goals of that plan. The most recent AQMP was adopted by the Governing Board of the South Coast Air Quality Management District (SCAQMD) on December 7, 2012 (“Final 2012 AQMP”). The transportation strategy and transportation control measures (TCMs), included as part of the 2012 AQMP and SIP for the South Coast Air Basin, are based on SCAG’s adopted 2012-2035 Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS) and 2011 Federal Transportation Improvement Program (FTIP). For purposes of assessing a project’s consistency with the AQMP, Projects that are consistent with the growth forecast projections of employment and population forecasts identified in the 2016-2040 RTP/SCS are considered consistent with the AQMP, since the growth projections contained in the 2016-2040 RTP/SCS from the basis of the land use and transportation control portions of the AQMP.

The Proposed Project is an infill development that would result in the demolition of the existing buildings and the construction of a unified development with two four-story mixed-use buildings (including 79 apartment units and 10,300 square feet of commercial uses) and a small lot subdivision with 61 small lot homes. The proposed mixed-use buildings and small lot homes would redevelopment the commercial land uses and re-activate the Project Site with new residential and neighborhood serving retail and restaurant

---

<sup>6</sup> *State of California Department of Conservation, Division of Land Resource Protection, Farmland Mapping and Monitoring Program, Los Angeles County Important Farmland 2012 Map, January 2015.*

<sup>7</sup> *Ibid.*

land uses that are substantially consistent with the allowable uses under the current zoning. As such, the Proposed Project is consistent with the regional growth projections for the Los Angeles Subregion. Furthermore, as discussed in Question XVI (a), the Project Site is served by four Metro bus routes and one LADOT DASH bus route. Studies by the California Department of Transportation, the U.S. Environmental Protection Agency and the Metropolitan Transportation Commission have found that focusing development in areas served by transit can result in local, regional and statewide benefits including reduced air pollution and energy consumption. As discussed in the Project's Traffic Study (see Appendix G to this MND), the Proposed Project's close proximity to surrounding commercial areas and the availability of alternative transit options in the vicinity would result in fewer trips and a reduction to the Proposed Project's vehicle miles traveled (VMTs) as compared to the base trip rates for similar stand-alone land uses. Thus, because the Proposed Project would be consistent with the growth projections and regional land use planning policies of the 2016-2040 RTP/SCS, the Project would not conflict with or obstruct implementation of the 2012 AQMP. Project impacts would be less than significant.

**b) Would the project violate any air quality standard or contribute substantially to an existing or projected air quality violation?**

**Less Than Significant Impact.** Based on the *L.A. CEQA Thresholds Guide*, a project may have a significant impact where project-related emissions would exceed federal, State, or regional standards or thresholds, or where project-related emissions would substantially contribute to an existing or projected air quality violation.

**Construction Emissions**

For purposes of analyzing impacts associated with air quality, this analysis assumes a construction schedule of approximately 30 months with buildout anticipated in 2019. This assumption is conservative and yields the maximum daily impacts. Construction activities associated with the Proposed Project would be undertaken in five main steps: (1) demolition/site clearing, (2) excavation, grading and foundations and (3) building construction, (4) paving, and (5) architectural coatings / finishing. The construction phases include the demolition of the existing commercial buildings, construction of the proposed buildings, connection of utilities to the buildings, laying irrigation for landscaping, architectural coatings, paving, and landscaping the Project Site. Construction activities would temporarily create emissions of dusts, fumes, equipment exhaust, and other air contaminants. Construction activities involving site excavation, grading and foundation preparation would primarily generate PM<sub>2.5</sub> and PM<sub>10</sub> emissions. Mobile sources (such as diesel-fueled equipment on-site and traveling to and from the Project Site) would primarily generate NO<sub>x</sub> emissions. The application of architectural coatings would primarily result in the release of ROG emissions. The amount of emissions generated on a daily basis would vary, depending on the amount and types of construction activities occurring at the same time.

The Proposed Project's construction emissions were quantified utilizing the California Emissions Estimator Model (CalEEMod *Version 2013.2.2*) as recommended by the SCAQMD. Table III-1, Estimated Peak Daily Construction Emissions, identifies the maximum daily emissions that are estimated to occur on peak construction days for each phase of project construction. These calculations assume that appropriate dust

control measures would be implemented as part of the Proposed Project during each phase of development, as required and regulated by SCAQMD. For purposes of this analysis, the following regulatory compliance measures have been identified as being applicable to the Proposed Project's construction activities:

- **Regulatory Compliance Measure RC-AQ-1** (Demolition, Grading and Construction Activities): Compliance with provisions of the SCAQMD District Rule 403. The project shall comply with all applicable standards of the Southern California Air Quality Management District, including the following provisions of District Rule 403:
  - a) All unpaved demolition and construction areas shall be wetted at least twice daily during excavation and construction, and temporary dust covers shall be used to reduce dust emissions and meet SCAQMD District Rule 403. Wetting could reduce fugitive dust by as much as 50 percent.
  - b) The construction area shall be kept sufficiently dampened to control dust caused by grading and hauling, and at all times provide reasonable control of dust caused by wind.
  - c) All clearing, earth moving, or excavation activities shall be discontinued during periods of high winds (i.e., greater than 15 mph), so as to prevent excessive amounts of dust.
  - d) All dirt/soil loads shall be secured by trimming, watering or other appropriate means to prevent spillage and dust.
  - e) All dirt/soil materials transported off-site shall be either sufficiently watered or securely covered to prevent excessive amount of dust.
  - f) General contractors shall maintain and operate construction equipment so as to minimize exhaust emissions.
  - g) Trucks having no current hauling activity shall not idle but be turned off.
  
- **Regulatory Compliance Measure RC-AQ-2:** In accordance with Sections 2485 in Title 13 of the California Code of Regulations, the idling of all diesel fueled commercial vehicles (weighing over 10,000 pounds) during construction shall be limited to five minutes at any location.
  
- **Regulatory Compliance Measure RC-AQ-3:** In accordance with Section 93115 in Title 17 of the California Code of Regulations, operation of any stationary, diesel-fueled, compression-ignition engines shall meet specified fuel and fuel additive requirements and emission standards.
  
- **Regulatory Compliance Measure RC-AQ-4:** The Project shall comply with South Coast Air Quality Management District Rule 1113 limiting the volatile organic compound content of architectural coatings.

As shown in Table III-1, construction-related daily emissions associated with the Proposed Project would not exceed any regional SCAQMD significance thresholds for criteria pollutants during the construction phases. Therefore, construction impacts are considered to be less than significant.

**Table III-1  
Estimated Peak Daily Construction Emissions**

<b>Emission Source</b>	<b>ROG</b>	<b>NO<sub>x</sub></b>	<b>CO</b>	<b>SO<sub>2</sub></b>	<b>PM<sub>10</sub></b>	<b>PM<sub>2.5</sub></b>
<b>Demolition</b>						
On-Site Fugitive Dust	--	--	--	--	2.20	0.33
On-Site Off-Road (Diesel Equipment)	4.29	45.66	35.03	0.04	2.29	2.14
Off Site (Hauling, Vendor, Worker)	0.25	3.00	3.16	<0.01	0.39	0.13
<b>Total Emissions</b>	<b>4.54</b>	<b>48.66</b>	<b>38.19</b>	<b>0.04</b>	<b>4.88</b>	<b>2.60</b>
<b>SCAQMD Thresholds</b>	<b>75</b>	<b>100</b>	<b>550</b>	<b>150</b>	<b>150</b>	<b>55</b>
<b>Significant Impact?</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>
<b>Grading</b>						
On-Site Fugitive Dust	--	--	--	--	6.11	3.32
On-Site Off-Road (Diesel Equipment)	3.46	35.98	25.38	0.03	2.04	1.88
Off Site (Hauling, Vendor, Worker)	0.77	11.04	9.78	0.03	1.06	0.40
<b>Total Emissions</b>	<b>4.23</b>	<b>47.02</b>	<b>35.16</b>	<b>0.06</b>	<b>9.21</b>	<b>5.60</b>
<b>SCAQMD Thresholds</b>	<b>75</b>	<b>100</b>	<b>550</b>	<b>150</b>	<b>150</b>	<b>55</b>
<b>Significant Impact?</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>
<b>Building Construction Phase</b>						
On-Site Off-Road Diesel Equipment	3.10	26.41	18.13	0.03	1.78	1.67
Off Site (Hauling, Vendor, Worker)	0.64	2.75	8.95	0.02	1.43	0.41
<b>Total Emissions</b>	<b>3.74</b>	<b>29.16</b>	<b>27.08</b>	<b>0.05</b>	<b>3.21</b>	<b>2.08</b>
<b>SCAQMD Thresholds</b>	<b>75</b>	<b>100</b>	<b>550</b>	<b>150</b>	<b>150</b>	<b>55</b>
<b>Significant Impact?</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>
<b>Paving Phase</b>						
On-Site Off-Road Diesel Equipment	1.41	14.32	12.26	0.02	0.83	0.76
Off-Site Hauling/Vendor/Worker Trips	0.07	0.09	0.98	<0.01	0.23	0.06
<b>Total Emissions</b>	<b>1.48</b>	<b>14.41</b>	<b>13.24</b>	<b>0.02</b>	<b>1.06</b>	<b>0.82</b>
<b>SCAQMD Thresholds</b>	<b>75</b>	<b>100</b>	<b>550</b>	<b>150</b>	<b>150</b>	<b>55</b>
<b>Significant Impact?</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>
<b>Architectural Coating</b>						
On-Site Architectural Coating	36.73	--	--	--	0.00	0.00
On-Site Off-Road Diesel Equipment	0.27	1.84	1.84	<0.01	0.13	0.13
Off-Site Hauling/Vendor/Worker Trips	0.07	0.09	0.98	<0.01	0.25	0.07
<b>Total Emissions</b>	<b>37.07</b>	<b>1.93</b>	<b>2.82</b>	<b>&lt;0.01</b>	<b>0.38</b>	<b>0.20</b>
<b>SCAQMD Thresholds</b>	<b>75</b>	<b>100</b>	<b>550</b>	<b>150</b>	<b>150</b>	<b>55</b>
<b>Significant Impact?</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>
<i>Note: Calculations assume compliance with SCAQMD Rule 403 – Fugitive Dust. Calculation sheets are provided in Appendix A to this IS/MND. Parker Environmental Consultants, 2016.</i>						

### Operational Emissions

Air pollutant emissions are currently generated at the Project Site by the existing auto body shops and commercial office building. These uses generate air pollutant emissions from stationary sources, such as space and water heating, architectural coatings (paint), and mobile vehicle traffic traveling to and from the Project Site. The average daily emissions generated by the existing uses at the Project Site have been estimated utilizing the California Emissions Estimator Model (CalEEMod *Version 2013.2.2*) recommended

by the SCAQMD. As shown in Table III-2, motor vehicles are the primary source of air pollutant emissions associated with existing uses at the Project Site.

**Table III-2  
Existing Daily Operational Emissions from the Project Site**

Emissions Source	Emissions in Pounds per Day					
	ROG	NO <sub>x</sub>	CO	SO <sub>x</sub>	PM <sub>10</sub>	PM <sub>2.5</sub>
<b>Summertime (Smog Season) Emissions</b>						
Area Source	0.72	<0.01	<0.01	0.00	<0.01	<0.01
Energy (Natural Gas)	0.01	0.13	0.11	<0.01	<0.01	<0.01
Mobile (Vehicles)	4.30	7.74	34.56	0.07	4.38	1.24
<b>Total Emissions</b>	<b>5.03</b>	<b>7.87</b>	<b>34.67</b>	<b>0.07</b>	<b>4.38</b>	<b>1.24</b>
<b>Wintertime (Non-Smog Season) Emissions</b>						
Area Source	0.72	<0.01	<0.01	0.00	<0.01	<0.01
Energy (Natural Gas)	0.01	0.13	0.11	<0.01	<0.01	<0.01
Mobile (Vehicles)	4.51	8.10	36.16	0.06	4.38	1.24
<b>Total Emissions</b>	<b>5.24</b>	<b>8.23</b>	<b>36.27</b>	<b>0.06</b>	<b>4.38</b>	<b>1.24</b>
<i>Calculation data are provided in Appendix A to this Draft IS/MND. Source: Parker Environmental Consultants, 2016.</i>						

Operational emissions generated by both stationary and mobile sources would result from normal day-to-day activities of the Proposed Project. Area source emissions would be generated by the consumption of natural gas and landscape maintenance. The following regulatory compliance measure have been identified as being applicable to the operational aspects of the Proposed Project:

- **Regulatory Compliance Measure RC-AQ-5:** New on-site facility nitrogen oxide emissions shall be minimized through the use of emission control measures (e.g., use of best available control technology for new combustion sources such as boilers and water heaters) as required by South Coast Air Quality Management District Regulation XIII, New Source Review.

Similar to existing conditions, operational emissions generated by both stationary and mobile sources would result from normal day-to-day activities of the Proposed Project. Area source emissions would be generated by the consumption of natural gas and landscape maintenance. Mobile emissions would be generated by the motor vehicles traveling to and from the Project Site. The Proposed Project's regional operational emissions are presented in Table III-3, Proposed Project Estimated Daily Operational Emissions. As shown, the Proposed Project's net operational emissions would not exceed the regional thresholds of significance set by the SCAQMD. Therefore, impacts associated with regional operational emissions from the Proposed Project would be less than significant.

**Table III-3  
Proposed Project Estimated Daily Operational Emissions**

Emissions Source	Emissions in Pounds per Day					
	ROG	NO <sub>x</sub>	CO	SO <sub>x</sub>	PM <sub>10</sub>	PM <sub>2.5</sub>
<b>Summertime (Smog Season) Emissions</b>						
Mobile (Vehicle) Sources	4.83	13.10	53.27	0.16	10.61	2.97
Energy (Natural Gas)	0.07	0.65	0.28	<0.01	0.05	0.05
Area Source	6.52	0.13	11.63	<0.01	0.06	0.06
<b>Total Project Emissions</b>	<b>11.42</b>	<b>13.88</b>	<b>65.18</b>	<b>0.16</b>	<b>10.72</b>	<b>3.08</b>
<i>Less Existing Project Site Emissions</i>	<i>-5.03</i>	<i>-7.87</i>	<i>-34.67</i>	<i>-0.07</i>	<i>-4.38</i>	<i>-1.24</i>
<b>NET Project Emissions</b>	<b>6.39</b>	<b>6.01</b>	<b>30.51</b>	<b>0.09</b>	<b>6.34</b>	<b>1.84</b>
<b>SCAQMD Thresholds</b>	<b>55</b>	<b>55</b>	<b>550</b>	<b>150</b>	<b>150</b>	<b>55</b>
<b>Potentially Significant Impact?</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>
<b>Wintertime (Non-Smog Season) Emissions</b>						
Mobile (Vehicle) Sources	4.99	13.75	52.83	0.15	10.61	2.97
Energy (Natural Gas)	0.07	0.65	0.28	<0.01	0.05	0.05
Area Source	6.52	0.13	11.63	<0.01	0.06	0.06
<b>Total Project Emissions</b>	<b>11.58</b>	<b>14.53</b>	<b>64.74</b>	<b>0.15</b>	<b>10.72</b>	<b>3.08</b>
<i>Less Existing Project Site Emissions</i>	<i>-5.24</i>	<i>-8.23</i>	<i>-36.27</i>	<i>-0.06</i>	<i>-4.38</i>	<i>-1.24</i>
<b>NET Project Emissions</b>	<b>6.34</b>	<b>6.30</b>	<b>28.47</b>	<b>0.09</b>	<b>6.34</b>	<b>1.84</b>
<b>SCAQMD Thresholds</b>	<b>55</b>	<b>55</b>	<b>550</b>	<b>150</b>	<b>150</b>	<b>55</b>
<b>Potentially Significant Impact?</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>	<b>No</b>
<i>Note: Calculation worksheets are provided in Appendix A to this IS/MND.</i>						
<i>Source: Parker Environmental Consultants, 2016.</i>						

- c) **Would the project result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions, which exceed quantitative threshold for ozone precursors)?**

**Less Than Significant Impact.** Based on the *L.A. CEQA Thresholds Guide*, a significant impact may occur if a project adds a considerable cumulative contribution to federal or State non-attainment pollutants. As the Basin is currently in State non-attainment for ozone, PM<sub>10</sub>, and PM<sub>2.5</sub>, related projects could exceed an air quality standard or contribute to an existing or projected air quality exceedance. In regards to determining the significance of the Project contribution, the SCAQMD neither recommends quantified analyses of construction and/or operational emissions from multiple development projects nor provides methodologies or thresholds of significance to be used to assess the cumulative emissions generated by multiple cumulative projects. Instead, the SCAQMD recommends that a project's potential contribution to cumulative impacts should be assessed utilizing the same significance criteria as those for project specific impacts. Furthermore, SCAQMD states that if an individual development project generates less than significant construction or operational emissions, then the development project would not generate a cumulatively considerable increase in emissions for those pollutants for which the Basin is in non-attainment.

As discussed under Question III(b) above, the Project would not generate construction or operational emissions that exceed the SCAQMD's recommended regional thresholds of significance. Therefore, the Proposed Project would not generate a cumulatively considerable increase in emissions of the pollutants for which the Basin is in nonattainment, and impacts would be less than significant.

**d) Would the project expose sensitive receptors to substantial pollutant concentrations?**

**Less Than Significant Impact.** Based on the *L.A. CEQA Thresholds Guide*, a significant impact may occur if a project were to generate pollutant concentrations to a degree that would significantly affect sensitive receptors. Sensitive receptors are populations that are more susceptible to the effects of air pollution than are the population at large. The SCAQMD identifies the following as sensitive receptors: long-term health care facilities, rehabilitation centers, convalescent centers, retirement homes, residences, schools, playgrounds, child care centers, and athletic facilities.<sup>8</sup>

The SCAQMD has developed localized significance thresholds (LSTs) that are based on the amount of pounds of emissions per day that can be generated by a project that would cause or contribute to adverse localized air quality impacts. These localized thresholds, which are found in the mass rate look-up tables in the "Final Localized Significance Threshold Methodology" document prepared by the SCAQMD,<sup>9</sup> apply to projects that are less than or equal to five acres in size and are only applicable to the following criteria pollutants: NO<sub>x</sub>, CO, PM<sub>10</sub>, and PM<sub>2.5</sub>. LSTs represent the maximum emissions from a project that are not expected to cause or contribute to an exceedance of the most stringent applicable federal or State ambient air quality standards, and are developed based on the ambient concentrations of that pollutant for each SRA. For PM<sub>10</sub>, the LSTs were derived based on requirements in SCAQMD Rule 403 — Fugitive Dust. For PM<sub>2.5</sub>, the LSTs were derived based on a general ratio of PM<sub>2.5</sub> to PM<sub>10</sub> for both fugitive dust and combustion emissions.

LSTs are provided for each of SCAQMD's 38 source receptor areas (SRA) at various distances from the source of emissions. The Project Site is located within SRA 6, which covers the West San Fernando area. The nearest sensitive receptors that could potentially be subject to localized air quality impacts associated with construction of the Proposed Project include small lot residences to the west of the Project Site. Given the proximity of these sensitive receptors to the Project Site, the LSTs with receptors located within 25 meters (82.02 feet) are used to address the potential localized air quality impacts associated with the construction-related NO<sub>x</sub>, CO, PM<sub>10</sub>, and PM<sub>2.5</sub> emissions for each construction phase. Sensitive receptors located further than 25 meters would be less impacted by localized emissions.

---

<sup>8</sup> *South Coast Air Quality Management District, CEQA Air Quality Handbook, 1993, page 5-1.*

<sup>9</sup> *South Coast Air Quality Management District, Final Localized Significance Threshold Methodology, June 2003, Revised July 2008.*

### Localized Construction Emissions

Emissions from construction activities have the potential to generate localized emissions that may expose sensitive receptors to harmful pollutant concentrations, especially during the demolition and grading phases. The Project Site's lot area is approximately 3.29 acres. Because the SCAQMD sets thresholds for 1-, 2-, and 5-acre sites, the localized thresholds for the Project Site were estimated using linear regression, which is suggested in the SCAQMD Final LST Document for project sites that are between 2 and 5 acres. As shown in Table III-4, Localized On-Site Peak Daily Construction Emissions, peak daily emissions generated within the Project Site during construction activities for each phase would not exceed the applicable construction LSTs for an approximate 3.5-acre site in SRA 6.<sup>10</sup> These calculations assume that appropriate dust control measures would be implemented as part of the Proposed Project during each phase of development, as required by SCAQMD Rule 403 - Fugitive Dust. Specific Rule 403 control requirements include, but are not limited to, applying water in sufficient quantities to prevent the generation of visible dust plumes, applying soil binders to uncovered areas, reestablishing ground cover as quickly as possible, utilizing a wheel washing system to remove bulk material from tires and vehicle undercarriages before vehicles exit the Project Site, and maintaining effective cover over exposed areas. Therefore, with implementation of the regulatory code compliance measures identified above, localized air quality impacts from construction activities on the off-site sensitive receptors would be less than significant.

**Table III-4  
Localized On-Site Peak Daily Construction Emissions**

Construction Phase <sup>a</sup>	Total On-site Emissions (Pounds per Day)			
	NO <sub>x</sub> <sup>b</sup>	CO	PM <sub>10</sub>	PM <sub>2.5</sub>
Demolition	45.66	35.03	3.15	2.27
Grading	35.98	25.38	4.42	3.17
Building Construction	26.41	18.13	1.78	1.67
Paving	14.32	12.26	0.83	0.76
Architectural Coatings	1.84	1.84	0.13	0.13
<b>SCAQMD Localized Thresholds <sup>c</sup></b>	<b>184</b>	<b>901</b>	<b>9</b>	<b>5</b>
<i>Potentially Significant Impact?</i>	<i>No</i>	<i>No</i>	<i>No</i>	<i>No</i>

<sup>a</sup> The localized thresholds for all phases are based on a receptor distance of 82 feet in SCAQMD's SRA 6 for a Project Site of 3.5 acre. Thresholds for a 3.5 acre site was estimated using linear regression.

<sup>b</sup> The localized thresholds listed for NO<sub>x</sub> takes into consideration the gradual conversion of NO<sub>x</sub> to NO<sub>2</sub>, and are provided in the mass rate look-up tables in the SCAQMD's "Final Localized Significance Threshold Methodology" guidance document. The analysis of localized air quality impacts associated with NO<sub>x</sub> emissions is focused on NO<sub>2</sub> levels as they are associated with adverse health effects.

<sup>c</sup> Thresholds for each pollutant is taken from Appendix C of the Final LST Methodology Document, revised October 21, 2009.

Source: (1) CalEEMod 2013.2.2, Calculation sheets are provided in Appendix A to this IS/MND.

<sup>10</sup> Thresholds are provided for 1-acre, 2-acre, and 5-acre sites. Thresholds for a 3.5-acre site was estimated using linear regression. Source: SCAQMD, Sample Construction Scenarios for Projects Less than Five Acres in Size, Appendix K, February 2005.

With regard to localized emissions from motor vehicle travel, traffic congested roadways and intersections have the potential to generate localized high levels of carbon monoxide (CO). Because the Basin is currently in attainment and existing congested intersections do not exceed state thresholds, CO hotspots are less than significant under extreme conditions. Therefore, no further analysis for CO hotspots is warranted and localized operational emissions would be less than significant.

### ***Toxic Air Contaminants (TAC)***

#### *Construction Emissions*

The Proposed Project's construction activities would generate toxic air contaminants in the form of diesel particulate emissions associated with the use of heavy trucks and construction equipment. The SCAQMD has not published guidance directly related to quantitatively assessing health risk impacts associated with construction activities. According to the Office of Environmental Health Hazard Assessment, estimating the cancer risk from toxic air contaminants should be based on a lifetime (i.e., 70-year) exposure period. The construction period would occur over an approximately 34-month period. Therefore, it is not meaningful to evaluate long-term cancer impacts from construction activities that occur over a relatively short duration of an individual's lifetime. The Proposed Project will comply with the CARB Air Toxics Control Measure that limits diesel powered equipment and vehicle idling to no more than 5 minutes at a location. In addition, as discussed above, the Project would not result in significant localized air quality impact. Construction activities would be subject to the regulations and laws relating to toxic air pollutants at the regional, State, and federal level that would protect sensitive receptors from substantial concentrations of these emissions. Therefore, the Proposed Project would result in a less than significant impact related to construction TACs.

#### *Operation Emissions*

The Proposed Project would consist of a unified development with multi-family residential, small lot residential and commercial uses that would not support any land uses or activities that would involve the use, storage, or processing of carcinogenic or non-carcinogenic toxic air contaminants. As such no significant toxic airborne emissions would result from Proposed Project implementation. Therefore, impacts associated with the release of toxic air contaminants would be less than significant.

### **e) Would the project create objectionable odors affecting a substantial number of people?**

**Less Than Significant Impact.** A significant impact may occur if objectionable odors occur which would adversely impact sensitive receptors. Odors are typically associated with industrial projects involving the use of chemicals, solvents, petroleum products, and other strong-smelling elements used in manufacturing processes, as well as sewage treatment facilities and landfills. As the Project involves no elements related to these types of activities, no odors from these types of uses are anticipated. Garbage collection areas for the Project would have the potential to generate foul odors if the areas are located in close proximity to habitable areas. Garbage collection areas for the mixed-use portion of the Proposed Project would be provided in communal, designated areas on the ground floor that will be secure and enclosed with walls,

and trash and recycling receptacles would have lids to prevent odors. For the small lot subdivision portion of the Proposed Project, each dwelling unit would have trash and recycling receptacles provided in the garage. Further, good housekeeping practices would be sufficient to prevent nuisance odors from garbage collection areas. In addition, SCAQMD Rule 402 (Nuisance), and SCAQMD Best Available Control Technology Guidelines would limit potential objectionable odor impacts during the Proposed Project's long-term operations phase. The Project would also be subject to the following regulatory compliance measure with respect to controlling odors from any operational activities within the proposed commercial uses:

- **Regulatory Compliance Measure RC-AQ-6:** The Project shall install odor-reducing equipment in accordance with South Coast Air Quality Management District Rule 1138.

With implementation of Regulatory Compliance Measure RC-AQ-6, potential operational odor impacts would be less than significant.

### **Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project in conjunction with the related projects in the Project Site vicinity would result in an increase in construction and operational emissions in the already urbanized area of the City of Los Angeles.

Cumulative development can affect implementation of the 2012 AQMP. The 2012 AQMP was prepared to accommodate growth, reduce pollutants within the areas under SCAQMD jurisdiction, improve the overall air quality of the region, and minimize the impact on the economy. Growth considered to be consistent with the 2012 AQMP would not interfere with attainment because this growth is included in the projections utilized in the formulation of the AQMP. Consequently, as long as growth in the Basin is within the projections for growth identified by SCAG, implementation of the 2012 AQMP will not be obstructed by such growth and cumulative impacts would be less than significant. Since the Proposed Project is consistent with SCAG's growth projections, it would not have a cumulatively considerable contribution to an impact regarding a potential conflict with or obstruction of the implementation of the applicable air quality plan. Thus, cumulative impacts related to conformance with the 2012 AQMP would be less than significant.

Cumulative air quality impacts from construction and operation of the Proposed Project, based on SCAQMD guidelines, are analyzed in a manner similar to Project-specific air quality impacts. The SCAQMD recommends that a project's potential contribution to cumulative impacts should be assessed utilizing the same significance criteria as those for project specific impacts. Therefore, according to the SCAQMD, individual development projects that generate construction or operational emissions that exceed the SCAQMD recommended daily thresholds for project-specific impacts would also cause a cumulatively considerable increase in emissions for those pollutants for which the Basin is in non-attainment. Thus, as discussed in Question III(c) above, because the construction-related and operational daily emissions associated with Proposed Project would not exceed the SCAQMD's recommended thresholds, these emissions associated with the Proposed Project would not be cumulatively considerable. Therefore, cumulative air quality impacts would be less than significant.

With respect to cumulative odor impacts, potential sources that may emit odors during construction activities at each related project include the use of architectural coatings, solvents, and asphalt paving. SCAQMD Rule 1113 would limit the amount of volatile organic compounds from cutback asphalt and architectural coatings and solvents, respectively. Based on mandatory compliance with SCAQMD Rules, construction activities and materials used in the construction of the Proposed Project and related projects would not combine to create objectionable construction odors. With respect to operations, SCAQMD Rules 402 (Nuisance) and Rule 1138 (Odor Reducing Equipment) would regulate any objectionable odor impacts from the related projects and the proposed Project's long-term operations phase. Thus, cumulative odor impacts would be less than significant.

#### IV. BIOLOGICAL RESOURCES

- a) **Would the project have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulation, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?**

**Potentially Significant Unless Mitigation Incorporated.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on biological resources if it could result in: (a) the loss of individuals, or the reduction of existing habitat, of a state or federal listed endangered, threatened, rare, protected, candidate, or sensitive species or a Species of Special Concern; (b) the loss of individuals or the reduction of existing habitat of a locally designated species or a reduction in a locally designated natural habitat or plant community; or (c) interference with habitat such that normal species behaviors are disturbed (e.g., from the introduction of noise or light) to a degree that may diminish the chances for long-term survival of a sensitive species. The Project Site is improved with a car resale center, an auto body shop, a commercial office building, and surface parking lots.

Based on the information presented in the Protected Tree Report (Appendix H to this IS/MND), there are 22 trees on site, 18 of which are 12 inches or greater in diameter at breast height (significant trees). All of the trees on-site are non-native and are not protected tree species under the Native Protected Tree Ordinance. Development of the Proposed Project would require the removal of all existing trees. The Project would be required to plant replacement trees at a ratio of 1 to 1 for all removed significant trees on-site (this replacement ratio may be increased at the discretion of the City's Urban Forestry Division). Further, the Proposed Project would be required to comply with the Federal Migratory Bird Treaty Act (MBTA) of 1918 (50 C.F.R Section 10.13). Sections 3503, 3503.5 and 3513 of the California Fish and Game Code prohibit take of all birds and their active nests including raptors and other migratory non-game birds (as listed under the Federal MBTA). See Regulatory Compliance Measure RC-BIO-1, below.

- **Regulatory Compliance Measure RC-BIO-1 (Habitat Modification (Nesting Native Birds)):**
  - Proposed project activities (including disturbances to native and non-native vegetation, structures and substrates) should take place outside of the breeding bird season which generally runs from March 1- August 31 (as early as February 1 for raptors) to avoid take (including disturbances which would cause abandonment of active nests containing eggs

- and/or young). Take means to hunt, pursue, catch, capture, or kill, or attempt to hunt, pursue, catch, capture or kill (Fish and Game Code Section 86).
- If project activities cannot feasibly avoid the breeding bird season, beginning thirty days prior to the disturbance of suitable nesting habitat, the applicant shall:
    - Arrange for weekly bird surveys to detect any protected native birds in the habitat to be removed and any other such habitat within 300 feet of the construction work area (within 500 feet for raptors) as access to adjacent areas allows. The surveys shall be conducted by a Qualified Biologist with experience in conducting breeding bird surveys. The surveys shall continue on a weekly basis with the last survey being conducted no more than 3 days prior to the initiation of clearance/construction work.
    - If a protected native bird is found, the applicant shall delay all clearance/construction disturbance activities within 300 feet of suitable nesting habitat for the observed protected bird species (within 500 feet for suitable raptor nesting habitat) until August 31.
    - Alternatively, the Qualified Biologist could continue the surveys in order to locate any nests. If an active nest is located, clearing and construction within 300 feet of the nest (within 500 feet for raptor nests) or as determined by a qualified biological monitor, shall be postponed until the nest is vacated and juveniles have fledged and when there is no evidence of a second attempt at nesting. The buffer zone from the nest shall be established in the field with flagging and stakes. Construction personnel shall be instructed on the sensitivity of the area.
    - The applicant shall record the results of the recommended protective measures described above to document compliance with applicable State and Federal laws pertaining to the protection of native birds. Such record shall be submitted and received into the case file for the associated discretionary action permitting the project.

The Project Site does not contain any critical habitat or support any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. Further, no protected tree species exist on site. The Proposed Project would comply with regulatory compliance measures RC-BIO-1 to ensure that the removal of the twenty-two non-protected trees on site would result in less than significant impact. Further, the Proposed Project would comply with Mitigation Measure BIO-1 to ensure that the tree removal and replacement on-site would result in a less than significant. With compliance with the following mitigation measure and regulatory compliance measure, the Proposed Project would result in a less than significant impact.

---

**Mitigation Measure:****BIO-1 (Tree Removal (Non-Protected Trees)):**

- Prior to the issuance of any permit, a plot plan shall be prepared indicating the location, size, type, and general condition of all existing trees on the site and within the adjacent public right(s)-of-way.
- All significant (8-inch or greater trunk diameter, or cumulative trunk diameter if multi-trunked, as measured 54 inches above the ground) non-protected trees on the site proposed for removal shall be replaced at a 1:1 ratio with a minimum 24-inch box tree. Net, new trees, located within the parkway of the adjacent public right(s)-of-way, may be counted toward replacement tree requirements.
- Removal or planting of any tree in the public right-of-way requires approval of the Board of Public Works. Contact Urban Forestry Division at: 213-847-3077. All trees in the public right-of-way shall be provided per the current standards of the Urban Forestry Division the Department of Public Works, Bureau of Street Services.

**b) Would the project have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, and regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?**

**No Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on biological resources if it could result in: (a) the loss of individuals, or the reduction of existing habitat, of a state or federal listed endangered, threatened, rare, protected, candidate, or sensitive species or a Species of Special Concern; (b) the loss of individuals or the reduction of existing habitat of a locally designated species or a reduction in a locally designated natural habitat or plant community; (c) the alternation of an existing wetland habitat; or (d) interference with habitat such that normal species behaviors are disturbed (e.g., from the introduction of noise, light) to a degree that may diminish the chances for long-term survival of a sensitive species. The Project Site is located in a developed urban area and occupied by a car resale center, an auto body shop, a commercial office building, and surface parking areas. No riparian or other sensitive natural vegetation communities are located on or adjacent to the Project Site. Therefore, implementation of the Proposed Project would not result in any adverse impacts to riparian habitat or other sensitive natural communities. Therefore, no impact would occur.

**c) Would the project have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?**

**No Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on biological resources if it could result in the alteration of an existing wetland habitat. The Project Site is entirely developed with impermeable surfaces and does not contain any wetlands or natural drainage channels. Therefore, the Project Site does not support any riparian or wetland

habitat, as defined by Section 404 of the Clean Water Act (see Question IV(b), above), and no impacts to riparian or wetland habitats would occur with the development of the Proposed Project.

**d) Would the project interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?**

**No Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally result in a significant impact on biological resources if it results in the interference with wildlife movement/migration corridors that may diminish the chances for long-term survival of a sensitive species. The Project Site is located in an urbanized area of the Northridge community within the City of Los Angeles. Due to the urbanized surroundings, there are no wildlife corridors or native wildlife nursery sites in the Project vicinity. Thus, the Proposed Project would not interfere with the movement of any residents or migratory fish or wildlife. Therefore, no impact would occur.

**e) Would the project conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance (e.g., oak trees or California walnut woodlands)?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project-related significant adverse effect could occur if a project were to cause an impact that is inconsistent with local regulations pertaining to biological resources, such as the City of Los Angeles Protected Tree Ordinance, 177,404. The Project Site is improved with car resale center, an auto body shop, a commercial office building, and surface parking areas. There are no protected tree species located on the Project Site. Therefore, the Proposed Project would not have the potential to conflict with the City of Los Angeles Protected Tree Ordinance. However, it is anticipated that all trees on-site would be removed during the construction of the Proposed Project. Further, the Proposed Project may require the removal of four street trees along Reseda Boulevard. In the event that the construction of the Proposed Project would require the removal of street trees, the removal of street trees is subject to the approval and tree replacement conditions set forth by the Board of Public Works, Urban Forestry Division. Further, the Project would be required to comply with the Federal Migratory Bird Treaty Act and Sections 3503, 3503.5, and 3513 of the California Fish and Game Code, which prohibits take of all birds and their active nests including raptors and other migratory non-game birds. Compliance with regulatory compliance measures RC BIO-1 and RC-BIO-2, above, would ensure that impacts upon the removal of on-site trees and street trees would be less than significant.

**f) Would the project conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?**

**No Impact.** A significant impact would occur if the Proposed Project would be inconsistent with maps or policies in any conservation plans of the types cited. The Project Site and its vicinity are not part of any draft or adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional or state habitat conservation plan. Therefore, no impact would occur with implementation of the Proposed Project.

---

## Cumulative Impacts

**Less Than Significant Impact.** The Proposed Project would have a less than significant impact upon biological resources with compliance with regulatory compliance measures. Development of the Proposed Project in combination with the nine related projects would not significantly impact wildlife corridors or habitat for any candidate, sensitive, or special status species identified in local plans, policies, or regulations, or by the CDFG or the USFWS. No such habitat occurs in the vicinity of the Project Site or related projects due to the existing urban development. Development of any of the related projects would be subject to the City of Los Angeles Protected Tree Ordinance, Federal Migratory Bird Treaty Act, and Sections 3503, 3503.5, and 3513 of the California Fish and Game Code. Thus, cumulative impacts to biological resources would be considered less than significant.

## V. CULTURAL RESOURCES

The following section summarizes and incorporates by reference the information provided in the following Historic Resources Report: Neill Lehr Cadillac Showroom & Service Buildings, 8400-8430 N. Reseda Boulevard, 8423-8429 N. Canby Avenue, Los Angeles, California, prepared by GPA Consulting, dated February 9, 2016. The Historic Resources Report is provided as Appendix B.

### a) **Would the project cause a substantial adverse change in the significance of an historic resource pursuant to CEQA § 15064.5?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a significant impact may occur if the Proposed Project results in a substantial adverse change in the significance of a historic resource. Section 15064.5 of the State CEQA Guidelines defines a historical resource as: (1) a resource listed in or determined to be eligible by the State Historical Resources Commission for listing in the California Register of Historical Resources; (2) a resource listed in a local register of historical resources or identified as significant in an historical resource survey meeting certain State guidelines; or (3) an object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California, provided that the lead agency's determination is supported by substantial evidence in light of the whole record. A substantial adverse change in the significance of a historic resource means demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of a historical resource would be materially impaired.<sup>11</sup>

---

<sup>11</sup> *CEQA Guidelines, Section 15064.5(b)(1)*.

Section 15064.5(b)(2) of the CEQA Guidelines provides that “[t]he significance of an historical resource is materially impaired when a project:

- (a) Demolishes or materially alters in an adverse manner those physical characteristics of an historical resource that convey its historical significance and that justify its inclusion in, or eligibility for, inclusion in the California Register of Historical Resources; or*
- (b) Demolishes or materially alters in an adverse manner those physical characteristics that account for its inclusion in a local register of historical resources pursuant to section 5020.1(k) of the Public Resources Code or its identification in an historical resources survey meeting the requirements of section 5024.1(g) of the Public Resources Code, unless the public agency reviewing the effects of the project establishes by a preponderance of evidence that the resource is not historically or culturally significant; or*
- (c) Demolishes or materially alters in an adverse manner those physical characteristics of a historical resource that convey its historical significance and that justify its eligibility for inclusion in the California Register of Historical Resources as determined by a lead agency for purposes of CEQA.*

The Project Site encompasses the Neill Lehr Cadillac showroom and service buildings located at 8400-8430 N. Reseda Boulevard and 8423-8429 N. Canby Avenue in the Northridge Community Plan area of the City of Los Angeles. The Neill Lehr Cadillac showroom was evaluated in July 2015 as part of SurveyLA, the citywide historical resource survey of Los Angeles. SurveyLA concluded that the property appeared eligible for listing in the National Register of Historic Places, California Register of Historical Resources, and for designation as a Los Angeles Historic-Cultural Monument (HCM). SurveyLA identifies the Neill Lehr Cadillac showroom as potentially historic for being an excellent example of the 1960s automobile showroom in Northridge and exhibits the essential characteristics of the property type.<sup>12</sup> GPA Consulting’s observations in the field, research, and comparisons with historic documentation found that while the showroom section of the property represents a late example of its type and retains integrity, large sections to the rear and sides of the showroom have been altered. As a whole, the property is not a significant and intact example of an automobile showroom. GPA Consulting determined that the car showroom on-site is not a significant cultural or historic resource for the reasons below. Further, the Office of Historic Resources reviewed the Proposed Project’s Historic Resources Report and concurs with the conclusions of the report.

---

<sup>12</sup> *SurveyLA, the Los Angeles Historic Resources Survey, is a comprehensive program to identify potentially historic resources throughout the City of Los Angeles. Professional historic preservation consultant teams conduct field surveys under the direction of the Department of City Planning, Office of Historic Resources. (Source: City of Los Angeles Department of City Planning, Office of Historic Resources, SurveyLA, Northridge Report, Individual Resources July 13, 2015. website: <http://preservation.lacity.org/sites/default/files/Northridge%20Individual.pdf>, accessed February 2016.*

Research did not reveal the subject property to be the site of any historical events. In terms of broad patterns of history, automobile showrooms are part of the auto-oriented commercial development that took place in Los Angeles as car ownership and use increased exponentially starting in the 1910s. Car sales boomed after World War II and as a result, car showrooms, like other commercial buildings constructed for the sale, service, maintenance, or collective off-street parking of the automobile, were constructed citywide in Los Angeles, typically along highways and arterial streets, and especially in areas built up after World War II. By the 1940s, automobile showrooms followed prescriptive design guidelines developed by automobile manufacturers to maximize the sales, advertising, and brand identification of their cars. Cadillac exercised special control over the design of its factory outlet showrooms until 1965 when it converted to a more widely used dealership model.

The property was designed in 1964 and opened in 1965. It embodies many of the character-defining features of the automobile showroom property type, such as the transparency between the interior and exterior. While the property is associated with the trend of auto-oriented commercial development, it is not significant within this historic context. The building is a late example of the property type and reflects an earlier period of development and innovation more than the time of its own construction in the mid-1960s. It lacks features that tie it directly to the Cadillac brand or a showroom designer of note. It is not an important, early, or innovative example of the automobile showroom property type that reflects a significant contribution to the development of automobile sales and purchases. Further, the building's significance was examined in relation to its original owner Henry (H.) Neill Lehr of Neill Lehr Cadillac. Although Mr. Lehr owned and operated a successful Cadillac dealership for many decades, there are no indications that he can be considered especially significant in the field of automobile showroom development. There is no evidence that Mr. Lehr was significant in the history of the automobile sales industry in Los Angeles. No further information was found to suggest that Mr. Lehr was a historic personage, or that any other individuals of historic significance were associated with the property.

Additionally, research did not reveal the property to be an important, innovative, or intact example of the property type exhibiting features that would associate it with a particular brand or designer. The property is not associated with an important historic event, architectural style, nor is it the work of a master. Furthermore, alterations and additions to the property have diminished the integrity of the property as an automobile showroom. As such, the Neill Lehr Cadillac showroom does not meet the criteria for inclusion on the National Register of Historic Places, California Register of Historical Resources, or the City of Los Angeles Historic-Cultural Monuments. Therefore, the demolition of the on-site buildings and the construction of the Proposed Project would result in a less than significant impact upon historic resources, and no mitigation measures are required.

**b) Would the project cause a substantial adverse change in the significance of an archaeological resource pursuant to CEQA § 15064.5?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a significant impact may occur if grading or excavation activities associated with the Proposed Project would disturb archaeological resources. No known archaeological sites are identified on the Project Site. There is no evidence that suggests any archaeological sites or archaeological resources exist on the Project Site. The

Project Site has been previously developed and is located in an urbanized area of the Northridge Community. The Project Site currently contains a resale car center, an auto body shop, a commercial office building, and surface parking lots. Thus, the Project Site has been previously disturbed. The Proposed Project would include excavation to a depth of approximately 12 feet below grade to construct the one-level subterranean parking garage. Thus, the potential exists for the accidental discovery of unknown and unrecorded archaeological materials. In the unlikely event any archaeological resources are encountered during the construction phase, the discovery of such materials would be mitigated to less than significant levels through compliance with the following applicable regulatory compliance measure:

- **Regulatory Compliance Measure RC-CR-1 (Archaeological):** If archaeological resources are discovered during excavation, grading, or construction activities, work shall cease in the area of the find until a qualified archaeologist has evaluated the find in accordance with federal, State, and local guidelines, including those set forth in California Public Resources Code Section 21083.2. Personnel of the proposed Project shall not collect or move any archaeological materials and associated materials. Construction activity may continue unimpeded on other portions of the Project site. The found deposits would be treated in accordance with federal, State, and local guidelines, including those set forth in California Public Resources Code Section 21083.2.

Although there are no known archaeological resources, the presence or absence of such materials cannot be determined until the Project Site is excavated, compliance with regulatory compliance measure RC-CR-1 would ensure that if any archaeological resources are encountered during construction, impacts to such resources would be less than significant.

**c) Would the project directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a significant impact may occur if grading or excavation activities associated with the Proposed Project were to disturb paleontological resources or geologic features which presently exist within the Project Site. The Project Site has been previously graded and currently exists as a commercial building and automobile land uses with associated surface parking areas. The Project Site and immediate surrounding areas do not contain any known vertebrate paleontological resources.<sup>13</sup> Although no paleontological resources are known to exist on-site, there is a potential for paleontological resources to exist at sub-surface levels on the Project Site, which may be uncovered during excavation of the proposed parking structure. The regulatory compliance measure RC-CR-2, identified below, would ensure that if any such resources are found during construction of the Proposed Project, they would be handled according to the proper regulations and any potential impacts would be reduced to less than significant levels.

---

<sup>13</sup> *City of Los Angeles Department of City Planning, Environmental and Public Facilities Maps: Vertebrate Paleontological Resources in the City of Los Angeles, September 1996.*

- **Regulatory Compliance Measure RC-CR-2 (Paleontological):** If paleontological resources are discovered during excavation, grading, or construction, the City of Los Angeles Department of Building and Safety shall be notified immediately, and all work shall cease in the area of the find until a qualified paleontologist evaluates the find. Construction activity may continue unimpeded on other portions of the Project Site. The paleontologist shall determine the location, the time frame, and the extent to which any monitoring of earthmoving activities shall be required. The found deposits would be treated in accordance with federal, State, and local guidelines, including those set forth in California Public Resources Code Section 21083.2.

Although there are no known paleontological resources, the presence or absence of such materials cannot be determined until the Project Site is excavated, compliance with regulatory compliance measure RC-CR-2 would ensure that if any paleontological resources are encountered during construction, impacts to such resources would be less than significant.

**d) Would the project disturb any human remains, including those interred outside of formal cemeteries?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project-related significant adverse effect could occur if grading or excavation activities associated with the proposed project would disturb previously interred human remains. No known human burials have been identified on the Project Site or its vicinity. However, it is possible that unknown human remains could occur on the Project Site, and if proper care is not taken during construction, damage to or destruction of these unknown remains could occur. The following regulatory compliance measure would ensure potential impacts related to the disturbance of unknown human remains are less than significant.

- **Regulatory Compliance Measure RC-CR-3 (Human Remains):** If human remains are encountered unexpectedly during construction demolition and/or grading activities, State Health and Safety Code Section 7050.5 requires that no further disturbance shall occur until the County Coroner has made the necessary findings as to origin and disposition pursuant to California Public Resources Code (PRC) Section 5097.98. In the event that human remains are discovered during excavation activities, the following procedure shall be observed:
  - Stop immediately and contact the County Coroner:
    - 1104 N. Mission Road  
Los Angeles, CA 90033  
323-343-0512 (8 a.m. to 5 p.m. Monday through Friday) or  
323-343-0714 (After Hours, Saturday, Sunday, and Holidays)
  - If the remains are determined to be of Native American descent, the Coroner has 24 hours to notify the Native American Heritage Commission (NAHC).
  - The NAHC will immediately notify the person it believes to be the most likely descendent of the deceased Native American.

- The most likely descendent has 48 hours to make recommendations to the owner, or representative, for the treatment or disposition, with proper dignity, of the human remains and grave goods.
- If the owner does not accept the descendant's recommendations, the owner or the descendent may request mediation by the NAHC.

Although there are no known human remains or burial sites, the presence or absence of human remains cannot be determined until the Project Site is excavated, compliance with regulatory compliance measure RC-CR-3 would ensure that if any human remains are encountered during construction, impacts to such resources would be less than significant.

**e) Would the project cause a substantial adverse change in the significance of a site, feature, place, cultural landscape, sacred place, or object with cultural value to a California Native American Tribe that is listed or determined eligible for listing on the California register of historical resources, listed on a local historical register, or otherwise determined by the lead agency to be a tribal cultural resource?**

**Less Than Significant Impact.** Pursuant to AB 52, the City Planning Department sent pre-consultation request letters to the recognized Native American Tribal Representatives within the Los Angeles region. The City received two responses, one response from the Soboba Band of Luiseño Indians, dated April 12, 2016, and one response from the Fernandeno Tataviam Band of Mission Indians, dated April 12, 2016. No other responses have been received. The Soboba Band's response letter indicated that they do not have any specific concerns regarding known cultural resources in the area the Project encompasses, but does request that the appropriate consultation continue to take place between concerned tribes, project proponents and the local agencies. The Fernandeno Tataviam Band of Mission Indians requested that the lead agency provide (1) the estimated cubic yards of soil disturbance for the Project, (2) the tree report encompassing all 22 trees planned for removal, and (3) all archaeological surveys and reports conducted within 1 mile of the project property for their review. With regards to the Fernandeno Tataviam Band of Mission Indians requests, the Lead Agency has provided the information, and the Proposed Project Applicant would follow necessary regulatory compliance measures in the event of a find (refer to RC-CR-2 and RC-CR-3). Thus, the Project would not cause a substantial adverse change in the significance of a known site, feature, place, cultural landscape, sacred place, or object with cultural value to a California Native American Tribe. Furthermore, adherence to the regulatory compliance measures referenced above (i.e., RC-CR-2 and RC-CR-3), would ensure the appropriate tribal references are contacted in the event of an accidental discovery of any archaeological resources or human remains, thus ensuring potential impacts to Native American resources would be avoided or reduced to less than significant levels.

### **Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project, in combination with the other nine related projects in the Project Site vicinity, would result in the continued redevelopment and revitalization of the surrounding area. Impacts to cultural resources tend to be site-specific and are assessed on a site-by-site basis. The analysis of the Proposed Project's impacts to cultural resources concluded that the Proposed Project would have no significant impacts with respect to cultural resources. Therefore, the Proposed

Project's incremental contribution to a cumulative impact would not be considerable, and cumulative impacts to cultural resources would be less than significant.

## VI. GEOLOGY AND SOILS

The following section summarizes and incorporates the reference information from the Geotechnical Investigation prepared by Feffer Geological Consulting, Inc.: Proposed New Three-Story On Grade Residences and Three Story Commercial Building with One Subterranean Level, 8350-8430 N. Reseda Boulevard, Northridge, CA 91324, dated November 18, 2014 ("Geotechnical Report"). This section also refers to the Proposed Project's Soils Report Approval Letter issued by the City of Los Angeles Department of Building and Safety dated July 29, 2016. The Geotechnical Report and the Soils Report Approval Letter are included as Appendix C.

Feffer Geological Consulting, Inc. conducted a field investigation on October 8, 2014 at the Project Site, which consisted of a review of the existing conditions and exploration involving drilling of borings and soil sampling. Six borings (B1, B2, B3, B4, B5, and B6) were drilled to a maximum depth of 40.5 feet below the existing ground surface. The soil materials encountered in the borings consisted of up to seven feet of fill over alluvium. Feffer Geological Consulting's review of geologic maps indicates that the material underlying the Project Site is comprised of Quaternary age Alluvium. The fill consists of silty sand with minor gravel. The color varies from brown to yellow brown. The fill is moist and medium dense to dense. The Alluvium consists of admixtures of silts, sands and clay with gravel, which vary from brown to yellow brown to orange-brown. The Alluvium was slightly moist to moist and medium dense to dense, firm to soft. The Alluvium is generally weakly horizontally layered with no significant structural planes. Groundwater was encountered at 20 to 27 feet below grade during the excavations. This area of Northridge is not known to have a high groundwater table. Historically, the highest groundwater in this area of Northridge is estimated to be 20 feet below the ground surface.<sup>14</sup> More detailed descriptions of the earth materials encountered and the subsurface excavations may be obtained from the individual logs for each boring in the Geological Report (Appendix C).

- a) **Would the project expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a significant impact may occur if a project site is located within a State-designated Alquist-Priolo Zone or other designated fault zone. The Project Site is not within a currently established Alquist-Priolo Earthquake

---

<sup>14</sup> *State of California, Department of Conservation, Division of Mines and Geology, Seismic Hazards Zone Report for the Canoga 7.5-Minute Quadrangle, Los Angeles County, California, 1998.*

Fault Zone for surface fault rupture hazards.<sup>15</sup> The Project Site is located within the seismically active area, as is all of Southern California and could be subjected to moderate to strong ground shaking in the event of an earthquake on one of the many active Southern California faults. Although there are no active faults on or within the immediate vicinity of the Project Site, earthquakes generated on large regional faults such as the San Andreas Fault could affect the Project Site. The closest known potentially active fault to the Project Site is the Sylmar and Mission Wells faults, located within five kilometers (approximately 3.1 miles). Since no active faults cross the Project Site, the surface rupture hazard at the Project Site is considered nil. Therefore, the potential for surface rupture due to faulting occurring beneath the Project Site during the design life of the Proposed Project is considered low, and the potential for impacts associated with surface fault rupture would be considered less than significant.

The Project would adhere to current engineering standards, the seismic safety requirements set forth in the Earthquake Regulations of the 2014 City of Los Angeles Building Code (LABC), the Los Angeles Municipal Code (LAMC), and design recommendations set forth in the Geotechnical Report so that the proposed structures may withstand typical seismic ground shaking. In addition, geologic and geotechnical evaluations of the Proposed Project would follow the guidelines presented in CGS *Special Publication 117, Guidelines for Evaluating and Mitigating Seismic Hazards in California*, which provides guidance for evaluation and mitigation of earthquake-related hazards (other than fault rupture). Thus, impacts related to strong seismic shaking would be reduced to less than significant levels. Potential impacts associated with seismic safety would be further reduced to less than significant levels with incorporation of the following regulatory compliance measure:

- **Regulatory Compliance Measure RC-GEO-1 (Seismic):** The design and construction of the project shall conform to the California Building Code seismic standards as approved by the Department of Building and Safety.

**b) Would the project expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving strong seismic ground shaking?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a significant impact may occur if a project represents an increased risk to public safety or destruction of property by exposing people, property, or infrastructure to seismically induced ground shaking hazards that are greater than the average risk associated with other locations in Southern California. The Project Site is located within a seismically active region, as is all of Southern California. The intensity of ground shaking depends upon the earthquake magnitude, the distance from the source, and the site response characteristics. The Project Site is not located within a seismic hazard zone for landsliding or faulting, as delineated by the State of California, in accordance with the Seismic Hazards Mapping Act and the Alquist-Priolo Act. The

---

<sup>15</sup> City of Los Angeles, Department of City Planning, *City of Los Angeles Zoning Information and Map Access System (ZIMAS)*, website: [www.zimas.lacity.org](http://www.zimas.lacity.org), accessed March 2016.

primary seismic hazard for this Project Site is the potential for strong ground motion from future earthquakes within the Los Angeles Basin. However, the potential for strong ground motion at the Project Site is not unusual for Southern California.

The Project Site is considered suitable for the construction of the Proposed Project provided that the recommendations included in the design and construction of the Proposed Project are to the satisfaction of the Department of Building and Safety. Review and approval from the Department of Building and Safety would ensure that the Proposed Project meets the applicable performance measures, and impacts would be less than significant. Accordingly, Regulatory Compliance Measure RC-GEO-1, above, would ensure impacts associated with seismic hazards are less than significant.

**c) Would the project expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving seismic-related ground failure, including liquefaction?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a significant impact may occur if a Project Site is located within a liquefaction zone. Liquefaction is a phenomenon in which loose, saturated, relatively cohesionless soil deposits lose shear strength during strong ground motions. Primary factors controlling liquefaction include intensity and duration of ground motion, gradation characteristics of the subsurface soils, in-situ stress conditions, and the depth to groundwater. Liquefaction is typified by a loss of shear strength in the liquefied layers due to rapid increases in pore water pressure generated by earthquake accelerations. Liquefaction typically occurs in areas where the soils below the water table are composed of poorly consolidated, fine to medium-grained, primarily sandy soil.

Based on the State of California Seismic Hazards Map for the Canoga 7½-Minute Quadrangle, the Project Site is not located in an area designated as “liquefiable.” However, based on the City of Los Angeles General Plan Safety Element, the Project Site is located in an area designated as “liquefiable.” Nevertheless, Feffer Geological Consulting concluded that the alluvial soils below the historic high groundwater level would not be susceptible to liquefaction settlement during a design level earthquake ground motion and a maximum considered earthquake. Additionally, the Proposed Project would adhere to current engineering standards, the seismic safety requirements set forth in the City of Los Angeles Building Code (LABC), the Los Angeles Municipal Code (LAMC), and design recommendations set forth in the Geotechnical Report. Thus, any potential impacts related to liquefaction would be less than significant.

**d) Would the project expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving landslides?**

**No Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant geologic hazard impact if it would cause or accelerate geologic hazards which would result in substantial damage to structures or infrastructure, or expose people to substantial risk of injury. A project-related significant adverse effect may occur if a project is located in a hillside area with soil conditions that would suggest a high potential for sliding. The Project Site is not located within a City of Los Angeles Hillside Grading Area or Hillside Ordinance Area. The City of Los Angeles Safety Element (1996) indicates the Project Site is not within an area identified as having a potential for slope instability.

Additionally, the State of California, Department of Conservation, Canoga Park Quadrangle Map shows that the Project Site is not within an area identified as having a potential for seismic slope instability.<sup>16</sup> There are no known landslides near the Project Site, nor is the Project Site in the path of any known or potential landslides. Additionally, Feffer Geological Consulting concluded that landslides are not possible due to the lack of slopes on or near the Project Site. Therefore, potential for slope stability hazards to adversely affect the Proposed Project is considered low, and no impact would occur.

**e) Would the project result in substantial soil erosion or the loss of topsoil?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have significant sedimentation or erosion impact if it would: (a) constitute a geologic hazard to other properties by causing or accelerating instability from erosion; or (b) accelerate natural processes of wind and water erosion and sedimentation, resulting in sediment runoff or deposition which would not be contained or controlled on-site. Although development of the Proposed Project has the potential to result in the erosion of soils during site preparation, excavation, and construction activities, erosion would be reduced by implementation of stringent erosion controls imposed by the City of Los Angeles through grading and building permit regulations. Minor amounts of erosion and siltation could occur during grading. The potential for soil erosion during the ongoing operation of the Proposed Project is extremely low due to the fact that the Project Site would be mostly paved-over or built upon so little soil would be exposed. All grading activities require grading permits from the Department of Building and Safety, which include requirements and standards designed to limit potential impacts to acceptable levels. In addition, all on-site grading and site preparation would comply with applicable provisions of Chapter IX, Division 70 of the LAMC, which addresses grading, excavations, and fills. Additionally, Regulatory Compliance Measure RC-GEO-2, below, would further ensure a less-than-significant impact would occur with respect to erosion or loss of topsoil.

- **Regulatory Compliance Measure RC-GEO-2 (Erosion/Grading/Short-Term Construction Impacts):** Chapter IX, Division 70 of the Los Angeles Municipal Code addresses grading, excavations, and fills. All grading activities require grading permits from the Department of Building and Safety. Additional provisions are required for grading activities within Hillside areas. The application of BMPs includes but is not limited to the following mitigation measures:
  - Excavation and grading activities shall be scheduled during dry weather periods. If grading occurs during the rainy season (October 15 through April 1), diversion dikes shall be constructed to channel runoff around the site. Channels shall be lined with grass or roughened pavement to reduce runoff velocity.
  - Stockpiles, excavated, and exposed soil shall be covered with secured tarps, plastic sheeting, erosion control fabrics, or treated with a bio-degradable soil stabilizer.

---

<sup>16</sup> *State of California, Department of Conservation, Division of Mines and Geology, Seismic Hazards Zones, Canoga Quadrangle Map, March 25, 1999.*

- f) **Would the project be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant geologic hazard impact if it could cause or accelerate geologic hazards causing substantial damage to structures or infrastructure, or expose people to substantial risk of injury. A significant impact may occur if the Proposed Project is built in an unstable area without proper site preparation or design features to provide adequate foundations for buildings, thus posing a hazard to life and property. The Geotechnical Report concluded that the potential for seismically induced settlement at the Project Site is considered small, and the geotechnical conditions are favorable for the Project provided that the recommendations specified in the Geotechnical Report are included in the design and construction of the Proposed Project to the satisfaction of the Department of Building and Safety (refer to Regulatory Compliance Measure RC-GEO-3, below). The Geotechnical Report finds that the Project Site is not located within an area that is susceptible to landslides. Further, the Geotechnical Report finds that although the Project Site is located within an area identified as “liquefiable,” the soils on the Project Site would not be susceptible to liquefaction settlement during a design level earthquake ground motion and a maximum considered earthquake. The Proposed Project would comply with the Los Angeles Building Code and in accordance with the conditions approved by the Department of Building and Safety. The Proposed Project would not create unstable geologic conditions on or off-site, and the Project impacts would be less than significant impact.

- **Regulatory Compliance Measure RC-GEO-3:** The Project shall comply with the conditions contained within the Department of Building and Safety’s Geology and Soils Report Approval Letter for the Proposed Project (dated July 29, 2016), and as it may be subsequently amended or modified.

- g) **Would the project be located on expansive soil, as identified in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant geologic hazard impact if it would cause or accelerate geologic hazards, which would result in substantial damage to structures or infrastructure, or expose people to substantial risk of injury. A significant impact may occur if the Proposed Project is built on expansive soils without proper site preparation or design features to provide adequate foundations for buildings, thus posing a hazard to life and property. Based on the Geotechnical Report, Feffer Geological Consulting concluded that the on-site near surface soil was found to possess low to medium expansive characteristics based upon field soil classifications. The Project Site is suitable for the proposed development provided that the recommendations on the Geotechnical Report are implemented during the construction. All grading activities would comply with the requirements and standards designed to limit potential impacts to acceptable levels under provision of the Department of Building and Safety. In addition, all on-site grading and site preparation would comply with applicable provisions of Chapter IX, Division 70 of the LAMC, which addresses grading, excavations, and fills. With adherence to the City of Los Angeles Department of

Building and Safety requirements and regulatory compliance measures, a less than significant impact would occur with respect to expansive soils.

**h) Would the project have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?**

**No Impact.** This question would apply to the Proposed Project only if it was located in an area not served by an existing sewer system. The Project Site is located in a developed area of the City of Los Angeles, which is served by a wastewater collection, conveyance and treatment system operated by the City of Los Angeles. No septic tanks or alternative disposal systems neither are necessary, nor are they proposed. Thus, no impact would occur.

**Cumulative Impacts**

**Less Than Significant Impact.** Geotechnical hazards are site-specific and there is little, if any, cumulative geological relationship between the Proposed Project and any of the nine related projects. Similar to the Proposed Project, potential impacts related to geology and soils would be assessed on a case-by-case basis and, if necessary, the applicants of the related projects would be required to implement the appropriate mitigation measures. Furthermore, the analysis of the Proposed Project's geology and soils impacts concluded that, through the implementation of the regulatory compliance measures, Proposed Project impacts would be less than significant. Therefore, the Proposed Project would not make a cumulatively considerable contribution to any potential cumulative impacts, and cumulative geology and soil impacts would be less than significant.

**VII. GREENHOUSE GAS EMISSIONS**

Greenhouse gas (GHG) emissions refer to a group of emissions that have the potential to trap heat in the atmosphere and consequently affect global climate conditions. Scientific studies have concluded that there is a direct link between increased emission of GHGs and long-term global temperature. The principal GHGs are carbon dioxide (CO<sub>2</sub>), methane (CH<sub>4</sub>), nitrous oxide (N<sub>2</sub>O), sulfur hexafluoride (SF<sub>6</sub>), perfluorocarbons (PFCs), hydrofluorocarbons (HFCs), and water vapor (H<sub>2</sub>O). CO<sub>2</sub> is the reference gas for climate change because it is the predominant greenhouse gas emitted. To account for the varying warming potential of different GHGs, GHG emissions are often quantified and reported as CO<sub>2</sub> equivalents (CO<sub>2</sub>e).

*California Global Warming Solutions Act of 2006*

The California Global Warming Solutions Act of 2006, widely known as AB 32, set a mandate for the California Air Resources Board (CARB) to develop and enforce regulations for the reporting and verification of statewide GHG emissions. The heart of the bill is the requirement that statewide GHG emissions be reduced to 1990 levels by 2020. In its Climate Change Scoping Plan (2008), ARB developed a California statewide GHG emission inventory for years 1990–2004 to support the effort of determining the 1990 level and 2020 near-term emissions limit. To determine the amount of GHG emission reductions needed to reduce to 1990 emissions, ARB then developed a forecast of 2020 emissions in a business-as-

usual scenario (2020 BAU), which is an estimate of the emissions expected to occur in the year 2020 if none of the foreseeable measures included in the Scoping Plan were implemented.

In May 2014, CARB published the First Update to the Climate Change Scoping Plan, where it revised the previously adopted 1990 GHG emissions level from 427 MMTCO<sub>2</sub>e to 431 MMTCO<sub>2</sub>e based on the scientifically updated global warming potential (GWP) values in the Intergovernmental Panel on Climate Change’s (IPCC’s) Fourth Assessment Report.<sup>17</sup> The total future emissions forecasted in the 2020 BAU scenario were also updated from the previously adopted estimate of 596 MMTCO<sub>2</sub>e to 509 MMTCO<sub>2</sub>e. The updated 2020 BAU scenario includes reductions anticipated from the implementation of several policies aimed at reducing the statewide greenhouse gas emissions inventory which are now adopted into law (i.e., California’s Low Carbon Fuel Standard, Pavley I and the Renewable Electricity Standard). As shown in Table III-5, below, the State anticipates it will meet its 2020 GHG emissions limit of 431 MMTCO<sub>2</sub>e through reductions in energy, transportation, waste and high-GWP sectors. The Cap-and-Trade Regulation provides a firm cap, ensuring that the 2020 statewide emission limit will not be exceeded. Thus, the estimated emission reductions attributed to the Cap-and-Trade Program depend on the emissions forecast. For example, if the emissions forecast increases, the reductions associated with the Cap-and- Trade Program will increase.

**Table III-5  
Climate Change Scoping Plan 2020 Emissions Target**

Category	2020 CO <sub>2</sub> e Emissions (MMTOC <sub>2</sub> e ) <sup>[a]</sup>
AB 32 Baseline 2020 Forecast Emissions (2020 BAU)	509
Expected Reductions from Sector-Based Measures	
Energy	25
Transportation	23
High-GWP	5
Waste	2
Cap and Trade Reductions	23 <sup>[b]</sup>
2020 Limit	431
<sup>[a]</sup> Based on AR4 GWP values.	
<sup>[b]</sup> Cap and Trade emissions reductions depend on the emission forecast.	
Source: CARB, First Update to the Climate Change Scoping Plan, May 2014.	

While the Scoping Plan does not provide any specific mandates or policies that directly applies to CEQA Projects, statewide reductions in GHG emissions from construction is being accomplished through

<sup>17</sup> The IPCC is the leading international body for the scientific assessment of climate change established in 1988 under the auspices of the United Nations.

continuous updates to the California Green Building Standards (CALGreen) Code and other State-mandated laws and regulations. Originally adopted in 2008, the CALGreen Code included all voluntary standards that went beyond the basic building code requirements and introduced new standards for reducing water use, provisions for reducing and recycling construction and demolition waste, criteria for site development to locate buildings near public transit, and measures for improving indoor air quality to protect the health of building occupants. In 2010, the CALGreen Code became mandatory on a statewide basis. Effective January 2014, the scope of the CALGreen Code was expanded to all residential buildings, including high-rise residential, as well as to additions or alterations with increases in conditioned space.

#### *LA Green Plan*

The City of Los Angeles has addressed the issue of global climate change through implementation of the *Green L.A., An Action Plan to Lead the Nation in Fighting Global Warming (L.A. Green Plan)* and has updated its zoning Code to mandate increased energy efficiency measures in new construction. The *L.A. Green Plan* outlines the goals and actions that the City has established to reduce the generation and emission of GHGs from both public and private activities. According to the *L.A. Green Plan*, Los Angeles is committed to the goal of reducing emissions of CO<sub>2</sub> to 35 percent below 1990 levels. To achieve this, the City is increasing the generation of renewable energy, improving energy conservation and efficiency, and changing transportation and land use patterns to reduce dependence on automobiles.

#### *LA Green Building Code*

The City of Los Angeles *L.A. Green Building Code* (Ordinance No. 181480), which incorporates applicable provisions of the CALGreen Code, and in many cases outlines more stringent GHG reduction measures available to development projects in the City of Los Angeles is consistent with statewide goals and policies in place for the reduction of greenhouse gas emissions, including AB 32 and the corresponding Scoping Plan. Among the many GHG reduction measures outlined later in this Section, the *L.A. Green Building Code* requires new development projects to achieve a 20 percent reduction in potable water use and wastewater generation, meet and exceed Title 24 Standards adopted by the California Energy Commission on December 17, 2008, and meet 50 percent construction waste recycling levels. New development projects are required to comply with the *L.A. Green Building Code*, and therefore are generally considered consistent with statewide GHG-reduction goals and policies, including AB 32.

#### *2012–2035 RTP/SCS*

On April 4, 2012, SCAG adopted the 2012–2035 Regional Transportation Plan/Sustainable Communities Strategy: Towards a Sustainable Future (2012–2035 RTP/SCS). Within the RTP, the SCS demonstrates the region’s ability to attain and exceed the GHG emission-reduction targets set forth by CARB. The SCS sets forth a regional plan for integrating the transportation network and related strategies with an overall land use pattern that responds to projected growth, housing needs, changing demographics, and transportation demands. The regional vision of the SCS maximizes current voluntary local efforts that support the goals of SB 375, as evidenced by several Compass Blueprint Demonstration Projects and various county transportation improvements. The SCS focuses the majority of new housing and job growth in high-quality transit areas and other opportunity areas in existing main streets, downtowns, and

commercial corridors, resulting in an improved jobs-housing balance and more opportunity for transit-oriented development. This overall land use development pattern supports and complements the proposed transportation network that emphasizes system preservation, active transportation, and transportation demand management measures. Finally, the 2012–2035 RTP/SCS fully integrates the two subregional SCSs prepared by the Gateway Cities and Orange County Council of Governments. On June 4, 2012, CARB accepted SCAG’s quantification of GHG emission reductions from the 2012–2035 RTP/SCS and the determination that the 2012–2035 RTP/SCS would, if implemented, achieve the 2020 and 2035 GHG emission reduction targets established by CARB.

### *SCAQMD*

SCAQMD has released draft guidance regarding interim CEQA GHG significance thresholds. In October 2008, SCAQMD proposed the use of a percent emission reduction target to determine significance for commercial/residential projects that emit greater than 3,000 metric tons of CO<sub>2</sub>e per year. On December 5, 2008, the SCAQMD Governing Board adopted the staff proposal for an interim GHG significance threshold for stationary source/industrial projects where SCAQMD is lead agency. However, SCAQMD has yet to formally adopt a GHG significance threshold for land use development projects (e.g., residential/commercial projects) and has formed a GHG Significance Threshold Working Group to further evaluate potential GHG significance thresholds.

**a) Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?**

**Less than Significant Impact.** Notwithstanding the regulatory plans and building code updates discussed above, there are no federal, state or local adopted thresholds of significance for addressing a residential project’s GHG emissions pursuant to CEQA. Section 15064.4 of the CEQA Guidelines provides direction to lead agencies in determining the significance of the impacts of GHGs, however, it does not establish a specific threshold of significant. Since neither the SCAQMD nor the City of Los Angeles have adopted quantitative thresholds of significance for a residential project’s generation of greenhouse gas emissions, the following analysis is based on a combination of the requirements outlined in the CEQA Guidelines. As required in Section 15064.4 of the CEQA Guidelines, this analysis includes an impact determination based on the following: (1) The extent to which the project may increase or reduce greenhouse gas emissions as compared to the existing environmental setting; (2) Whether the project emissions exceed a threshold of significance that the lead agency determines applies to the project; (3) The extent to which the project complies with regulations or requirements adopted to implement a statewide, regional, or local plan for the reduction or mitigation of greenhouse gas emissions. The Guidelines do not mandate the use of absolute numerical thresholds to measure the significance of greenhouse gas emissions.

For purposes of this analysis, a significant impact would occur if the Proposed Project’s design features are not substantially consistent with the applicable policies and/or regulations outlined in the Scoping Plan, SB 375, SCAG’s 2016-2040 RTP/SCS, and the LA Green Building Code.

## Construction

Construction of the Proposed Project would emit GHG emissions through the combustion of fossil fuels by heavy-duty construction equipment and through vehicle trips generated by construction workers traveling to and from the Project Site. These impacts would vary day to day over the approximate 30-month duration of construction activities.

Emissions of GHGs were calculated using CalEEMod *Version 2013.2.2* for the construction duration of the Proposed Project and the results of this analysis are presented in Table III-6, Project Construction-Related Greenhouse Gas Emissions. As shown in Table III-6, the total GHG emissions from construction activities related to the Proposed Project would be approximately 17,632.70 metric tons with the greatest annual emissions of approximately 6,256 metric tons occurring in year 2017.

**Table III-6**  
**Project Construction-Related Greenhouse Gas Emissions**

Year	CO <sub>2</sub> e Emissions (Metric Tons per Year) <sup>a</sup>
2016	5,034.39
2017	6,255.97
2018	4,315.46
2019	2,026.88
<b>Total Construction GHG Emissions</b>	<b>17,632.70</b>
<i><sup>a</sup> Construction CO<sub>2</sub> values were derived using CalEEMod Version 2013.2.2 Calculation data and results are provided in Appendix D, Greenhouse Gas Emissions Calculations Worksheets.</i>	

## Operation

The average daily GHG emissions generated by the existing Project Site have been estimated utilizing CalEEMod as recommended by the SCAQMD. Table III-7, Existing Project Site Greenhouse Gas Emissions, presents the GHG emissions associated with existing operations at the Project Site. As shown in Table III-7, the existing operations on the Project Site generate approximately 1,195.19 CO<sub>2</sub>e MTYCO<sub>2</sub>e.

**Table III-7**  
**Existing Project Site Greenhouse Gas Emissions**

Emissions Source	CO <sub>2</sub> e Emissions (Metric Tons per Year)
Area	<0.01
Energy	217.23
Mobile	900.13
Waste	40.51
Water	37.32
<b>Total</b>	<b>1,195.19</b>
<i>Calculation data and results provided in Appendix D, Greenhouse Gas Emissions Calculations Worksheets. Source: Parker Environmental Consultants, 2016.</i>	

---

***Project GHG Emissions***

The GHG emissions resulting from operation of the Proposed Project, which involves the usage of on-road mobile vehicles, electricity, natural gas, water, landscape equipment and generation of solid waste and wastewater, were calculated under two separate scenarios in order to illustrate the effectiveness of the Project's compliance with AB 32, the *L.A. Green Building Code*, *SCAG's 2012–2035 RTP/SCS*, *SB 375*, and other mitigating features that would be effective in reducing GHG emissions, such as the Project Site being an infill lot, its proximity to transit and walking distance to a major employment center. For purposes of demonstrating the Proposed Project's consistency with AB32 and the State's goals for reducing GHG emissions to 1990 levels by 2020, the Project's greenhouse gas emissions were quantified to reflect the project's design features such as being an infill development with applicable trip credits for increased density, walkability, transit accessibility, proposing Energy Star rated appliances, and as otherwise being built in compliance with all applicable Green Building Code requirements and applicable regulatory measures (i.e., compliance with Rule 403 (dust suppression), low VOC coatings, increasing energy conservation beyond Title 24, implementing on-site solid waste recycling program).

As shown in Table III-8, below, the Proposed Project's annual generation of GHG emissions is estimated to be 2,115.26 CO<sub>2</sub>e MTY, representing an approximate 920.07 CO<sub>2</sub>e MTY increase as compared to the existing emissions generated on-site under current conditions. For comparative purposes, the GHG emissions from a base project of the same size and proposed land uses, but without the GHG-reducing design features described above for the Proposed Project was quantified. This comparative analysis demonstrates the effect the Proposed Project's compliance with SB 375's citing criteria, re-use of existing buildings, and the structural and operational design features such as installing energy efficient lighting, low flow plumbing fixtures, Energy Star-rated appliances, and implementing a construction and operational recycling program during the life of the Project would have with respect to reducing GHG emissions. As shown in Table III-8, the Proposed Project's consistency with applicable plans, policies and code requirements imposed through the Green Building Code for purposes of conserving resources and reducing GHG emissions, yields an approximate 8% reduction as compared to a base project without such design features and compliance measures.

**Table III-8  
Project Operational Greenhouse Gas Emissions**

Emissions Source	Estimated Project Generated CO <sub>2</sub> e Emissions (Metric Tons per Year)		
	Base Project Without GHG Reduction Features	Proposed Project	Percent Reduction
Area	2.41	2.41	0%
Energy	878.34	757.42	14%
Mobile (Motor Vehicles)	1,845.47	1,839.79	0.3%
Waste	53.91	26.95	50%
Water	122.64	96.12	21%
Construction Emissions <sup>a</sup>	587.76	587.76	--
<b>Project Total</b>	<b>3,490.53</b>	<b>3,310.45</b>	--
<i>Less Existing Project Site</i>	<i>-1,195.19</i>	<i>-1,195.19</i>	--
<b>Project NET TOTAL</b>	<b>2,295.34</b>	<b>2,115.26</b>	<b>8%</b>

<sup>a</sup> The total construction GHG emissions were amortized over 30 years and added to the operation of the Project. Calculation data and results provided in Appendix D, Greenhouse Gas Emissions Calculations Worksheets.

Through required implementation of the Green Building Code, the Project's mixed-use design, and the Project's location on an infill site, the Proposed Project would be consistent with local and statewide goals and policies aimed at reducing the generation of GHGs, including CARB's AB 32 Scoping Plan aimed at achieving 1990 GHG emission levels by 2020. The following describes the benefits and applicability of the Proposed Project's compliance measures and design features that serve to reduce the carbon footprint of the development:

- 1. Infill Development.** The Proposed Project is located on an infill site that is currently developed with auto service and medical office uses. The Project is also located in an area that is adequately served by existing infrastructure and would not require the extension of utilities or roads to accommodate the proposed development.
- 2. Energy Conservation.** As mandated by the Green Building Code, the Project will be required to exceed Title 24 2013 standards and include ENERGY STAR appliances. California's Building Energy Efficiency Standards are updated on an approximately three-year cycle. The 2016 Standards will continue to improve upon the 2013 Standards for new construction of, and additions and alterations to, residential and nonresidential buildings. The effective date of the 2016 Standards is January 1, 2017.
- 3. Solid Waste Reduction Efforts.** The Project is subject to construction waste reduction of at least 50 percent. In addition, operation of the Project is subject to AB 939 requirements to divert 50 percent of solid waste to landfills through source reduction, recycling, and composting. As required by the California Solid Waste Reuse and Recycling Access Act of 1991, the Project would provide adequate storage areas for collection and storage of recyclable waste materials.

4. **Water Conservation.** The Project would be required to provide a schedule of plumbing fixtures and fixture fittings that reduce potable water use within the development by at least 20 percent. It must also provide irrigation design and controllers that are weather- or soil moisture-based and automatically adjust in response to weather conditions and plants' needs. Therefore, the Project's generation of GHG emissions would not make a project-specific or cumulatively considerable contribution to GHG emissions, and impacts would be less than significant.

Therefore, as demonstrated above, the Proposed Project's design features and compliance with regulatory measures would be consistent with local and statewide goals and policies aimed at reducing the generation of GHGs, including CARB's AB 32 Scoping Plan aimed at achieving 1990 GHG emission levels by 2020. Therefore, the Project's generation of GHG emissions would not make a project-specific or cumulatively considerable contribution to conflicting with an applicable plan, policy or regulation for the purposes of reducing the emissions of greenhouse gases and, the Proposed Project's impact would be less than significant. Notwithstanding the Project's less than significant impact upon GHG emissions, the Department of City Planning recommends the following regulatory compliance measure (RC-GHG-1) and Mitigation Measures (GHG-1) be implemented to further reduce GHG emissions in new developments.

- **Regulatory Compliance Measure RC-GHG-1 (Green Building Code):** with the Project shall comply with the City of Los Angeles Green Building Code (Chapter IX, Article 9, of the Los Angeles Municipal Code), the Project shall comply with all applicable mandatory provisions as applicable based on the timing of construction.

#### **Mitigation Measure:**

##### **GHG-1 (Greenhouse Gas Emissions)**

- Low- and non-VOC containing paints, sealants, adhesives, solvents, asphalt primer, and architectural coatings (where used), or pre-fabricated architectural panels shall be used in the construction of the Project to reduce VOC emissions to the maximum extent practicable.

#### **b) Would the project conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?**

**Less than Significant Impact.** As described above and in Question VII(a), the Proposed Project would be consistent with local and statewide goals and policies aimed at reducing the generation of GHGs, including CARB's AB 32 Scoping Plan aimed at achieving 1990 GHG emission levels by 2020. Therefore, the Project's generation of GHG emissions would not make a project-specific or cumulatively considerable contribution to conflicting with an applicable plan, policy or regulation for the purposes of reducing the emissions of greenhouse gases and, the Proposed Project's impact would be less than significant. Similarly, notwithstanding the Project's less than significant impact upon GHG emissions, the Department of City Planning recommends Mitigation Measure GHG-1 (identified above) be implemented to further reduce GHG emissions in new developments.

## Cumulative Impacts

The GHG emissions from a mixed-use development with residential, office, and commercial uses is relatively very small in comparison to state or global GHG emissions and, consequently, they would, in isolation, have no significant direct impact on climate change. Rather, it is the increased accumulation of GHG from more than one project and many sources in the atmosphere that may result in global climate change, which can cause the adverse environmental effects previously discussed. Accordingly, the threshold of significance for GHG emissions determines whether a project's contribution to global climate change is "cumulatively considerable." Many regulatory agencies, including the SCAQMD, concur that GHG and climate change should be evaluated as a potentially significant cumulative impact, rather than a project direct impact. Accordingly, the GHG analysis presented above analyzes whether the Proposed Project's impact would be cumulatively considerable using a plan-based approach (and quantitative and qualitative analysis) to determine the Proposed Project's contributing effect on global warming. As concluded above, the Proposed Project's generation of GHG emissions would represent a 8% reduction in GHG emissions with GHG reduction measures in place as compared to the Project's emissions in the absence of all of the GHG reducing measures and project design features. Furthermore, the Proposed Project would be consistent with all applicable local ordinances, regulations and policies that have been adopted in furtherance of the state and City's goals of reducing GHG emissions. Thus, the Proposed Project would not make a cumulatively considerable contribution to GHG emissions and impacts would be less than significant.

## VIII. HAZARDS AND HAZARDOUS MATERIALS

The following section summarizes and incorporates the reference information from the Environmental Site Assessment prepared by California Environmental Geologists & Engineers, Inc. ("California Environmental"): Environmental Site Assessment – Phase I and Phase II Screening Subsurface Soil Gas Survey, dated November 2014 ("Environmental Site Assessment"). The Project Environmental Site Assessment is included as Appendix E.

### a) **Would the project create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a significant impact may occur if a project would involve the use or disposal of hazardous materials as part of its routine operations, or would have the potential to generate toxic or otherwise hazardous emissions that could adversely affect sensitive receptors. The Proposed Project includes the construction of a unified development with two mixed-use buildings with up to 79 apartment units and 10,300 square feet of ground-floor commercial uses and a small lot subdivision with 61 small lot homes. No hazardous materials other than modest amounts of typical cleaning supplies and solvents used for housekeeping and janitorial purposes would routinely be transported to the Project Site. The use of these substances would comply with State Health Codes and Regulations.

Construction could involve the use of potentially hazardous materials, including vehicle fuels, oils, and transmission fluids. However, all potentially hazardous materials would be contained, stored, and used in accordance with manufacturers' instructions and handled in compliance with applicable standards and

regulations, which include requirements for disposal of hazardous materials at a facility licensed to accept such waste based on its waste classification and the waste acceptance criteria of the permitted disposal facilities. Operation of the proposed residential and commercial uses would involve the use of cleaning materials and supplies that are typical of such land uses. Therefore, the Proposed Project would not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials and impacts would be less than significant.

**b) Would the project create significant hazard to the public or the environment through reasonably foreseeable upset and accidental conditions involving the release of hazardous materials into the environment?**

**Potentially Significant Unless Mitigation Incorporated.** A significant impact may occur if a project utilizes quantities of hazardous materials as part of its routine operations and could potentially pose a hazard to nearby sensitive receptors under accident or upset conditions. An Environmental Site Assessment was prepared for the Project Site, which includes a Phase I analysis and Phase II analysis. The purpose of the Phase I analysis was to provide information regarding the potential for hazardous material impacts to soil and groundwater beneath the properties in order to identify any Recognized Environmental Conditions (RECs). A REC refers to the presence or likely presence of any hazardous substances or petroleum products in, on, or at a property due to any release to the environment, under conditions indicative of a release to the environment, or under conditions that pose a material threat of a future release to the environment. The Phase I also evaluated the presence of Historical Recognized Environmental Conditions (HRECs) and Controlled Recognized Environmental Conditions (CRECs). The Phase II analysis includes a subsurface soil gas survey to evaluate for residual fuel impacts and for evidence of releases associated with the long-term automotive repair use of the property considering the proposed residential redevelopment.

***Database Search***

According to the Environmental Site Assessment, historical site utilization research indicates that the Project Site was developed with agricultural cultivation and associated farmhouses from 1928 to 1952. By 1964, approximately seven structures (residential/commercial) were on site. By 1967, the existing automotive showroom and adjoining repair bay structure are present. The existing configuration of structures is present by 1977. The Project Site is currently developed with three structures: one two-story car showroom and office building with connected service bay buildings, one spray booth and service bay building, and one retail/office building. The buildings were constructed in 1956 (8354 Reseda) and 1965 (8400 Reseda) with significant additions occurring to the 8400 Reseda structures in the subsequent years. The 8400 Reseda buildings are occupied by Picture Car Warehouse specializing in the rehabilitation and maintenance of specialty vehicles used primarily for the entertainment industry (film, television, etc.). The 8400 Reseda parcel includes two paint spray booths and approximately 40 service bays for the painting/automotive maintenance/body work performed on vehicles. The 8350-8354 Reseda office/retail structure is currently occupied by Gary's Custom Optik, an eyeglasses retailer, and the San Fernando Valley Counseling Center. The remainder of the Project Site is asphalt paved parking and storage for vehicles being maintained by Picture Car Warehouse.

The Project Site is identified on the HIST CORTESE, LUST, CA FID UST, HIST UST, and SWEEPS UST

databases. The subject property is the former site of the Neil Lehr Cadillac sales and repair facility. California Environmental reviewed a file pertaining to the Project Site at the City of Los Angeles Fire Department Underground Storage Tank or Hazardous Materials Division. The file contained the installation permits for three underground tanks dated 6/18/65 (permit no. 8640), 6/21/65 (permit no. 8660) and 10/9/70 (permit no. 18503). An approved application for the operation of an auto fueling station dated 1965 was also contained in the file. In 1989, several underground storage tanks were removed from the Project Site and a release of gasoline was discovered. From 1989 to 1997, the Project Site underwent a series of evaluation, assessment, and remediation processes resulting in a final case closure from the LARWQCB in 1997. The nearest listed contaminated site to the Project Site is the former Chevron Station (8301 Reseda Blvd) located approximately 400 feet to the south. The site had a reported release of gasoline and received case closure status in 1996. It is considered unlikely that the soil and/or groundwater beneath the Project Site have been impacted by the down-gradient off-site release.

The Environmental Site Assessment revealed no evidence of recognized environmental conditions (RECs), or controlled recognized conditions (CRECs) in connection with the Project Site. The removal of three USTs, the remediation activities undertaken, and the subsequent closure letter issued by the LARWQCB are considered an historical recognized environmental condition (HREC) in connection with the Project Site.

The documented on-site fuel spill, the long-term history of automotive paint/solvent use and automotive waste generation at the Project Site warranted the need for a subsurface soil gas survey in order to assess the soil conditions. In addition, due to the age of the on-site buildings, there is a potential that asbestos-containing materials (ACMs), lead-based paints (LBPs), and polychlorinated biphenyls (PCBs) are present on-site. These do not qualify as RECs but require discussion. On-site ACMs, LBPs, and PCBs are further discussed, below.

### ***Site Reconnaissance***

California Environmental conducted a site reconnaissance on the Project Site on September 24, 2014 to observe site conditions. Hazardous substance use in the form of automotive servicing products such as solvent based parts cleaner, waste oil/oil filters, used anti-freeze, automotive paints, and used coolant was observed on the Project Site. Several existing aboveground storage tanks for used motor oil were observed on the Project Site at the time of the site reconnaissance. A number of 55-gallon storage drums were also observed. A clarifier was also observed in the area of the waste oil AST and drum storage area. Containers of motor oil and other automotive fluids were observed. Several trash bins were observed. Some overflow of non-hazardous debris was observed in the vicinity of the bins. No evidence of significant spills and/or stains was observed at the time of the site reconnaissance. No evidence of other containers of hazardous or unidentified substances were noted on the Project Site at the time of the site reconnaissance.

#### *Asbestos-Containing Materials (ACMs)*

Sampling of suspect asbestos containing material (ACM) was not included in the scope of work for the Phase I investigation. Since the existing buildings on-site were constructed prior to the ban on the use of asbestos, there is the potential that demolition of these buildings could release ACMs present in the structures. Therefore, prior to demolition activities, a complete asbestos survey would be conducted to identify all sources of asbestos, as required by the U.S. EPA National Emission Standard for Hazardous Air

Pollutants (NESHAP) regulation and the South Coast Air Quality Management District's (SCAQMD's) Rule 1403. Bulk samples of all materials that are suspected of containing asbestos would be collected and analyzed for asbestos content. Asbestos removal is stringently controlled by Federal Regulations and SCAQMD Rule 1403. Removal of asbestos in a building is not unusual and can be readily accomplished. In accordance with the EPA's NESHAP regulation and SCAQMD's Rule 1403, all materials that are identified as ACMs would be removed by a trained and licensed asbestos abatement contractor. The asbestos removal operations would be conducted in accordance with CAL-OSHA Asbestos for the Construction Industry Standard, SCAQMD and EPA rules and regulations and industry standards. The contractor selected for the removal process would be chosen based on experience, reputation, and relationship with local agencies such as SCAQMD and OSHA regional offices. Generally, asbestos removal operations are low risk. When following asbestos-related regulations, the possibility of exposure to airborne asbestos fibers from asbestos removal projects is limited. The SCAQMD has very specific regulations for asbestos emissions. Provided the removal and disposal of ACMs from the Project Site follows the various guidelines required by SCAQMD Rule 1403, as well as all other applicable state and federal rules and regulations, hazardous materials impacts relative to exposure to asbestos would be less than significant.

#### *Lead Based Paint (LBP)*

Due to the age of the existing buildings, lead-based paint may be present on site. Exposure of workers to lead-based paint during demolition of the existing structures could be a hazardous to the health of the construction workers. A qualified lead-paint abatement consultant would be required to comply with applicable state and federal rules and regulations governing lead paint abatement. Such regulations that would be followed during demolition include Construction Safety Orders 1532.1 (pertaining to lead) from Title 8 of the California Code of Regulations, and lead exposure guidelines provided by the U.S. Department of Housing and Urban Development (HUD). Compliance with mandatory state and federal regulations would ensure that the potential lead-based paint on-site would be handled properly and impacts associated with the exposure to lead-based paint would be less than significant.

#### *Polychlorinated Biphenyls (PCBs)*

No evidence of PCB containing transformers or equipment was observed on the Project Site. Fluorescent light fixtures were observed in the subject buildings. Fluorescent light fixtures manufactured prior to 1977 (and fluorescent light fixtures without a date of manufacture) may have ballasts capacitors that contain PCBs, which is recognized by the EPA as a suspect carcinogen. Used fluorescent lamp tubes are considered to be hazardous mercury-bearing waste requiring proper disposal in accordance with local, state, and federal requirements. The on-site ballasts were not inspected during the site reconnaissance. Due to the date of construction of the subject building, it is considered likely that the on-site ballasts contain PCB concentrations greater than the federal action limit of 50 parts per million (ppm). It is recommended that the fluorescent light fixtures be inspected for PCB content labels prior to disposal, and should be handled in accordance with applicable regulations upon demolition. Prior to issuance of a demolition permit, a polychlorinated biphenyl (PCB) abatement contractor shall conduct a survey of the Project Site to identify and assist with compliance with applicable state and federal rules and regulations governing PCB removal and disposal. Therefore, impacts with respect to handling and disposal of PCBs would be less than

significant.

#### *Potential Vapor Encroachment Condition (p-VEC)*

The State of California has adopted Indoor Air Quality Guidelines (CHHSLs) issued by CALEPA in 2005/2010. Potential sources for vapor intrusion to indoor air include degassing of solvents and other compounds from contaminated soil and contaminated groundwater. The long term automotive servicing use of the property, along with the previously identified release of total petroleum hydrocarbons (TPH), and former presence of USTs suggests the potential for a vapor encroachment condition in future on-site structures. The handling and controlling of p-VECs is further discussed below.

#### ***Soil Gas Sampling / Soil Conditions***

Due to the documented on-site fuel spill, the long-term history of automotive paint/solvent use and automotive waste generation at the Project Site, a screening subsurface soil gas survey was implemented to evaluate for residual fuel impacts and for evidence of releases associated with the long-term automotive repair use of the property considering the proposed residential redevelopment of the property. Soil-gas sampling was implemented on-site on October 28, 2014. The soil gas survey consisted of seven soil gas probes installed at five feet below ground surface (bgs). The soil gas probes were located in the vicinity of the removed three USTs, near the clarifier, and in background locations.

The soil gas survey detected benzene, tetrachloroethylene (PCE), and methyl-tert-butyl ether (MTBE) all above their respective California Human Health Screening Levels for residential and commercial properties. All other VOCs were below the method detection limits in the samples obtained. The detections of benzene and MTBE are likely related to residual gasoline contamination beneath the repair bay building to the southeast of the removed gasoline UST. The PCE detections are considered de minimus and likely related to small surface spills of parts cleaning solvent. A contingency for the removal of TPH impacted soil encountered beneath the building footprint during redevelopment should be provided for. TPH (hydraulic oil) is also present beneath the central building. These areas would be uncovered during future grading.

A Soil Management Plan (SMP) would be developed for the future grading work, especially for the identification and segregation of the TPH impacted soil. It is recommended that a post-grading soil gas survey be conducted to confirm that a vapor intrusion hazard does not exist for future on-site structures. The Soils Management Plan (SMP) would be prepared and implemented to provide a framework under which work can proceed safely and contaminated soils can be properly handled, segregated, stockpiled and disposed of at a licensed disposal facility. Proper handling of the contaminated media would be required regardless of the contamination source. Further, if the Proposed Project's subterranean development extends to depths in which groundwater is encountered, the groundwater would be properly handled and managed. If dewatering activities are required, such activities would comply with the requirements of the Waste Discharge Requirements for Discharges of Groundwater from Construction and Project Dewatering to Surface Waters in Coastal Watersheds of Los Angeles and Ventura Counties (Order No. R4-2008-0032, National Pollutant Discharge Elimination System No. CAG994004) or subsequent permit. This would include submission of a Notice of Intent for coverage under the permit to the Los Angeles Regional Water Quality Control Board at least 45 days prior to the start of dewatering and compliance with all applicable

provisions in the permit, including water sampling, analysis, and reporting of dewatering-related discharges.

The Proposed Project's compliance with mandatory state and federal regulatory compliance measures and implementation of Mitigation Measure HAZ-1 would ensure that potential impacts associated with the release of a hazardous material would be less than significant.

**Regulatory Compliance Measure:**

**RC-HAZ-1 Explosion/Release (Existing Toxic and Hazardous Construction Materials)**

- (Asbestos) Prior to the issuance of the demolition permit, the applicant shall provide a letter to the Department of Building and Safety from a qualified asbestos abatement consultant that no ACM are present in the building. If ACM are found to be present, it will need to be abated in compliance with the South Coast Air Quality Management District's Rule 1403 as well as other state and federal regulations.
- (Lead Paint) Prior to the issuance of any permit for demolition or alteration of the existing structure(s), a lead-based paint survey shall be performed to the written satisfaction of the Department of Building and Safety. Should lead-based paint materials be identified, standard handling and disposal practices shall be implemented pursuant to OSHA regulations.
- (Polychlorinated Biphenyl – Commercial and Industrial Buildings) Prior to issuance of a demolition permit, a polychlorinated biphenyl (PCB) abatement contractor shall conduct a survey of the Project Site to identify and assist with compliance with applicable state and federal rules and regulations governing PCB removal and disposal.

**Mitigation Measure:**

**HAZ-1:** To verify whether residual areas of impacted soil maybe present under buildings, a Soil Management Plan (SMP) shall be developed for the future grading work, especially for the identification and segregation of the petroleum hydrocarbons (TPH) impacted soil. It is recommended that a post-grading soil gas survey be conducted to confirm that a vapor intrusion hazard does not exist for future onsite structures. The SMP and results of the post-grading soil gas survey shall be submitted to the LARWQCB File #913240316 for their review and incorporation into the agency file.

**c) Would the project emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?**

**Potentially Significant Unless Mitigation Incorporated.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact to hazards and hazardous materials if: (a) the project involved a risk of accidental explosion or release of hazardous substances (including, but not limited to oil, pesticides, chemicals or radiation); or (b) the project involved the creation

of any health hazard or potential health hazard. The determination of significance shall be made on a case-by-case basis considering the following factors: (a) the regulatory framework for the health hazard; (b) the probable frequency and severity of consequences to people or property as a result of a potential accidental release or explosion of a hazardous substance; (c) the degree to which project design will reduce the frequency or severity of a potential accidental release or explosion of a hazardous substance; (d) the probable frequency and severity of consequences to people from exposure to the health hazard; and (e) the degree to which project design would reduce the frequency of exposure or severity of consequences of exposure to the health hazard.

There is one school within one-quarter mile the Project Site: Magnolia Science Academy 7, located at 18355 Roscoe Boulevard, approximately immediately southeast of the Project Site. Localized construction impacts associated with noise, dust and localized air quality emissions, and construction traffic/hauling activities generally occur within a radius of 500 feet or less of the Project Site. Because of the short distance between the Project Site and the Magnolia Science Academy 7, the Proposed Project has the potential to expose students and staff of the Magnolia Science Academy 7 to potentially hazardous materials, substances, or waste during the construction period. The Proposed Project would provide appropriate construction measures to reduce the Project's impacts upon the nearby school facility. Further, the Project's proposed haul route would be designed to minimize, to the greatest degree possible, hauling impacts on Magnolia Science Academy 7. The proposed haul route would utilize the Ronald Reagan Freeway (CA-118). Traveling to and from the 118 Freeway, the haul route would utilize the Reseda Boulevard on- and off-ramp and would not pass the aforementioned school. Implementation of Mitigation Measures HAZ-2, below, would reduce potential construction impacts related to nearby schools to less than significant levels.

### **Mitigation Measure:**

#### **HAZ-2 Construction Activity Near Schools**

- The Applicant and contractors shall maintain ongoing contact with administrator of Magnolia Science Academy 7. The administrative offices shall be contacted when demolition, grading and construction activity begin on the project site so that students and their parents will know when such activities are to occur. The developer shall obtain school walk and bus routes to the schools from either the administrators or from the LAUSD's Transportation Branch (323) 342-1400 and guarantee that safe and convenient pedestrian and bus routes to the school be maintained.
- The Applicant shall install appropriate traffic signs around the site to ensure pedestrian and vehicle safety.
- There shall be no staging or parking of construction vehicles, including vehicles to transport workers on Roscoe Boulevard, adjacent to the school.
- Due to noise impacts on the schools, no construction vehicles or haul trucks shall be staged or idled on Roscoe Boulevard, adjacent to the school, during school hours.

Furthermore, no hazardous materials other than modest amounts of typical cleaning supplies and solvents used for housekeeping and janitorial purposes would be typically associated with the operation and activities of the proposed land uses, and the use of these substances would comply with State Health Codes

and Regulations. The Proposed Project's operational impacts would have a less than significant impact.

**d) Would the project be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?**

**Potentially Significant Unless Mitigation Incorporated.** California Government Code Section 65962.5 requires various state agencies to compile lists of hazardous waste disposal facilities, unauthorized releases from underground storage tanks, contaminated drinking water wells, and solid waste facilities from which there is known migration of hazardous waste, and submit such information to the Secretary for Environmental Protection on at least an annual basis. A significant impact may occur if the Project Site is included on any of the above lists and poses an environmental hazard to surrounding sensitive uses.

As described above, the Project Site is identified on the following databases: HIST CORTESE, LUST, CA FID UST, HIST UST, and SWEEPS UST. The Environmental Site Assessment determined that there were no RECs in connection with the Project Site. The Project Site is the site of a former leaky underground tank that was evaluated, remediated, and granted case closure by LARWQCB in 1996. A screening subsurface soil gas survey was later prepared to evaluate residual fuel impacts. The soil gas survey detected benzene, tetrachloroethylene (PCE), and methyl-tert-butyl ether (MTBE) all above their respective California Human Health Screening Levels for residential and commercial properties. All other VOCs were below the method detection limits in the samples obtained. A contingency for the removal of TPH impacted soil encountered beneath the building footprint during redevelopment would be implemented.

A Soils Management Plan (SMP) would be prepared and implemented to provide a framework under which work can proceed safely and contaminated soils can be properly handled, segregated, stockpiled and disposed of at a licensed disposal facility, especially for the identification and segregation of the TPH impacted soil. The Project would comply with mandatory state and federal regulatory compliance measures. Incorporation of regulatory compliance measures and Mitigation Measures HAZ-1 and HAZ-2, above, would ensure potential impacts would be reduced to less than significant levels.

**e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?**

**No Impact.** A significant project-related impact may occur if the Proposed Project were placed within a public airport land use plan area, or within two miles of a public airport, and subject to a safety hazard. The closest airport is the Van Nuys Airport, approximately 2.5 miles east of the Project Site. The airport is not located within two miles of the Project Site. The Proposed Project is located in an airport hazard area and has a 200-foot height limit above a mean sea level (MSL) elevation of 790 feet, which is an approximate 990 foot maximum elevation above MSL for a proposed building. The elevation of the Project Site is

approximately 775 feet above MSL.<sup>18</sup> The Proposed Project would reach a maximum height of 45 feet above grade. Therefore, the Proposed Project would reach a maximum height elevation of 820 feet above MSL. As such, the Proposed Project is well within the maximum height limit and would not expose its residents to any airport related hazard. Furthermore, the Project Site is not located within an airport land use plan. Therefore, no impact would occur.

**f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?**

**No Impact.** This question would apply to the Proposed Project only if it were in the vicinity of a private airstrip and would subject area residents and workers to a safety hazard. The Project Site is not within the vicinity of a private airstrip. Therefore, no impact would occur.

**g) Would the project impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?**

**No Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact to hazards and hazardous materials if: (a) the project involved possible interference with an emergency response plan or emergency evacuation plan. According to the *L.A. CEQA Thresholds Guide*, the determination of significance shall be made on a case-by-case basis considering the degree to which the project may require a new, or interfere with an existing emergency response or evacuation plan, and the severity of the consequences. The Project Site is not located on an identified disaster route or an adopted emergency response or evacuation plan.<sup>19,20</sup> Development of the Project Site may require temporary and/or partial street closures due to construction activities. Nonetheless, while such closures may cause temporary inconvenience, they would not be expected to substantially interfere with emergency response or evacuation plans. The Proposed Project would not cause permanent alterations to vehicular circulation routes and patterns, impede public access or travel upon public rights-of-way. Therefore, the Proposed Project would not be expected to interfere with any adopted emergency response plan or emergency evacuation plan, and no significant impacts would occur.

---

<sup>18</sup> *California Environmental Geologists & Engineers, Inc., Environmental Site Assessment – Phase I and Phase II Screening Subsurface Soil Gas Survey, Commercial Property, 8350-8354, 8400, & 8420 Reseda Blvd, APN 2786-021-017 & -018; 2786-022-021, Northridge, CA 91324, November 2014.*

<sup>19</sup> *Los Angeles County Department of Public Works, City of Los Angeles Valley Area Disaster Route Map, August 13, 2008.*

<sup>20</sup> *City of Los Angeles, Safety Element Exhibit H, Critical Facilities and Lifeline Systems in the City of Los Angeles, April 1995.*

- h) Would the project expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?**

**No Impact.** The Project Site is located in a highly urbanized area of the Northridge community within the City of Los Angeles and does not include wildlands or high fire hazard terrain or vegetation. The Project Site is not located in a Very High Fire Hazard Severity Zone (VHFHSZ).<sup>21</sup> Therefore, no impacts from wildland fires are expected to occur.

### **Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project in combination with the nine related projects has the potential to increase to some degree the risks associated with the use and potential accidental release of hazardous materials in the City of Los Angeles. However, the potential impact associated with the Proposed Project would be less than significant and, therefore, not cumulatively considerable. With respect to the related projects, the potential presence of hazardous substances would require evaluation on a case-by-case basis, in conjunction with the development proposals for each of those properties. Further, local municipalities are required to follow local, state, and federal laws regarding hazardous materials, which would further reduce impacts associated with the related projects. Therefore, with compliance with local, state, and federal laws pertaining to hazardous materials, the Proposed Project in conjunction with related projects would be expected to result in less-than-significant cumulative impacts with respect to hazardous materials.

## **IX. HYDROLOGY AND WATER QUALITY**

- a) Would the project violate any water quality standards or waste discharge requirements?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on surface water quality if discharges associated with the project would create pollution, contamination, or nuisance as defined in Section 13050 of the California Water Code (CWC) or that cause regulatory standards to be violated, as defined in the applicable National Pollution Discharge Elimination System (NPDES) stormwater permit or Water Quality Control Plan for the receiving body of water. A significant impact may occur if a project would discharge water which does not meet the quality standards of agencies which regulate surface water quality and water discharge into stormwater drainage systems. Significant impacts would also occur if a project does not comply with all applicable regulations with regard to surface water quality as governed by the State Water Resources Control Board (SWRCB) through its nine Regional Boards. The Project Site lies within the Los Angeles Regional Water Quality Control Board (LARWQCB). Applicable regulations include compliance with the

---

<sup>21</sup> *City of Los Angeles, Department of City Planning, City of Los Angeles Zoning Information and Map Access System (ZIMAS), Parcel Profile Report, website: [www.zimas.lacity.org](http://www.zimas.lacity.org), accessed September 2015.*

---

Standard Urban Storm Water Mitigation Plan (SUSMP) and the Stormwater Low Impact Development (LID) Ordinance (No. 181899) requirements to reduce potential water quality impacts.

### ***Construction***

Three general sources of potential short-term, construction-related stormwater pollution associated with the Proposed Project include: 1) the handling, storage, and disposal of construction materials containing pollutants; 2) the maintenance and operation of construction equipment; and 3) earth moving activities which, when not controlled, may generate soil erosion via storm runoff or mechanical equipment. As required under the National Pollution Discharge Elimination System (NPDES), the Project Applicant is responsible for preparing a Storm Water Pollution Prevention Plan (SWPPP) to mitigate the effects of erosion and the inherent potential for sedimentation and other pollutants entering the stormwater system. The primary objectives of the NPDES stormwater program requirements are to: 1) effectively prohibit non-stormwater discharges; and 2) reduce the discharge of pollutants from stormwater conveyance systems to the Maximum Extent Practicable (“MEP” statutory standard). The SWPPP would incorporate the required implementation of Best Management Practices (BMPs) for erosion control and other measures to meet the NPDES requirements for stormwater quality. Implementation of the BMPs identified in the SWPPP and compliance with the NPDES and City discharge requirements would ensure that the construction of the Proposed Project would not violate any water quality standards or discharge requirements, or otherwise substantially degrade water quality. Furthermore, the implementation of the code required SWPPP would ensure that the Proposed Project’s construction-related water quality impacts would be less than significant.

### ***Operation***

The Project Site is currently developed with a car resale center, an auto body shop, and commercial offices with a total building area of approximately 27,411 square feet with designated surface parking areas. The Project Site is almost completely developed with impervious surfaces. Therefore, the Project Site is nearly 100 percent impervious. Thus, nearly 100 percent of the surface water runoff from the Project Site is currently directed to adjacent storm drains and only a very small percentage of stormwater percolates into the groundwater table beneath the Project Site.

The Proposed Project includes the construction of unified development that would include a total of 140 dwelling units (which includes 79 apartment units and 61 small-lot subdivision home) and 10,300 square feet of commercial space. The apartment unit and commercial element would be provided in two mixed-use buildings fronting Reseda Boulevard. The small lot subdivision would be east of the mixed-use buildings. The Proposed Project would provide open space, yards, and setbacks associated with the small lot subdivision and multi-family development. As such, approximately 90 percent of the Project Site would be developed with impervious surfaces. Development of the Project would result in an increase of pervious surfaces on-site and a decrease in Project Site runoff. The Proposed Project would generate surface water runoff, which would be directed to stormwater drains. Further, the Proposed Project would be required to demonstrate compliance with Low Impact Development (LID) Ordinance standards and retain or treat the first ¾-inch of rainfall in a 24-hour period. Compliance with LID Ordinance would further reduce the amount of surface water runoff leaving the Project Site. The Project would also be required to comply with

the City of Los Angeles Ordinance No. 172,176 and Ordinance No. 173,494 specify Stormwater and Urban Runoff Pollution Control, which require the application of Best Management Practices (BMPs). As such, the Proposed Project would decrease the amount of surface water runoff as compared to the current conditions. Full compliance with the following regulatory compliance measures would ensure that the operation of the Proposed Project would not violate any water quality standards or discharge requirements or otherwise substantially degrade water quality.

- **Regulatory Compliance Measure RC-WQ-1: National Pollutant Discharge Elimination System General Permit.** Prior to issuance of a grading permit, the Applicant shall obtain coverage under the State Water Resources Control Board National Pollutant Discharge Elimination System General Permit for Storm Water Discharges Associated with Construction and Land Disturbance Activities (Order No. 2009-0009-DWQ, National Pollutant Discharge Elimination System No. CAS000002) (Construction General Permit) for Phase 1 of the Proposed Project. The Applicant shall provide the Waste Discharge Identification Number to the City of Los Angeles to demonstrate proof of coverage under the Construction General Permit. A Storm Water Pollution Prevention Plan shall be prepared and implemented for the Proposed Project in compliance with the requirements of the Construction General Permit. The Storm Water Pollution Prevention Plan shall identify construction Best Management Practices to be implemented to ensure that the potential for soil erosion and sedimentation is minimized and to control the discharge of pollutants in stormwater runoff as a result of construction activities.
- **Regulatory Compliance Measure RC-WQ-2: Low Impact Development Plan.** Prior to issuance of grading permits, the Applicant shall submit a Low Impact Development Plan and/or Standard Urban Stormwater Mitigation Plan to the City of Los Angeles Bureau of Sanitation Watershed Protection Division for review and approval. The Low Impact Development Plan and/or Standard Urban Stormwater Mitigation Plan shall be prepared consistent with the requirements of the Development Best Management Practices Handbook.

Stormwater management design would be required to conform to the City of Los Angeles Stormwater LID Ordinance. The Stormwater LID Ordinance was adopted in November 2011 and requires stormwater mitigation for a larger number of development and redevelopment projects than was previously required under SUSMP. The Ordinance has expanded to include all development and redevelopment projects within the City of Los Angeles that require a building permit and that create, add, or replace 500 square feet or more of impervious area.<sup>22</sup> This Ordinance requires developments to capture and treat the first 3/4-inch rainfall in accordance with established stormwater treatment priorities.

The Proposed Project falls within the second tier of the LID Ordinance requirements, which state that

---

<sup>22</sup> *City of Los Angeles, Development Best Management Practices Handbook, Low Impact Development Manual, Part B Planning Activities. Fourth Edition, June 2011.*

development projects that involve residential use with five or more units and result in an alteration of at least 50% or more of the impervious surfaces on an existing developed site, the entire site must comply with the standards and requirements of Article 4.4 of Chapter VI of the Los Angeles Municipal Code and with the Development Best Management Practices Handbook. The Project Site would be designed to manage and capture stormwater runoff to the maximum extent feasible utilizing various LID Ordinance techniques, including but not limited to infiltration, evapotranspiration, capture for use, high efficiency bio-filtration and retention systems BMP (listed in priority order). If partial or complete on-site compliance of any type is technically infeasible, the Project Site and LID Plan shall be required to comply with all applicable SUSMP requirements in order to maximize on-site compliance.<sup>23</sup> Therefore, as the Proposed Project would be subject to the LID requirements, operational water quality impacts would be less than significant.

**b) Would the project substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?**

**No Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on groundwater level if it would change potable water levels sufficiently to: (a) reduce the ability of a water utility to use the groundwater basin for public water supplies, conjunctive use purposes, storage of imported water, summer/winter peaking, or respond to emergencies and drought; (b) reduce yields of adjacent wells or well fields (public or private); (c) adversely change the rate or direction of flow of groundwater; or (d) result in demonstrable and sustained reduction in groundwater recharge capacity. As discussed in Question VIII(a) the Project Site is nearly 100 percent impervious. As such, almost all of the surface water runoff from the Project Site is currently directed to adjacent storm drains and a small percentage percolates into the groundwater table beneath the Project Site. According to the Geotechnical Report, groundwater was encountered at a depth of 20 to 27 feet below the ground surface. The Proposed Project would excavate soils beneath the Project Site to allow for a one-level subterranean parking structure beneath the two mixed-use buildings. The subterranean parking structure would extend to a depth of approximately 12 feet below grade and would not impact the potential groundwater table. Further, as discussed above, development of the Proposed Project would increase the amount of pervious surfaces on the Project Site. Development of the Project would increase the amount of surface area in which rainwater can percolate into the groundwater. As such, development of the Proposed Project is not expected to cause the depletion of the groundwater supplies or the interference of groundwater recharge. The Proposed Project would continue to be supplied with potable water by the LADWP. Further, the Proposed Project would comply with the LID Ordinance and LAMC Section 64.70, Stormwater Runoff and Urban Pollution Control. Thus, construction of the Proposed Project would not deplete groundwater supplies or interfere substantially with groundwater recharge, and no impact would

---

<sup>23</sup> *Stormwater LID Ordinance (No. 181899), 2011.*

occur.

- c) Would the project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner, which would result in substantial erosion or siltation on- or off-site?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on surface water hydrology if it would result in a permanent, adverse change to the movement of surface water sufficient to produce a substantial change in the current or direction of water flow. The Project Site is located in an urbanized area of the City of Los Angeles, and no streams or river courses are located on or within the Project vicinity. The Project Site is currently nearly 100 percent impervious. Implementation of the Proposed Project is expected to decrease the amount of stormwater runoff leaving the Project Site, since the Project would increase pervious surfaces on-site and would comply with the LID Ordinance. The Proposed Project would not significantly increase site runoff or result in any changes in the local drainage patterns. Further, the Proposed Project would comply with LAMC Section 64.70, Stormwater Runoff and Urban Pollution Control. Impacts associated with localized drainage and surface water runoff would therefore be considered less than significant.

- d) Would the project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on surface water hydrology if it would result in a permanent, adverse change to the movement of surface water sufficient to produce a substantial change in the current or direction of water flow. The Proposed Project would not result in a significant increase in site runoff, or any changes in the local drainage patterns. Therefore, as the Proposed Project would not substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site, no impact would occur. In addition, the Proposed Project will comply with the required regulatory compliance measures, including the implementation of the LID Ordinance, and impacts would be less than significant.

- e) Would the project create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on surface water quality if discharges associated with the Project would create pollution, contamination, or nuisance as defined in Section 13050 of the California Water Code (CWC) or that cause regulatory standards to be violated, as defined in the applicable National Pollution Discharge Elimination System (NPDES) stormwater permit or Water Quality Control Plan for the receiving water body. A significant impact may occur if the volume of stormwater runoff from the Project Site were to increase to a level that exceeds the capacity of the storm drain system serving the Project Site. A significant adverse effect would also occur if a project substantially increases the probability that polluted

runoff would reach the storm drain system.

Currently, the Project Site is completely developed with impervious surfaces and nearly 100 percent of surface water runoff is directed to adjacent street storm drains. Existing storm drain lines serving the Project Site are located on Reseda Boulevard. The Proposed Project would not result in a significant increase in site runoff, or any changes in the local drainage pattern. Runoff from the Project Site currently is and would continue to be collected on the Project Site and directed towards existing storm drains in the Project vicinity that have adequate capacity. Pursuant to local practice and City policy stormwater retention will be required as part of the Low Impact Development (LID) Ordinance / SUSMP implementation features (despite no increase in imperviousness of the site). Any contaminants gathered during routine cleaning of construction equipment would be disposed of in compliance with applicable stormwater pollution prevention permits. Further any pollutants from the parking areas would be subject to the requirements and regulations of the NPDES and applicable LID Ordinance standards and retain or treat the first  $\frac{3}{4}$  -inch of rainfall in a 24-hour period, which will reduce the Proposed Project's impact to the stormwater infrastructure. Therefore, the Proposed Project would not create or contribute to runoff water that would exceed capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff. Potential impacts to surface water quality would be less than significant. Further, the Proposed Project will comply with LAMC Section 64.70, Stormwater Runoff and Urban Pollution Control, and all applicable laws and regulations pertaining to stormwater runoff and water quality would ensure impacts are reduced to a less than significant level.

**f) Would the project otherwise substantially degrade water quality?**

**No Impact.** A significant impact may occur if a project includes potential sources of water pollutants that would have the potential to substantially degrade water quality. The Proposed Project does not include potential sources of contaminants, which could potentially degrade water quality and would comply with all federal, state and local regulations governing stormwater discharge. Therefore, no impact would occur.

**g) Would the project place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?**

**No Impact.** A significant impact would occur if the Project were to place housing within a 100-year flood hazard area. A 100-year flood is defined as a flood which results from a severe rainstorm with a probability of occurring approximately once every 100 years. According to the Federal Emergency Management Agency (FEMA), the Project Site is not located in an area designated as a 100-year flood hazard area. The Project Site is in a zone designated as Zone X, which classifies the area as a minimal flood hazard area.<sup>24</sup> Therefore, the Proposed Project would not place housing within a 100-year flood hazard area, and no impact would occur.

---

<sup>24</sup> *Federal Emergency Management Agency (FEMA), Flood Insurance Rate Map, Los Angeles County, California and Incorporated Areas, Map number 06037C1285F, September 26, 2008.*

**h) Would the project place within a 100-year flood hazard area structures which would impede or redirect flood flows?**

**No Impact.** A significant impact may occur if the Project was located within a 100-year flood zone, which would impede or redirect flood flows. The Project Site is not in an area designated as a 100-year flood hazard area as mapped by the FEMA's Flood Insurance Rate Map. The Project Site is in a zone designated as Zone X (a minimal flood hazard area).<sup>25</sup> The Project Site is located in an urbanized area. As no changes to the local drainage pattern would occur with implementation of the Proposed Project, the Proposed Project would not have the potential to impede or redirect floodwater flows. Therefore, no impact would occur.

**i) Would the project expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?**

**No Impact.** A significant impact may occur if the Proposed Project exposes people or structures to a significant risk of loss or death caused by the failure of a levee or dam, including but not limited to a seismically-induced seiche. Seiches are large waves generated in very large enclosed bodies of water or partially enclosed arms of the sea in response to ground shaking. Tsunamis are waves generated in large bodies of water by fault displacement or major ground movement. Review of the City of Los Angeles General Plan Safety Element, the Proposed Project does not lie within an inundation or tsunami hazard area.<sup>26</sup> Thus, the Proposed Project would not expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam. Therefore, no impact would occur.

**j) Would the project expose people or structures to a significant risk of loss, injury or death involving inundation by seiche, tsunami, or mudflow?**

**No Impact.** A significant impact would occur if the Project Site is sufficiently close to the ocean or other water body to be potentially at risk of the effects of seismically-induced tidal phenomena (i.e., seiche and tsunami), or if the Project Site is located adjacent to a hillside area with soil characteristics that would indicate potential susceptibility to mudslides or mudflows. Review of the City of Los Angeles General Plan Safety Element, the Proposed Project does not lie within an inundation or tsunami hazard area.<sup>27</sup> The Project Site and the surrounding area are highly urbanized and relatively flat. The Project's Geotechnical Report finds that the site elevation is not considered capable of landsliding due to the lack of slopes on or near the Project Site. Thus, the occurrence of mudflows on the Site is considered remote. Therefore, the Project Site is not subject to slope instability, tsunamis, and seiches. Therefore, no impact would occur.

---

<sup>25</sup> *Ibid.*

<sup>26</sup> *City of Los Angeles Department of City Planning, General Plan Safety Element, Safety Element Exhibit G: Inundation & Tsunami Hazard Areas In the City of Los Angeles, March 1994.*

<sup>27</sup> *Ibid.*

## Cumulative Impacts

**Less Than Significant Impact.** Development of the Proposed Project in combination with the nine related projects would result in the further infilling of uses in a highly developed area within Downtown Los Angeles. As discussed above, the Project Site and the surrounding areas are served by the existing storm drain system. Runoff from the Project Site and adjacent urban uses is typically directed into the adjacent streets, where it flows to the nearest drainage improvements. It is likely that most, if not all, of the related projects would also drain to the surrounding street system. However, little if any additional cumulative runoff is expected from the Proposed Project and the related project sites, since the surrounding area is highly developed with impervious surfaces. Under the requirements of the LID Ordinance, each related project will be required to implement stormwater BMPs to retain or treat the runoff from a storm event producing  $\frac{3}{4}$ -inch of rainfall in a 24-hour period. Mandatory structural BMPs in accordance with the NPDES water quality program will therefore result in a cumulative reduction to surface water runoff, as the development in the surrounding area is limited to infill developments and redevelopment of existing urbanized areas. Therefore, the Proposed Project would not make a cumulatively considerable contribution to impacting the volume or quality of surface water runoff, and cumulative impacts to the existing or planned stormwater drainage systems would be less than significant. Therefore, cumulative water quality impacts would be less than significant.

## X. LAND USE AND PLANNING

### a) Would the project physically divide an established community?

**No Impact.** A significant impact may occur if the project would be sufficiently large enough or otherwise configured in such a way as to create a physical barrier within an established community. According to the *L.A. CEQA Thresholds Guide*, the determination of significance shall be made on a case-by-case basis considering the following factors: (a) the extent of the area that would be impacted, the nature and degree of impacts, and the types of land uses within that area; (b) the extent to which existing neighborhoods, communities, or land uses would be disrupted, divided or isolated, and the duration of the disruptions; and (c) the number, degree, and type of secondary impacts to surrounding land uses that could result from implementation of the Proposed Project.

The Project Site is located in an urbanized area of the Northridge Community and is consistent with the existing physical arrangement of the properties within the vicinity of the Project Site. The Project Site is currently developed with a car resale center, auto body shop, and a commercial office building along with areas designated for surface parking. The total lot area of the Project Site is approximately 143,552 square feet. The Proposed Project would include the demolition of the existing buildings on-site, and the construction of a unified development with a small lot subdivision with 61 small lot residential units and two mixed-use residential buildings with up to 79 multi-family dwelling units and 10,300 square feet of ground-floor commercial. The Proposed Project would be a unified development with a mix of small lot homes, multi-family apartments, and ground-floor commercial space. The Proposed Project would also establish transition between the commercial uses along Reseda Boulevard and the single-family residential community east of the Project Site. The Proposed Project would increase the housing stock in the

Northridge community. No separations of uses or disruption of access between land use types would occur as a result of the Proposed Project. Accordingly, implementation of the Proposed Project would not disrupt or divide the physical arrangement of the established community, and no impact would occur.

- b) Would the project conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?**

**Less Than Significant Impact.** A significant impact may occur if a project is inconsistent with the General Plan or zoning designations applicable to the Project Site, and would cause adverse environmental effects, which the General Plan and zoning designations are created to avoid or mitigate.

The Project proposes the development of a small lot subdivision with 61 small lot homes and two four-story mixed-use buildings with a maximum of 79 multi-family dwelling units and 10,300 square feet of ground-floor commercial. The Project Site is zoned C2-1VL and P-1VL with a General Plan land use designation of General Commercial. The partial areas zoned P-1VL are Automobile Parking Zones within the General Commercial zones. Pursuant to LAMC Section 12.14.C, for all portions of C2 buildings used for residential purposes, side and rear yards must conform to the requirements of an R4 Zone at the lowest residential floor level. The minimum lot area requirements of the R4 Zone also apply to all portions of C2 buildings used for residential purposes. The 1VL designation indicates that the Project Site is located within a Very Limited Height District in Height District No. 1, which specifies a maximum height of three stories and 45 feet above grade. However, a building in Height District 1VL designed and used entirely for residential purposes shall be limited to the number of feet in height, but not the number of stories. Height District No. 1VL also limits the maximum floor area ratio (FAR) to 1.5 times the buildable area of the lot.

The Project Site is located within the Northridge Community Plan area (CPA) of the City of Los Angeles. The SCAQMD Air Quality Management Plan and the Southern California Association of Governments (SCAG) 2012-2035 Regional Transportation Plan/Sustainable Communities Plan (RTP/SCS) are regional plans that guide development within the City of Los Angeles. On a local level, the City of Los Angeles General Plan (General Plan), the City of Los Angeles Municipal Code (LAMC), and the Northridge Community Plan (“Community Plan”) guide development on the Project Site. These plans and their applicability to the Proposed Project are further discussed below.

## **Regional Plans**

### ***SCAQMD Air Quality Management Plan***

The Proposed Project is located within the South Coast Air Basin (Basin) and, therefore, falls under the jurisdiction of the SCAQMD. In conjunction with SCAG, the SCAQMD is responsible for formulating and implementing air pollution control strategies. The SCAQMD’s most recent Air Quality Management Plan (AQMP) was updated in 2012 to establish a comprehensive air pollution control program leading to the attainment of State and federal air quality standards in the Basin, which is a non-attainment area. The Proposed Project conforms to the zoning and land use designations for the Project Site as identified in the

General Plan, and, as such, would not add emissions to the Basin that were not already accounted for in the approved AQMP. Furthermore, as noted in Checklist Section 2 (b), Air Quality, the Proposed Project would not exceed the daily emission thresholds during the construction or operational phases of the Proposed Project. Therefore, the Project would be consistent with the AQMP.

### ***SCAG Regional Comprehensive Plan***

The Project Site is located within the six-county region that comprises the SCAG planning area. The SCAG Regional Comprehensive Plan (RCP) includes growth management policies that strive to improve the standard of living, maintain the regional quality of life, and provide social, political, and cultural equity. The Proposed Project would be consistent with policies set forth in the RCP, as the Proposed Project would redevelop an existing parking lot and underutilized commercial property with a high density multi-family residential development with project-serving retail and restaurant uses, thereby maximizing a property that is easily accessible to mass transit, and that is least likely to cause an adverse environmental impact. Furthermore, as the Proposed Project would add approximately 140 residential units to the City of Los Angeles, generating as many as 389 residents, which is consistent with SCAG growth projections.

### **Local Plans**

#### ***Northridge Community Plan***

The Project Site is located within the Northridge Community Plan Area (CPA). Therefore, all development activity on-site is subject to the land use regulations of the Northridge Community Plan (Community Plan). The Community Plan sets forth goals to maintain the community's individuality that include: preserving, enhancing and providing a variety of housing opportunities, improving the function, design and economic vitality of the commercial corridors, preserving and enhancing characteristics of existing uses, maximizing rail transit system opportunities, and planning the remaining commercial and industrial sites for needed job producing uses to improve the economic and physical condition of the Northridge area.<sup>28</sup> The Proposed Project, which would provide a mixed-use development and small lot residential units to the Northridge area, would conform to the goals, objectives, and land uses identified in the Community Plan by providing a variety of housing options and providing commercial to the commercial corridor along Reseda Boulevard.

The Proposed Project would revitalize the area with the development of two four-story mixed-use residential and commercial buildings and 61 three-story small lot residential units. The mixed-use buildings would provide a maximum of 79 dwelling units (consisting of 26 studio apartments, 34 one-bedroom apartments, and 19 two-bedroom apartments) and 10,300 square feet of ground-floor commercial space with a total of 297 automobile parking spaces and 163 bicycle spaces. The Proposed Project would provide a variety of on-site amenities, which include common open space areas, amenity rooms, roof garden/terrace,

---

<sup>28</sup> *City of Los Angeles Department of City Planning, Northridge Community Plan, 1998.*

and private open space decks for the residents and guests. A detailed analysis of the consistency of the Proposed Project with the applicable objectives and policies of the Northridge Community Plan for Residential and Commercial Land Uses is presented in Table III-9, below.

**Table III-9  
Project Consistency with Applicable Objectives and Policies of the  
Northridge Community Plan Land Use Element for Residential and Commercial Land Uses**

Objective / Policy	Project Consistency Analysis
<i>Residential</i>	
<p><b>Objective 1-1:</b> To provide for the preservation of existing and the development of new housing to meet the diverse economic and physical needs of the existing residents and project population of the Plan area to the year 2010.</p>	<p>Consistent. The Proposed Project would provide up to 140 total residential units including 79 multi-family dwelling units and 61 small lot homes to meet the needs of the projected population. The Proposed Project would provide a diverse number of housing options for the community at market rate for all persons, without discrimination. Thus, the Proposed Project would be consistent with this objective.</p>
<p><b>Policy 1-1.1:</b> Designate lands for single and multi-family residential development. Maintain single-family residential to the maximum extent possible. Commercial development should not intrude into residential areas.</p>	<p>Consistent. The Project Site is currently developed with commercial uses and is zoned for commercial development. The Proposed Project would construct two mixed-use building along Reseda Boulevard, which supports and is consistent with the properties along Reseda Boulevard, and development a small lot subdivision to the east of the mixed-use buildings. The small lot residential subdivision would be consistent with the single-family homes along the east side of Canby Avenue. The 10,300 square feet of ground-floor retail and restaurant would not intrude into a residential zoned areas east of the Project Site and would be located along the commercial corridor of Reseda Boulevard. Thus, the Proposed Project would consistent with this policy.</p>
<p><b>Policy 1-1.2:</b> Protect existing single-family residential neighborhoods from encroachment by higher density residential and other incompatible uses.</p>	<p>Consistent. The Project Site is zoned C2-1VL and P-1VL with a General Plan land use designation of General Commercial. The Project Site is currently occupied by a car resale center, an auto body shop, a commercial office building, and surface parking lots that immediately abut the single-family homes long Canby Avenue. The Proposed Project would not displace any single-family homes, since no residential units exist on-site. Further, the Proposed Project would provide a small-lot subdivision with 61 small lot homes that would provide a visual transition between commercial development along Reseda Boulevard and the single-family homes along Canby Avenue. The Proposed Project would be compatible with the surrounding land uses. Thus, the Proposed Project would be consistent with this policy.</p>
<p><b>Policy 1-1.3:</b> New single and multi-family residential development should be designed in accordance with the Urban Design Chapter.</p>	<p>Consistent. The Urban Design chapter in the Community Plan lays out policies and standards for residential developments to ensure good design that promote the quality of life, living conditions and neighborhood pride. The Proposed Project would comply with all the design policies that include providing a pedestrian entrance at the front of the Project Site and required useable open space for outdoor activities, especially for children. Thus, the Proposed Project would consistent with this policy.</p>
<p><b>Policy 1-1.4:</b> The City should promote neighborhood preservation, both in existing</p>	<p>Consistent. Although this Goal is directed towards the City and does not specifically apply to the Proposed Project, the</p>

<p>single- family neighborhoods, as well as existing multi-family areas.</p>	<p>development of the Proposed Project would not hinder this policy. The Proposed Project aims to increase the residential housing stock by developing 79 multi-family dwelling units and 61 small lot homes and would not disturb or demolish any existing single and multi-family neighborhoods. Thus, the Proposed Project would be consistent with this policy.</p>
<p><b>Objective 1-2:</b> To locate new housing appropriately in a manner which reduces vehicular trips and which increases accessibility to services and facilities.</p>	<p>Consistent. The Proposed Project is located in close proximity to many services and employment opportunities along Reseda Boulevard and Roscoe Boulevard, which would provide residents with access to employment opportunities, facilities and services in walking distance from the Project Site. Further, many bus stops are offered within walking distance from the Project Site, which connects the Project Site to the greater Los Angeles area. The Proposed Project’s commercial element would increase employment opportunities and services in the Project Site area. The Project would provide new housing opportunities to the community that is in close proximity to employment, services, facilities, and alternative transportation. Thus, the Proposed Project supports this objective.</p>
<p><b>Policy 1-2.1:</b> Locate higher residential densities near commercial centers, commuter rail stations, and bus routes where public service facilities, utilities, and topography will accommodate this development.</p>	<p>Consistent. The Project Site is an infill site that is adequately served by existing public service facilities and public utilities. The Project would increase the density and utilization of the Project Site, and as discussed in Section XIV, Public Services, and Section XVII, Utilities and Service Systems, and would continue to be adequately served by existing public service systems. The Project Site can adequately accommodate the development. The Project Site is located in walking distance to numerous services, retail, and employment opportunities along Reseda Boulevard and Roscoe Avenue. Additionally, the Project Site is in close proximity to public transportation options, including Metro and LADOT bus stops and services. The Proposed Project would be consistent with this policy.</p>
<p><b>Policy 1-2.2:</b> Locate senior citizen housing projects in neighborhoods within reasonable walking distance of health and community facilities, services, and public transportation.</p>	<p>Consistent. This policy does not particularly pertain to the Proposed Project because it is not specifically a senior citizen housing project. However, the Proposed Project would include up to 79 multi-family dwelling units and 61 small lot homes that would be available to all persons, without discrimination, including senior citizens. The Project Site is located in walking distance to a the Northridge Hospital Medical Center on the intersection of Roscoe Boulevard and Reseda Boulevard, approximately 0.1 mile south of the Project Site. Thus, the Project Site would allow potential senior citizen residents reasonable access to a health and community facility. Thus, the Proposed Project is consistent with this policy.</p>
<p><b>Policy 1-2.3:</b> Provide for an adequate supply of housing to meet the needs of students attending California State University of Northridge, without creating adverse impacts on adjacent permanent residential neighborhoods. Fraternity and sorority housing should not be scattered through the community.</p>	<p>Consistent. The Proposed Project is located approximately 0.9 mile south of California State University of Northridge. The Proposed Project would include a total of 140 residential units that would be available to all persons, including students attending the University. The Proposed Project would not provide fraternity or sorority housing. Thus, the Proposed Project would not conflict with this policy.</p>
<p><b>Objective 1-3:</b> To preserve and enhance the varied and distinct residential character and integrity of existing single and multi-family neighborhoods.</p>	<p>Consistent. The Proposed Project is currently occupied by a car resale center, an auto body shop, a commercial office building, and surface parking lots, and would not demolish any existing residential neighborhoods. The Proposed Project would increase</p>

	<p>the housing stock in the Northridge community with safe, attractive, and centrally located studios, one-bedroom, and two-bedroom apartments, along with two-bedroom and three-bedroom small lot homes. The small lot homes would provide a visual transition between commercial development along Reseda Boulevard and the single-family homes along Canby Avenue; thus, enhancing the residential character along Canby Avenue. The diverse availability of housing would enhance the residential character and integrity of the surrounding neighborhoods. Thus, the Proposed Project would consistent with this policy.</p>
<p><b>Policy 1-3.1:</b> Maintain and preserve the character of existing neighborhoods and encourage participation in self-help preventive maintenance to promote neighborhood conservation, beautification, and rehabilitation.</p>	<p>Consistent. The Proposed Project mixed-use buildings would be consistent with the commercial development and character along Reseda Boulevard. The proposed small lot homes would provide a visual transition between commercial development along Reseda Boulevard and the single-family homes along Canby Avenue. Thus, the Project would enhance and preserve the commercial character along Reseda Boulevard and residential character along Canby Avenue. Thus, the Proposed Project would be consistent with this policy.</p>
<p><b>Policy 1-3.2:</b> Consider factors such as neighborhood character and identity, compatibility of land uses, impacts on livability, impacts on services and public facilities, and impacts on traffic levels, and environmental impacts when changes in residential densities are proposed.</p>	<p>Consistent. The Project Site is currently developed with a car resale center, an auto body shop, and a commercial office building. No residential uses currently exist on site. The Project would redevelop the site with residential and commercial uses. The Proposed Project would add approximately 389 residents on site and to the Northridge community. As discussed above, the Project would place two mixed-use buildings on the western portion of the Project Site, along Reseda Boulevard, which would be consistent with existing commercial uses along Reseda Boulevard. The eastern portion of the Project Site would be developed with a small lot, single-family subdivision, which is would provide visual consistency with the single-family residences on the east side of Canby Avenue. The small lot subdivision would provide a visual transition between the commercial and multi-family residential uses along Reseda Boulevard to the single-family residential land uses along Canby Avenue. Moreover, the Proposed Project is consistent with the existing C2-1VL zoning designation and the General Commercial land use designation on site. As such the Project would result in a less than significant impact to neighborhood character, identity, livability. The Project would be compatible with existing land uses.</p> <p>As discussed in Sections, XIV. Public Services, XVI. Transportation and Traffic, and XVII. Utilities and Service Systems, the Proposed Project would be adequately served by existing public services and utilities. The addition of Project traffic would result in a less than significant impact to transportation systems.</p> <p>As such, the Proposed Project is consistent with this policy.</p>
<p><b>Policy 1-3.3:</b> Seek a high degree of architectural compatibility and landscaping for new infill development to protect the character and scale of</p>	<p>Consistent. The Proposed Project would transform the Project Site from its existing condition to a modern mixed-use and small lot residential neighborhood. The Project aims to be</p>

<p>existing residential neighborhoods.</p>	<p>consistent with the unique vision aimed for the Northridge community by providing areas of open space surrounding the single-family neighborhood and the mixed-use development. The Project Site is zoned C2-1VL and P-1VL, and as part of the Project, the Applicant requests a zone change for a consistent zoning of C2-1VL across the entire Project Site. The proposed development is consistent with the C2-1VL zone and General Commercial Land Use designation.</p> <p>The Project Applicant requests a Zoning Administrator’s Determination for Transitional Height that would allow for greater building heights than the limits set forth in LAMC 12.21.1-A.10, which would alter the existing character and scale of the Project Site. The Project proposed small lot homes on the eastern side of the Project Site that would be approximately 37 feet above grade. These proposed small lot homes would alter the residential character and scale along Canby Avenue in a manner that would complement the commercial uses along Reseda Boulevard and provide a visual transition to the residential uses along Canby Avenue.</p> <p>The Proposed Project would be designed and landscaped with the guidance of the Northridge Community Plan, Chapter 5, Urban Design. As such, would have a high degree of architectural and landscaping compatibility with the immediate community. The Proposed Project would not conflict with this policy.</p>
<p><b>Objective 1-4:</b> To preserve and enhance structures that have a distinctive and significant historical character.</p>	<p>Consistent. The Proposed Project would demolish the on-site structures which include a car resale center, an auto body shop, and a commercial office. The Neill Lehr Cadillac Showroom was identified on SurveyLA as potentially historic for being an excellent example of the 1960s automobile showroom in Northridge and exhibits the essential characteristics of the property type. However, as described in the Historic Resources Report conducted by GPA Consulting, observations found that while the showroom section of the property displays a late example of its type and retains integrity, large sections to the rear and sides of the showroom have been altered. As a whole, the property is not a significant and intact example of an automobile showroom. GPA Consulting determined that the car showroom on-site is not a significant cultural or historic resource. (Refer to Section V, Cultural Resources) Therefore, the Proposed Project would not demolish a historic structure. Thus, the Proposed Project supports this objective.</p>
<p><b>Policy 1-4.1:</b> Protect and encourage reuse of the area’s historic resources.</p>	<p>Consistent. As discussed above, the Proposed Project would not demolish or reuse any historic structures. As determined in the Project’s Historic Resources Report and discussed in Section V, Cultural Resources, the buildings on-site are not historic. The Proposed Project would be consistent with this policy.</p>
<p><b>Objective 1-5:</b> To promote and insure the provision of adequate housing for all person regardless of income, age or ethnic background.</p>	<p>Consistent. The Proposed Project would provide 79 multiple dwelling units (including 26 studio units, 34 one-bedroom units, and 19 two-bedroom units) and 61 small lot homes, ranging from two to three bedrooms. The residential dwelling units would be available at market rate. The Proposed Project would increase the housing options available in Northridge community</p>

	to all persons, without discrimination. The Project can attract new, economically, and ethnically diverse households, which is a goal of the General Plan and Community Plan. Thus, the Proposed Project supports this objective.
<b>Policy 1-5.1:</b> Promote greater individual choice in type, quality, and location of housing.	The Proposed Project would provide a variety of residential dwelling units including a mix of multi-family and small lot, single-family dwelling units. This promotes and encourages the development of different housing types and greater individual choice in quality of housing. Thus, the Proposed Project is consistent with this policy.
<b>Policy 1-5.2:</b> Provide for the development of townhomes and other similar condominium type housing units to increase home ownership options, without expanding into single-family designated neighborhoods.	Consistent. The Proposed Project would be consistent with this policy. The Proposed Project would include and the development of 61 small-lot residential homes. The proposed units would be available at market rate and provide for greater home ownership options. As discussed above, the proposed small-lot subdivision would complement the commercial uses along Reseda Boulevard and provide visual transition to the existing single-family homes along Canby Avenue. The Proposed Project is consistent with the C2-1VL zone that exists on-site.
<b>Policy 1-5.3:</b> Promote housing in mixed-use projects in pedestrian-oriented and transit intensive locations.	Consistent. The Proposed Project includes a mixed-use development that includes 79 multi-family residential units and 10,300 square feet of ground-floor retail and restaurant space fronting Reseda Boulevard. The Project Site is located in an urbanized area of Northridge community and is in walking distance to numerous services, retail, and employment opportunities along the commercial corridor on Reseda Boulevard, including transit bus stops in walking distance from the Project Site. The Proposed Project would promote and support pedestrian activity along Reseda Boulevard. Thus, the Proposed Project is consistent with this policy.
<b>Commercial</b>	
<b>Objective 2-1:</b> To maintain the viability and vitality of the existing Northridge Central Community Business District as a community focal point.	Consistent. Reseda Boulevard generally between Roscoe Boulevard to the south and Lassen Street to the north is identified as Northridge central commercial area. This Proposed Project is located within the Northridge central commercial area. The Proposed Project includes up to 10,300 square feet of ground-floor commercial space for offices, businesses or retail uses. The Proposed Project's commercial space would serve as a focal point for shopping, civic, and social activities in the community. The Proposed Project would foster new business and employment opportunities and potential customers, which helps to make the Northridge Central Community Business District a community focal point. Thus, the Proposed Project supports this objective.
<b>Policy 2-1.2:</b> Encourage retail and service commercial uses, including professional services and restaurants on both sides of Reseda Boulevard. Fast food restaurants and mini-malls should be discouraged.	Consistent. The Proposed Project includes up to 10,300 square feet of neighborhood-serving ground-floor commercial space for offices, businesses, or retail uses along Reseda Boulevard. The Proposed Project would provide new opportunities for new professional businesses or the expansion or relocation of existing businesses; thus, increasing business opportunities in the area. The Project would provide commercial land to support community needs along Reseda Boulevard. Thus, the Proposed Project is consistent with this policy.
<b>Policy 2-1.2:</b> Conserve and strengthen viable	Consistent. As part of the Proposed Project, two modern mixed-

<p>commercial development throughout the community.</p>	<p>use developments would front Reseda Boulevard and would provide for new commercial opportunities in the community. Additionally, the Proposed Project would promote pedestrian activity along Reseda Boulevard. Thus, the Proposed Project is consistent with this policy.</p>
<p><b>Policy 2-1.3:</b> New commercial uses should be located in existing commercial areas. There should be no increase in existing building height limitations.</p>	<p>Consistent. The Project Site is currently zoned C2-1VL and P-1VL with a corresponding land use designation of general commercial. The Proposed Project would demolish the existing commercial development on-site and construct a unified development with two mixed-use buildings (totaling 10,300 square feet of new commercial space) fronting Reseda Boulevard. The Project Applicant requests a Zoning Administrator’s Determination for Transitional Height that would allow for greater building heights than the limits set forth in LAMC 12.21.1-A.10. The C2-1VL limits buildings to three stories above grade and the Project proposes a maximum of four stories above grade. Although the Project would increase building heights, the Proposed Project would reach a maximum height of 45 feet above grade, which is the maximum height limitation for a C2-1VL zone. Thus, the Proposed Project would be consistent with this policy.</p>
<p><b>Policy 2-1.4:</b> Require that projects be designed and developed to achieve a high level of quality, distinctive character, and compatibility with existing uses and developed in accordance with design standards.</p>	<p>Consistent. The Proposed Project would include up to 10,300 square feet of commercial space. Compliant to the commercial section of the Urban Design chapter in the Community Plan and the applicable plans listed in this section, the commercial portion of the Proposed Project would be designed to create an environment that encourages pedestrian and economic activity. The commercial space would serve the needs of the residential portion of the building and the wider community and developed in accordance with applicable design standards. Thus, the Proposed Project would be consistent with this policy.</p>
<p><b>Objective 2-2:</b> To enhance the community identity in distinctive commercial districts.</p>	<p>Consistent. The Project Site is currently developed with a car resale center, an auto body shop, and a commercial office building. The Project proposes up to 10,300 square feet of ground-floor commercial space in the two mixed-use buildings, which would provide new opportunities for new businesses or the expansion or relocation of existing businesses. The Proposed Project would enhance the commercial corridor along Reseda Boulevard. Thus, the Proposed Project supports this objective.</p>
<p><b>Policy 2-2.1:</b> Improve security and parking standards in commercial areas. Where new development occurs, parking should be located in the rear of buildings.</p>	<p>Consistent. The Proposed Project would include a total of 297 parking spaces (117 multi-family residential spaces, 122 small lot residential spaces, and 58 commercial spaces). The mixed-use buildings would include one level of subterranean parking and parking at grade level with designated parking for commercial uses located at the rear of the buildings. Thus, the Proposed Project would be consistent with this policy.</p>
<p><b>Policy 2-2.2:</b> Require that mixed-use projects be designed with commercial uses on the ground floor and developed to achieve a high level of quality, distinctive character, and compatibility with existing uses.</p>	<p>Consistent. The Proposed Project would locate the 10,300 square feet of commercial space on the ground-floor. The commercial space would be located along street-level frontage on Reseda Boulevard near its surrounding commercial developments. The commercial space would be compatible with the existing surrounding commercial developments. Thus, the Proposed Project would be consistent with this policy.</p>
<p><b>Policy 2-2.3:</b> Encourage pedestrian activity on both sides of Reseda Boulevard, particularly</p>	<p>Consistent. This policy does not particularly pertain to the Proposed Project because the Project Site is not located between</p>

<p>between Nordhoff Street and Halsted Street near the University.</p>	<p>Nordhoff Street and Halsted Street near the University. However, the Proposed Project is located within the Northridge central commercial area, and the commercial portion of the Proposed Project would front Reseda Boulevard and promote pedestrian activity on Reseda Boulevard.</p>
<p><b>Objective 2-3:</b> To enhance the appearance of commercial districts.</p>	<p>Consistent. Reseda Boulevard generally between Roscoe Boulevard to the south and Lassen Street to the north is identified as Northridge central commercial area. This Proposed Project is located within the Northridge central commercial area. The Community Plan notes that this area should be strongly considered for improving and upgrading. The Proposed Project would enhance the Project Site with a unified development that would include a small-lot subdivision on the eastern side of the Project Site and two mixed-use buildings containing 10,300 square feet of ground-floor commercial fronting Reseda Boulevard. The proposed development would transform and enhance the current condition and appearance of the Project Site. The Proposed Project would be guided by the Urban Design chapter of the Community Plan and would be designed to enhance the commercial districts along Reseda Boulevard. Thus, the Proposed Project is supports this objective.</p>
<p><b>Policy 2-3.1:</b> Improve the landscaping of commercial properties.</p>	<p>Consistent. The Proposed Project would be well landscaped with the guidance of the Community Plan’s urban design guidelines. Further, the Proposed Project’s landscape plans are subject to review and approval by the Department of City Planning. Thus, the Proposed Project is consistent with this policy.</p>
<p><b>Policy 2-3.2:</b> Landscaped corridors should be created and enhanced through the planting of street trees.</p>	<p>Consistent. The Proposed Project would plant street trees along Reseda Boulevard in coordination with the City of Los Angeles Division of Urban Forestry. Further, the Project would provide well-designed open space areas along Reseda Boulevard in the form of courtyards and landscaped areas. Thus, the Proposed Project is consistent with this policy.</p>
<p><b>Policy 2-3.3:</b> Any proposed development should be designed to enhance and be compatible with adjacent development.</p>	<p>Consistent. As discussed above, Project design would enhance the Project Site and be visually consistent with adjacent development. The Project would add commercial uses that would be compatible with adjacent, existing commercial uses along Reseda Boulevard. Additionally, Project design and services would promote pedestrian experience at and around the Project Site. The Proposed Project would be consistent with this policy.</p>
<p><b>Policy 2-3.4:</b> Preserve community character, scale, and architectural diversity.</p>	<p>Consistent. The Proposed Project would supplement and enhance the commercial character along Reseda Boulevard by constructing the two mixed-use buildings that would be enhance community character and architectural diversity existing community. As discussed above, the Project Applicant requests a Zoning Administrator’s Determination for Transitional Height that would allow for greater building heights than the limits set forth in LAMC 12.21.1-A.10. The C2-1VL limits buildings to three stories above grade and the Project proposes a maximum of four stories above grade. Although the Project would increase building heights, the Proposed Project would reach a maximum height of 45 feet above grade, which is the maximum height limitation for a C2-1VL zone. The development would be consistent with the priorities and unique vision contemplated for</p>

	the community by incorporating modern architecture. As such, the Proposed Project would preserve community character, scale, and architectural diversity. The Proposed Project would be consistent with this policy.
<b>Policy 2-3.5:</b> Improve safety and aesthetics of parking areas in commercial areas.	Consistent. The Proposed Project would be designed to include adequate lighting and accessibility to on-site parking areas to promote safety within the parking areas. Therefore, the Proposed Project would consistent with this policy.
<b>Policy 2-3.6:</b> Improve the visual appearance of commercial districts through better control of signage, including billboard proliferation. Large bill-boards are not in keeping with the desired appearance of Reseda Boulevard and all commercial districts and are strongly discouraged.	Consistent. The Proposed Project would enhance the appearance of the Project Site with new, well-designed mixed-use building with ground-floor commercial space that would be consistent with the signage and existing commercial development in the vicinity of the Project Site. The Proposed Project does not propose a billboard along Reseda Boulevard. Thus, the Proposed Project would be consistent with this policy.
<i>Source: City of Los Angeles, Department of City Planning Northridge Community Plan, February 24, 1998; and Parker Environmental Consultants, April 2016.</i>	

The Northridge Community Plan addresses planning, land use issues and opportunities in various sectors, such as residential, commercial, industrial, transportation, among others. The Northridge Community Plan designates residential land use densities based on a population of 66,351 persons and 23,627 dwelling units by 2010 within the Community Plan area.<sup>29</sup> The 2010 United States Census shows that the Northridge Community Plan area had an actual population of 67,339 persons and 23,942 dwelling units.<sup>30</sup> The 2010 Census data shows that the actual population and housing units in the Northridge Community Plan area in 2010 was higher than what was projected in the Community Plan. Nevertheless, as discussed in Section XIII. Population and Housing, the Project is consistent with SCAG’s population and housing growth projections. The Proposed Project would meet the needs of the Community Plan by adding housing stock to an area that would reduce vehicular trips and increase accessibility to local services and facilities.

The Proposed Project would be consistent with the criteria for residential uses in commercial areas. The Project would revitalize an area that is currently occupied by a car resale center, an auto body shop, a commercial office building, and surface parking lots with the development of two four-story mixed-use buildings with a total of 79 multi-family units and 10,300 square feet of ground-floor commercial space, and a 61 small lot homes. The Project’s land uses are consistent with the surrounding neighborhood that is characterized by single-family homes to the east and commercial land uses to the west. Additionally, the

<sup>29</sup> *City of Los Angeles Department of City Planning, Northridge Community Plan, 2009 Population Estimate: <http://cityplanning.lacity.org/DRU/LocL/LocFrame.cfm?geo=CP&loc=Nrd&sgo=ct&rpt=PnH&yryx=Y09>, accessed October 2015.*

<sup>30</sup> *The Northridge Community Plan Area contains the following tracts: 1112.04, 1112.05, 1112.06, 1113.02, 1151.01, 1151.03, 1151.04, 1152.01, 1152.02, 1153.01, 1153.02, 1154.01, 1154.03, 1154.04, and 1173.02. The population and dwelling units were calculated by summing the individual tracts together. Source: United States Census Bureau, 2010 Census Interactive Population Map, website: <http://www.census.gov/2010census/popmap/>, accessed October 2015.*

Proposed Project requests a zone change from C2-1VL and P-1VL to a consistent C2-1VL across the entire Project Site. The Proposed Project would be consistent the land uses permitted in a C2-1VL zone and the General Plan land use designation (General Commercial). As such, the Proposed Project is compatible and appropriate for the land uses located in the vicinity of the Project Site.

The Proposed Project would be consistent with the goals, objectives, and policies set forth in the Northridge Community Plan. Therefore, the Project is consist with the applicable land use and planning policies in the Northridge Community Plan.

### ***Los Angeles Municipal Code***

The Project Site consists of approximately 143,552 square feet (3.29 acres) of total lot area. The Project Site is currently improved with a car resale center, an auto body shop, a commercial office building, and surface parking lots. The Proposed Project includes the construction of two four-story mixed-use buildings with up to 79 apartment units and 10,300 square feet of ground-floor retail and restaurant on the western side of the Project Site and a small lot subdivision with 61 three-story small lot homes on the eastern side of the Project Site.

The Project Site is located within the jurisdiction of the City of Los Angeles and is, therefore, subject to the applicable land use and zoning requirements in the Los Angeles Municipal Code (LAMC). The General Plan land use designation for the Project Site is General Commercial and the zoning designation is C2-1VL and P-1VL, which allows for residential and commercial land uses. The Proposed Project would be comprised of multi-family residential, small lot small lot residential, and commercial uses. Residential uses are permitted on lots zoned for C2 uses that are located within the Northridge CPA. The LAMC land use and zoning requirements applicable to the Proposed Project are further discussed below.

### ***Building Height***

The Project Site is zoned C2-1VL and P-1VL with the primary land use designation of General Commercial. The corresponding zones for General Commercial are the C1.5, C2, C4, and RAS3 Zones. Height District No. 1VL (Very Limited Height District) limits the building height to three stories and shall not exceed 45 feet above grade. However, a building in Height District 1VL designed and used entirely for residential purposes shall be limited to the number of feet in height, but not the number of stories Further, pursuant to LAMC Section 12.21.1 A.10, the buildings in C zone within 50 to 99 feet from a RW1 Zone or a more restrictive zone is limited to 33 feet above grade. This limitation applies to the small lot homes fronting Canby Avenue.

The mixed-use buildings on the western side of the Project Site is proposed for four stories and would reach a maximum height of 45 feet above grade. The small lot homes on the eastern side of the Project Site is proposed for three stories and would reach a maximum height of 37 feet above grade, including the proposed small lot homes fronting Canby Avenue. However, due to the close proximity properties zoned RS-1 and RA-1 to the east, the Proposed Project is subject to the requirements of Transitional Height, pursuant to LAMC Section 12.21.1-A,10. The Applicant has requested a determination for the number of stories for the mixed-use buildings and a Zoning Administrator's Determination for transitional building

height to allow for a greater building height than required. Upon approval, the proposed building heights would be compliant with the allowable heights in the LAMC.

### *Floor Area*

The Height District No. 1VL also limits the total floor area from exceeding 1.5 times the total area of the lot. The Project Site consists of approximately 140,925 square feet (3.23 acres) of net lot area with a buildable area of 140,925. The allowable floor area on-site is 211,387 square feet. The Applicant requests a conditional use permit to average the floor area of the Unified Development. The Proposed Project would provide a total of approximately 202,215 square feet of floor area for an approximate 1.43:1 FAR averaged across the site. Therefore, development of the Proposed Project is within the allowable floor area ratio for the Project Site and a less than significant impact would occur.

### *Density*

Pursuant to LAMC Section 12.14.C, residential land uses are permitted in a C2 zone with compliance to the residential development regulations of an R4 Zone. Therefore, the residential uses of the Proposed Project must follow the R4 zone lot area requirements to all portions of buildings used for residential purposes. As such, the Project Site is permitted a maximum density of one dwelling unit per 400 square feet. The Applicant requests a conditional use permit to average the density of the Unified Development. With approval of conditional use permit, the Proposed Project would be consistent the permitted density for the Project Site. Therefore, a less than significant impact would occur.

### *Setbacks*

Pursuant to LAMC Section 12.14.C, no front setbacks are required for the Proposed Project for the mixed-use buildings. The side and rear setbacks for residential uses in a C2 zone must follow the same setbacks required for a R4 zone at the lowest residential story. Rear yards must be 16 feet plus one foot for each story over the 3<sup>rd</sup> floor. Side yards must be 5 feet plus one foot for every story over the 2<sup>nd</sup> floor. Therefore, the required side and rear yards for the proposed mixed-use portion of the Project are 7 feet and 16 feet, respectively. The proposed mixed-use portion of the Project would provide the following: a zero-foot front setback, a 7-foot side setback, and a 16-foot rear setback. Therefore, the Proposed Project would be consistent with the required setbacks for a C2 zone.

Pursuant to the LAMC Section 12.22.C 27(e) and Small Lot Ordinance (Ordinance No. 176,354), no front, side or rear yard shall be required between lots within an approved small lot subdivision. However, a five-foot setback shall be provided where a lot abuts a lot that is not created pursuant to this subdivision. The Proposed Project would be consistent with this requirement.

### *Open Space*

As shown in Table II-3 in Section II, Project Description, the Proposed Project would be in compliance with the minimum open space requirements of the LAMC. The 61 small lots do not require open space. Pursuant to LAMC 12.21.G, the mixed-use portions of the Proposed Project require approximately 10,175 square

feet of open space. The Proposed Project would include 27,819 square feet of open space, including 17,589 square feet of open space for the small lot portion of the Project and 10,230 square feet of common and private open space areas incorporated in the mixed-use portion of the Project. As part of the open space requirements, the mixed-use residential component of the Project includes planting trees at a rate of one tree for every four dwelling units, which requires 20 trees. Thus, the Proposed Project would be consistent with the open space requirements of the LAMC.

### *Parking*

The Off-Street Automobile Parking Requirements (LAMC Section 12.21 A 4 (a)), requires two (2) spaces per small lot home, one (1) space per multi-family dwelling unit with less than 3 habitable rooms, one and a half (1.5) spaces for each multi-family dwelling unit of 3 habitable rooms, and two (2) spaces for each multi-family dwelling unit with more than 3 habitable rooms. The Parking Requirements also states that four (4) spaces shall be provided for every 1,000 square feet of general retail space and one (1) space for every 100 square feet of restaurant space. The Proposed Project also qualifies for a reduction in required parking spaces utilizing a bike parking reduction for being in close proximity to a transit facility.

Parking for the proposed commercial and residential uses in the mixed-use buildings would be provided in the one level of subterranean parking beneath the building and the parking at grade level. Each small lot home would provide a 2-vehicle parking garage on the first floor of each dwelling unit. As summarized in Table II-4, in the Project Description Chapter, the Proposed Project would be consistent with the applicable parking requirements of the LAMC. The Proposed Project would require a total of 295 parking spaces (115 multi-family residential spaces, 122 small lot residential spaces, and 58 commercial spaces). The Proposed Project plans to provide a total of 297 parking spaces.

The Proposed Project would additionally provide on-site bicycle parking in bicycle storage spaces for short-term and long-term bike storage. All short-term bike parking for the small lots would be located in the small lot subdivision for residential guests. Long-term and short-term parking for the multi-family and commercial uses would be provided in the parking levels in the two mixed-use buildings. Pursuant to LAMC Section 12.21 A.16, the Proposed Project is required to supply 13 short-term bicycle parking spaces, 84 long-term bicycle parking spaces, and 60 additional bicycle spaces for the 20% parking reduction for a total of 157 bicycle parking spaces in the mixed-use buildings and an additional 6 short-term spaces for the single-family lot for a total of 163 bicycle parking spaces. The Project proposes to provide 163 spaces. Thus, the Proposed Project would be consistent with the LAMC requirements for vehicle and bicycle parking.

The Proposed Project would not conflict with the goals, objectives, and allowable land uses in the LAMC. With the approval of requested entitlements, the Proposed Project would be consistent with local and regional plans applicable to the Project Site, and impacts would be less than significant level.

**c) Would the project conflict with any applicable habitat conservation plan or natural community conservation plan?**

**No Impact.** A project-related significant adverse impact could occur if the Project Site were located within an area governed by a habitat conservation plan or natural community conservation plan. As discussed in Question IV(f) above, no such plans presently exist which govern any portion of the Project Site. Further, the Project Site is located in an urbanized area, and the Project Site is currently developed with a car resale center, an auto body shop, a commercial office building, and surface parking lots. Therefore, the Proposed Project would not have the potential to cause such effects, and no impact would occur.

**Cumulative Impacts**

**No Impact.** Development of any related project is expected to occur in accordance with adopted plans and regulations. It is also expected that most of the related projects would be compatible with the zoning and land use designations of each related project site and its existing surrounding uses. In addition, it is reasonable to assume that the related projects under consideration would implement and support local and regional planning goals and policies. Therefore, the Proposed Project's land use impacts would not be cumulatively considerable since the Proposed Project would not conflict with applicable local or regional plans. The Proposed Project's land use would not create any significant impacts.

**XI. MINERAL RESOURCES**

**a) Would the project result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?**

**No Impact.** A significant impact may occur if a project site is located in an area used or available for extraction of a regionally-important mineral resource, or if the project development would convert an existing or future regionally-important mineral extraction use to another use, or if the project development would affect access to a site used or potentially available for regionally-important mineral resource extraction. According to the *L.A. CEQA Thresholds Guide*, the determination of significance shall be made on a case-by-case basis considering: (a) whether, or the degree to which, the project might result in the permanent loss of, or loss of access to, a mineral resource that is located in a State Mining and Geology Board Mineral Resource Zone MRZ-2 zone or other known or potential mineral resource area, and (b) whether the mineral resource is of regional or statewide significance, or is noted in the Conservation Element as being of local importance. The Project Site is not located within mineral resource zone area (MRZ-2) nor is the Project Site located in an oil field or oil drilling area.<sup>31</sup> The Project Site is currently

---

<sup>31</sup> *City of Los Angeles, Department of City Planning, Environmental And Public Facilities Maps: Areas Containing Significant Mineral Deposits in the City of Los Angeles, September 1996.*

*City of Los Angeles, Department of City Planning, Environmental And Public Facilities Maps: Oil Field and Oil Drilling Areas in the City of Los Angeles, September 1996.*

developed as a car resale center, an auto body shop, a commercial office building, and surface parking lots. The Project Site is not currently used for the extraction of mineral resources, and there is no evidence to suggest that the Project Site has been historically used for the extraction of mineral resources.<sup>32</sup> Therefore, the development of the Proposed Project would not result in the loss of availability of a known mineral resource and no impact would occur.

**b) Would the project result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?**

**No Impact.** A significant impact may occur if the Project Site is located in an area used or available for extraction of a regionally-important mineral resource, or if the development would convert an existing or future regionally-important mineral extraction use to another use, or if the development would affect access to a site used or potentially available for regionally-important mineral resource extraction. The Project Site is not located within mineral resource zone area (MRZ-2) nor is the Project Site located in an oil field or oil drilling area.<sup>33</sup> The Project Site is not currently used for the extraction of mineral resources, and there is no evidence to suggest that the Site has historically been used for the extraction of mineral resources. Therefore, no impact to locally important mineral resources would occur.

## **XII. NOISE**

### *Fundamentals of Noise*

Sound is technically described in terms of amplitude (loudness) and frequency (pitch). The standard unit of sound amplitude measurement is the decibel (dB). The decibel scale is a logarithmic scale that describes the physical intensity of the pressure vibrations that make up any sound. The pitch of the sound is related to the frequency of the pressure vibration. Since the human ear is not equally sensitive to a given sound level at all frequencies, a special frequency-dependent rating scale has been devised to relate noise to human sensitivity. The A-weighted decibel scale (dBA) provides this compensation by discriminating against frequencies in a manner approximating the sensitivity of the human ear.

Noise, on the other hand, is typically defined as unwanted sound. A typical noise environment consists of a base of steady “background” noise that is the sum of many distant and indistinguishable noise sources. Superimposed on this background noise is the sound from individual local sources. These can vary from an

---

<sup>32</sup> *California Environmental Geologists & Engineers, Inc., Environmental Site Assessment – Phase I and Phase II Screening Subsurface Soil Gas Survey, Commercial Property, 8350-8354, 8400, & 8420 Reseda Blvd, APN 2786-021-017 & -18; 2876-022-021, Northridge, CA 91324, November 2014.*

<sup>33</sup> *City of Los Angeles, Department of City Planning, Environmental And Public Facilities Maps: Areas Containing Significant Mineral Deposits in the City of Los Angeles, September 1996.*

*City of Los Angeles, Department of City Planning, Environmental And Public Facilities Maps: Oil Field and Oil Drilling Areas in the City of Los Angeles, September 1996.*

occasional aircraft or train passing by to virtually continuous noise from, for example, traffic on a major highway.

Several rating scales have been developed to analyze the adverse effect of community noise on people. Since environmental noise fluctuates over time, these scales consider that the effect of noise upon people is largely dependent upon the total acoustical energy content of the noise, as well as the time of day when the noise occurs. Those that are applicable to this analysis are as follows:

- $L_{eq}$  – An  $L_{eq}$ , or equivalent energy noise level, is the average acoustic energy content of noise for a stated period of time. Thus, the  $L_{eq}$  of a time-varying noise and that of a steady noise are the same if they deliver the same acoustic energy to the ear during exposure. For evaluating community impacts, this rating scale does not vary, regardless of whether the noise occurs during the day or the night.
- $L_{max}$  – The maximum instantaneous noise level experienced during a given period of time.
- $L_{min}$  – The minimum instantaneous noise level experienced during a given period of time.
- CNEL – The Community Noise Equivalent Level is a 24-hour average  $L_{eq}$  with a 5 dBA “weighting” during the hours of 7:00 P.M. to 10:00 P.M. and a 10 dBA “weighting” added to noise during the hours of 10:00 P.M. to 7:00 A.M. to account for noise sensitivity in the evening and nighttime, respectively. The logarithmic effect of these additions is that a 60 dBA 24 hour  $L_{eq}$  would result in a measurement of 66.7 dBA CNEL.

Noise environments and consequences of human activities are usually well represented by median noise levels during the day, night, or over a 24-hour period. For residential uses, environmental noise levels are generally considered low when the CNEL is below 60 dBA, moderate in the 60–70 dBA range, and high above 70 dBA. Noise levels greater than 85 dBA can cause temporary or permanent hearing loss. Examples of low daytime levels are isolated, natural settings with noise levels as low as 20 dBA and quiet suburban residential streets with noise levels around 40 dBA. Noise levels above 45 dBA at night can disrupt sleep. Examples of moderate level noise environments are urban residential or semi-commercial areas (typically 55–60 dBA) and commercial locations (typically 60 dBA). People may consider louder environments adverse, but most will accept the higher levels associated with more noisy urban residential or residential-commercial areas (60–75 dBA) or dense urban or industrial areas (65–80 dBA).

It is widely accepted that in the community noise environment the average healthy ear can barely perceive CNEL noise level changes of 3 dBA. CNEL changes from 3 to 5 dBA may be noticed by some individuals who are extremely sensitive to changes in noise. A 5 dBA CNEL increase is readily noticeable, while the human ear perceives a 10 dBA CNEL increase as a doubling of sound.

Noise levels from a particular source generally decline as distance to the receptor increases. Other factors, such as the weather and reflecting or barriers, also help intensify or reduce the noise level at any given location. A commonly used rule of thumb for roadway noise is that for every doubling of distance from the source, the noise level is reduced by about 3 dBA at acoustically “hard” locations (i.e., the area between the noise source and the receptor is nearly complete asphalt, concrete, hard-packed soil, or other solid

materials) and 4.5 dBA at acoustically “soft” locations (i.e., the area between the source and receptor is normal earth or has vegetation, including grass). Noise from stationary or point sources is reduced by about 6 to 7.5 dBA for every doubling of distance at acoustically hard and soft locations, respectively. In addition, noise levels are also generally reduced by 1 dBA for each 1,000 feet of distance due to air absorption. Noise levels may also be reduced by intervening structures – generally, a single row of buildings between the receptor and the noise source reduces the noise level by about 5 dBA, while a solid wall or berm reduces noise levels by 5 to 10 dBA. The normal noise attenuation within residential structures with open windows is about 17 dBA, while the noise attenuation with closed windows is about 25 dBA.<sup>34</sup>

### *Ambient Noise Levels*

To assess the existing ambient noise conditions in the area, ambient noise measurements were taken with a Larson Davis 831 sound level meter, which conforms to industry standards set forth in ANSI S1.4-1983 (R2001) - American National Standard Specification for Sound Level Meters. Figure III-1, Noise Monitoring and Sensitive Receptor Location Map, depicts the noise measurement locations fronting the surrounding land uses and adjacent residential uses as the most likely sensitive receptors to experience noise level increases during construction. The detailed noise monitoring data are presented in Appendix F, Noise Monitoring Data, and are summarized below in Table III-10, Existing Ambient Daytime Noise Levels in Project Site Vicinity. As shown in Table III-10, the ambient noise in the vicinity of the Project Site ranges from 51.1 to 73.1  $L_{eq}$ . The maximum noise level during the four 15-minute recordings was 89.1 dB  $L_{max}$  at the intersection of Reseda Boulevard and Roscoe Boulevard, which was attributable to heavy vehicle traffic. The primary noise sources that contributed most to the measured ambient noise levels were vehicles and buses primarily along Reseda Boulevard. The Project Site is currently occupied with a car resale center, an auto body shop, a commercial office building, and surface parking lots, which contributes to the ambient noise levels associated with commercial activity during the daytime and evening hours.

---

<sup>34</sup> *National Cooperative Highway Research Program Report 117, Highway Noise: A Design Guide for Highway Engineers, 1971.*

**Table III-10  
Existing Ambient Daytime Noise Levels in Project Site Vicinity**

No.	Location	Primary Noise Sources	Noise Level Statistics <sup>a</sup>		
			L <sub>eq</sub>	L <sub>min</sub>	L <sub>max</sub>
1	On the southeast corner of the Reseda Boulevard and Chase Street intersection	Vehicle traffic, bus stop, buses, pedestrian activity	69.9	54.1	82.0
2	On the east side of the Reseda Boulevard, west of the Project Site	Roadway traffic	69.4	49.7	77.3
3	On the northeast corner of Reseda Boulevard and Roscoe Boulevard	Vehicle traffic, bus stop, buses, pedestrian activity, car honking	73.1	58.8	89.1
4	On the south end of Canby Avenue, east of the Project Site	Light mechanic/auto repair noise from Project Site, light residential traffic	51.1	43.0	69.5

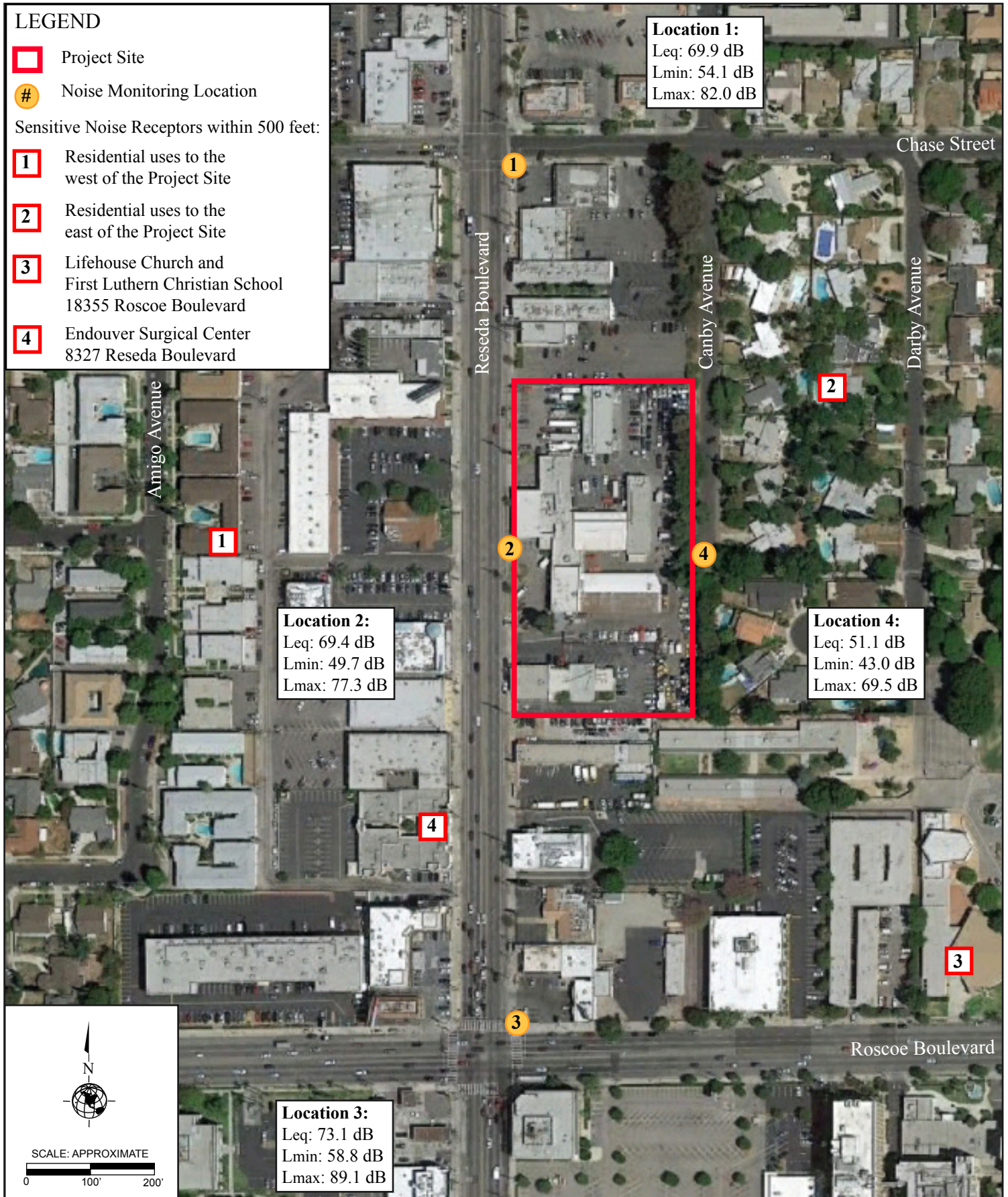
<sup>a</sup> Noise measurements were taken on Thursday, October 22, 2015 at each location for a duration of 15 minutes. See Appendix F of this IS/MND for noise monitoring data sheets.

**LEGEND**

- Project Site
- # Noise Monitoring Location

Sensitive Noise Receptors within 500 feet:

- 1 Residential uses to the west of the Project Site
- 2 Residential uses to the east of the Project Site
- 3 Lifehouse Church and First Lutheran Christian School  
18355 Roscoe Boulevard
- 4 Endouver Surgical Center  
8327 Reseda Boulevard



SCALE: APPROXIMATE

Source: Parker Environmental Consultants, October 22, 2015.



Figure III-1  
Noise Monitoring Locations and Sensitive Receptors

---

*Sensitive Receptors*

Several noise sensitive land uses are located adjacent to and in the vicinity of the Proposed Project. For purposes of assessing noise impacts on sensitive populations, the following sensitive receptors in close proximity to the Project Site were identified:

- 1) Single-family residences located to east of the Project Site on Canby Avenue;
- 2) Single-family residences located to the east of the Project Site on Community Street;
- 3) Lifehouse Church and First Lutheran Christian School, located at 18355 Roscoe Boulevard; and
- 4) Endeavor Surgical Center, located at 8327 Reseda Boulevard.

The locations of these land uses relative to the Project Site are depicted in Figure III-1, Noise Monitoring and Sensitive Receptor Location Map. For purposes of assessing construction-generated vibration impacts on building damage, there are no buildings immediately abutting the Project Site or within a close enough proximity to the Project Site to be susceptible to building damage from groundborne vibration during construction.

- a) **Would the project result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?**

**Potentially Significant Unless Mitigation Incorporated.** A significant impact may occur if the Proposed Project would generate excess noise that would cause the ambient noise environment at the Project Site to exceed noise level standards set forth in the City of Los Angeles General Plan Noise Element (Noise Element) and the City of Los Angeles Noise Ordinance (Noise Ordinance). Implementation of the Proposed Project would result in an increase in ambient noise levels during both construction and operation, as discussed in further detail below.

*Construction Noise*

Construction-related noise impacts upon adjacent land uses would be significant if, as indicated in LAMC Section 112.05, noise from construction equipment within 500 feet of a residential zone exceeds 75 dBA at a distance of 50 feet from the noise source. However, the above noise limitation does not apply where compliance is technically infeasible. Technically infeasible means that the above noise limitation cannot be complied with despite the use of mufflers, shields, sound barriers and/or any other noise reduction device or techniques during the operation of the equipment. Additionally, as defined in the *L.A. CEQA Thresholds Guide* for construction noise impacts, a significant impact would occur if construction activities lasting more than one day would increase the ambient noise levels by 10 dBA or more at any off-site noise-sensitive location. Furthermore, the *L.A. CEQA Thresholds Guide* also states that construction activities lasting more than ten days in a three-month period, which would increase ambient exterior noise levels by 5 dBA or more at a noise sensitive use, would also normally result in a significant impact.

Construction of the Proposed Project would require the use of heavy equipment for demolition/site clearing, grading and site preparation, the installation of utilities, paving, and building construction. During each construction phase there would be a different mix of equipment operating and noise levels would vary based on the amount of equipment in operation and the location of each activity. The U.S. Environmental Protection Agency (EPA) has compiled data regarding the noise generating characteristics of specific types of construction equipment and typical construction activities. The data pertaining to the types of construction equipment and activities that would occur at the Project Site are presented in Table III-11, Typical Outdoor Construction Noise Levels, respectively, at a distance of 50 feet from the noise source (i.e., reference distance).

**Table III-11  
Typical Outdoor Construction Noise Levels**

<b>Construction Phase</b>	<b>Noise Levels at 50 Feet with Mufflers (dBA L<sub>eq</sub>)</b>	<b>Noise Levels at 60 Feet with Mufflers (dBA L<sub>eq</sub>)</b>	<b>Noise Levels at 100 Feet with Mufflers (dBA L<sub>eq</sub>)</b>	<b>Noise Levels at 200 Feet with Mufflers (dBA L<sub>eq</sub>)</b>
Ground Clearing	82	80	76	70
Excavation, Grading	86	84	80	74
Foundations	77	75	71	65
Structural	83	81	77	71
Finishing	86	84	80	74

*Source: United States Environmental Protection Agency, Noise from Construction Equipment and Operations, Building Equipment and Home Appliances, PB 206717, 1971.*

The noise levels shown in Table III-11 represent composite noise levels associated with typical construction activities, which take into account both the number of pieces and spacing of heavy construction equipment that are typically used during each phase of construction. Construction noise during the heavier initial periods of construction could be expected to be 86 dBA L<sub>eq</sub> when measured at a reference distance of 50 feet from the center of construction activity.<sup>35</sup> These noise levels would diminish rapidly with distance from the construction site at a rate of approximately 6 dBA per doubling of distance. For example, a noise level of 84 dBA L<sub>eq</sub> measured at 50 feet from the noise source to the receptor would reduce to 78 dBA L<sub>eq</sub> at 100 feet from the source to the receptor, and reduce by another 6 dBA L<sub>eq</sub> to 72 dBA L<sub>eq</sub> at 200 feet from the source to the receptor. Construction activities associated with the Proposed Project would be expected to generate similar noise levels to those shown in Table III-11, below during the approximate 30-month construction period.

As set forth in the *L.A. CEQA Thresholds Guide*, a significant construction noise impact would occur if

<sup>35</sup> *Although the peak noise levels generated by certain construction equipment may be greater than 86 dBA at a distance of 50 feet, the equivalent noise level would be approximately 86 dBA L<sub>eq</sub> (i.e., the equipment does not operate at the peak noise level over the entire duration).*

construction activities lasting more than one day would increase the ambient noise levels by 10 dBA or more at any off-site noise-sensitive location. Construction activities lasting more than ten days in a three-month period, which would increase ambient exterior noise levels by 5 dBA or more at a noise sensitive use, would also normally result in a significant impact. Since construction activities associated with the proposed development at the Project Site would last for more than ten days in a three-month period, is possible that the Proposed Project could cause a significant noise impact during construction if the ambient exterior noise levels at the identified off-site and on-site sensitive receptors increase by 5 dBA or more. The ambient exterior noise levels at the identified off-site sensitive receptors could possibly exceed 5 dBA or more on a temporary and intermittent basis during the construction period, especially for the residents located to east of the Project Site along Canby Avenue. As noted in Table III-10, the ambient noise level as measured during the site survey was 51.1 dBA  $L_{eq}$ . The nearest residences (on Community Avenue) are located immediately adjacent to the southern end of the Project Site and share a common wall with the Project Site. The residences along Canby Avenue are set back approximately 50 feet from the Project Site. As such, the Proposed Project's construction activities would have the potential to temporarily increase the ambient noise levels in proximity to sensitive receptors by 35 dBA  $L_{eq}$ , on a temporary and intermittent basis throughout the duration of the construction period.

The City of Los Angeles Building Regulations Ordinance No. 178048 requires a construction site notice to be provided that includes the following information: job site address, permit number, name and phone number of the contractor and owner or owner's agent, hours of construction allowed by code or any discretionary approval for the Site, and City telephone numbers where violations can be reported. The notice is required to be posted and maintained at the construction site prior to the start of construction and displayed in a location that is readily visible to the public. Pursuant to LAMC Section 41.40, exterior demolition and construction activities that generate noise are prohibited between the hours of 9:00 P.M. and 7:00 A.M. Monday through Friday, and between 6:00 P.M. and 8:00 A.M. on Saturday. Demolition and construction are prohibited on Sundays and all federal holidays. The construction activities associated with the Proposed Project would comply with these LAMC requirements. Mitigation Measure N-1 would further restrict the permissible hours of construction to the hours of 7:00 am to 6:00 pm Monday through Friday, and 8:00 am to 6:00 pm on Saturday. In accordance with LAMC Section 112.05, construction noise levels are exempt from the 75 dBA noise threshold if all technically feasible noise attenuation measures are implemented. Although the estimated construction-related noise levels associated with the Proposed Project would exceed the numerical noise threshold of 75 dBA at 50 feet from the noise source as outlined in the City Noise Ordinance, and the typical construction noise levels associated with the Proposed Project would exceed the existing ambient noise levels at all of the identified off-site sensitive receptors by more than the 5 dBA threshold established by the *L.A. CEQA Thresholds Guide* during all construction phases, implementation of the following mitigation measures would reduce the noise levels associated with construction of the Proposed Project to the maximum extent that is technically feasible. Thus, based on the provisions set forth in LAMC 112.05, and implementation of Mitigation Measures N-1 and N-2, below, the project's construction noise impacts would be considered less than significant.

---

**Mitigation Measure:****N-1 Demolition, Grading, and Construction Activities**

- Construction and demolition shall be restricted to the hours of 7:00 am to 6:00 pm Monday through Friday, and 8:00 am to 6:00 pm on Saturday.
- To the maximum extent practical, demolition and construction activities shall be scheduled so as to avoid operating several pieces of equipment simultaneously, which causes high noise levels.
- The project contractor shall use power construction equipment with state-of-the-art noise shielding and muffling devices.

**N-2 Demolition, Grading, and Construction Activities**

- The project contractor shall erect a temporary noise-attenuating sound barrier along the perimeter of the Project Site's easterly property line to block the line of sight between the construction equipment and the adjacent residential land uses. The sound wall shall be a minimum of 8 feet in height and shall be comprised of either ¾-inch plywood or an alternative sound absorbing material capable of achieving a 10 dBA reduction in sound level.

***Operational Noise****HVAC Equipment Noise*

Upon completion and operation of the Proposed Project, on-site operational noise would be generated by heating, ventilation, and air conditioning (HVAC) equipment installed on the new structures. However, the noise levels generated by these equipment types are not anticipated to be substantially greater than those generated by the current HVAC equipment serving the existing buildings on the Project Site and in the Project vicinity. As such, the HVAC equipment associated with the Proposed Project would not represent a new source of noise in the Project Site vicinity. In addition, the operation of this and any other on-site stationary sources of noise would be required to comply with the LAMC Section 112.02, which prohibits noise from air conditioning, refrigeration, heating, pumping, and filtering equipment from exceeding the ambient noise level on the premises of other occupied properties by more than five decibels.

*Noise from Mixed Use Commercial and Residential Land Uses*

Due to the mixed-use nature of the mixed-use buildings and small residential subdivision, noise generated from the operation of proposed commercial uses have the potential to impact the proposed residential uses. In order to ensure that on-site residences would not be adversely impacted by ambient urban noise levels, Regulatory Compliance Measure RC-N-1 shall be implemented to ensure that dwelling units associated with the Proposed Project would be constructed in accordance with Title 24 insulation standards of the California Code of Regulations for residential buildings, which serves to provide an acceptable interior noise environment for sensitive uses. With implementation of Regulatory Compliance Measure RC-N-1, impacts associated with interior noise levels at the proposed residences would be less than significant.

- **Regulatory Compliance Measures RC-N-1 (Increased Noise Levels (Mixed-Use Development)):** Pursuant to the 2013 California Green Building Code Section 5.507.4, the commercial portion of the proposed mixed-use buildings would employ building assemblies and components with Sound Transmission Class (STC) values determined in accordance with ASTM E90 and STM E413 or Outdoor-Indoor Sound Transition Class (OITC) determined in accordance with ASTM E1332. Pursuant to the 2013 California Green Building Code Section 5.507.4.1, the commercial portion of the proposed mixed use buildings shall include wall and roof-ceiling assemblies exposed to the noise source making up the building or addition envelope or altered envelop shall meet a composition STC rating of at least 50 or a composite OITC rating of no less than 40, with exterior windows of a minimum STC of 40 or OITC of 30 within the 65 CNEL noise contour of a freeway or expressway, railroad, industrial source, or fixed-guideway source as determined by the Noise Element of the General Plan.
- b) **Would the project result in exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?**

**Potentially Significant Unless Mitigation Incorporated.** Vibration is sound radiated through the ground. Vibration can result from a source (e.g., subway operations, vehicles, machinery equipment, etc.) causing the adjacent ground to move, thereby creating vibration waves that propagate through the soil to the foundations of nearby buildings. This effect is referred to as groundborne vibration. The peak particle velocity (PPV) or the root mean square (RMS) velocity is usually used to describe vibration levels. PPV is defined as the maximum instantaneous peak of the vibration level and is typically used for evaluating potential building damage. RMS is defined as the square root of the average of the squared amplitude of the level. RMS velocity in decibels (VdB) is typically more suitable for evaluating human response.

The background vibration velocity level in residential areas is usually around 50 VdB. The vibration velocity level threshold of perception for humans is approximately 65 VdB. A vibration velocity level of 75 VdB is the approximate dividing line between barely perceptible and distinctly perceptible levels for most people. Most perceptible indoor vibration is caused by sources within buildings such as operation of mechanical equipment, movement of people, or the slamming of doors. Typical outdoor sources of perceptible groundborne vibration are construction equipment, steel-wheeled trains, and traffic on rough roads. If a roadway is smooth, the groundborne vibration from traffic is rarely perceptible. The range of interest is from approximately 50 VdB, which is the typical background vibration velocity level, to 100 VdB, which is the general threshold where minor damage can occur in fragile buildings.

### ***Construction***

Excavation and earthwork activities for the Proposed Project have the potential to generate low levels of groundborne vibration. The operation of construction equipment generates vibrations that propagate through the ground and diminishes in intensity with distance from the source. Vibration impacts can range from no perceptible effects at the lowest vibration levels, to low rumbling sounds and perceptible vibration at moderate levels, to slight damage of buildings at the highest levels. Thus, construction activities

associated with the Proposed Project could have an adverse impact on both sensitive structures (i.e., building damage) and populations (i.e., annoyance).

For purposes of addressing construction-related vibration impacts on buildings, the City of Los Angeles has not adopted any policies or guidelines relative to groundborne vibration impacts. While the Los Angeles County Code (LACC Section 12.08.350) states a presumed perception threshold of 0.01 inch per second RMS, this threshold applies to groundborne vibrations from long-term operational activities, not temporary construction activities. Consequently, neither the City of Los Angeles nor the County of Los Angeles have adopted significance thresholds to assess vibration impacts during construction, the FTA and Caltrans vibration standards were used to evaluate potential impacts related to project construction. Based on Caltrans criteria, construction impacts relative to structural damage from groundborne vibration would be considered significant if the following thresholds were to occur as shown in Table III-12, below.

**Table III-12  
Vibration Damage Potential Threshold Criteria**

Threshold Criteria	Maximum PPV (in/sec)	
	Transient Sources	Continuous/Frequent Intermittent Sources
<b>Structure and Condition</b>		
Extremely fragile historic buildings, ruins, ancient monuments	0.12	0.08
Fragile buildings	0.2	0.1
Historic and some old buildings	0.5	0.25
Older residential structures	0.5	0.3
New residential structures	1.0	0.5
Modern industrial/commercial buildings	2.0	0.5
<i>Source: California Department of Transportation, Transportation and Construction Vibration Guidance Manual, Chapter 7: Vibration Prediction and Screening Assessment for Construction Equipment, Table 19. September 2013.</i>		

Table III-13, Vibration Source Levels for Construction Equipment, identifies various PPV and RMS velocity (in VdB) levels for the types of construction equipment that would operate at the Project Site during construction. As shown in Table III-13, vibration velocities could range from 0.003 to 0.089 inch/sec PPV at 25 feet from the source activity, with corresponding vibration levels ranging from 58 VdB to 87 VdB at 25 feet from the source activity, depending on the type of construction equipment in use.

**Table III-13  
Vibration Source Levels for Construction Equipment**

Equipment	Approximate PPV (in/sec)					Approximate RMS (VdB)				
	25 Feet	50 Feet	60 Feet	75 Feet	100 Feet	25 Feet	50 Feet	60 Feet	75 Feet	100 Feet
Large Bulldozer	0.089	0.031	0.024	0.017	0.011	87	78	76	73	69
Caisson Drilling	0.089	0.031	0.024	0.017	0.011	87	78	76	73	69
Loaded Trucks	0.076	0.027	0.020	0.015	0.010	86	77	75	72	68
Jackhammer	0.035	0.012	0.009	0.007	0.004	79	70	68	65	61
Small Bulldozer	0.003	0.001	0.0008	0.0006	0.0004	58	49	47	44	40

*Source: Federal Transit Administration, Transit Noise and Vibration Impact Assessment, Final Report, 2006.*

In terms of construction vibration impacts on buildings, the Project's anticipated vibration impact of 0.089 PPV (in/sec) would not exceed the structural vibration damage threshold of 0.5 PPV (in/sec) for older residential buildings. Thus, impacts resulting from groundborne vibration would be less than significant.

For purposes of addressing vibration impacts relative to human annoyance, the following analysis relies on the FTA's vibration impact thresholds, which are 80 VdB and above at residences and buildings where people normally sleep (e.g., nearby residences) and 83 VdB and above at institutional buildings, which includes schools and churches. The single family residential land uses east of the Project Site are sensitive receptors that would potentially be exposed to increased vibration levels on a temporary and intermittent basis during the construction period. Based on the estimated construction vibration levels identified in Table III-13, the sensitive receptors within 25 feet of the Project Site (i.e., the residences on Community Street) would be exposed to vibration levels of up to 87 VdB (approximately 7 VdB above the vibration annoyance threshold). The residences on Canby Avenue would be less impacted by groundborne vibration as the 50 feet of separation between these residences and the Project Site would reduce groundborne vibration impacts to approximately 78 VdB, which is below the 80 VdB vibration threshold. Implementation of the measures identified under Mitigation Measures N-1 would serve to reduce construction related vibration levels to the maximum extent feasible, and thus would reduce the annoyance factor to an acceptable level. Furthermore, all construction activity would be restricted to the hours of 7:00 A.M. to 6:00 P.M. Monday through Friday, and 8:00 A.M. to 6:00 P.M. on Saturday. Because any vibration level increases experienced at the residential uses in close proximity to the Project Site would occur during the acceptable time periods for construction activities, and would only occur on a temporary and intermittent basis during the construction period. Therefore, with implementation of Mitigation Measure N-1, impacts associated with groundborne vibration would be considered less than significant.

### **Operation**

The Proposed Project would be a unified development with two mixed-use buildings and a small lot subdivision designated for single-family, small lot homes, which would not involve the use of stationary equipment that would result in high vibration levels. Although groundborne vibration at the Project Site and immediate vicinity may currently result from heavy-duty vehicular travel (e.g., refuse trucks and transit buses) on Reseda Boulevard, the proposed land uses would not result in a substantial increased in the use of

these heavy-duty vehicles on the public roadways. While refuse trucks would be used for the removal of solid waste at the Project Site, these trips would typically only occur once a week and would not be any different than those presently occurring in the vicinity of the Project Site. As such, vibration impacts associated with operation of the Proposed Project would be less than significant.

**c) Would the project result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?**

**Potentially Significant Unless Mitigation Incorporation.** A significant impact may occur if the Proposed Project were to result in a substantial permanent increase in ambient noise levels above existing ambient noise levels without the Proposed Project. As defined in the *L.A. CEQA Thresholds Guide* for operational noise impacts, a project would normally have a significant impact on noise levels from Proposed Project operations if the Proposed Project causes the ambient noise level measured at the property line of affected uses that are shown in Table III-14, Community Noise Exposure (CNEL), to increase by 3 dBA in CNEL to or within the “normally unacceptable” or “clearly unacceptable” category, or any 5 dBA or greater noise increase. Thus, a significant impact would occur if noise levels associated with operation of the Proposed Project would increase the ambient noise levels by 3 dBA CNEL at homes where the resulting noise level would be at least 70 dBA CNEL. In addition, any long-term increase of 5 dBA CNEL or more is considered to cause a significant impact. Generally, in order to achieve a 3 dBA CNEL increase in ambient noise from traffic, the volume on any given roadway would need to double. In addition to analyzing potential impacts in terms of CNEL, the analysis also addresses increases in on-site noise sources per the provisions of the LAMC, which establishes a  $L_{eq}$  standard of 5 dBA over ambient conditions as constituting a LAMC violation.

**Table III-14  
Community Noise Exposure (CNEL)**

<b>Land Use</b>	<b>Normally Acceptable<sup>a</sup></b>	<b>Conditionally Acceptable<sup>b</sup></b>	<b>Normally Unacceptable<sup>c</sup></b>	<b>Clearly Unacceptable<sup>d</sup></b>
Single-family, Duplex, Mobile Homes	50 - 60	55 - 70	70 - 75	above 75
Multi-Family Homes	50 - 65	60 - 70	70 - 75	above 75
Schools, Libraries, Churches, Hospitals, Nursing Homes	50 - 70	60 - 70	70 - 80	above 80
Transient Lodging – Motels, Hotels	50 - 65	60 - 70	70 - 80	above 75
Auditoriums, Concert Halls, Amphitheaters	---	50 - 70	---	above 70
Sports Arena, Outdoor Spectator Sports	---	50 - 75	---	above 75
Playgrounds, Neighborhood Parks	50 - 70	---	67 - 75	above 75
Golf Courses, Riding Stables, Water Recreation, Cemeteries	50 - 75	---	70 - 80	above 80
Office Buildings, Business and Professional Commercial	50 - 70	67 - 77	above 75	---
Industrial, Manufacturing, Utilities, Agriculture	50 - 75	70 - 80	above 75	---

<sup>a</sup> *Normally Acceptable:* Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction without any special noise insulation requirements.

<sup>b</sup> *Conditionally Acceptable:* New construction or development should be undertaken only after a detailed analysis of the noise reduction requirements is made and needed noise insulation features included in the design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning will normally suffice.

<sup>c</sup> *Normally Unacceptable:* New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of the noise reduction requirements must be made and needed noise insulation features included in the design.

<sup>d</sup> *Clearly Unacceptable:* New construction or development should generally not be undertaken.

Source: Office of Planning and Research, State of California General Plan Guidelines, October 2003 (in coordination with the California Department of Health Services); City of Los Angeles, General Plan Noise Element, adopted February 1999.

---

## ***Operational Noise***

### *Traffic Noise*

The Proposed Project would increase traffic volumes on the surrounding roadways, which in turn has the potential to increase roadway noise. According to the *L.A. CEQA Thresholds Guide*, a significant impact may occur if a project would result in traffic that doubles the existing traffic. According to the Project's Traffic Impact Analysis, the proposed development would result in 43 AM peak hour trips and 60 PM peak hour trips. The Proposed Project would not have the potential to double the traffic volumes on any of the ten intersections analyzed for the Project Site. As such, the Proposed Project would not have the potential to increase roadway noise levels by 3 dBA.

To quantify the Proposed Project's mobile noise impacts on the study intersections, traffic noise was modeled using the California Department of Transportation, Technical Noise Supplement (2009). Traffic noise was modeled under the Existing (2016) and Existing (2016) Plus Project scenarios to determine the environmental baseline and Project impact. The changes in future noise levels associated with the Proposed Project at locations in the surrounding intersections in the Project vicinity where sensitive receptors are located are identified in Table III-15, Project Noise Impacts at Study Intersections. As shown, the Proposed Project would increase local noise levels by a maximum of 0.04 dBA CNEL at the intersection of Reseda Boulevard and Chase Street during the PM peak hour, which would be inaudible/imperceptible to most people and would not exceed the thresholds of significance. The remaining street intersections analyzed would all experience a 0.03 dBA CNEL increase or less. Thus, the Project's mobile noise impacts would not exceed the 3 dBA CNEL threshold, set forth in the *L.A. CEQA Thresholds Guide*, and the Proposed Project's mobile source noise impact would be less than significant.

### *Stationary Noise Sources*

New stationary sources of noise, such as mechanical HVAC equipment would be installed for the proposed residences at the Project Site. As discussed in Question XI(a) above, the design of this equipment would be required to comply with LAMC Section 112.02, which prohibits noise from air conditioning, refrigeration, heating, pumping, and filtering equipment from exceeding the ambient noise level on the premises of other occupied properties by more than five decibels. Thus, because the noise levels generated by the HVAC equipment serving the Proposed Project would not be allowed to exceed the ambient noise level by five decibels on the premises of the adjacent properties, a substantial permanent increase in noise levels would not occur at the nearby sensitive receptors. This impact would be less than significant.

### *Parking Noise*

Activities within the designated parking structure areas associated with the Proposed Project would have the potential to increase ambient noise levels in the area. Sources of noise within the parking areas would include engines accelerating, doors slamming, car alarms, and people talking. Noise levels within the parking areas would fluctuate with the amount of automobile and human activity. Noise levels would be highest in the early morning and evening when the largest number of people would enter and exit the Project Site. However, any parking noise that may be audible from outside of the parking areas would be

substantially similar to the existing noise generated at the surface parking areas of the existing commercial buildings, which includes a car resale center, an auto body shop, and a commercial office building. In addition, operational-related noise generated by motor driven vehicles within the Project Site is regulated under the LAMC. Specifically, with regard to motor driven vehicles, LAMC Section 114.02 prohibits the operation of any motor driven vehicles upon any property within the City such that the created noise would cause the noise level on the premises of any occupied residential property to exceed the ambient noise level by more than 5 dBA. With implementation of Mitigation Measure N-3, noise impacts associated with the Proposed Project's parking garage within the two mixed-use buildings would ensure operational noise impacts are less than significant.

**Table III-15  
Project Noise Impacts at Study Intersections**

Street Intersection	Peak Hour	Noise Levels in dBA CNEL			
		Existing (2016) w/o Project	Existing (2016) Plus Project	Increase	Significant Impact?
1. Reseda Boulevard and Nordhoff Street	AM	69.90	69.91	0.01	No
	PM	69.90	69.91	0.01	No
2. Reseda Boulevard and Parthenia Street	AM	69.90	69.92	0.02	No
	PM	69.90	69.92	0.02	No
3. Reseda Boulevard and Chase Street	AM	69.90	69.93	0.03	No
	PM	69.90	69.94	0.04	No
4. Reseda Boulevard and Roscoe Boulevard	AM	73.10	73.12	0.02	No
	PM	73.10	73.12	0.02	No
5. Reseda Boulevard and Saticoy Street	AM	73.10	73.11	0.01	No
	PM	73.10	73.11	0.01	No
6. Roscoe Boulevard and Tampa Avenue	AM	73.10	73.10	0.00	No
	PM	73.10	73.11	0.01	No
7. Roscoe Boulevard and Wilbur Avenue	AM	73.10	73.11	0.01	No
	PM	73.10	73.11	0.01	No
8. Roscoe Boulevard and Etiwanda Avenue	AM	73.10	73.11	0.01	No
	PM	73.10	73.11	0.01	No
9. Roscoe Boulevard and Lindley Avenue	AM	73.10	73.11	0.01	No
	PM	73.10	73.11	0.01	No
10. Roscoe Boulevard and White Oak Avenue	AM	73.10	73.11	0.01	No
	PM	73.10	73.11	0.01	No

*Source: Calculations based on the California Department of Transportation (Caltrans), Technical Noise Supplement (Nov. 2009) formula for adding and subtracting equal sound pressure levels. Traffic volumes are based on the Project Traffic Study prepared by Overland Traffic Consultants, Inc. dated March 2016.*

### **Mitigation Measure:**

#### **N-3 Parking Structure Ramps**

- Concrete, not metal, shall be used for construction of parking ramps.
- The interior ramps shall be textured to prevent tire squeal at turning areas.

- d) **Would the project result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?**

**Potentially Significant Unless Mitigation Incorporated.** A significant impact may occur if the Proposed Project were to result in a substantial temporary or periodic increase in ambient noise levels above existing ambient noise levels without the Proposed Project. As discussed above, temporary and periodic construction noise impacts are expected to be mitigated to less than significant level for both construction noise and vibration with implementation of Mitigation Measures N-1 and N-2. With mitigation, the Proposed Project's temporary and periodic increase in ambient noise levels would be mitigated to a less than significant level.

- e) **For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?**

**No Impact.** A significant impact may occur if the Proposed Project were located within an airport land use plan and would introduce substantial new sources of noise or substantially add to existing sources of noise within or in the vicinity of the Project Site. There are no airports within a two-mile radius of the Project Site. The closest airport is the Van Nuys Airport approximately 2.5 miles east of the Project Site. The Proposed Project would not expose people to excessive noise levels associated with airport uses. Therefore, no impact would occur.

- f) **For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?**

**No Impact.** This question would apply to a project only if it were in the vicinity of a private airstrip and would subject area residents and workers to a safety hazard. The Project Site is not located in the vicinity of a private airstrip. As no such facilities are located in the vicinity of the Project Site, no impact would occur.

### **Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project in conjunction with the nine related projects identified in Section II, Project Description, would result in an increase in construction-related and traffic-related noise as well as on-site stationary noise sources in the already urbanized area of the City of Los Angeles. The Project Applicant has no control over the timing or sequencing of the related projects that have been identified within the Proposed Project study area. Therefore, any quantitative analysis that assumes multiple, concurrent construction projects would be speculative. Construction-period noise for the Proposed Project and each related project (that has not yet been built) would be localized. In addition, each of the related projects would be required to comply with the City's noise ordinance, as well as mitigation measures that may be prescribed pursuant to CEQA provisions that require potentially significant impacts to be reduced to the extent feasible.

With respect to cumulative traffic noise impacts, traffic noise was modeled under the Existing (2016) and Future (2019) with Project scenarios to determine the environmental baseline, Project impact, related projects, and future growth impacts in the Project area. Thus, the future predicted noise levels include the traffic volumes from the Proposed Project and future traffic levels associated with ambient growth and the related projects, as shown in Table III-16. The highest increase in local noise levels shows a maximum of 0.44 dBA CNEL at the intersection of Reseda Boulevard and Chase Street during the AM peak hour, which would not exceed the 3 dBA CNEL threshold of significance. As such, the Proposed Project's noise volumes would not be cumulatively considerable. Thus, the cumulative impact associated with construction noise would be less than significant.

**Table III-16  
Cumulative Noise Impacts at Study Intersections**

Street Intersection	Peak Hour	Noise Levels in dBA CNEL			
		Existing (2016) Without Project	Future (2019) with Project	Increase	Significant Impact?
1. Reseda Boulevard and Nordhoff Street	AM	69.90	70.27	0.37	No
	PM	69.90	70.24	0.34	No
2. Reseda Boulevard and Parthenia Street	AM	69.90	70.22	0.32	No
	PM	69.90	70.29	0.39	No
3. Reseda Boulevard and Chase Street	AM	69.90	70.34	0.44	No
	PM	69.90	70.32	0.42	No
4. Reseda Boulevard and Roscoe Boulevard	AM	73.10	73.53	0.43	No
	PM	73.10	73.51	0.41	No
5. Reseda Boulevard and Saticoy Street	AM	73.10	73.46	0.36	No
	PM	73.10	73.45	0.35	No
6. Roscoe Boulevard and Tampa Avenue	AM	73.10	73.46	0.36	No
	PM	73.10	73.46	0.36	No
7. Roscoe Boulevard and Wilbur Avenue	AM	73.10	73.45	0.35	No
	PM	73.10	73.45	0.35	No
8. Roscoe Boulevard and Etiwanda Avenue	AM	73.10	73.53	0.43	No
	PM	73.10	73.51	0.41	No
9. Roscoe Boulevard and Lindley Avenue	AM	73.10	73.46	0.36	No
	PM	73.10	73.46	0.36	No
10. Roscoe Boulevard and White Oak Avenue	AM	73.10	73.49	0.39	No
	PM	73.10	73.50	0.40	No

*Source: Calculations based on the California Department of Transportation (Caltrans), Technical Noise Supplement (Nov. 2009) formula for adding and subtracting equal sound pressure levels. Traffic volumes are based on the Project Traffic Study prepared by Overland Traffic Consultants, Inc. dated March 2016.*

### XIII. POPULATION AND HOUSING

- a) **Would the project induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?**

**Less Than Significant Impact.** A significant impact may occur if a project would locate new development such as homes, businesses, or infrastructure, with the effect of substantially inducing growth in the

proposed area that would otherwise not have occurred as rapidly or in as great a magnitude. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether a project results in a significant impact on population and housing growth shall be made considering: (a) the degree to which a project would cause growth (i.e., new housing or employment generators) or accelerate development in an undeveloped area that exceeds projected/planned levels for the year of project occupancy/buildout, and that would result in an adverse physical change in the environment; (b) whether the project would introduce unplanned infrastructure that was not previously evaluated in the adopted Community Plan or General Plan; and (c) the extent to which growth would occur without implementation of the project.

In October 2008, SCAG approved and adopted the “2008 Regional Comprehensive Plan for the SCAG Region – Helping Communities Achieve A Sustainable Future” (2008 RCP). The 2008 RCP is a long-term comprehensive plan that provides a strategic vision for handling the region’s land use, housing, economic, transportation, environmental, and overall quality of life needs. The 2008 RCP is intended to serve as an advisory document for local agencies in the SCAG region. The following vision statement and guiding principles are based on the region’s adopted Compass Growth Vision Principles for Sustaining a Livable Region. These statements further articulate how the RCP can promote and sustain the region’s mobility, livability, and prosperity for future generations.

#### *RCP Vision*

*To foster a Southern California region that addresses future needs while recognizing the interrelationship between economic prosperity, natural resource sustainability, and quality of life. Through measured performance and tangible outcomes, the RCP serves as both a voluntary action plan with short-term guidance and strategic, long-term initiatives that are guided by the following Guiding Principles for sustaining a livable region.*

#### *RCP Guiding Principles*

- *Improve mobility for all residents.* Improve the efficiency of the transportation system by strategically adding new travel choices to enhance system connectivity in concert with land use decisions and environmental objectives.
- *Foster livability in all communities.* Foster safe, healthy, walkable communities with diverse services, strong civic participation, affordable housing and equal distribution of environmental benefits.
- *Enable prosperity for all people.* Promote economic vitality and new economies by providing housing, education, and job training opportunities for all people.
- *Promote sustainability for future generations.* Promote a region where quality of life and economic prosperity for future generations are supported by the sustainable use of natural resources.

***SCAG's Compass Growth Vision Strategy***

SCAG's Compass Growth Vision, adopted in 2004, and incorporated into the 2008 RCP, encourages better relationships between housing, transportation, and employment. The Growth Vision is driven by four key principles: (1) Mobility – Getting where we want to go, (2) Livability – Creating positive communities, (3) Prosperity – Long-term health for the region, and (4) Sustainability – Preserving natural surroundings. Additionally, the Compass Growth Vision incorporates a 2% Growth Strategy that will increase the region's mobility by:

- Putting new employment centers and new neighborhoods near major transit systems so that people can have transportation choices other than their cars.
- Designing safe, attractive transit centers and plazas that people enjoy using.
- Creating mini-communities around transit stations, with small businesses, urban housing and restaurants all within an easy walk.

***Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS)***

On April 2016, SCAG's Regional Council adopted the 2016-2040 Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS): A Plan for Mobility, Accessibility, Sustainability, and a High Quality of Life. The RTP/SCS is the culmination of a multi-year effort involving stakeholders from across the SCAG Region. The 2016-2040 RTP/SCS balances the Southern California region's future mobility and housing needs with economic, environmental, and public health goals.

Based on the regional growth projections in the 2016-2040 RTP/SCS, in 2012, the City of Los Angeles had an estimated permanent population of approximately 3,845,500 persons and approximately 1,325,500 residences. By the year 2040, SCAG forecasts the City of Los Angeles will increase to 4,609,400 persons (or a 20% increase since the year 2012) and approximately 1,690,300 residences (or an 28% increase since the year 2012) citywide. SCAG's population and housing projections for the City of Los Angeles and the SCAG region as a whole for 2012 and 2040 are further summarized in Table III-17, below.

**Table III-17  
SCAG Population and Housing Projections for the  
City of Los Angeles and the SCAG Region**

<b>Population</b>			
	<b>2012</b>	<b>2040</b>	<b>% Growth (2012-2040)</b>
Los Angeles City <sup>a</sup>	3,845,500	4,609,400	20%
Los Angeles County <sup>b</sup>	9,923,000	11,514,000	16%
SCAG Region <sup>b</sup>	18,322,000	22,138,000	21%
<b>Households</b>			
	<b>2012</b>	<b>2040</b>	<b>% Growth (2012-2040)</b>
Los Angeles City <sup>a</sup>	1,325,500	1,690,300	28%
Los Angeles County <sup>b</sup>	3,257,000	3,946,000	21%
SCAG Region <sup>b</sup>	5,885,000	7,412,000	26%
<b>Employment</b>			
	<b>2012</b>	<b>2040</b>	<b>% Growth (2012-2040)</b>
Los Angeles City <sup>a</sup>	1,696,400	2,169,100	28%
Los Angeles County <sup>b</sup>	4,246,000	5,226,000	23%
SCAG Region <sup>b</sup>	7,440,000	9,872,000	33%
<i>Source:</i>			
<sup>a</sup> SCAG, adopted 2016-2040 RTP/SCS Growth Forecast, Demographics and Growth Forecast Appendix, adopted April 2016.			
<sup>b</sup> SCAG, adopted 2016-2040 RTP/SCS Growth Forecast, adopted April 2016.			

The Proposed Project will revitalize an underutilized, fully developed property in an existing commercial area. The Proposed Project is an infill development Project within the Northridge Community Plan Area within the City of Los Angeles. With respect to regional growth forecasts, SCAG forecasts the City of Los Angeles Subregion will experience a population increase to 4.6 million persons by 2040. The U.S. Census Bureau reported the City of Los Angeles as having a population of 3,792,621 persons and 1,413,995 housing units in 2010.<sup>36</sup> As shown in Table III-17, SCAG Population and Housing Projections for the City of Los Angeles and the SCAG Region the forecast from 2012 through 2040 envisions a population growth of 763,900 additional persons (an approximate 20% growth rate) and 3,816,000 additional persons (an approximate 21% growth rate), respectively. The number of households within the City of Los Angeles is anticipated to increase by 364,800 households, or approximately 28% between 2012 and 2040. The number of households within the SCAG Region is anticipated to increase by 1,527,000 households, or approximately 26% between 2012 and 2040. By 2040, the City of Los Angeles is expected to experience a 20% population growth, 28% household unit growth, and a 28% employment growth as compared to the

<sup>36</sup> United States Census Bureau, *Quick Facts: Los Angeles City, California*, website: <https://www.census.gov/quickfacts/table/PST045215/0644000,00>, accessed April 2016.

2012 values.

Based on the community's current household demographics (e.g., an average of 2.78 persons per household for the Northridge area), the construction of up to 140 residential dwelling units (79 multi-family and 61 small lot) would result in an increase of approximately 389 net permanent residents in the City of Los Angeles.<sup>37</sup> The proposed increase in housing units and population would be consistent with the SCAG growth forecast of 364,800 additional households and approximately 763,900 persons in the City of Los Angeles between 2012 and 2040. As such, the Proposed Project would not cause growth (i.e., new housing or employment generators) or accelerate development in an undeveloped area that exceeds projected/planned levels for the year of Proposed Project occupancy/buildout or that would result in an adverse physical change in the environment. The Project would not introduce unplanned infrastructure that was not previously evaluated in the adopted SCAG RTP/SCS.

According to the Department of City Planning, the Northridge Community Plan designates residential land use densities based on a reasonable expected population of 66,351 persons and 23,627 dwelling units by 2010 within the Northridge Community Plan area.<sup>38</sup> The 2010 United States Census shows that the Northridge Community Plan area had an actual population of approximately 67,339 persons and 23,942 dwelling units in 2010.<sup>39</sup> The 2010 Census data shows that the actual population and number of households in the Northridge CPA are higher than anticipated, by exceeding the Community Plan projection by 988 persons and 315 households. Historically, the community was planned for residential purposes, and aims to preserve existing residential neighborhoods throughout the community, retaining existing single-family areas and multi-family corridors. The Proposed Project would not remove any existing dwelling units or displace any residents. Additionally, as discussed above, the Project addition of up to 140 dwelling units and approximately 389 net permanent residents is consistent with SCAG's growth projections for the Los Angeles region. The Proposed Project would meet the needs of the Community Plan by adding housing stock to an area that would reduce vehicular trips and increase accessibility to local services and facilities. Therefore, impacts related to population growth would be less than significant.

---

<sup>37</sup> Los Angeles Department of City Planning Demographic Research Unit, *City of Los Angeles: 2009 Population Estimate Population by Housing Type, Northridge Community Plan Area*, website: <http://cityplanning.lacity.org/DRU/Loc/LocFrame.cfm?geo=CP&loc=Nrd&sgo=ct&rpt=PnH&yrx=Y09>, accessed October 2015.

<sup>38</sup> City of Los Angeles Department of City Planning, *Northridge Community Plan, Plan Population and Dwelling Unit Capacity*, pg. III-2.

<sup>39</sup> The Northridge Community Plan Area contains the following tracts: 1112.04, 1112.05, 1112.06, 1113.02, 1151.01, 1151.03, 1151.04, 1152.01, 1152.02, 1153.01, 1153.02, 1154.01, 1154.03, 1154.04, and 1173.02. The population and dwelling units were calculated by summing the individual tracts together. Source: United States Census Bureau, *2010 Census Interactive Population Map*, website: <http://www.census.gov/2010census/popmap/>, accessed October 2015.

**b) Would the project displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?**

**No Impact.** A significant impact may occur if the Proposed Project would result in the displacement of existing housing units, necessitating the construction of replacement housing elsewhere. The Proposed Project would consist of the development of new housing and commercial land uses on a site that is currently developed with a car resale center, an auto body shop, and a commercial office building. As such, the Proposed Project would not displace any existing housing. The proposed development of small lot homes and mixed-use residential and commercial buildings are consistent with the allowable uses as permitted by the zoning and General Plan land use designations. Therefore, no impact would occur.

**c) Would the project displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?**

**No Impact.** The Proposed Project would consist of the development of a unified development with two mixed-use residential and commercial buildings (79 multi-family units and 10,300 square feet of commercial use) and a small-lot subdivision with 61 small lot residential units. The Project Site is currently developed with a car resale center, an auto body shop, and a commercial office building. No displacement of existing housing would occur with the development of the Proposed Project. Therefore, no impact would occur.

**Cumulative Impacts**

**Less Than Significant Impact.** The related projects would introduce additional residential related uses to the City of Los Angeles. Any residential related projects would result in direct population growth in the City of Los Angeles. As shown in Table III-18, the Proposed Project and related projects that involve residential developments would cumulatively contribute 1,033 new residential dwelling units to the Northridge Community Plan area, generating approximately 2,872 new residents.

**Table III-18  
Estimated Cumulative Housing Units**

<b>Related Projects (By Housing Type)</b>	<b>Total Housing Units <sup>a</sup></b>	<b>Total Residents <sup>b</sup></b>
Apartments	720	2,002
Condominiums	173	481
<b>Related Projects Total:</b>	<b>893</b>	<b>2,483</b>
Proposed Project Net Total:	140	389
<b>Cumulative Total:</b>	<b>1,033</b>	<b>2,872</b>
<sup>a</sup> Based on the housing units provided in the Related Projects table of the Traffic Study, prepared by Overland Traffic Consultants, Inc., dated March 2016. <sup>b</sup> Based on a generation rate of 2.78 residents per dwelling unit. Los Angeles Department of City Planning Demographic Research Unit, City of Los Angeles: 2009 Population Estimate Population by Housing Type, Central City Community Plan Area, website: <a href="http://cityplanning.lacity.org/DRU/Loc/LocFrame.cfm?geo=CP&amp;loc=Nrd&amp;sgo=ct&amp;rpt=PnH&amp;yrx=Y09">http://cityplanning.lacity.org/DRU/Loc/LocFrame.cfm?geo=CP&amp;loc=Nrd&amp;sgo=ct&amp;rpt=PnH&amp;yrx=Y09</a> , accessed October 2015. Source: Parker Environmental Consultants, 2015.		

As discussed in Question XIII(a), the Proposed Project would not exceed the growth projections of SCAG's RCP for the City of Los Angeles subregion. Furthermore, the Proposed Project is the type of project encouraged by SCAG and City policies, as the Project would promote and help accommodate growth in urban centers that are close to existing employment centers and alternative transportation. Because the Proposed Project would not displace any residents, and population growth potentially associated with the Proposed Project has already been anticipated per SCAG projections, the Proposed Project's population growth would not be cumulatively considerable. Therefore, the Proposed Project's cumulative impacts to population and housing would be less than significant.

#### **XIV. PUBLIC SERVICES**

**a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objective for any of the following public services:**

**(i) Fire protection?**

**Less Than Significant Impact.**

##### ***Construction***

Construction of the Proposed Project would increase the potential for accidental on-site fires from the operation of construction equipment and the use of flammable construction materials. The implementation of best management practices (BMPs) for the operation of mechanical equipment and the use of flammable construction materials by construction contractors and work crews would minimize fire hazards associated with the construction of the Proposed Project. The BMPs that would be implemented during construction of the Project would include: keeping mechanical equipment in good operating condition, and as required by law, carefully storing flammable materials in appropriate containers, and the immediate and complete cleanup of spills of flammable materials when they occur.

Construction activities also have the potential to affect fire protection services, such as emergency vehicle response times, by adding construction traffic to the street network and potentially requiring partial lane closures during street improvements and utility installations. Thus, construction could have the potential to adversely affect fire access. However, these impacts are considered to be less than significant because emergency access would be maintained to the Project Site during construction through marked emergency access points approved by the LAFD, construction impacts are temporary in nature and do not cause lasting effects, and no complete lane closures are anticipated. Additionally, if any partial street closures are required, flagmen would be used to facilitate the traffic flow until construction is complete.

---

### ***Operation***

Based on the *L.A. CEQA Thresholds Guide*, a project would normally have a significant impact on fire protection if it requires the addition of a new fire station or the expansion, consolidation or relocation of an existing facility to maintain service. Section 15382 of the CEQA guidelines defines “significant effect on the environment” as “a substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by the project including land, air, water, minerals, flora, fauna, ambient noise, and objects of historic or aesthetic significance. An economic or social change by itself shall not be considered a significant effect on the environment. A social or economic change related to a physical change may be considered in determining whether the physical change is significant.” Thus, the addition of a new fire station or the expansion, consolidation or relocation of an existing facility to maintain service would only be considered significant if such activities result in a physical adverse impact upon the environment.

The City of Los Angeles Fire Department (LAFD) considers fire protection services for a project adequate if a project is within the maximum response distance for the land use proposed. Pursuant to Section 57.09.07A of the LAMC, the maximum response distance between high density residential and commercial neighborhood land uses and a LAFD fire station that houses an engine company is 1.5 miles and a truck company is 2 miles. If the distance is exceeded, all structures located in the applicable residential or commercial area would be required to install automatic fire sprinkler systems. With such systems installed, fire protection would be considered adequate even if the project is located beyond the maximum response distance.

The Proposed Project would include a small lot subdivision with 61 small lot homes and a mixed-use development with up to 79 dwelling units and 10,300 square feet of ground floor commercial. The Proposed Project would generate approximately 389 new residents and 61 employees.<sup>40,41</sup> The Proposed Project would increase the utilization of the Project Site, which is currently used as a car resale center, an auto body shop, and a commercial office building and would potentially increase the demand for LAFD services. The Project Site is primarily served by LAFD Station No. 103, located at 18143 Parthenia Street, approximately 0.6 mile northeast of the Project Site, which includes an assessment engine and paramedic ambulance. The Project Site 1.2 miles north of LAFD Station No. 73, located at 7419 Reseda Boulevard. Fire Station No. 73 includes an engine company, assessment light force, and paramedic ambulance, which satisfies the Los Angeles Fire Code requirement of being within 1.5 miles of an engine company. Based on the response distance criteria specified in LAMC 57.09.07A and the relatively short distances from Fire Station No. 73 and Fire Station No. 103 to the Project Site, fire protection response would be considered adequate. Compliance with regulatory compliance measure RC-PS-1, below, would ensure impacts upon fire services are reduced to less than significant levels.

---

<sup>40</sup> *A residential generation rate of 2.78 used.*

<sup>41</sup> *Source: U.S. Green Building Council, Building Area per Employee by Business Type, May 13 2008.*

- **Regulatory Compliance Measure RC-PS-1 (Fire):** The recommendations of the Fire Department relative to fire safety shall be incorporated into the building plans, which includes the submittal of a plot plan for approval by the Fire Department either prior to the recordation of a final map or the approval of a building permit. The plot plan shall include the following minimum design features: fire lanes, where required, shall be a minimum of 20 feet in width; all structures must be within 300 feet of an approved fire hydrant, and entrances to any dwelling units or guest room shall not be more than 150 feet in distance in horizontal travel from the edge of the roadway of an improved street or approved fire lane.

## Cumulative Impacts

**Less Than Significant Impact.** The Proposed Project, in combination with the nine related projects, could increase the demand for fire protection services in the Project area. Specifically, there could be increased demands for additional LAFD staffing, equipment, and facilities over time. This need would be funded via existing mechanisms (e.g., property taxes, government funding, and developer fees) to which the Proposed Project and related projects would contribute. Similar to the Proposed Project, each of the related projects would be individually subject to LAFD review and would be required to comply with all applicable fire safety requirements of the LAFD in order to adequately mitigate fire protection impacts. Specifically, any related project that exceeded the applicable response distance standards described above would be required to install automatic fire sprinkler systems in order to mitigate the additional response distance. To the extent cumulative development causes the need for additional fire stations to be built throughout the City, the development of such stations would be on small infill lots within existing developed areas and would not likely cause a significant impact upon the environment. Nevertheless, the siting and development of any new fire stations would be subject to further CEQA review and evaluated on a case-by-case basis. However, as the LAFD does not currently have any plans for new fire stations to be developed in proximity to the Project Site, no impacts are currently anticipated to occur. On this basis, the Proposed Project would not make a cumulatively considerable impact to fire protection services, and, as such cumulative impacts on fire protection would be less than significant.

### (ii) Police Protection?

**Potentially Significant Unless Mitigation Incorporated.** A significant impact may occur if the City of Los Angeles Police Department (LAPD) could not adequately serve a project, necessitating a new or physically altered station. Section 15382 of the CEQA guidelines defines “significant effect on the environment” as “a substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by the Project including land, air, water, minerals, flora, fauna, ambient noise, and objects of historic or aesthetic significance. An economic or social change by itself shall not be considered a significant effect on the environment. A social or economic change related to a physical change may be considered in determining whether the physical change is significant.” Thus, the addition of a new police station or police substation, if warranted, would only be considered significant if the construction or operation of a new facility results in a physical adverse impact upon the environment. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether the project results in a significant impact on police protection shall be made considering the following factors: (a) the population increase resulting from the Proposed Project, based on the net increase of residential units or square footage of non-residential floor

area; (b) the demand for police services anticipated at the time of project buildout compared to the expected level of service available. Consider, as applicable, scheduled improvements to LAPD services (facilities, equipment, and officers) and the project's proportional contribution to the demand; and (c) whether the project includes security and/or design features that would reduce the demand for police services.

The Proposed Project would include up to 140 dwelling units (79 multi-family and 61 small lot) and up to 10,300 square feet of ground floor commercial and would generate approximately 389 new residents and 61 employees. The Proposed Project would increase the utilization of the Project Site, which is currently used as a car resale center, an auto body shop, and a commercial office building and would potentially increase the demand for LAPD services. The Project Site is located in the Devonshire Area division of the LAPD's Valley Bureau. The Project Site is served by the Devonshire Community Police Station, located at 10250 Etiwanda Avenue, which is approximately 2.3 miles north of the Project Site. Table III-19, Devonshire Area Police Station Crime Statistics, provides crime statistics for Devonshire area in the City of Los Angeles.

**Table III-19  
Devonshire Area Police Station Crime Statistics**

Crimes	2013 (Year to Date) <sup>a</sup>	2014 (Year to Date)	2015 (Year to Date)
<i>Violent Crimes</i>			
Homicide	4	1	3
Rape	36	48	39
Robbery	109	104	119
Aggravated Assault	203	225	343
<b>Total Violent Crimes</b>	<b>352</b>	<b>378</b>	<b>504</b>
<i>Property Crimes</i>			
Burglary	750	687	855
Motor Vehicle Theft	385	341	370
BTFV	1,129	864	1,154
Personal / Other Theft	1,026	1,300	1,324
<b>Total Property Crimes</b>	<b>3,290</b>	<b>3,192</b>	<b>3,703</b>
<b>Total Part 1 Crimes</b>	<b>3,642</b>	<b>3,570</b>	<b>4,207</b>
Child / Spousal Abuse (Part I & II) <sup>b</sup>	318	416	408
Shots Fired	25	22	27
Shooting Victims	8	6	8
<i>Notes:</i> <sup>a</sup> Crime Statistics for week ending October 10, 2015. <sup>b</sup> Part II Child/Spousal Abuse Simple Assaults not included in Part I Aggravated Assaults above to comply with the FBI's Uniform Crime Reporting guidelines. Source: LAPD, COMPSTAT Unit, Devonshire Area Profile, accessed October 2015.			

Construction sites, if left unsecured, have the potential to attract trespassers and/or vandals that would potentially result in graffiti, excess trash, and potentially unsafe conditions for the public. Such occurrences would adversely affect the aesthetic character of the Project Site and surrounding area and could potentially

cause public health and safety concerns. With compliance to Mitigation Measure PS-1, below, Project impacts would be less than significant during the construction period.

Implementation of the Proposed Project would result in an increase of site visitors, residents, and employees to the Project Site, thereby generating a potential increase in the number of service calls from the Project Site. Responses to thefts, vehicle burglaries, vehicle damage, traffic-related incidents, and crimes against persons would be anticipated to escalate as a result of the increased on-site activity and increased traffic on adjacent streets and arterials. The Proposed Project would include adequate and strategically positioned functional and thematic lighting to enhance public safety. Visually obstructed and infrequently accessed “dead zones” would be limited and, where possible, security controlled to limit public access. The two mixed-use buildings and layout design of the Proposed Project would also include crime prevention features, such as nighttime security lighting and secure parking facilities. In addition, the continuous visible and non-visible presence of residents at all times of the day would provide a sense of security during evening and early morning hours. As such, the Project residents of the mixed-use buildings would be able to monitor suspicious activity at the building entry points. These preventative and proactive security measures would decrease the amount of service calls to the LAPD. With adherence to the mitigation measure identified below, the Proposed Project’s potential impact upon LAPD services would be mitigated to a less than significant level.

#### **Mitigation Measure:**

##### **PS-1 Police:**

The plans shall incorporate the Design Guidelines (defined in the following sentence) relative to security, semi-public and private spaces, which may include but not be limited to access control to building, secured parking facilities, walls/fences with key systems, well-illuminated public and semi-public space designed with a minimum of dead space to eliminate areas of concealment, location of toilet facilities or building entrances in high-foot traffic areas, and provision of security guard patrol throughout the project site if needed. Please refer to “Design Out Crime Guidelines: Crime Prevention Through Environmental Design”, published by the Los Angeles Police Department. Contact the Community Relations Division, located at 100 W. 1<sup>st</sup> Street, #250, Los Angeles, CA 90012; (213) 486-6000. These measures shall be approved by the Police Department prior to the issuance of building permits.

#### **Cumulative Impacts**

**Less Than Significant Impact.** The Proposed Project, in combination with the nine related projects, would increase the demand for police protection services in the Project area. Specifically, there would be an increased demand for additional LAPD staffing, equipment, and facilities over time. This need would be funded via existing mechanisms (e.g., sales taxes, government funding, and developer fees), to which the Proposed Project and related projects would contribute. In addition, each of the related projects would be individually subject to LAPD review and would be required to comply with all applicable safety requirements of the LAPD and the City of Los Angeles in order to adequately address police protection service demands. Furthermore, each of the related projects would likely install and/or incorporate adequate crime prevention design features in consultation with the LAPD, as necessary, to further decrease the

demand for police protection services. To the extent cumulative development causes the need for additional police stations to be built throughout the City, the development of such stations would be on small infill lots within existing developed areas and would not likely cause a significant impact upon the environment. Nevertheless, the siting and development of any new police stations would be subject to further CEQA review and evaluated on a case-by-case basis. However, as the LAPD does not currently have any plans for new police stations to be developed in proximity to the Project Site. No impacts are currently anticipated to occur. On this basis, the Proposed Project would not make a cumulatively considerable impact to police protection services, and cumulative impacts on police protection would be less than significant.

**(iii) Schools?**

**Less Than Significant Impact.** A significant impact may occur if a project includes substantial employment or population growth, which could generate a demand for school facilities that would exceed the capacity of the Los Angeles Unified School District (LAUSD). The Project Site is located in LAUSD Board District 3. The Project Site is currently served by one elementary school, one middle school, and three high schools. Table III-20, Resident Schools Serving the Project Site, details the names, grades served, and location of each school.

**Table III-20  
Resident Schools Serving the Project Site**

School Name	Grades	Address
Cantara Street Elementary	K-5	17950 Cantara Street
Northridge Middle School	6-8	17960 Chase Street
Grover Cleveland Charter High School	9-12	8140 Vanalden Avenue
Northridge Academy Senior High	9-12	9601 Zelzah Avenue
Valley Academy of Arts and Sciences	9-12	10445 Balboa Boulevard
<i>Notes: Enrollment in Northridge Academy Senior High and Valley Academy of Arts and Sciences is by application only. Source: Los Angeles Unified School District, Resident School Identifier, website: <a href="http://rsi.lausd.net/ResidentSchoolIdentifier/">http://rsi.lausd.net/ResidentSchoolIdentifier/</a>, accessed October 2015.</i>		

As shown in Table III-21, Proposed Project Estimated Student Generation, the Proposed Project would generate at most 25 elementary students, 7 middle school students and 14 high school students, for a total of approximately 46 students. This would create an increased demand on local school districts. As shown in the Related Projects List (Table II-6 of Section II. Project Description), Related Project No. 5 includes the development of a private school with a capacity of 265 students. The construction of this related project may further lessen the school demands in the area. Furthermore, the Project Applicant would be required to pay all applicable developer fees to the LAUSD to offset the Proposed Project’s demands upon local schools. Pursuant to Government Code Section 65995, the development fees authorized by SB 50 are deemed to be “full and complete school facilities mitigation.” Thus, the Proposed Project’s potential impact upon public school services would be less than significant by the following regulatory compliance measure:

- **Regulatory Compliance Measure RC-PS-2 (Payment of School Development Fee):** Prior to issuance of a building permit, the General Manager of the City of Los Angeles, Department of

Building and Safety, or designee, shall ensure that the Applicant has paid all applicable school facility development fees in accordance with California Government Code Section 65995.

**Table III-21  
Proposed Project Estimated Student Generation**

Land Use	Size	Elementary School Students	Middle School Students	High School Students	Total Students
<b>Existing Uses</b>					
Car Resale Center / Auto Repair Shop (Commercial) <sup>a</sup>	21,986 sf	0.3	0.2	0.1	0.6
Commercial Office <sup>b</sup>	5,425 sf	0.1	0.2	0.1	0.4
<b>Total Existing Students:</b>		<b>0.4</b>	<b>0.4</b>	<b>0.2</b>	<b>1.0</b>
<b>Proposed Project</b>					
Small Lot <sup>c</sup>	61 du	12.2	3.3	7.0	22.5
Multi-Family <sup>d</sup>	79 du	13.0	3.6	7.4	24.0
Commercial	10,300 sf	0.2	0.1	0.1	0.4
<b>Total Proposed Project Students:</b>		<b>25.4</b>	<b>7.0</b>	<b>14.5</b>	<b>46.9</b>
<i>Less Existing Students:</i>		<i>-0.4</i>	<i>-0.4</i>	<i>-0.2</i>	<i>-1.0</i>
<b>NET Student Generation:</b>		<b>25.0</b>	<b>6.6</b>	<b>14.3</b>	<b>45.9</b>
<p><i>Notes:</i>  <sup>a</sup> <i>sf = square feet; du = dwelling units</i>  <sup>a</sup> <i>Student generation rates are as follows for retail/commercial uses: .0149 elementary, .0069 middle and .0067 high school students per 1,000 square feet.</i>  <sup>b</sup> <i>Student generation rates are as follows for office uses: .0233 elementary, .0108 middle and .0104 high school students per 1,000 square feet.</i>  <sup>c</sup> <i>Student generation rates are as follows for small lot residential uses (which used single-family detached rates for a conservative estimate): .0199 elementary, .0546 middle and .1143 high school students per unit.</i>  <sup>d</sup> <i>Student generation rates are as follows for multi-family residential uses: .1649 elementary, .0450 middle and .0943 high school students per unit.</i>  <i>Source: For bullet points (a) above: Los Angeles Unified School District, School Facilities Needs Analysis for Los Angeles Unified School District, September 2012.</i>  <i>-For bullet points (b) and (c) above: Los Angeles Unified School District, School Fee Justification Study, September 2002.</i></p>					

## Cumulative Impacts

**Less Than Significant Impact.** The Proposed Project, in combination with the nine related projects is expected to result in a cumulative increase in the demand for school services. Development of the related projects would likely generate additional demands upon school services. These related projects would have the potential to generate students that would attend the same schools as the Proposed Project. As shown in Table III-22, Estimated Cumulative Student Generation, the Proposed Project and related projects would cumulatively contribute approximately 49 elementary school students, 45 middle school students and 94 high school students, for a total of almost 188 students. This would create an increased cumulative demand on local school districts. As noted in Table II-6 in Section II. Project Description, Related Project No. 5 proposes the development of a private school, which may further lessen the overall demand on schools in the Project area. Additionally, each of the new housing units would be responsible for paying mandatory

school fees to mitigate the increased demand for school services. Cumulative impacts on schools would be less than significant.

**Table III-22  
Estimated Cumulative Student Generation**

Land Use	Size	Elementary School Students	Middle School Students	High School Students	Total Students
Single-Family Attached <sup>a</sup>	173 du	9.2	2.5	5.2	16.9
Multi-Family Residences <sup>b</sup>	776 du	12.0	34.9	73.2	120.1
Industrial <sup>c</sup>	80,133 sf	1.4	0.7	0.6	2.7
Office <sup>d</sup>	5,300 sf	0.1	0.1	0.1	0.3
Retail <sup>e</sup>	63,807 sf	0.8	0.4	0.4	1.6
<b>Related Projects Total:</b>		<b>23.5</b>	<b>38.6</b>	<b>79.5</b>	<b>141.6</b>
Proposed Project Net Total:		25.0	6.6	14.3	45.9
<b>Cumulative Total:</b>		<b>48.5</b>	<b>45.2</b>	<b>93.8</b>	<b>187.5</b>

Notes: sf = square feet; du = dwelling units  
Uses not listed are estimated by the closest type of use available in the table.

<sup>a</sup> Student generation rates are as follows for single-family attached residential uses: .053 elementary, .0145 middle and .0303 high school students per unit.

<sup>b</sup> Student generation rates are as follows for multi-family residential uses: .1649 elementary, .0450 middle and .0943 high school students per unit.

<sup>c</sup> Student generation rates are as follows for industrial uses: .018 elementary, .0083 middle and .008 high school students per 1,000 square feet.

<sup>d</sup> Student generation rates are as follows for office uses: .0233 elementary, .0108 middle and .0104 high school students per 1,000 square feet.

<sup>e</sup> Student generation rates are as follows for retail/commercial uses: .0149 elementary, .0069 middle and .0067 high school students per 1,000 square feet.

Source:  
-For bullet points (a) and (b) above: Los Angeles Unified School District, School Facilities Needs Analysis for Los Angeles Unified School District, September 2012.  
-For bullet points (c) through (g) above: Los Angeles Unified School District, School Fee Justification Study, September 2002.  
-Related Project No. 1 was not included in generation due to lack of project information.  
-Conversions and estimates of square feet per occupant based on California Building Code (2013), Ch.10, Table 1004.1.2.

**(iv) Parks?**

**Less Than Significant Impact.** A significant impact would occur if the recreation and park services available could not accommodate the projected population increase resulting from implementation of a project or if the proposed project resulted in the construction of new recreation and park facilities that create significant direct or indirect impacts to the environment. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether the project results in a significant impact on recreation and parks shall be made considering the following factors: (a) the net population increase resulting from the Proposed Project; (b) the demand for recreation and park services anticipated at the time of project buildout compared to the expected level of service available. Consider, as applicable, scheduled improvements to recreation and park services (renovation, expansion, or addition) and the project's proportional contribution to the demand; and (c) whether the project includes features that would reduce the demand for park services (e.g., on-site recreation facilities, land dedication, or direct financial support to the Department of Recreation and Parks).

The Public Recreation Plan (PRP), a portion of the Service Systems Element of the City of Los Angeles General Plan, provides standards for the provision of recreational facilities throughout the City and includes Local Recreation Standards. The desired long-range standard for local parks is based on two acres per 1,000 persons for neighborhood parks and two acres per 1,000 persons for community parks or four acres per 1,000 persons of combined neighborhood and community parks. However, the PRP also notes that these long-range standards may not be reached during the life of the plan, and, therefore, includes more attainable short- and intermediate-range standards of one (1) acre per 1,000 persons for neighborhood parks and one (1) acre per 1,000 persons for community parks, or two (2) acres per 1,000 people of combined neighborhood and community parks. These standards are Citywide goals and are not intended to be requirements for individual development projects. The Public Recreation Element of the City's General Plan also recognizes that the achievement of such goals is not the responsibility of individual development projects and that such goals will be met by "seek[ing] federal, state and private funds to implement acquisition and development of parks and recreational facilities."

The Proposed Project is located within an urbanized area within the Northridge Community Plan area. As shown in Table III-23, there are approximately 20.1 acres of parkland and public recreation facilities within a 2-mile radius of the Project Site. These facilities range from 0.78-acres (Cleveland High School Pool) to 9.16 acres (Dearborn Park). As discussed in Checklist Question XII (a), it is estimated that the development of the Proposed Project would result in an increase of 389 new residents to the area. Based on the standard parkland ratio goal of 4 acres per 1,000 residents, the Proposed Project would generate a Citywide goal of serving such residents with approximately 1.56 acres of additional public parkland. The Project will contribute towards the achievement of such goal through a combination of (1) on-site open space proposed within the Project, (2) payment of applicable taxes in accordance with LAMC Section 21.10.3(a)(1), and (3) the availability of existing park and recreation facilities within the area. The Proposed Project would provide approximately 10,230 square feet (0.23 acres) of total open space and amenities on-site available exclusively to serve Project residents and their guests in the two mixed-use buildings, thereby achieving the required square feet of open space required by the LAMC. The small lot subdivision does not require open space for the future residents; although the Proposed Project would provide approximately 17,589 square feet of common open space at grade to serve residences to of the small lot subdivision. As such, the Proposed Project would provide a total of 27,819 square feet of common and private open space on-site. In addition to the on-site open space provided within the Proposed Project, the Proposed Project is subject to a tax of \$200 per dwelling unit pursuant to LAMC Section 21.10.3(a)(1) (Dwelling Unit Construction Tax). This tax, payable to the Department of Building and Safety, shall be deposited into a "Park and Recreational Sites and Facilities Fund" to be used exclusively for the acquisition and development of park and recreational sites. In accordance with LAMC Section 21.10.3(a)(1), this tax may be offset or reduced based on the amount of on-site open space and recreational amenities provided on-site. Further, the Proposed Project would be required to comply with LAMC Section 12.33, which determines the dedication of land or payment for park and recreational facilities as a requirement for a zone change for a multiple residential use in any multiple residential or commercial zone. Therefore, under the City's mandatory Dwelling Unit Construction Tax and Quimby Fees and adherence with LAMC Section 12.33, which are collected prior to a certificate of occupancy for residential land uses (see regulatory compliance measure RC-PS-3, below), the Proposed Project's impact upon parks and recreational facilities would be reduced to a less-than-significant level.

- **Regulatory Compliance Measure RC-PS-3 (Increased Demand For Parks Or Recreational Facilities):**

- (*Subdivision*) Pursuant to Section 17.12-A or 17.58 of the Los Angeles Municipal Code, the applicant shall pay the applicable Quimby fees for the construction of dwelling units.
- (*Apartments*) Pursuant to Section 21.10 of the Los Angeles Municipal Code, the applicant shall pay the Dwelling Unit Construction Tax for construction of apartment buildings.
- The applicant shall comply with the applicable provisions of LAMC Section 12.33.

**Table III-23  
Recreation and Park Facilities within the Project Area**

<b>Park Name</b>	<b>Park Size (acres)</b>	<b>Park Amenities</b>	<b>Approx. Distance to Project Site (miles)</b>
1. Cleveland High School Pool	0.78	Indoor pool, sports programs	0.83
2. Vanalden Park	8.38	Picnic tables, horseshoe pit, jogging path	1.06
3. Jesse Owens Mini-Park	1.74	Open space, picnic tables, jogging path	1.80
4. Dearborn Park	9.16	Basketball courts (lighted/outdoor), children's play area, picnic tables, tennis courts	1.90
<b>Total Parkland (Approximate):</b>	<b>20.1</b>		
Sources: Park distances, size, and amenities were determined using: (1) City of Los Angeles Department of Recreation and Parks, Facility Locator, <a href="http://www.laparks.org/">http://www.laparks.org/</a> ; (2) Navigate LA, <a href="http://navigatea.lacity.org/navigatea/">http://navigatea.lacity.org/navigatea/</a> ; and (3) Google Earth (when necessary), accessed October 2015.			

**Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project in conjunction with the nine related projects could result in an increase in permanent residents residing in the greater Project area. Additional cumulative development would contribute to lowering the City's existing parkland to population ratio, which is currently below the preferred standard. However, each of the residential related projects are required to comply with payment of Quimby (for subdivisions) and Parks and Recreation Fee (for apartment units). Each residential related project would also be required to comply with the on-site open space requirements of the LAMC. Therefore, with payment of the applicable recreation fees on a project-by-project basis, the Proposed Project would not make a cumulatively considerable impact to parks and recreational facilities, and cumulative impacts would be less than significant.

**(v) Other Public Facilities?**

**Less Than Significant Impact.** A significant impact may occur if a project includes substantial employment or population growth that could generate a demand for other public facilities (such as libraries), which would exceed the capacity available to serve the Project Site. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether the project results in a significant impact on libraries shall be made considering the following factors: (a) the net population increase resulting from the Project; (b) the

demand for library services anticipated at the time of project buildout compared to the expected level of service available. Consider, as applicable, scheduled improvements to library services (renovation, expansion, addition or relocation) and the project's proportional contribution to the demand; and (c) whether the project includes features that would reduce the demand for library services (e.g., on-site library facilities or direct financial support to the Los Angeles Public Library).

Within the City of Los Angeles, the Los Angeles Public Library (LAPL) provides library services at the Central Library, seven regional branch libraries, 56 community branches and two bookmobile units, consisting of a total of five individual bookmobiles. Approximately 6.5 million books and other materials comprise the LAPL collection. The LAPL branch currently serving the Project Site is the Northridge Branch Library, located at 9051 Darby Avenue, approximately 0.9 mile north of the Project Site.<sup>42</sup> It is anticipated that the Northridge Branch Library currently meets the demands of the Project area and would be able to meet the Proposed Project's demand for library services. Therefore, the Proposed Project's impacts upon library services would be less than significant.

### **Cumulative Impacts**

**Less Than Significant Impact.** Development of the related projects is projected to generate additional housing and residents within the study area, which would likely generate additional demands upon library services. This increase in resident population, combined with the 389 additional residents generated by the Proposed Project, would result in a cumulative increase in demands upon public library services. To meet the increased demands upon the City's Public Library system, Los Angeles voters passed a Library Bond Issue for \$178.3 million to improve, renovate, expand, and construct 32 branch libraries. Since the Program's inception in 1998, the Library Department and the Department of Public Works, Bureau of Engineering have made considerable progress in the design and construction of the branch library facilities. Based on the growth forecasts utilized in the 2007-2010 Strategic Plan, much of this growth has already been accounted for in planning new and expanded library facilities. Thus, the 389 additional residents generated by the Proposed Project would not make a cumulatively considerable impact upon the City's library system. Therefore, the cumulative impacts related to library facilities would be reduced to a less than significant level.

---

<sup>42</sup> *City of Los Angeles Public Library, Hours and Locations, website: <http://www.lapl.org/branches>, accessed October 2015.*

---

**XV. RECREATION**

- a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?**

**Less Than Significant Impact.** For the purpose of this Initial Study, a significant impact may occur if the project would include substantial employment or population growth, which would increase the use of existing neighborhood and regional parks or other recreational facilities, such that substantial physical deterioration of the facility would occur or be accelerated. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether the project results in a significant impact on recreation and parks shall be made considering the following factors: (a) the net population increase resulting from the proposed project; (b) the demand for recreation and park services anticipated at the time of project buildout compared to the expected level of service available. Consider, as applicable, scheduled improvements to recreation and park services (renovation, expansion, or addition) and the project's proportional contribution to the demand; and (c) whether the project includes features that would reduce the demand for park services (e.g., on-site recreation facilities, land dedication, or direct financial support to the Department of Recreation and Parks).

It is reasonable to assume that the future occupants of the Proposed Project would utilize recreation and park facilities in the surrounding area. As noted in Table III-23, above, there are four existing new and recently improved parks within the Project Area totaling more than 20 acres that are available to serve the future residents and retail visitors of the Project Site. The Proposed Project would be required to provide 10,175 square feet of open space for the mixed-use buildings residents. The Proposed Project would provide approximately 10,230 square feet (0.23 acres) of and recreational facilities within the two mixed-use buildings on-site that would be available exclusively to serve Project residents and their guests. Thus, the Proposed Project would comply with LAMC requirements for on-site open space. Although not required, the Proposed Project would additionally provide approximately 17,589 square feet of common open space at grade to serve residences to of the small lot subdivision. As such, the Proposed Project would provide a total of 27,819 square feet of common and private open space on-site. The availability of these on-site recreation amenities and opportunities would serve to reduce the demand for off-site park services, and accordingly the Proposed Project would not substantially increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated. In addition, the Applicant would pay the City's mandatory Dwelling Unit Construction Tax and Quimby fees, which are collected prior to a certificate of occupancy for residential land uses, and comply with LAMC Section 12.33 (refer to regulatory code compliance measure RC-PS-3 ,above). Accordingly, the Proposed Project's impact upon parks and recreational facilities would be less than significant.

- b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?**

**Less Than Significant Impact.** A significant impact may occur if a project includes or requires the construction or expansion of park facilities and such construction would have a significant adverse effect on

the environment. As noted above, there are four existing, new, or recently improved parks within the Project Area totaling more than 20 acres that are available to serve the future residents and retail visitors to the Project Site. The Proposed Project would also provide approximately 10,230 square feet of open space and recreational facilities within the two mixed-use buildings on-site. Thus, the Proposed Project would comply with LAMC requirements for on-site open space. Although not required, the Proposed Project would additionally provide approximately 17,589 square feet of common open space at grade to serve residences to of the small lot subdivision. As such, the Proposed Project would provide a total of 27,819 square feet of common and private open space on-site. As discussed in Section XIV (iv) above, Citywide park standards are Citywide goals and are not intended to be requirements for individual development projects. The Public Recreation Plan within the City's General Plan also recognizes that the achievement of such goals is not the responsibility of individual development projects and that such goals will be met by "seek[ing] federal, state and private funds to implement acquisition and development of parks and recreational facilities." The Proposed Project itself does not include the expansion of park facilities and does not require the construction or expansion of recreational facilities that might have an adverse impact on the environment. Therefore, a less than significant impact would occur.

### **Cumulative Impacts**

**Less Than Significant Impact.** Section 15355 of the State CEQA Guidelines defines cumulative impacts as "two or more individual effects which, when considered together, are considerable or which compound or increase other environmental impacts." As discussed above, the Proposed Project would have a less than significant impact on recreational resources. The Proposed Project in combination with the nine related projects would be expected to increase the cumulative demand for parks and recreational facilities in the City of Los Angeles. A number of new parks and recently renovated park improvements have been made in to accommodate cumulative demands created by increased residential development. Similar to the Proposed Project's requirement to pay a Dwelling Unit Construction Tax and Quimby Fees to improve recreation and park facilities, the related projects that include residential units would be required to pay similar recreation taxes and/or applicable Quimby fees to mitigate impacts upon park and recreational facilities and to provide additional funds to meet Citywide park goals. Additionally, each related project would be subject to the provisions of the LAMC for providing on-site open space, which is proportionately based on the amount of new development. Because the Proposed Project would have a less than significant incremental contribution to the potential cumulative impact on recreational resources, the Proposed Project would have a less than significant cumulative impact on such resources.

## **XVI. TRANSPORTATION AND TRAFFIC**

The following section summarizes and incorporates by reference the information provided in the *Traffic Impact Analysis for a Mixed-Use Development, Located at 8350-8430 Reseda Boulevard Avenue in the City of Los Angeles*, prepared by Overland Traffic Consultants, Inc., dated April 2016. The Traffic Study and related correspondence from the Los Angeles Department of Transportation (dated July 13, 2016) are provided as Appendix G to this IS/MND.

- a) Would the project conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of**

**transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?**

**Potentially Significant Unless Mitigation Incorporated.** The Transportation Research Board Circular 212 Critical Movement Analysis (CMA) Planning Method was used to analyze traffic operating conditions at study intersections. CMA methodology compares the amount of traffic an intersection is able to process (capacity) to the level of traffic during peak hours (volume). The resulting volume-to-capacity ratio (V/C) is expressed in terms of level of service (LOS). LOS A represents free-flow activity and LOS F represents overcapacity operation. LOS is a qualitative assessment of the quantitative effects of such factors as traffic volume, roadway geometrics, speed, delay, and maneuverability on roadway and intersection operations.

The City of Los Angeles determines whether a transportation impact at a signalized intersection is significant according to a sliding scale. At an intersection with a final LOS C, a project impact would occur if the project contributes 0.040 or greater to the intersection V/C. At an intersection with a final LOS D, a project impact would occur if the project contributes 0.020 or greater to the intersection V/C. At an intersection with a final LOS E or F, a project impact would occur if the project contributes 0.010 or greater to the intersection V/C. Refer to Table III-24, Definition of Significant Impact at Intersection, below.

**Table III-24  
Definition of Significant Impact at Intersection**

Level of Service	Volume-to-Capacity (V/C)	Project-related Increase in Volume-to-Capacity (V/C) Ratio
C	0.701–0.800	Equal to or greater than 0.04
D	0.801–0.900	Equal to or greater than 0.02
E, F	> 0.900	Equal to or greater than 0.01

**Study Intersections**

A total of ten intersections were identified, in conjunction with LADOT staff, for inclusion in the traffic analysis. The analyzed locations are shown in Figure 3 of the Traffic Study (see Appendix G). None of these intersections are Los Angeles County Congestion Management Plan (CMP) monitoring locations or freeway mainline monitoring locations. Detailed descriptions of the roadway facilities in the study area are provided in the Traffic Study. The following intersections were determined in consultation with LADOT:

- |  |   |
|--|---|
| 1. Reseda Boulevard and Nordhoff Street  | 6. Roscoe Boulevard and Tampa Avenue      |
| 2. Reseda Boulevard and Parthenia Street | 7. Roscoe Boulevard and Wilbur Avenue     |
| 3. Reseda Boulevard and Chase Street     | 8. Roscoe Boulevard and Etiwanda Avenue   |
| 4. Reseda Boulevard and Roscoe Boulevard | 9. Roscoe Boulevard and Lindley Avenue    |
| 5. Reseda Boulevard and Saticoy Street   | 10. Roscoe Boulevard and White Oak Avenue |

## Existing Transit Options

Public transportation in the study area is provided by the Metropolitan Transportation Authority (Metro). Metro provides transit routes 150 and 240 along Reseda Boulevard and Ventura Boulevard with major stops at Westfield Topanga and Promenade Malls, Warner Center Transit Hub, Pierce College, Sherman Oaks Galleria, Universal Studios City Walk and CSUN. Metro routes 152 and 353 limited provide service on Roscoe Boulevard in the vicinity of the Project Site, running from Woodland Hills via Fallbrook Avenue and Roscoe Boulevard to Sun Valley with stops at Van Nuys and Burbank Airports and ultimately to the North Hollywood Red and Orange Line Stations. Rapid Line 744 runs along Reseda Boulevard, Ventura Boulevard and Van Nuys Boulevard with major stops at CSUN, Panorama Mall, Van Nuys Civic Center, Metrolink Stations at Van Nuys and Northridge and the Hanson Dam Recreation area. The Metro Rapid 744 bus stop is located at the intersection of Roscoe Boulevard and Reseda Boulevard, approximately 0.1 mile south of the Project Site. Los Angeles Department of Transportation (LADOT) DASH also provides a bus service near the Project Site. The DASH Northridge route runs through major points of interest in Northridge and Reseda. The closest DASH bus stop is located on the southwest corner of Roscoe Boulevard and Reseda Boulevard, approximately 0.1 mile south of the Project Site. The transit route information is provided in Appendix C of the Traffic Study.

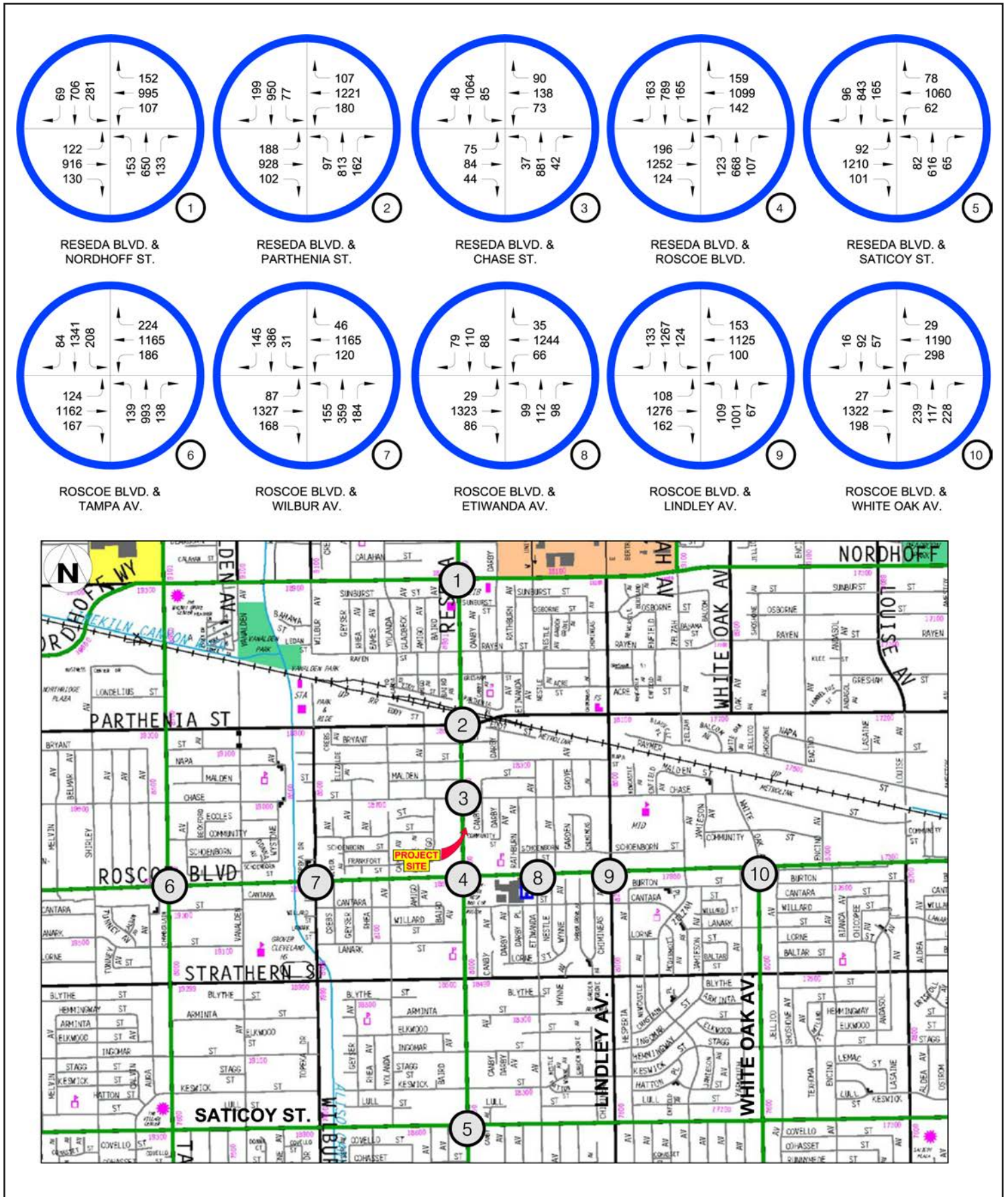
## Existing (2016) Traffic Volumes

The traffic conditions analysis at the study intersections was conducted using the City of Los Angeles Department of Transportation Critical Movement Analysis (CMA) method. The existing peak hour traffic counts were used along with intersection lane configurations and traffic controls to determine the intersection's current operating condition. As shown in Table III-25, below, all of the intersections operate at acceptable levels of service (LOS D or better). Figure III-2 and Figure III-3 illustrate the Existing (2016) traffic volumes of the ten study intersections.

**Table III-25**  
**Existing (2016) Condition – Intersection Level of Service**

Intersection	Existing Conditions			
	AM		PM	
	V/C	LOS	V/C	LOS
1. Reseda Boulevard and Nordhoff Street	0.576	A	0.723	C
2. Reseda Boulevard and Parthenia Street	0.897	D	0.846	D
3. Reseda Boulevard and Chase Street	0.481	A	0.518	A
4. Reseda Boulevard and Roscoe Boulevard	0.724	C	0.776	C
5. Reseda Boulevard and Saticoy Street	0.715	C	0.715	C
6. Roscoe Boulevard and Tampa Avenue	0.804	D	0.844	D
7. Roscoe Boulevard and Wilbur Avenue	0.717	C	0.604	B
8. Roscoe Boulevard and Etiwanda Avenue	0.599	A	0.536	A
9. Roscoe Boulevard and Lindley Avenue	0.887	D	0.740	C
10. Roscoe Boulevard and White Oak Avenue	0.699	B	0.660	B

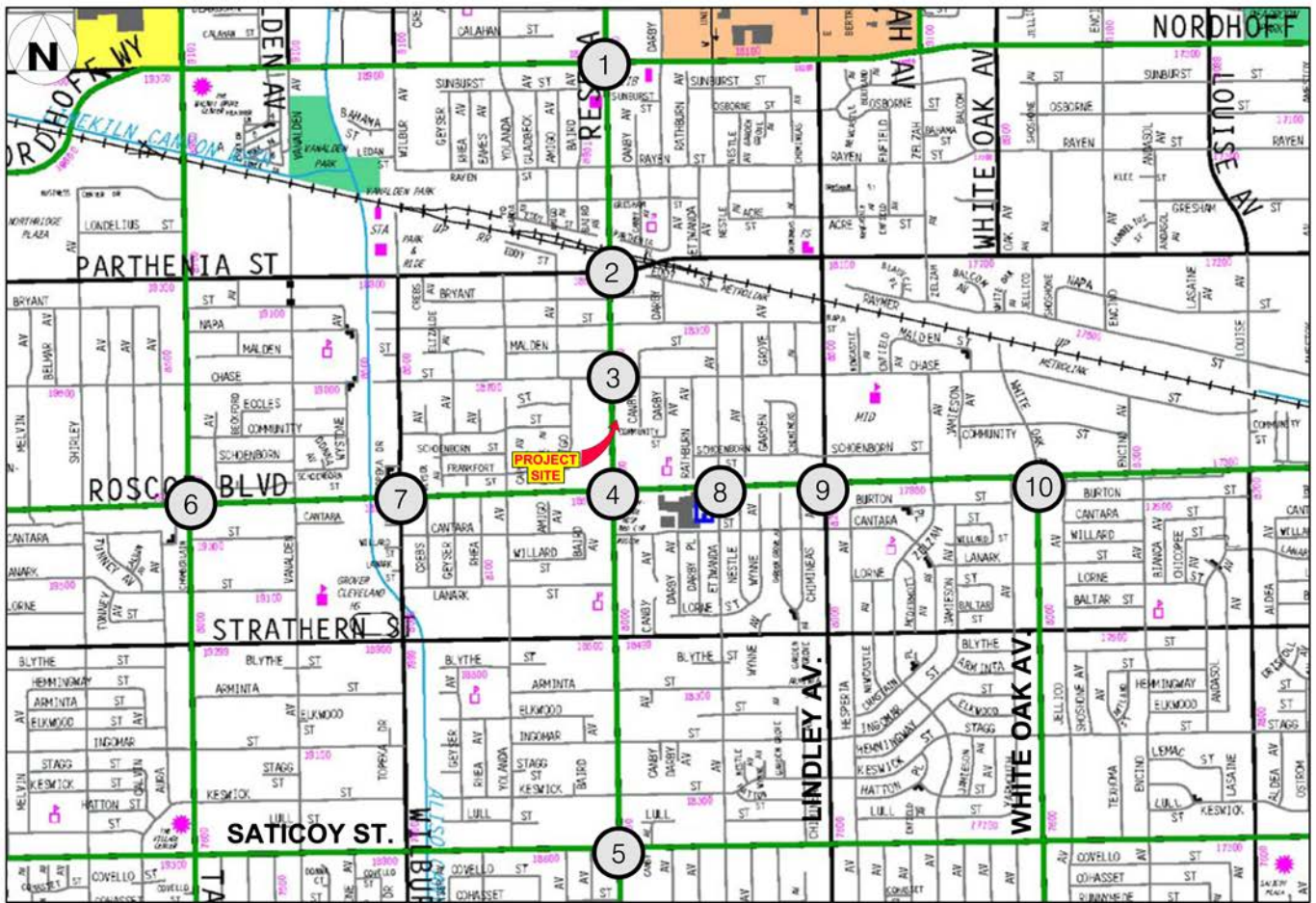
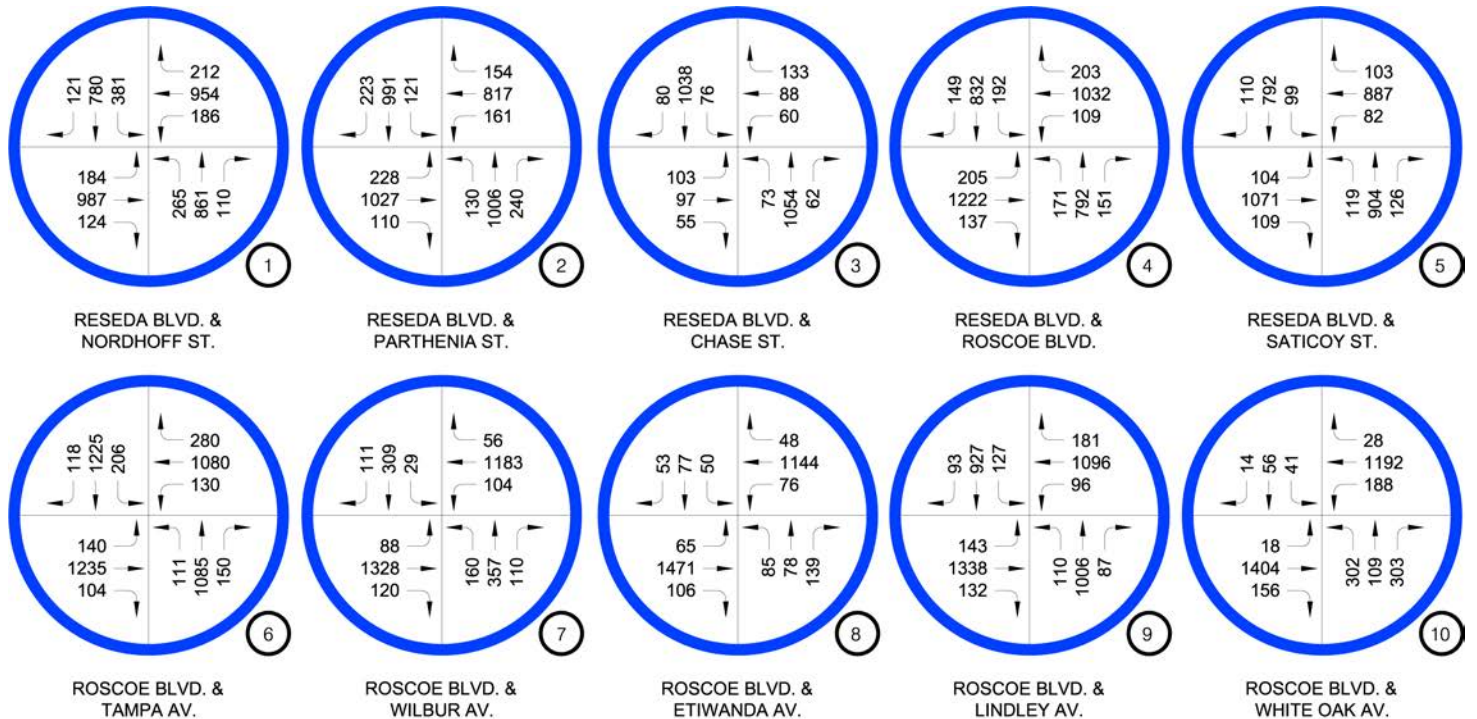
*LOS = level of service; V/C = volume-to-capacity ratio*  
*Source: Overland Traffic Consultants, Inc., Traffic Impact Analysis for a Mixed-Use Development, Located at 8350-8430 Reseda Boulevard Avenue, April 2016.*



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



Figure III-2  
Existing (2016) Traffic Volumes - AM Peak Hour



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



Figure III-3  
Existing (2016) Traffic Volumes - PM Peak Hour

## **Project Impacts**

### ***Project Parking Vehicular Access***

The main vehicular access to the Project Site would be provided from Reseda Boulevard with secondary access from Canby Avenue. Parking for the mixed-use buildings would be provided underneath the two proposed mixed-use buildings in a subterranean and at-grade parking garage. Each of the 61 small residential lots would provide two parking spaces on the first floor. The proposed parking supply is expected to meet the requirements of the City of Los Angeles Municipal Code. In addition, the Proposed Project would comply with the LAMC bicycle parking requirements by providing adequate long-term and short-term bicycle stalls.

### ***Estimated Trip Generation***

The Project Site is currently developed with 21,986 square feet of automobile uses and 5,425 square feet of commercial office land uses. The Proposed Project includes the development of two mixed-use buildings with 79 apartment units and 10,300 square feet of retail and restaurant use, and the development of 61 small lot homes. The daily and peak-hour trips for the Proposed Project were generated using trip rates from the Institute of Transportation Engineers (ITE) *Trip Generation* Manual (9<sup>th</sup> Edition, 2012). In accordance with the LADOT, vehicle trip generation is reduced by 5% for internal trips, 20% for retail pass-by trips, 10% for quality restaurant pass-by trips, and 20% high-turnover restaurant pass-by trips. Project trip generation is presented in Table III-26, below. As shown in Table III-26, the analysis estimates that the Project would generate a total of 43 AM peak hour vehicle trips and 60 PM peak hour vehicle trips without pass-by adjustments.

**Table III-26  
Project Trip Generation Estimates**

Land Use (Land Use Code)	Size	Unit	ADT <sup>a</sup>	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
<b>Proposed Project</b>									
<i>Apartments (220)</i>	79	du	525	8	32	40	32	17	49
<i>Small Lot Residences (210)</i>	61	du	354	5	22	27	21	10	31
<b>Subtotal Residential Trips:</b>			<b>879</b>	<b>13</b>	<b>54</b>	<b>67</b>	<b>53</b>	<b>27</b>	<b>80</b>
<i>Retail (820)</i>	5,150	sf	220	3	2	5	9	10	19
Internal Trips (5%) <sup>b</sup>			-11	0	0	0	0	-1	-1
Pass-by Trips (20%)			-42	-1	0	-1	-2	-2	-4
<i>Quality Restaurant (931)</i>	2,575	sf	232	1	1	2	13	6	19
Internal Trips (5%)			-12	0	0	0	-1	0	-1
Pass-by Trips (10%)			-22	0	0	0	-1	-1	-2
<i>High-Turnover Restaurant (932)</i>	2,575	sf	327	15	13	28	15	10	25
Internal Trips (5%)			-16	-1	0	-1	-1	0	-1
Pass-by Trips (20%)			-62	-3	-2	-5	-3	-2	-5
<b>Subtotal Commercial Trips:</b>			<b>614</b>	<b>14</b>	<b>14</b>	<b>28</b>	<b>29</b>	<b>20</b>	<b>49</b>
<b>Total Proposed Project Trips:</b>			<b>1,493</b>	<b>27</b>	<b>68</b>	<b>95</b>	<b>82</b>	<b>47</b>	<b>129</b>
<i>Total Project Pass-By Trips:</i>			<i>-126</i>	<i>-4</i>	<i>-2</i>	<i>-6</i>	<i>-6</i>	<i>-5</i>	<i>-11</i>
<b>Existing Development</b>									
<i>Automobile Services (942)</i>	21,986	sf	589	32	17	49	33	35	68
Pass-by Trips (10%)			-59	-3	-2	-5	-3	-4	-7
<i>Optician Office (710)</i>	5,425	sf	98	5	1	6	3	7	10
<i>&amp; Lens Manufacturing (140) <sup>c</sup></i>			10	2	0	2	1	1	2
Pass-by Trips (10%)			-10	-1	0	-1	0	-1	-1
<b>Total Existing Trips:</b>			<b>629</b>	<b>35</b>	<b>17</b>	<b>52</b>	<b>34</b>	<b>38</b>	<b>72</b>
Net New Trips (New – Existing):			864	-8	51	43	48	9	57
Pass-By Trips:			-69	-4	-2	-6	-3	-5	-8
<b>Net Trip Generation (without pass-by):</b>			<b>921</b>	<b>-8</b>	<b>51</b>	<b>43</b>	<b>51</b>	<b>9</b>	<b>60</b>
<small>ADT = average daily traffic; du =dwelling units; sf = square feet  <sup>a</sup> Trip rates based on the Institute of Transportation Engineers (ITE) Trip Generation Manual, 9<sup>th</sup> Edition (2012).  <sup>b</sup> Internal and pass-by trips based on City of Los Angeles Department of Transportation (LADOT) Traffic Study Policies and Procedures (2014).  <sup>c</sup> Optician sees patients and manufactures lenses. ½ sf dedicated to medical office and ½ sf to manufacturing.                      Source: Overland Traffic Consultants, Inc., Traffic Impact Analysis for a Mixed-Use Development, Located at 8350-8430 Reseda Boulevard Avenue, April 2016.</small>									

**Existing (2016) With Project Intersection Level of Service**

Existing Plus Project traffic volumes have been developed to analyze the existing traffic conditions after completion of the Proposed Project. The Project’s traffic impact has been calculated by adding the Project-only traffic volumes to the existing traffic volumes. Figure III-4 and Figure III-5 illustrate the Existing Plus Project traffic volumes for the AM and PM peak hours, respectively. As discussed above, the LOS level were determined using the LADOT spreadsheet for calculating CMA methodology. Table III-27, below, summarizes the results of the Existing (2016) and Existing (2016) Plus Project LOS analysis for the ten study intersections for the AM And PM peak hours. Under Existing Plus Project conditions, nine of the ten study intersections would continue to operate at LOS D or better. All study intersections are forecast to operate at the existing LOS level with the exception of Reseda Boulevard and Parthenia Street, which would change from LOS D to LOS E. However, the change in traffic flow generated by the Proposed Project would not exceed the City’s threshold of significance. Therefore, the Proposed Project would not

cause any significant traffic impacts in either the AM or PM peak hour for any of the ten study intersections. Further, Proposed Project would be reviewed by LADOT and would comply with the applicable conditions and recommendations provided by LADOT. The Proposed Project would result in a less than significant impact under the Existing with Project traffic condition.

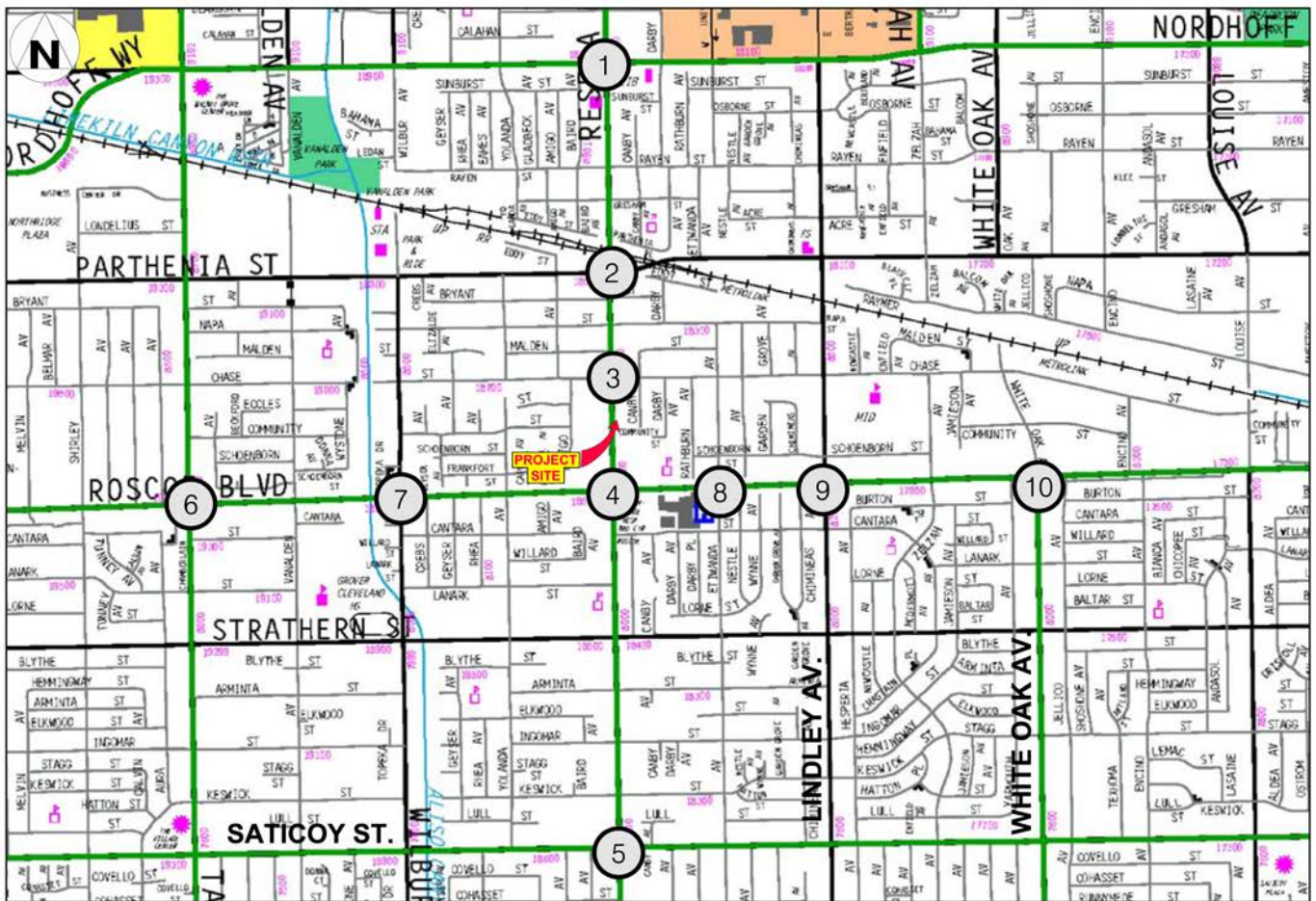
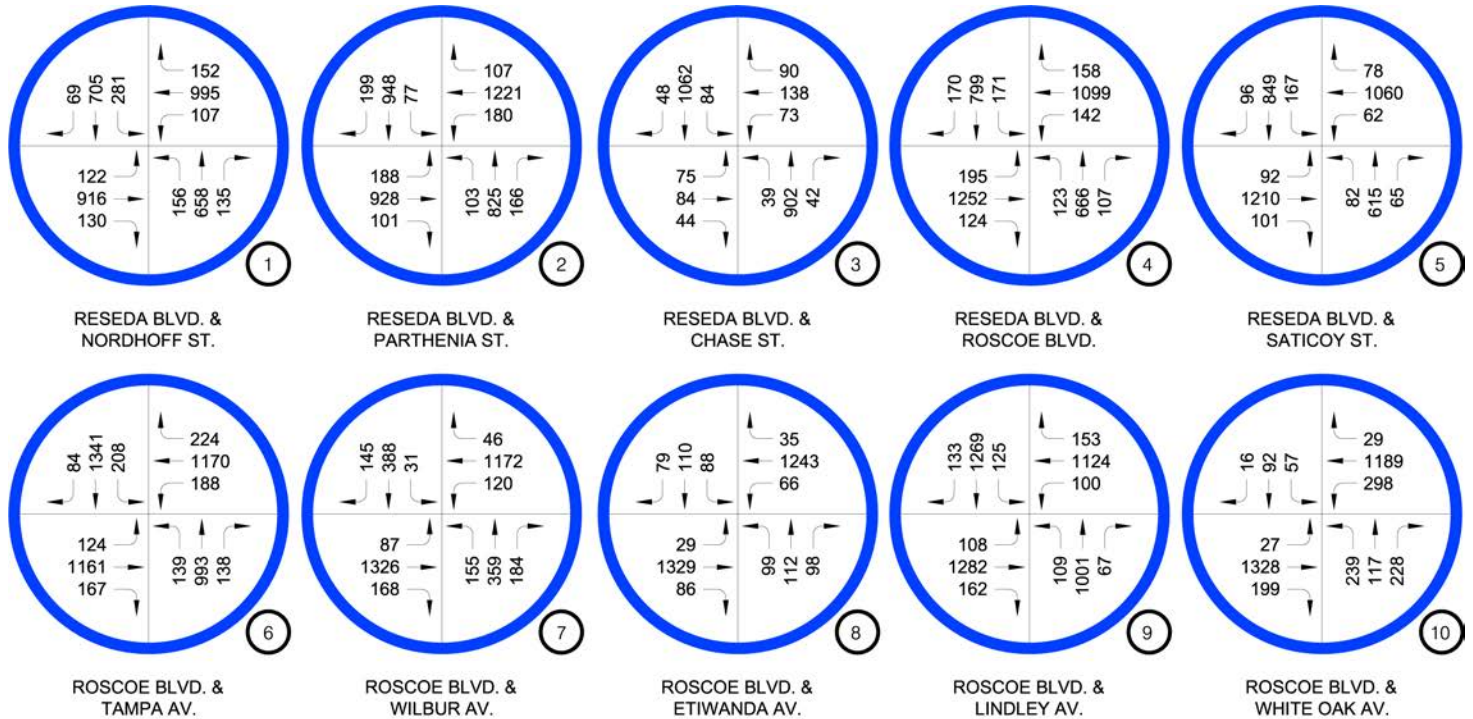
Further, the Proposed Project would be required to implement measures detailed in LADOT's communication with the Planning Department (DOT Case No. 43751) and dated July 13, 2016. The Proposed Project's construction and operational phases would coordinate and follow the guidance of LADOT, which would further ensure that Project impacts would be less than significant.

**Table III-27  
Existing (2016) Plus Project Condition Level of Service Summary**

Intersection	Peak Hour	Existing		Existing With Project		Project Impact
		V/C	LOS	V/C	LOS	
1. Reseda Boulevard and Nordhoff Street	AM	0.576	A	0.579	A	0.003
	PM	0.723	C	0.723	C	0.000
2. Reseda Boulevard and Parthenia Street	AM	0.897	D	0.901	E	0.004
	PM	0.846	D	0.853	D	0.007
3. Reseda Boulevard and Chase Street	AM	0.481	A	0.482	A	0.001
	PM	0.518	A	0.523	A	0.005
4. Reseda Boulevard and Roscoe Boulevard	AM	0.724	C	0.727	C	0.003
	PM	0.776	C	0.787	C	0.011
5. Reseda Boulevard and Saticoy Street	AM	0.715	C	0.717	C	0.002
	PM	0.715	C	0.717	C	0.002
6. Roscoe Boulevard and Tampa Avenue	AM	0.804	D	0.805	D	0.001
	PM	0.844	D	0.845	D	0.001
7. Roscoe Boulevard and Wilbur Avenue	AM	0.707	C	0.711	C	0.004
	PM	0.604	B	0.605	B	0.001
8. Roscoe Boulevard and Etiwanda Avenue	AM	0.599	A	0.599	A	0.000
	PM	0.536	A	0.536	A	0.000
9. Roscoe Boulevard and Lindley Avenue	AM	0.887	D	0.887	D	0.000
	PM	0.740	C	0.741	C	0.001
10. Roscoe Boulevard and White Oak Avenue	AM	0.699	B	0.700	B	0.001
	PM	0.660	B	0.661	B	0.001

*LOS = level of service; V/C = volume-to-capacity ratio*

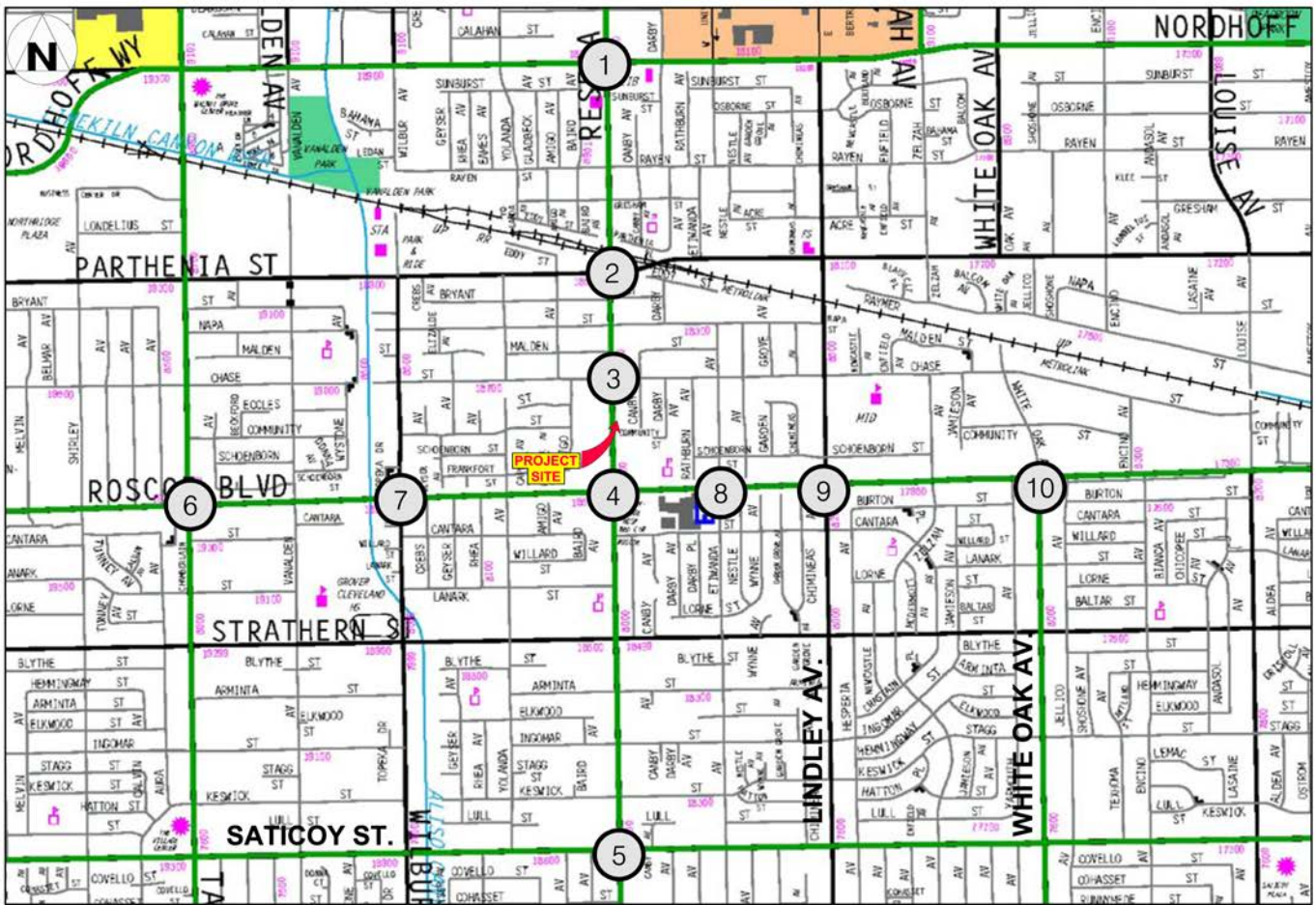
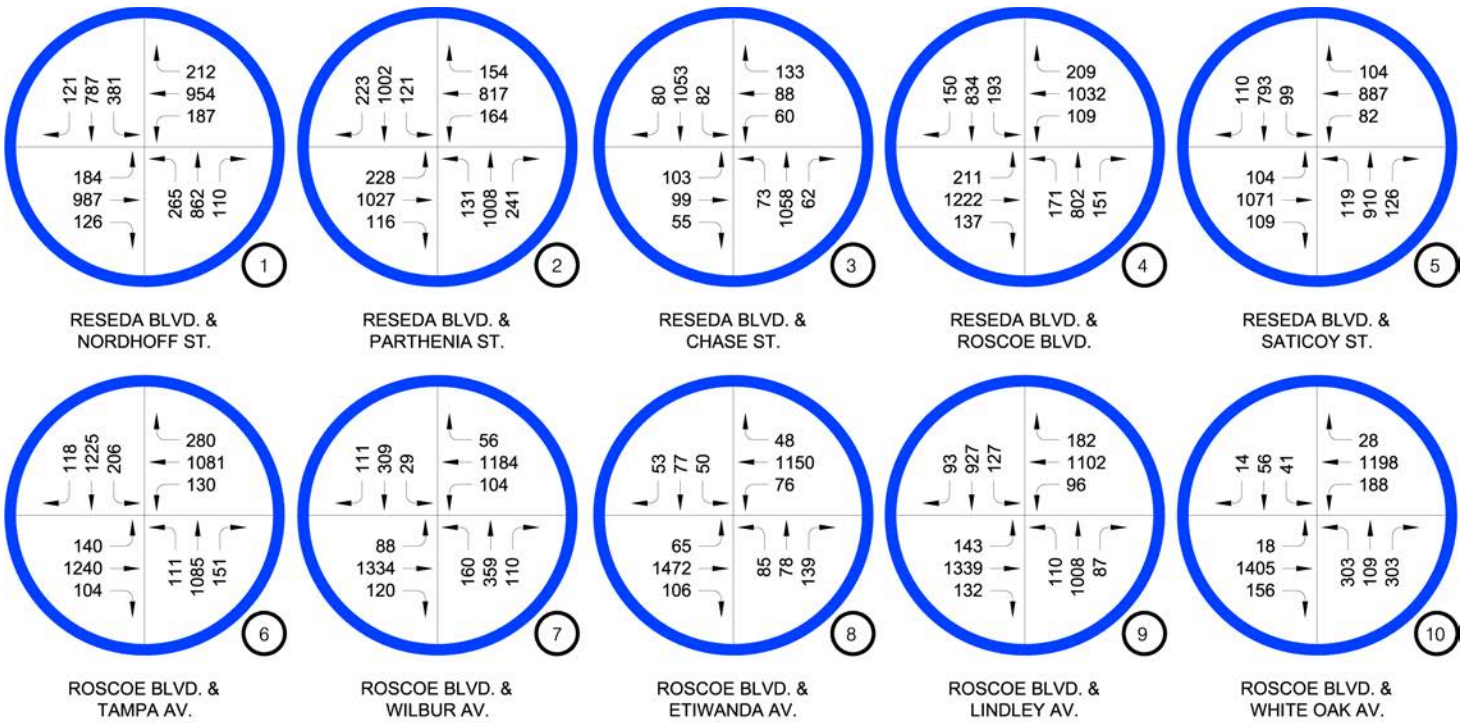
*Source: Overland Traffic Consultants, Inc., Traffic Impact Analysis for a Mixed-Use Development, Located at 8350-8430 Reseda Boulevard Avenue, April 2016.*



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



Figure III-4  
Existing (2016) with Project Traffic Volumes - AM Peak Hour



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



Figure III-5  
Existing (2016) with Project Traffic Volumes - PM Peak Hour

---

***Future (2019) Conditions Intersection Level of Service***

The potential traffic growth in the Future (2019) at the study intersections has been determined by adding the existing traffic volume, ambient traffic growth of one percent per year, and traffic from the other related development projects. In addition to the related project traffic, it was assumed that future bike lanes would be installed on Roscoe Boulevard through the study area. Figure III-6 and Figure III-7 illustrate the future traffic volumes at the ten intersections without the Proposed Project during the AM and PM peak hours, respectively. Figure III-8 and Figure III-9 illustrate the future traffic volumes for the studied intersections with the addition of the Proposed Project for the AM and PM peak hours, respectively.

Table III-28 summarizes the level of service for the Future (2019) without Project and Future (2019) with Project conditions at the analyzed intersections for the AM and PM peak hours. Under the Future without Project conditions, six of the ten study intersections are expected to continue to operate at LOS D or better during the weekday AM and PM peak hours with the addition of growth in ambient traffic and related projects. The remaining four study intersections, Reseda Boulevard and Parthenia Street, Reseda Boulevard and Roscoe Boulevard, Roscoe Boulevard and Tampa Avenue, and Roscoe Boulevard and Lindley Avenue are all expected to operate at LOS E during the AM and PM peak hours, with the exception of the intersection of Roscoe Boulevard and Lindley Avenue that is expected to operate at LOS F during the AM peak hour.

Under Future with Project conditions, the analysis indicates that for the AM and PM peak hours, the addition of Project traffic would not cause the level of service to change at any of the study intersections. Any increases in volume/capacity (V/C) ratios would be less than the threshold for a significant impact to occur. Therefore, the Proposed Project would not cause any significant traffic impacts in either the AM or PM peak hour. As the Proposed Project is not anticipated to result in a significant transportation impact at any of the study intersections, no traffic mitigation measures are recommended. Future traffic impacts would be less than significant.

Further, the Proposed Project would be required to implement measures detailed in LADOT's communication with the Planning Department (DOT Case No. 43751) and dated July 13, 2016. The Proposed Project's construction and operational phases would coordinate and follow the guidance of LADOT, which would further ensure that Project impacts would be less than significant.

**Table III-28  
Future (2019) Condition Level of Service Summary**

Intersection	Peak Hour	Future Without Project		Future With Project		Project Impact
		V/C	LOS	V/C	LOS	
1. Reseda Boulevard and Nordhoff Street	AM	0.638	B	0.641	B	0.003
	PM	0.786	C	0.787	C	0.001
2. Reseda Boulevard and Parthenia Street	AM	0.990	E	0.994	E	0.004
	PM	0.931	E	0.937	E	0.006
3. Reseda Boulevard and Chase Street	AM	0.536	A	0.537	A	0.001
	PM	0.572	A	0.577	A	0.005
4. Reseda Boulevard and Roscoe Boulevard	AM	0.925	E	0.929	E	0.004
	PM	0.953	E	0.957	E	0.004
5. Reseda Boulevard and Saticoy Street	AM	0.779	C	0.781	C	0.002
	PM	0.781	C	0.783	C	0.002
6. Roscoe Boulevard and Tampa Avenue	AM	0.991	E	0.992	E	0.001
	PM	0.982	E	0.984	E	0.002
7. Roscoe Boulevard and Wilbur Avenue	AM	0.855	D	0.855	D	0.000
	PM	0.787	C	0.789	C	0.002
8. Roscoe Boulevard and Etiwanda Avenue	AM	0.723	C	0.725	C	0.002
	PM	0.741	C	0.741	C	0.000
9. Roscoe Boulevard and Lindley Avenue	AM	1.024	F	1.027	F	0.003
	PM	0.939	E	0.940	E	0.001
10. Roscoe Boulevard and White Oak Avenue	AM	0.896	D	0.899	D	0.003
	PM	0.867	D	0.867	D	0.000

*LOS = level of service; V/C = volume-to-capacity ratio  
Source: Overland Traffic Consultants, Inc., Traffic Impact Analysis for a Mixed-Use Development, Located at 8350-8430 Reseda Boulevard Avenue, April 2016.*

### **Impact on Regional Transportation System**

The Congestion Management Program (CMP) was adopted to monitor regional traffic growth and related transportation improvements. The CMP designated a transportation network including all state highways and some arterials within the County to be monitored by local jurisdictions. If LOS standards deteriorate on the CMP network, then local jurisdictions must prepare a deficiency plan to be in conformance with the program. Local jurisdictions found to be in nonconformance with the CMP risk the loss of state gas tax funding.

For purposes of the CMP LOS analysis, a substantial change in freeway segments are defined as an increase or decrease of 0.10 in the demand to capacity ratio and a change in LOS. A CMP traffic impact analysis is required if a project will add 150 or more trips to a freeway segment in either direction during either the AM or PM weekday peak hour. As indicated on the Project peak hour trip assignment (Figures 6 and 7 of the Traffic Impact Study), the Project does not exceed the CMP thresholds for significance.

An analysis is also required at all CMP monitoring intersections where a project would add 50 or more peak hour trips. There are no designated CMP arterial monitoring stations in this study area. As indicated in the Project traffic impact analysis, the Project does not create a significant traffic impact at any freeway ramp intersections or freeway segments. Therefore, no additional freeway or CMP analysis is necessary.

---

***Transit Analysis***

Per Congestion Management Program (CMP) 2010 guidelines, person trips can be estimated by multiplying the total trips generated by 1.4 resulting in a net of 52 am person trips and 65 pm person trips. The commercial trips assigned to transit may then be calculated by multiplying the person trips by 15% with the residential person trips by 10%. The CMP transit trip generation calculation shows a potential net increase of 13 am transit trips and 17 pm transit trips generated by the Project. Based on the current transit service on Reseda and Roscoe Boulevards, a total of approximately 10 north/south Reseda buses per peak hour and 12 east/west Roscoe buses per peak hour serve the Project. Using a standard capacity of 40 passengers per bus, the transit service has the potential capacity of 880 passengers per hour (22 buses per hour at 40 passengers per bus). This level of potential transit increase by the Project (13 am and 17 pm transit trips) is not expected to adversely affect the current ridership of the transit services in the area.

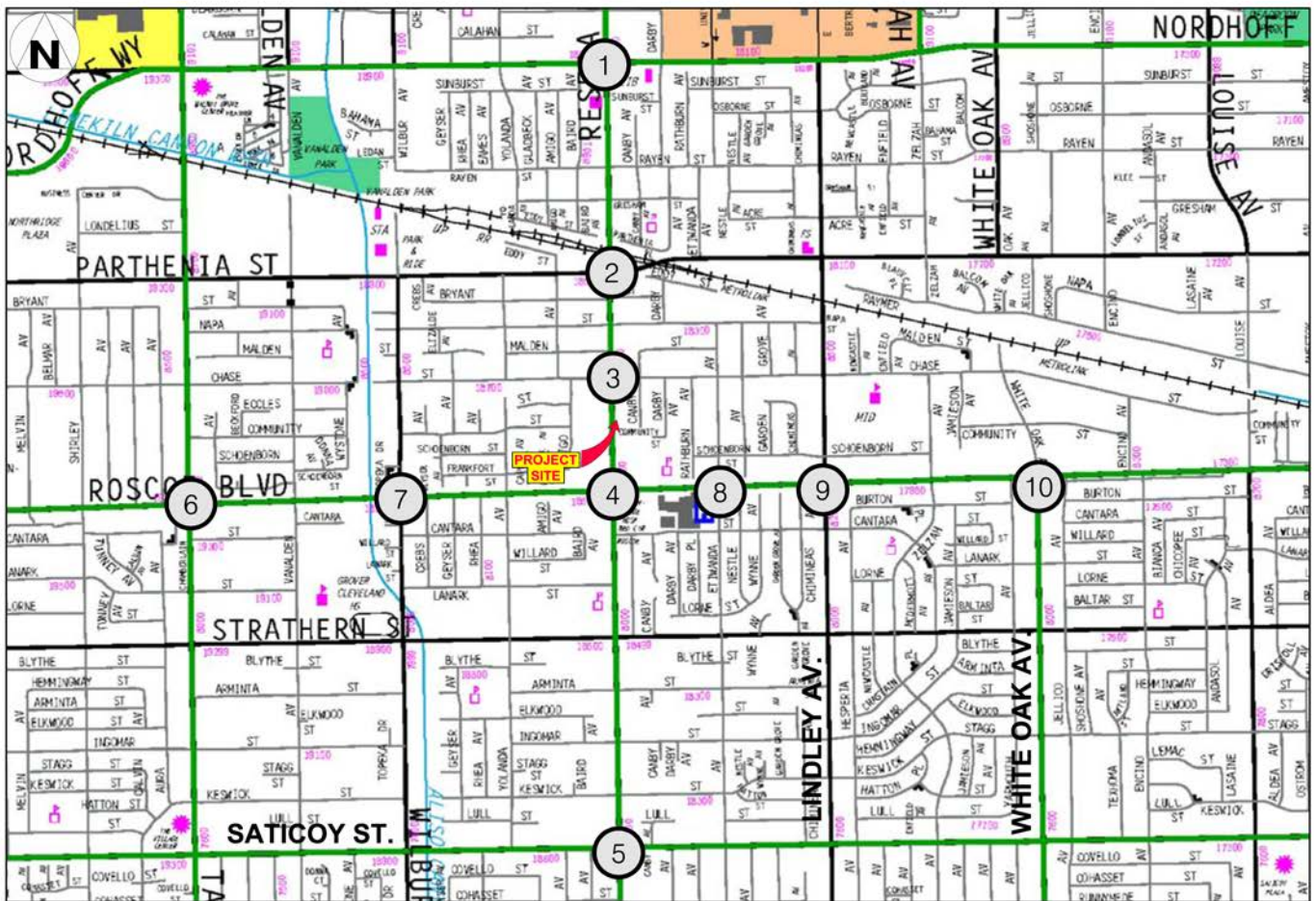
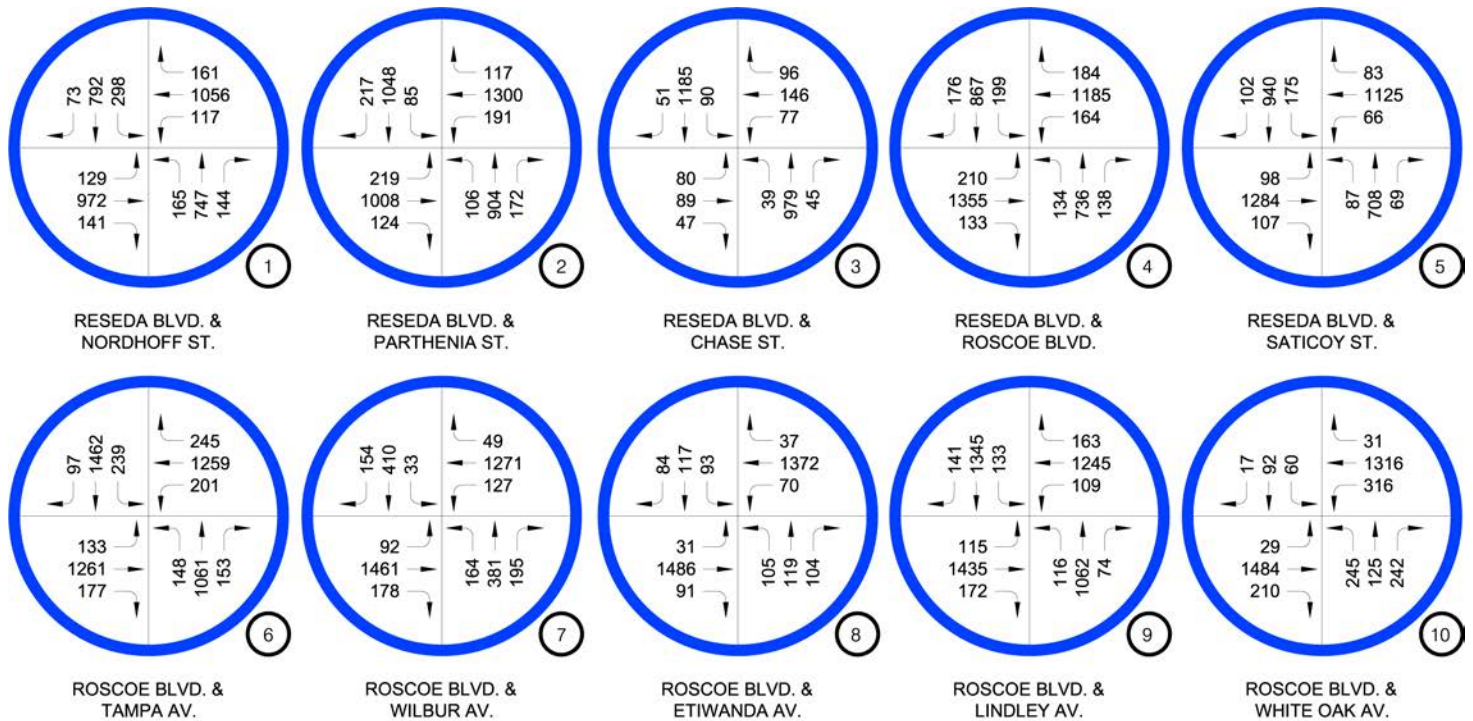
***LADOT and Caltrans Screening Agreement***

The City of Los Angeles Department of Transportation (LADOT) and Caltrans District 7 have developed an agreement on the Freeway Impact Procedures, the "Caltrans Agreement", October 2013 and renewed in December 2015. This multi-agency agreement describes a screening process to determine the level of analysis necessary for land development projects.

The screening criterion are based on the traffic volume and capacity of nearby freeway and ramp facilities, and the estimated volume of added project traffic. Four screening criterion have been developed by LADOT and Caltrans. If any of the four criteria described below are satisfied then additional traffic impact analysis is required.

1. The project's peak hour trips would result in a 1% or more increase to the freeway mainline capacity of a freeway segment operating at a level of service (LOS) E or F (based on a capacity of 2,000 vehicles per hour per lane (vphpl)).
2. The project's peak hour trips would result in a 2% or more increase to the freeway mainline capacity of a freeway segment operating at a level of service (LOS) D (based on a capacity of 2,000 vehicles per hour per lane (vphpl)).
3. The project's peak hour trips would result in a 1% or more increase to the freeway off ramp operating at a level of service (LOS) E or F (based on an off ramp capacity of 850 vehicles per hour per lane (vphpl) as measured at the intersection).
4. The project's peak hour trips would result in a 2% or more increase to the freeway off ramp operating at a level of service (LOS) E or F (based on an off ramp capacity of 850 vehicles per hour per lane (vphpl) as measured at the intersection).

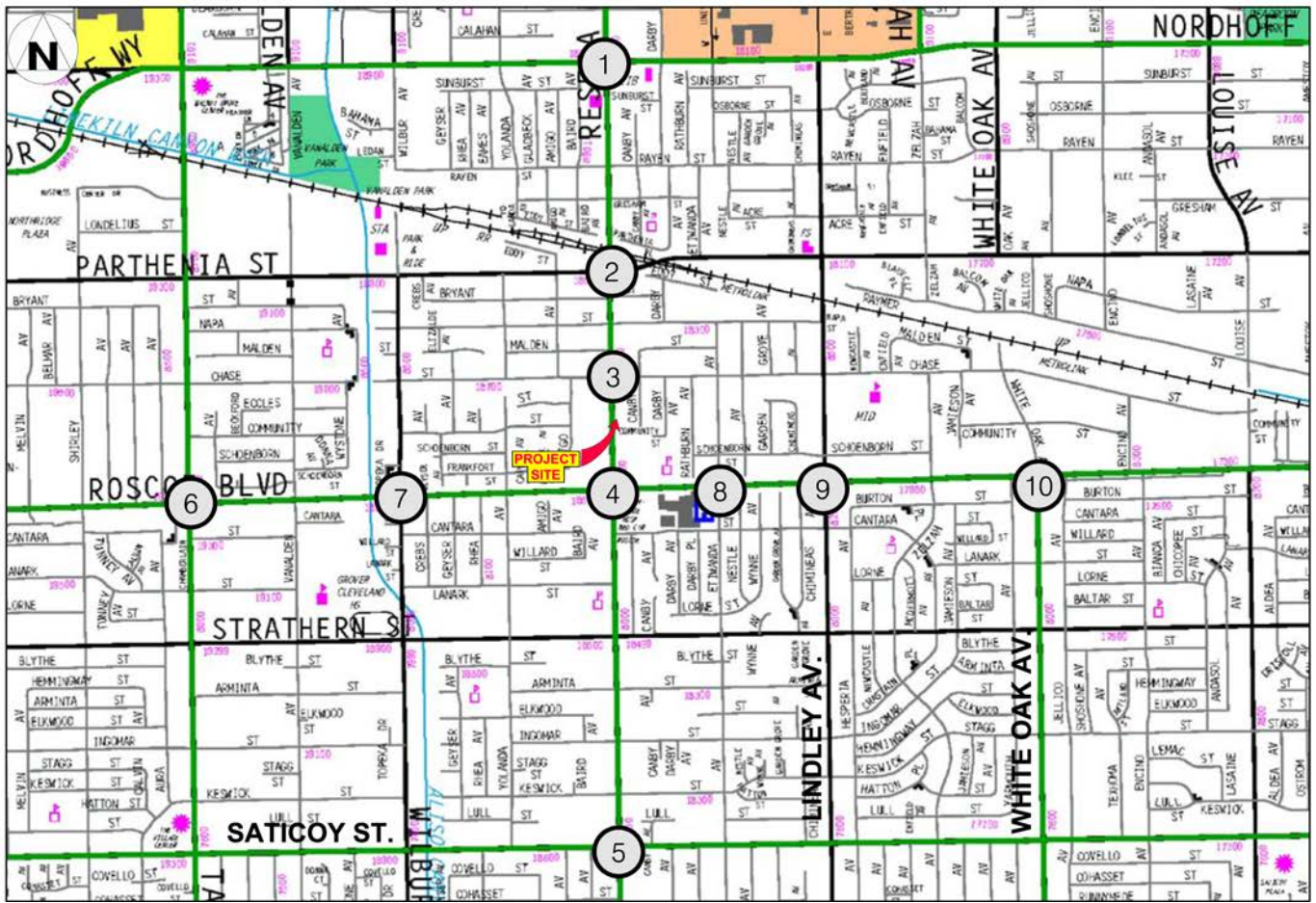
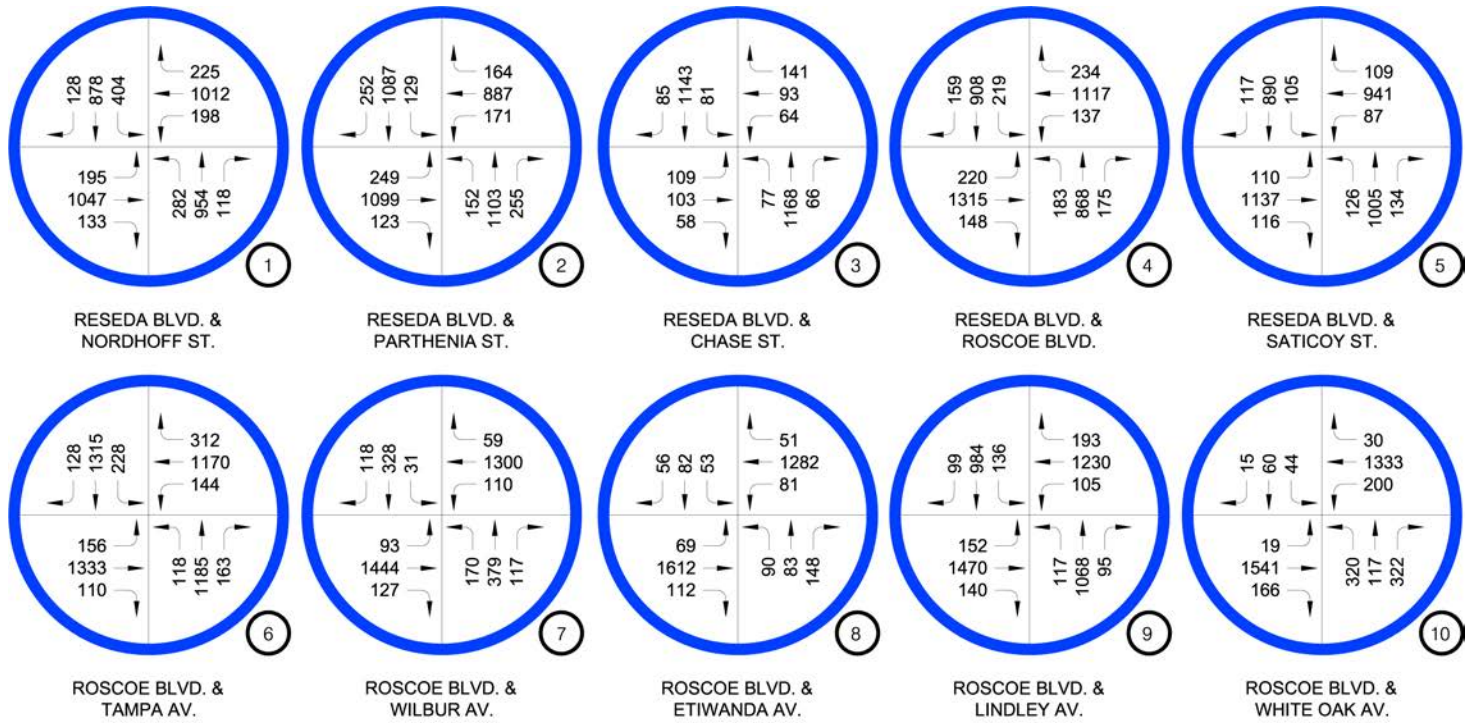
The screening criteria was evaluated in the MOU scoping document approved by LADOT, included as Appendix G within the Traffic Impact Study. As shown, none of the screening criteria exceeded the agreed upon thresholds requiring further review. No additional regional analysis is required.



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



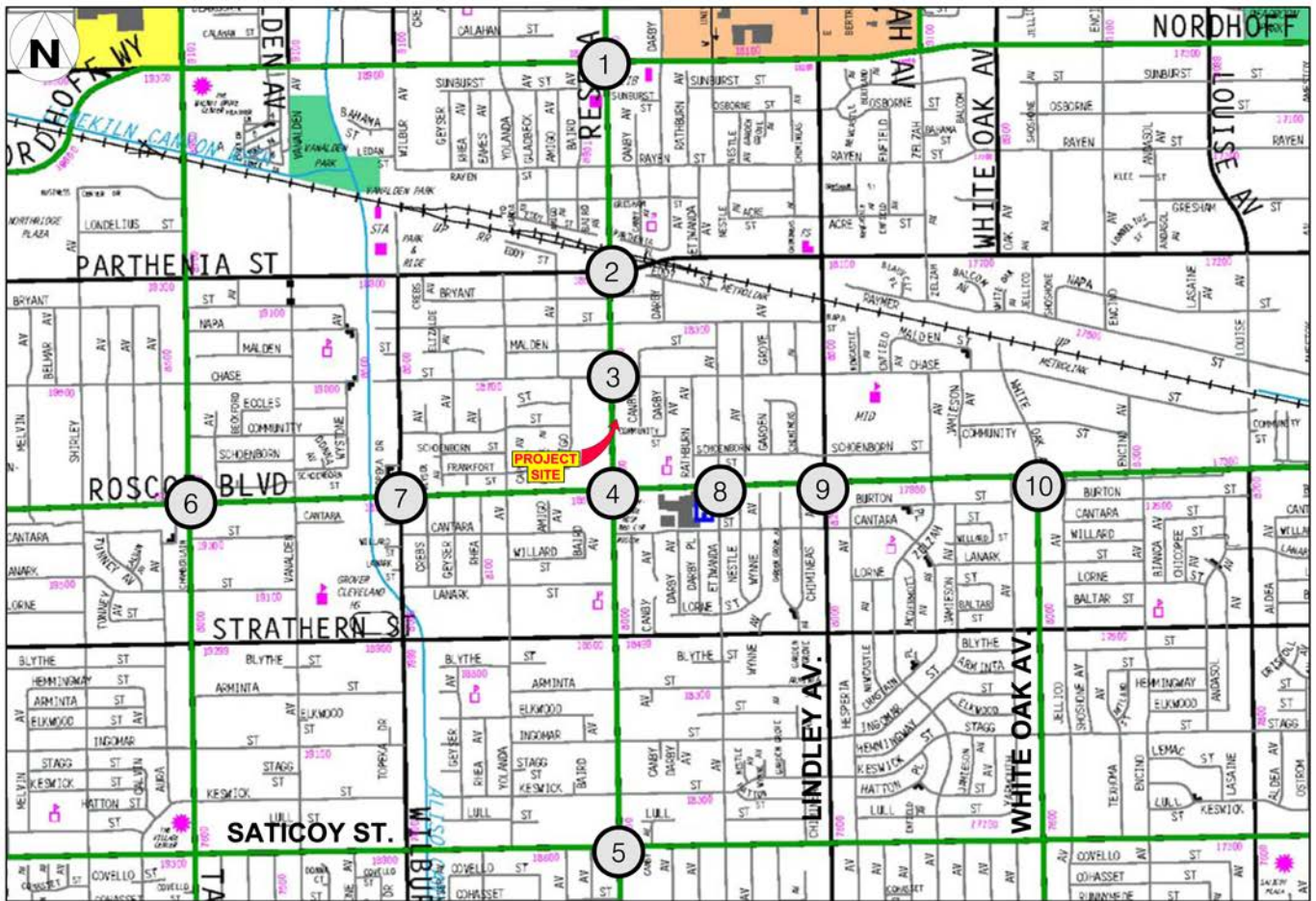
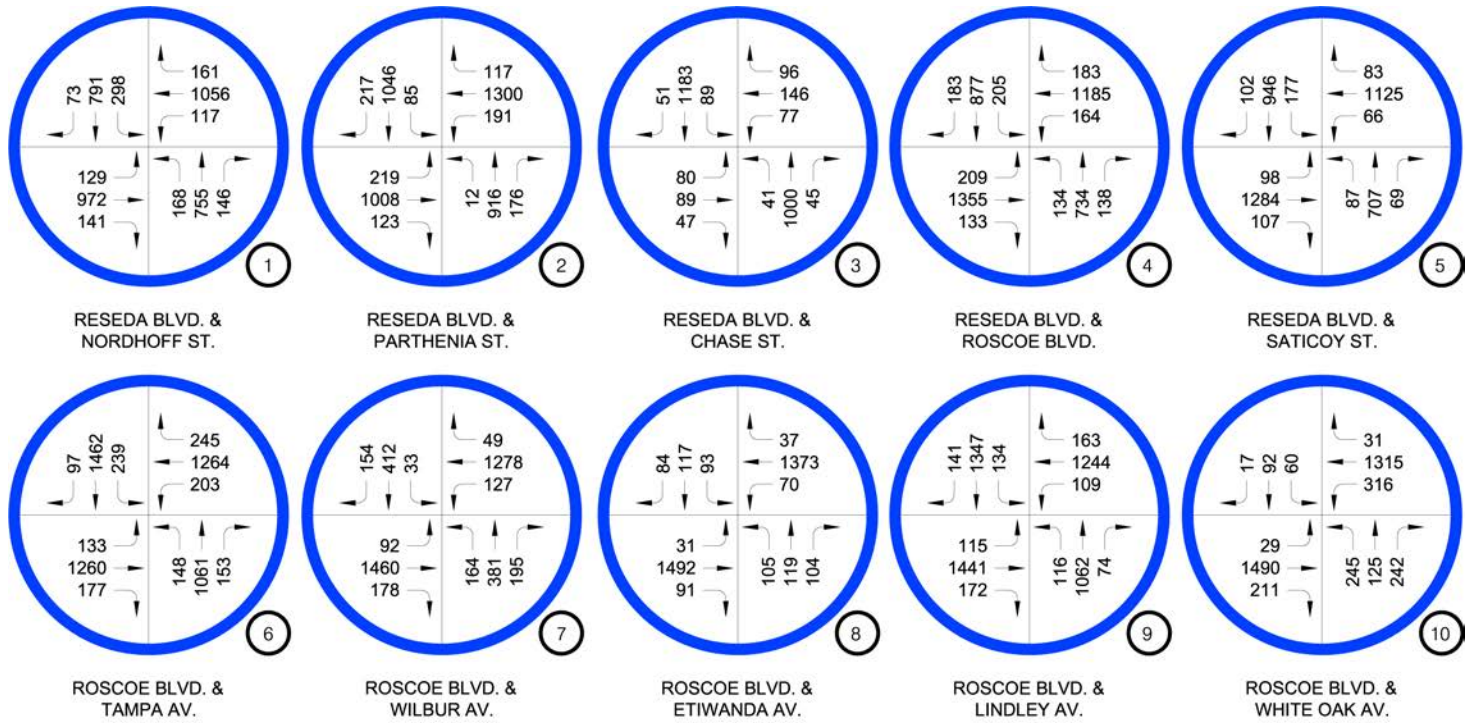
Figure III-6  
Future (2019) without Project Traffic Volumes - AM Peak Hour



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



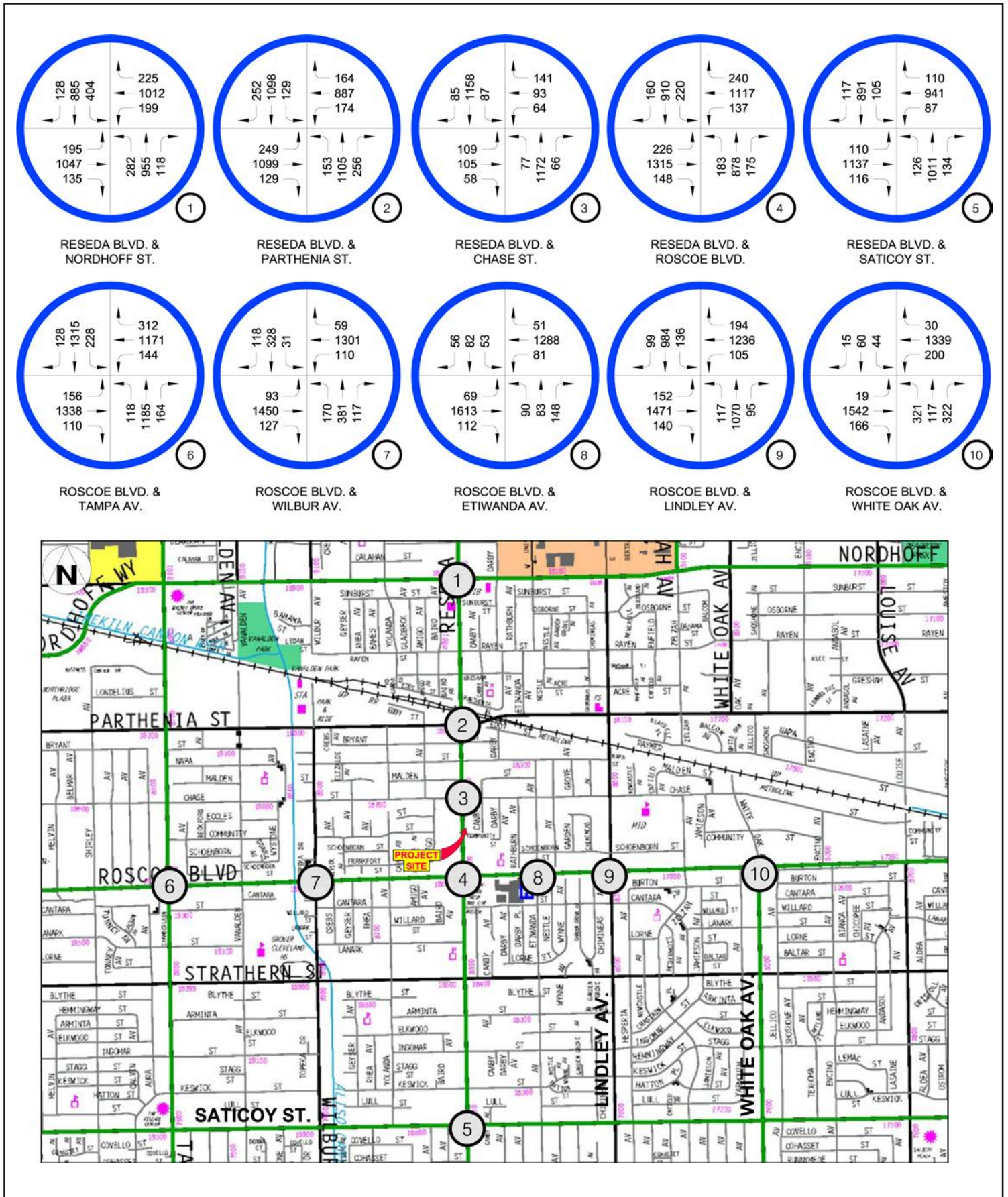
Figure III-7  
 Future (2019) without Project Traffic Volumes - PM Peak Hour



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



Figure III-8  
 Future (2019) with Project Traffic Volumes - AM Peak Hour



Source: Overland Traffic Consultants, Inc., Traffic Impact Study Mixed Use Development, April 2016.



Figure III-9  
Future (2019) with Project Traffic Volumes - PM Peak Hour

### ***Construction Traffic***

As part of the Project's construction mitigation, a Construction Management program would be implemented during the construction phase to minimize potential conflicts associated with construction activity. The Project applicant would be required to submit formal construction staging and traffic control plans for review and approval by the City prior to the issuance of any construction permits.

The Project's potential construction impacts may involve temporary construction activities within a roadway that would cause lane or street closures and a temporary loss of on-street parking. The Work Site Traffic Control Plan will be reviewed and approved by the City to provide safe operations during these temporary intermittent lane closures, as well as to provide safety measures around the construction site to reduce the risk to pedestrian traffic.

Construction workers are typically expected to arrive at the Project Site before 7:00 AM and depart before or after the weekday peak hours of 4:00 to 6:00 PM. During construction of the Proposed Project, parking for construction workers would be provided on-site or off-site. No construction worker parking would be allowed on the surrounding residential streets. To ensure construction workers do not park on streets in the surrounding neighborhood, the contractor will reserve parking spaces for construction workers at public parking lots in the vicinity of the Project Site. The final parking plan for construction workers would be determined at the time of construction and outlined in the Construction Management Plan.

The Proposed Project would require the use of haul trucks during demolition, site clearing and excavation, and the use of a variety of other construction vehicles throughout the construction of the Proposed Project. The Proposed Project would require approximately 2,000 cubic yards of asphalt debris and 22,000 cy of excavated soil to be exported off site. Truck staging would on-site through most of the construction period. During excavation, staging could occur off-site at a designated truck staging area near the Project Site and radioed in from the staging area when ready. The proposed haul route for the Project would utilize the Ronald Reagan (CA-118) Freeway. Traveling to and from the 118 Freeway, the haul route would utilize the Reseda Boulevard on- and off-ramp. The haul trips would occur outside of the peak hours and during the permissible hauling hours identified in the haul route to be approved by the Department of Building and Safety. Cement trucks and material delivery trucks would be staged on-site with an occasional curb lane closure for equipment staging and delivery of materials such as concrete. No detours around the construction site are expected, however, flagmen may be used to control traffic movement during the ingress and egress of trucks and heavy equipment.

The addition of construction vehicles onto the street system would contribute to increased traffic in the Project vicinity. However, temporary traffic impacts from construction would likely occur during the non-peak hours as a result of an increase in truck traffic associated with excavation; delivery of construction materials; an increase in automobile traffic associated with construction workers; and potential reductions in street capacity from temporary lane closures associated with construction activities for roadway and sidewalk improvement, utility changes, drainage facilities, and sewer improvements. The Proposed Project's construction trip traffic would be a fraction of the operational traffic that would not cause any significant impacts at the studied intersections. Therefore, it is not anticipated that they would contribute to

a significant increase in the overall congestion in the Project vicinity. In addition, any truck trips would be limited to the length of time required for the Project's construction. Due to the temporary nature of the traffic, construction impacts would be less than significant with the incorporation of Mitigation Measures TRAFFIC-1 and TRAFFIC-2, below.

Further, the Proposed Project would be required to implement measures detailed in LADOT's communication with the Planning Department (DOT Case No. 43751) and dated July 13, 2016. The Proposed Project would be constructed with the coordination and guidance of LADOT, which would further ensure that Project impacts would be less than significant.

### **Mitigation Measures**

**TRAFFIC-1** A Construction Work Site Traffic Control Plan shall be submitted to DOT for review and approval in accordance with the LAMC prior to the start of any construction work. The Construction Work Site Traffic Control Plan shall show the location of any roadway or sidewalk closures, traffic detours, haul routes, hours of operation, protective devices, warning signs and access to abutting properties. All construction related traffic shall be restricted to off-peak hours.

**TRAFFIC-2** All delivery truck loading and unloading shall take place on site or within the boundaries of an approved traffic control plan.

**b) Would the project conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?**

**Less Than Significant Impact.** A significant impact would occur if a project conflicts with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways. The local CMP requires that all CMP monitoring intersections be analyzed where a project would likely add 50 or more trips during the peak hours. The study area analyzed in the Traffic Impact Study includes the ten intersections listed above. None of these intersections are Los Angeles County Congestion Management Plan (CMP) monitoring locations. There are no designated CMP arterial monitoring stations in the study area. A review of the Project trip distribution and net Project traffic additions to the study vicinity shows that the Proposed Project would not add 50 or more trips to any CMP intersection. Therefore, no further CMP intersection analysis is warranted.

Furthermore, any CMP freeway-monitoring segment where a project is expected to add 150 or more trips in any direction during the peak hours is to be analyzed. Based on the Project trip generation described above, the Proposed Project would not exceed the CMP thresholds of significance. The Proposed Project would not create a significant traffic impact to any freeway ramp intersections or freeway segments. Therefore, no additional freeway or CMP analysis is necessary, and Project impacts would be less than significant.

**c) Would the project result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?**

**No Impact.** This question would apply to the Proposed Project only if it involved an aviation-related use or would influence changes to existing flight paths. The Proposed Project does not include any aviation-related uses and would have no airport impact. It would also not require any modification of flight paths for the existing airports in Los Angeles. Therefore, no impact would occur.

**d) Would the project substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?**

**No Impact.** A significant impact may occur if the Proposed Project includes new roadway design or introduces a new land use or features into an area with specific transportation requirements and characteristics that have not been previously experienced in that area, or if Project Site access or other features were designed in such a way as to create hazard conditions. The Proposed Project would not include unusual or hazardous design features. Current vehicular access to the land uses on the Project Site are provided by ingress/egress driveways along Reseda Boulevard. The Proposed Project would utilize a primary driveway on Reseda Boulevard with full ingress/egress access and provide a secondary driveway along Canby Avenue. Additionally, the Proposed Project would not include any sharp curves, dangerous intersections, or incompatible uses. No off-site traffic improvements are proposed in the area surrounding the Project Site. The internal circulation between the mixed-use buildings and the small lot residential subdivision would comply with any applicable conditions and recommendations from the Department of Transportation. As such, the Proposed Project would not include new vehicular access driveways that could potentially conflict with pedestrian circulation and traffic. Therefore, the Proposed Project would not substantially increase hazards due to design features or incompatible uses, and no impact would occur.

**e) Would the project result in inadequate emergency access?**

**Less Than Significant Impact.** A significant impact may occur if the Proposed Project design would not provide emergency access meeting the requirements of the LAFD, or in any other way threatened the ability of emergency vehicles to access and serve the Project Site or adjacent uses.

As previously discussed in Section VIII (g), the Proposed Project is not located on or near an adopted emergency response or evacuation plan. Development of the Project Site may require temporary and/or partial street closures due to construction activities. However, any such closures would be temporary in nature and would be coordinated with the Departments of Transportation, Building and Safety, and Public Works. Nonetheless, while such closures may cause temporary inconvenience, they would not be expected to substantially interfere with emergency response or evacuation plans. Therefore, the impacts would be less than significant.

As described in Section XIV (a), the Proposed Project would satisfy the emergency response requirements of the LAFD. There are no hazardous design features included in the access design or site plan for the Proposed Project that could impede emergency access. Furthermore, the Proposed Project would be subject to the site plan review requirements of the LAFD to ensure that all access roads, driveways and parking areas would remain accessible to emergency service vehicles. Therefore, the Proposed Project

would not be expected to result in inadequate emergency access, and the impact would be less than significant.

**f) Would the project conflict with adopted policies, plans, or programs regarding public transit, bicycles, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?**

**Potentially Significant Unless Mitigation Incorporated.** A significant impact may occur if the Proposed Project would conflict with adopted policies or involve modification of existing alternative transportation facilities located on- or off-site. The Mobility Element in the City's General Plan identifies Reseda Boulevard as a "Tier 1 Protected Bicycle Lane," and bicycle lanes are located on both sides of the street. The construction of the Proposed Project may result in temporary lane closures and pedestrian and bicycle detours. As discussed above, the Project would implement a Construction Work Site Traffic Control Plan noting the potential roadway and sidewalk closures and detours. Further, the Applicant would install appropriate traffic signs, protection fencing and/or canopies within the public right-of-way around the site to ensure pedestrian and vehicle safety. Pursuant to LAMC Section 62.45, the Applicant would obtain permits from the Bureau of Street Services prior to the closure of any adjacent sidewalk and/or construction of protection fences or canopies within the public right-of-way. With the implementation of Mitigation Measure TRAFFIC-1, impacts relating to conflicts with bicycle and pedestrian facilities would be less than significant.

The operation Proposed Project would not require the disruption of public transportation services or the alteration of public transportation routes. Since the operation Proposed Project would not modify or conflict with any alternative transportation policies, plans or programs, it would have no impact on such programs. Furthermore, the Proposed Project would be expected to comply with the 2010 Bicycle Plan, which is a component of the Mobility Element of the General Plan. Thus, the Proposed Project complies with all applicable bicycle and pedestrian-friendly policies, plans and programs, and less than significant would occur during the Project's operation.

### **Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project in conjunction with the nine related projects would result in an increase in average daily vehicle trips and peak hour vehicle trips in the Northridge Community Plan area. As noted in Table III-28, above, all increases in V/C ratios in the AM peak hour and PM peak hour would be less than the threshold for a significant impact to occur and the Proposed Project's contribution to cumulative impacts is less than significant for all of the study intersections analyzed. Therefore, the Proposed Project's cumulative impact is considered less than significant.

---

**XVII. UTILITIES AND SERVICE SYSTEMS****a) Would the project exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?**

**Less Than Significant Impact.** A significant impact would occur if a project exceeds wastewater treatment requirements of the applicable Regional Water Quality Control Board. Section 13260 of the California Water Code states that persons discharging or proposing to discharge waste that could affect the quality of the waters of the State, other than into a community sewer system, shall file a Report of Waste Discharge (ROWD) containing information which may be required by the appropriate Regional Water Quality Control Board (RWQCB). The RWQCB then authorizes an NPDES permit that ensures compliance with wastewater treatment and discharge requirements. The Los Angeles RWQCB (LARWQCB) enforces wastewater treatment and discharge requirements for properties in the Project area.

Wastewater from the Project Site is conveyed via municipal sewage infrastructure maintained by the Los Angeles Bureau of Sanitation to the Tillman Water Reclamation Plant (TWRP), which serves the west San Fernando Valley area. The TWRP is a public facility and is subject to the State's wastewater treatment requirements. Wastewater from the Project Site is and would continue to be treated according to the wastewater treatment requirements enforced by the LARWQCB. Therefore, impacts associated with wastewater treatment requirements would be less than significant.

**b) Would the project require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?**

**Less Than Significant Impact.** A significant impact may occur if a project would increase water consumption or wastewater generation to such a degree that the capacity of facilities currently serving the Project Site would be exceeded. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether a project results in a significant impact on water shall be made considering the following factors: (a) the total estimated water demand for the project; (b) whether sufficient capacity exists in the water infrastructure that would serve the project, taking into account the anticipated conditions at project buildout; (c) the amount by which the project would cause the projected growth in population, housing or employment for the Community Plan area to be exceeded in the year of the project completion; and (d) the degree to which scheduled water infrastructure improvements or project design features would reduce or offset service impacts.

*Water Treatment Facilities and Existing Infrastructure*

The Los Angeles Aqueduct (LAA), local groundwater, purchased water from MWD, and recycled water are the primary sources of water supplies for the City. From the early construction of LAA in the early 1900's, LAA has historically provided the vast majority of water for the City. It remains a significant source of water, providing an average of 36 percent of total water supplies from fiscal year 2005-06 to 2009-10. LAA supplies come primarily from snowmelt from the Eastern Sierra Nevada Mountains and can fluctuate due to the varying hydrologic conditions. In recent years, LAA supplies have been less than the historical average because of environmental restoration efforts in Mono Basin and Owens Valley.

Over the last ten years local groundwater has provided approximately 12 percent of the total water to the City. LADWP has adjudicated rights to pump groundwater from the San Fernando, Sylmar, Eagle Rock, Central, and West Coast Basins. LADWP has an annual entitlement of approximately 87,000 acre-feet per year (AFY) in the San Fernando Basin, 3,405 AFY in the Sylmar Basin, 15,000 AFY in the Central Basin, 1,503 AFY in the West Coast Basin, and 500 AFY in the Eagle Rock Basin, which totals approximately 107,408 AFY. LADWP does not exercise its pumping rights in Eagle Rock Basin and West Coast Basin at this time.

The Metropolitan Water District of Southern California is the largest water wholesaler for domestic and municipal uses in California and provides water to nearly 19 million people with an average of 1.7 billion gallons of water per day. MWD imports a portion of its water supplies from Northern California through the State Water Project's (SWP) California Aqueduct and from the Colorado River through MWD's own Colorado River Aqueduct. The City of Los Angeles purchases water from MWD to supplement its supplies from local groundwater, Los Angeles Aqueduct deliveries, and recycled water. Historically, LADWP has purchased MWD water to make up the difference between demand and City supplies. The City of Los Angeles relies more heavily on MWD water during drier years. As percentage of the City's total water supply, MWD water has historically varied from 4 percent in fiscal year 1983-84 to 71 percent in fiscal year 2008-09.<sup>43</sup>

The Los Angeles Department of Water and Power (LADWP) ensures the reliability and quality of water supply through an extensive distribution system that includes more than 7,200 miles of pipes, more than 100 storage tanks and reservoirs within the City, and eight storage reservoirs along the Los Angeles Aqueducts. Much of the water flows north to south, entering Los Angeles at the Los Angeles Aqueduct Filtration Plant (LAAFP) in Sylmar, which is owned and operated by LADWP. Water entering the LAAFP undergoes treatment and disinfection before being distributed throughout the LADWP's Water Service Area. The LAAFP has the capacity to treat approximately 600 million gallons per day (mgd).<sup>44</sup> The average plant flow is approximately 450 mgd during the non-summer months and 550 mgd during the summer months, and operates at between 75 and 90 percent capacity. Therefore, the LAAFP has a remaining capacity of treating approximately 50 to 150 mgd, depending on the season.<sup>45</sup>

As shown in Table III-29 below, the Proposed Project would generate a net increase in water demand of approximately 32,326 gallons per day (gpd) of water, significantly below available capacity. Because the Proposed Project is consistent with the zoning and General Plan land use designations, and the Project's population growth is within SCAG's forecast, the Project's increased water demand would not measurably reduce the LAAFP's treatment capacity; therefore, no new or expanded water treatment facilities would be

---

<sup>43</sup> *City of Los Angeles, Department of Water and Power, 2010 Urban Water Management Plan, 2010.*

<sup>44</sup> *Los Angeles Department of Water and Power, website: <http://wsoweb.ladwp.com/Aqueduct/historyoflaa/waterquality.htm>, accessed October 2015.*

<sup>45</sup> *Los Angeles Department of Water and Power, website: <http://www.ladwp.com/>, accessed October 2015.*

required. With respect to water treatment facilities, the Proposed Project would have a less-than-significant impact.

In the event that water main and/or other infrastructure upgrades are required for the proposed development, such infrastructure improvements would be conducted within the right-of-way easements serving the Project area, and would not create a significant impact to the physical environment. This is largely due to the fact that (a) any disruption of service would be short-term, (b) the replacement of the water mains would be within public rights-of-way, and (c) any foreseeable infrastructure improvements would be limited to the immediate Project vicinity. Therefore, potential impacts resulting from water infrastructure improvements would be less than significant.

**Table III-29  
Proposed Project Estimated Water Demand**

Type of Use	Size	Water Demand Rate (gpd/unit) <sup>a</sup>	Total Water Demand (gpd)
<b>Existing Uses</b>			
Commercial / Auto Body Shop	21,986 sf	0.096 gpd/sf	2,111
Commercial Office	5,425 sf	0.18 gpd/sf	977
<b>Total Existing Water Demand:</b>			<b>3,088</b>
<b>Proposed Project</b>			
<b>Multi-Family Residential Units (79 total du)</b>			
Studio	26 du	96 gpd/du	2,496
One Bedroom	34 du	144 gpd/du	4,896
Two Bedroom	19 du	192 gpd/du	3,648
<b>Small Lot Residential Units (61 total du)</b>			
Two Bedroom	20 du	216 gpd/du	4,320
Three Bedroom	41 du	276 gpd/du	11,316
<b>Commercial</b>			
Ground-floor Retail	5,150 sf	0.096 gpd/sf	494
Restaurant (5,150 sf) <sup>b</sup>	229 seats	36 gal/seat/day	8,244
<b>Total Project Water Demand:</b>			<b>35,414</b>
<i>Less Existing Water Demand:</i>			<i>-3,088</i>
<b>NET Additional Water Demand:</b>			<b>32,326</b>
<i>Notes: sf =square feet; du = dwelling units, gpd: gallons per day</i> <sup>a</sup> <i>L.A. CEQA Thresholds Guide (2006), Exhibit M.2-12. Consumption Rates based on 120% of the City of Los Angeles Department of Public Works, Bureau of Sanitation Sewer Generation Rates table, March 20, 2002.</i> <sup>b</sup> <i>For a conservative analysis, it was assumed that the restaurant would include full service indoor seating. Further, it was assumed that 1/3 of the restaurant square footage would be used as a kitchen/administration/bathrooms and 2/3 of the restaurant square footage would be used for seating. (As such, 5,150 * 2/3 = 3,433 square feet of seating). Further, it was assumed that each seat would require 15 sf. (As such, 3,433 / 15 = 229 seats).</i> <i>Source: Parker Environmental Consultants, 2016.</i>			

---

*Wastewater Treatment Facilities and Existing Infrastructure*

Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant wastewater impact if: (a) the project would cause a measurable increase in wastewater flows to a point where, and a time when, a sewer's capacity is already constrained or that would cause a sewer's capacity to become constrained; or (b) the project's additional wastewater flows would substantially or incrementally exceed the future scheduled capacity of any one treatment plant by generating flows greater than those anticipated in the Wastewater Facilities Plan or General Plan and its elements.

The Los Angeles Bureau of Sanitation provides sewer service to the Proposed Project area. Sewage from the Project Site is conveyed via sewer infrastructure to the Tillman Water Reclamation Plant (TWRP).<sup>46</sup> The TWRP treats an average daily flow of 67 million gallons per day (mgd), and has capacity to treat 80 mgd.<sup>47</sup> This equals a remaining capacity of 13 mgd of wastewater able to be treated at the TWRP. As shown in Table III-30, the Proposed Project would generate a net increase of approximately 26,939 gpd of wastewater, representing a fraction of one percent of the available capacity. Based on the configuration of the sewer lines serving the Project Site, the Proposed Project's sewer flows would be routed to the lines under Reseda Boulevard.<sup>48</sup> In accordance with the *L.A. CEQA Thresholds Guide*, the estimated sewer flows were based on the sewage generation factors for residential and commercial categories (City of Los Angeles, Bureau of Sanitation, 1996). The TWRP has a remaining capacity of 13 additional mgd, and as such would have adequate capacity to serve the Project Site. Therefore, impacts to sewer capacity and infrastructure would be less than significant.

**c) Would the project require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?**

**Less Than Significant Impact.** A significant impact may occur if the volume of stormwater runoff would increase to a level exceeding the capacity of the storm drain system serving a project site, resulting in the construction of new stormwater drainage facilities. As described in Question IX(c) the Proposed Project would not result in a significant increase in site runoff, or any changes in the local drainage patterns. The Proposed Project Site is currently developed as a commercial building, auto repair shop, and resale center. Runoff from the Project Site currently is and would continue to be directed towards existing storm drains in the Project vicinity. As stated previously in response to Checklist Question IX, the Project shall comply with the LID Plan, Standard Urban Stormwater Mitigation Plan (SUSMP), and/or the site-specific mitigation plan to mitigate stormwater pollution as required by Ordinance Nos. 172,176 and 173,494. The

---

<sup>46</sup> City of Los Angeles Department of Public Works, Bureau of Sanitation, Donald C. Tillman Water Reclamation Plant, website: [http://www.lasewers.org/treatment\\_plants/tillman/index.htm](http://www.lasewers.org/treatment_plants/tillman/index.htm), accessed October 2015.

<sup>47</sup> City of Los Angeles Department of Public Works, Bureau of Sanitation, Wastewater: About Wastewater, website: <http://www.lacitysan.org/wastewater/factsfigures.htm>, accessed October 2015.

<sup>48</sup> City of Los Angeles Bureau of Engineering, Navigate LA, website: <http://navigatela.lacity.org/navigatela/>, accessed: October 2015.

Low Impact Development Ordinance and SUSMP requires projects to retain or treat the first ¼-inch of rainfall in a 24-hour period. The Project would be required to implement Best Management Practices (BMP) and show compliance with the ordinances. The appropriate design and application of BMP devices and facilities shall be determined by the Watershed Protection Division of the Bureau of Sanitation, Department of Public Works. Thus, development of the Proposed Project would not create or contribute to runoff water, which may exceed the capacity of existing or planned stormwater drainage systems. Therefore, Project impacts would be considered less than significant.

**Table III-30  
Proposed Project Estimated Wastewater Generation**

Type of Use	Size	Wastewater Generation Rate (gpd/unit) <sup>a</sup>	Total Wastewater Generation (gpd)
<b>Existing Uses</b>			
Commercial / Auto Body Shop	21,986 sf	0.08 gpd/sf	1,759
Commercial Office	5,425 sf	0.15 gpd/sf	814
<b>Total Existing Wastewater Generation:</b>			<b>2,573</b>
<b>Proposed Project</b>			
<b>Multi-Family Residential Units (79 total du)</b>			
Studio	26 du	80 gpd/du	2,080
One Bedroom	34 du	120 gpd/du	4,080
Two Bedroom	19 du	160 gpd/du	3,040
<b>Small Lot Residential Units (61 total du)</b>			
Two Bedroom	20	180 gpd/du	3,600
Three Bedroom	41	230 gpd/du	9,430
<b>Commercial</b>			
Ground-floor Retail	5,150 sf	0.08 gpd/sf	412
Restaurant (5,150 sf) <sup>b</sup>	229 seats	30 gal/seat/day	6,870
<b>Total Project Wastewater Generation:</b>			<b>29,512</b>
<i>Less Existing Wastewater Generation:</i>			<i>-2,573</i>
<b>NET TOTAL Wastewater Generation:</b>			<b>26,939</b>
<i>Notes: sf =square feet; du = dwelling units, gpd: gallons per day</i> <sup>a</sup> <i>L.A. CEQA Thresholds Guide (2006), Exhibit M.2-12.</i> <sup>b</sup> <i>For a conservative analysis, it was assumed that the restaurant would include full service indoor seating. Further, it was assumed that 1/3 of the restaurant square footage would be used as a kitchen/administration/bathrooms and 2/3 of the restaurant square footage would be used for seating. (As such, 5,150 * 2/3 = 3,433 square feet of seating). Further, it was assumed that each seat would require 15 sf. (As such, 3,433 / 15 = 229 seats).</i> <i>Parker Environmental Consultants, 2016.</i>			

**d) Would the project have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?**

**Less Than Significant Impact.** A significant impact may occur if a project would increase water consumption to such a degree that new water sources would need to be identified. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether the project results in a significant impact on water

shall be made considering the following factors: (a) the total estimated water demand for the project; (b) whether sufficient capacity exists in the water infrastructure that would serve the project, taking into account the anticipated conditions at project buildout; (c) the amount by which the project would cause the projected growth in population, housing or employment for the Community Plan area to be exceeded in the year of the project completion; and (d) the degree to which scheduled water infrastructure improvements or project design features would reduce or offset service impacts.

The City's water supply comes from local groundwater sources, the Los Angeles-Owens River Aqueduct, State Water Project, and from the Metropolitan Water District (MWD) of Southern California, which is obtained from the Colorado River Aqueduct. The MWD utilizes a land-use based planning tool that allocates projected demographic data from the SCAG into water service areas for each of MWD's member agencies. The 2010 Urban Water Management Plan (UWMP), which estimates future demand based on population and growth estimated reported in SCAG's 2012-2035 RTP/SCS, projects a total water demand and supply of 710,800 AFY in 2035. With its current water supplies, planned future water conservation, and planned future water supplies, LADWP will be able to reliably provide water to its customers through the 25-year planning period covered by the 2010 UWMP. Through various conservation strategies, the LADWP will be able to reduce the City's water demand during dry years to respond to any reductions to water supplies during multiple dry years.

As shown in Table III-29, the Proposed Project's net increase in water demand would be 32,326 gallons per day. The Project is consistent with the allowable land uses and density that are planned for the Project Site and is therefore within the growth projections of SCAG's 2012-2035 RTP/SCS. Accordingly, the Project's anticipated water demand has been accounted for and would not exceed the water demand estimates of the City's 2010 UWMP. Thus, the Proposed Project would have a less-than-significant impact on water demand. In addition, pursuant to LAMC Section 122.03(a), the Proposed Project is required to utilize water saving devices including, but not limited to, urinals equipped with flush-o-meter valves, which flush with a maximum of 1.28 gallons, which would further reduce impacts associated with this issue to a level that is less than significant. Environmental impacts would further be reduced by implementation of the following regulatory compliance measures:

- **Regulatory Compliance Measure RC-WS-1 (Green Building Code):** The Project shall implement all applicable mandatory measures within the LA Green Building Code that would have the effect of reducing the Project's water use.
- **Regulatory Compliance Measure RC-WS-2 (Landscape):** The Project shall comply with Ordinance No. 170,978 (Water Management Ordinance), which imposes numerous water conservation measures in landscape, installation, and maintenance (e.g., use drip irrigation and soak hoses in lieu of sprinklers to lower the amount of water lost to evaporation and overspray, set automatic sprinkler systems to irrigate during the early morning or evening hours to minimize water loss due to evaporation, and water less in the cooler months and during the rainy season).

Compliance with the regulatory compliance measures identified above would reduce the Proposed Project's demands for potable water resources to a less than significant level and no mitigation measures are required.

---

**Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project, related projects and the cumulative growth throughout the City of Los Angeles, would further increase the demand for potable water within the City. Through the 2010 Urban Water Management Plan, the LADWP has demonstrated that it can provide adequate water supplies for the City through the year 2035. This estimate is based in part on demographic projections obtained for the LADWP service area from the Metropolitan Water District (MWD). The MWD utilizes a land-use based planning tool that allocates projected demographic data from the Southern California Association of Governments (SCAG) into water service areas for each of MWD's member agencies. MWD's demographic projections use data reported in SCAG's 2008 Regional Transportation Plan (RTP). Additionally, the Proposed Project's growth is consistent with SCAG's growth projections for the City of Los Angeles subregion. The Proposed Project is consistent with the underlying allowable uses per the LAMC and would not exceed the allowable density for the Project Site. As such, the additional water demands generated by the Project are accounted for in the 2010 Urban Water Management Plan.

Development of the Proposed Project in conjunction with the nine related projects would further increase regional demands on LAAFP's capacity. The impact of the continued growth of the region would likely have the effect of diminishing the daily excess capacity of the LAAFP's service to the City of Los Angeles. The Proposed Project and related projects cumulative water demand can be seen in Table III-31, below. As shown in Table III-31, the net water demand of the nine related projects and the Proposed Project would total approximately 334,294 gpd or 0.33 mgd. Of the 50 to 150 mgd available water treatment capacity in LAAFP, the cumulative demand of 0.33 mgd would not significantly reduce its capacity. As such, cumulative impacts with respect to water demand would be less than significant.

**Table III-31  
Estimated Cumulative Water Demand**

Type of Use	Size	Water Demand Rate (gpd/unit) <sup>a</sup>	Total Water Demand (gpd)
<b>Related Projects</b>			
Dwelling Units <sup>b</sup>	893 du	192 gpd/du	171,456
Fast-food Restaurant	4,587 sf	24 gpd/sf	110,088
Industrial	80,133 sf	0.096 gpd/sf	7,693
Retail	51,750 sf	0.096 gpd/sf	4,968
School	265 stu	9.6 gpd/stu	2,544
Senior Living <sup>c</sup>	56 du	90 gpd/du	5,040
Storage	7,470 sf	0.024 gpd/sf	179
<b>Total Related Projects Water Demand:</b>			<b>301,968</b>
Total Project Water Demand:			32,326
<b>TOTAL CUMULATIVE:</b>			<b>334,294</b>
<p><i>Notes: sf = square feet; du = dwelling units, gpd = gallons per day, stu = student</i>  <i>Uses not listed are estimated by the closest type of use available in the table.</i>  <sup>a</sup> <i>L.A. CEQA Thresholds Guide (2006), Exhibit M.2-12. Consumption Rates based on 120% of the City of Los Angeles Department of Public Works, Bureau of Sanitation Sewer Generation Rates table, March 20, 2002.</i>  <sup>b</sup> <i>Dwelling units include condominiums and multi-family residential units. Consumption rate was based on 2 bedrooms per unit as a conservative estimate.</i>  <sup>c</sup> <i>Senior living quarters assume to be rest home.</i>  -Related Project No. 1 was not included in water demand due to lack of project information.  -Conversions and assumptions of square feet per occupant based on California Building Code (2013), Ch.10, Table 1004.1.2.  Source: Parker Environmental Consultants, 2016.</p>			

- e) **Would the project result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?**

**Less Than Significant Impact.** Based upon the criteria established in the *L.A. CEQA Thresholds Guide*, a project would normally have a significant wastewater impact if: (a) the project would cause a measurable increase in wastewater flows to a point where, and a time when, a sewer's capacity is already constrained or that would cause a sewer's capacity to become constrained; or (b) the project's additional wastewater flows would substantially or incrementally exceed the future scheduled capacity of any one treatment plant by generating flows greater than those anticipated in the Wastewater Facilities Plan or General Plan and its elements. As stated in Checklist Question XVII(b), above, the sewage flow will ultimately be conveyed to the Tillman Water Reclamation Plant, which has sufficient capacity for the Proposed Project.<sup>49</sup> Therefore, impacts would be less than significant.

<sup>49</sup> *City of Los Angeles Department of Public Works, Bureau of Sanitation, Wastewater: About Wastewater, website: <http://www.lacitysan.org/wastewater/factsfigures.htm>, accessed October 2015.*

**Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project in conjunction with the nine related projects would further increase regional demands on the TWRP’s capacity. The impact of the continued growth of the region would likely have the effect of diminishing the daily excess capacity of TWRP’s service to the City of Los Angeles. The related projects cumulative wastewater demand can be seen in Table III-32, Projected Cumulative Wastewater Generation. As shown in Table III-32, the net wastewater demand of the nine related projects and the Proposed Project totals approximately 278,579 gpd or 0.28 mgd. Of the 13 mgd available in TWRP, the cumulative demand of 0.28 mgd accounts for approximately 2% of the available capacity and would not significantly reduce its capacity. As such, cumulative impacts with respect to wastewater demand would be less than significant.

**Table III-32  
Projected Cumulative Wastewater Generation**

Type of Use	Size	Wastewater Generation Rate (gpd/unit) <sup>a</sup>	Total Wastewater Generation (gpd)
<b>Related Projects</b>			
Dwelling Units <sup>b</sup>	893 du	160 gpd/du	142,880
Fast-food Restaurant	4,587 sf	20 gpd/sf	91,740
Industrial	80,133 sf	0.08 gpd/sf	6,411
Retail	51,750 sf	0.08 gpd/sf	4,140
School	265 stu	8 gpd/stu	2,120
Senior Living <sup>c</sup>	56 du	75 gpd/du	4,200
Storage	7,470 sf	0.02 gpd/sf	149
<b>Total Related Projects Wastewater Generation:</b>			<b>251,640</b>
Total Project Wastewater Generation:			26,939
<b>TOTAL CUMULATIVE:</b>			<b>278,579</b>
<p><i>Notes: sf =square feet; du = dwelling units, gpd = gallons per day, emp = employee, stu = student</i>  <i>Uses not listed are estimated by the closest type of use available in the table.</i>  <sup>a</sup> <i>L.A. CEQA Thresholds Guide (2006), Exhibit M.2-12.</i>  <sup>b</sup> <i>Dwelling units include condominiums and multi-family residential units. Consumption rate was based on 2 bedrooms per unit as a conservative estimate.</i>  <sup>c</sup> <i>Senior living quarters assume to be rest home.</i>                      -Related Project No. 1 was not included in water demand due to lack of project information.                      -Conversions and assumptions of square feet per occupant based on California Building Code (2013), Ch.10, Table 1004.1.2. Parker Environmental Consultants, 2016.</p>			

**f) Would the project be served by a landfill with sufficient permitted capacity to accommodate the project’s solid waste disposal needs?**

**Less Than Significant Impact.** A significant impact may occur if a project were to increase solid waste generation to a degree such that the existing and projected landfill capacity would be insufficient to accommodate the additional solid waste. Based on the *L.A. CEQA Thresholds Guide*, the determination of whether a project results in a significant impact on solid waste shall be made considering the following factors: (a) amount of projected waste generation, diversion, and disposal during demolition, construction,

and operation of the project, considering proposed design and operational features that could reduce typical waste generation rates; (b) need for additional solid waste collection route, or recycling or disposal facility to adequately handle project-generated waste; and (c) whether the project conflicts with solid waste policies and objectives in the Source Reduction and Recycling Element (SRRE) or its updates, the Solid Waste Management Policy Plan (SWMPP), Framework Element of the Curbside Recycling Program, including consideration of the land use-specific waste diversion goals contained in Volume 4 of the SRRE.

Solid waste generated within the City is disposed of at privately owned landfill facilities throughout Los Angeles County. While the Bureau of Sanitation provides waste collection services to single-family and some small multi-family developments, private haulers provide waste collection services for most multi-family residential and commercial developments within the City. Solid waste transported by both public and private haulers is either recycled, reused, transformed at a waste-to-energy facility, or disposed of at a landfill. Under the City's RENEW LA Plan, the City committed to reaching Zero Waste by diverting 70% of the solid waste generated in the City by 2013, diverting 90% by 2025, and becoming a zero waste city by 2030. State law currently requires at least 50% solid waste diversion and establishes a state-wide goal of 75% diversion by 2020. Moreover, state law requires mandatory commercial recycling in all businesses and multi-family complexes and imposes additional reporting requirements on local agencies, including the City of Los Angeles. In order to meet these requirements and goals, the City has established an exclusive, competitive franchise system for the collection, transportation and processing of commercial and multi-family solid waste that will aid the City in meeting its diversion goals by, among other things: (i) requiring franchisees to meet diversion targets; (ii) increasing the capacity for partnership between the City and solid waste haulers; (iii) allowing the City to establish consistent methods for diversion of recyclables and organics; (iv) increasing the City's ability to track diversion, which will enable required reporting and monitoring of state mandated commercial and multi-family recycling; (v) increasing the City's ability to ensure diversion quality in the processing facilities handling its waste and recyclables; and (vi) increasing the City's capacity to enforce compliance with federal, state, county, and local standards. Pursuant to Section 66.32 of the LAMC, the Project's solid waste contractor must obtain, in addition to all other required permits, an AB 939 Compliance Permit from the Bureau of Sanitation.

Within the City of Los Angeles, the Sunshine Canyon Landfill and the Chiquita Canyon Landfill serve existing land uses within the City. Both landfills accept residential, commercial, and construction waste. The Sunshine Canyon Landfill is jointly operated by the City and the County, has a remaining capacity of 64.68 million tons. Chiquita Canyon Landfill currently has a remaining capacity of 1.83 million tons. Thus, the Sunshine Canyon Landfill and the Chiquita Canyon Landfill combined have a remaining permitted capacity of approximately 66.51 million tons. The Sunshine Canyon Landfill has an estimated remaining life of 23 years, and the Chiquita Canyon Landfill has an estimated remaining life of 2 years.<sup>50</sup> An expansion of the Chiquita Canyon Landfill is currently proposed and would add a capacity of

---

<sup>50</sup> *County of Los Angeles Department of Public Works, 2014 Annual Report, Los Angeles Countywide Integrated Waste Management Plan, pages 31 and 32, December 2015.*

48,114,000 tons (a 43-year life expectancy based on 2014 average daily disposal of 3,558 tons per day or 15 years based on the maximum permitted rate of disposal of 10,000 tons per day).<sup>51</sup>

The Proposed Project would follow all applicable solid waste policies and objectives that are required by law, statute, or regulation. As shown in Table III-33 below, for the total unified development of 202,215 square feet of floor area plus the parking garage and residential support areas, it is estimated that the construction of the Proposed Project would generate approximately 2,682 tons of debris during the demolition and construction processes. The Proposed Project would also export approximately 2,000 cy of soil and 22,000 cy of asphalt debris during the demolition and site clearing phases. Under the requirements of the hauler's AB 939 Compliance Permit from the Bureau of Sanitation, all construction and demolition debris will be delivered to a Certified Construction and Demolition Waste Processing Facility.

As shown in Table III-34, Estimated Operational Solid Waste Generation, the Proposed Project's net generation during operation of the Proposed Project would be approximately 1,922 pounds per day. This estimate is conservative, as it does not factor in any recycling or waste diversion programs. The Proposed Project's solid waste would be handled by private waste collection services. The amount of solid waste generated by the Proposed Project is within the available capacities at area landfills, and the Project impacts to regional landfill capacity would be less than significant. Implementation of the following code compliance measures would further reduce the Proposed Project's impacts on solid waste generation.

**Table III-33  
Estimated Construction and Demolition Debris**

Construction Activity	Size	Rate <sup>a</sup> (lbs./sf)	Generated Waste (tons)
<b>Demolition</b>			
On-site Buildings	27,411 sf	155 lbs/sf	2,124
<b>Total Project Demolition Debris Generation:</b>			<b>2,124</b>
<b>Construction</b>			
Small Lot Homes	119,722 sf	4.38 lbs/sf	262
Multi-Family Residential	72,193 sf	4.38 lbs/sf	158
Ground-floor Commercial	10,300 sf	3.89 lbs/sf	20
Automobile Parking Garage	36,730 sf	3.89 lbs/sf	71
Storage, Mechanical Room, etc.	23,565 sf	3.89 lbs/sf	46
<b>Total Project Construction Debris Generation:</b>			<b>558</b>
<b>Proposed Project NET TOTAL:</b>			<b>2,682</b>
<i>Notes: sf = square feet; lbs = pounds</i> <sup>a</sup> USEPA Report No EPA530-98-010, <i>Characterization of Building Related Construction and Demolition Debris in the United States, July 1998.</i> <i>Source: Parker Environmental Consultants, 2016.</i>			

<sup>51</sup> County of Los Angeles Department of Public Works, 2014 Annual Report, Los Angeles Countywide Integrated Waste Management Plan, page 60, December 2015.

**Table III-34**  
**Estimated Operational Solid Waste Generation**

Type of Use	Size	Solid Waste Generation Rate <sup>a</sup> (lbs/unit/day)	Total Solid Waste Generated (lbs/day)
<b>Existing Uses</b>			
Commercial /Auto Body Shop (21,986 sf)	37 emp <sup>b</sup>	10.53 lbs/employee/day	390
Commercial Office	5,425 sf	0.006 lbs/sf/day	32
<b>Total Existing Solid Waste Generation:</b>			<b>432</b>
<b>Proposed Project</b>			
Residential	140 du	12.23 lbs/du/day	1,712
Commercial (10,300 sf)	61 emp <sup>b</sup>	10.53 lbs/employee/day	642
<b>Total Project Solid Waste Generation:</b>			<b>2,354</b>
<i>Less Existing Uses:</i>			<i>-432</i>
<b>NET TOTAL Solid Waste Generation:</b>			<b>1,922</b>
<p>Notes: sf =square feet; du = dwelling units, emp = employee</p> <p><sup>a</sup> L.A. CEQA Thresholds Guide, page M.3-2. Waste generation includes all materials discarded, whether or not they are later recycled or disposed of in a landfill.</p> <p><sup>b</sup> Total proposed commercial space is 10,300 square feet, which consists of approximately 5,150 sf of retail and 5,150 sf of restaurant. The U.S. Green Building Code metrics for building area per employee by building type was used to determine number of employee. Retail Employees were projected based on 1 employee per 588 square feet of retail/commercial space, and restaurant employees were projected based on 1 employee per 100 square feet of high turnover (sit down) restaurant space.</p> <p>Source: Parker Environmental Consultants, 2015.</p>			

- **Regulatory Compliance Measure RC-SW-1 (Designated Recycling Area):** In compliance with the LAMC, the proposed Project shall provide readily accessible areas that serve the entire building and are identified for the depositing, storage, and collection of nonhazardous materials for recycling, including (at a minimum) paper, corrugated cardboard, glass, plastics, and metals.
- **Regulatory Compliance Measure RC-SW-2 (Construction Waste Recycling):** In order to meet the diversion goals of the California Integrated Waste Management Act and the City of Los Angeles, which will total 70 percent by 2013, the Applicant shall salvage and recycle construction and demolition materials to ensure that a minimum of 70 percent of construction-related solid waste that can be recycled is diverted from the waste stream to be landfilled. Solid waste diversion would be accomplished through the on-site separation of materials and/or by contracting with a solid waste disposal facility that can guarantee a minimum diversion rate of 70 percent. In compliance with the LAMC, the General Contractor shall utilize solid waste haulers, contractors, and recyclers who have obtained an Assembly Bill (AB) 939 Compliance Permit from the City of Los Angeles Bureau of Sanitation.
- **Regulatory Compliance Measure RC-SW-3 (Commercial/Multifamily Mandatory Recycling):** In compliance with AB341, recycling bins shall be provided at appropriate locations to promote recycling of paper, metal, glass and other recyclable material. These bins shall be emptied and recycled accordingly as a part of the Proposed Project's regular solid waste disposal

---

program. The Project Applicant shall only contract for waste disposal services with a company that recycles solid waste in compliance with AB341.

**g) Would the project comply with federal, state, and local statutes and regulations related to solid waste?**

**Less Than Significant Impact.** A significant impact may occur if a project would generate solid waste that was not disposed of in accordance with applicable regulations. The Proposed Project would generate solid waste that is typical of small lot residential units and two residential mixed-use buildings with ground-floor retail and restaurant and would comply with all federal, state, and local statutes and regulations regarding proper disposal. Therefore, the Project's solid waste impacts would be less than significant.

**Cumulative Impacts**

**Less Than Significant Impact.** Development of the Proposed Project in conjunction with the nine related projects would further increase regional demands on landfill capacity. The impact of the continued growth of the region would likely have the effect of diminishing the daily excess capacity of the existing landfills serving the City of Los Angeles. Although there are several proposals for new landfills in the region, there are currently few viable options for City of Los Angeles waste past 2029. Table III-35 shows the cumulative solid waste generation in pounds per day. The cumulative operational solid waste generation of the related projects and Proposed Project would contribute approximately 3,605 tons of solid waste per year, which represents a fraction of one percent of the current remaining capacity of the Sunshine Canyon Landfill and the Chiquita Canyon Landfill, which combined have a remaining permitted capacity of approximately 66.51 million tons.

While in the short-term adequate landfill capacity exists to accommodate solid waste generated by the Proposed Project, in the future there will be a need to develop additional landfills and other waste disposal options to accommodate future growth. These options include diversion or transformation as the preferred methods for addressing solid waste and specific and practical applications (i.e., market development, public education and public policy initiatives) within the City.

The City of Los Angeles Solid Waste Management Plan (AB 939) sets forth strategies that would provide adequate landfill capacity through 2037 to accommodate anticipated growth. The Bureau of Sanitation has projected the need for waste disposal capacity based on SCAG's regional population growth projections. The growth associated with Proposed Project is within those projections. Furthermore, projects within the City of Los Angeles must comply with the City's SRRE.

**Table III-35  
Cumulative Operational Solid Waste Generation**

Type of Use	Size	Solid Waste Generation Rate <sup>a</sup> (lbs/unit/day)	Total Solid Waste Generated (lbs/day)
<b>Related Projects</b>			
Dwelling Units <sup>b</sup>	949 du	12.23 lbs/du/day	11,606
Retail / Commercial (63,807 sf) <sup>c</sup>	109 emp	10.53 lbs/employee/day	1,148
Industrial	80,133 sf	0.063 lbs/sf/day	5,048
Office	5,300 sf	0.006 lbs/sf/day	32
<b>Related Projects Total:</b>			<b>17,834</b>
Proposed Project Net Total:			1,922
<b>CUMULATIVE TOTAL:</b>			<b>19,756</b>
<i>Notes: sf = square feet; du = dwelling units; emp = employee</i> <i>Uses not listed are estimated by the closest type of use available in the table.</i> <sup>a</sup> <i>L.A. CEQA Thresholds Guide, page M.3-2. Waste generation includes all materials discarded, whether or not they are later recycled or disposed of in a landfill.</i> <sup>b</sup> <i>Dwelling units include condominiums and multi-family residential units.</i> <sup>c</sup> <i>Generation rates are based on 1 employee per 588 square feet of retail/commercial.</i> -Related Project No. 1 was not included in solid waste generation due to lack of project information. -Conversions and assumptions of square feet per occupant based on California Building Code (2013), Ch.10, Table 1004.1.2. Parker Environmental Consultants, 2016.			

As reported by the Bureau of Sanitation in 2009, the City had achieved a waste diversion rate of 65 percent. The City is exceeding the state-mandated diversion goal of 50 percent by 2000 set by the California Integrated Waste Management Act (CIWMA) of 1989.<sup>52</sup> Waste diversion rates are required to increase to 75 percent by 2025 and through on-going development of waste management infrastructure over the last decade and innovative source reduction, reuse, recycling and composting programs have been implemented. These programs include Green Mulching and Composting workshops, black yard trimming recycling cans, the City-owned Central Los Angeles Refuse Transfer Station (CLARTS) and Residential Special Material and Electronics Recycling or S.A.F.E. Centers. New programs are being implemented to increase the amount of waste diverted by the City, including: multi-family recycling, food waste recycling, commercial recycling and technical assistance and support for City departments to help meet their waste reduction and recycling goals. The City is also developing programs to ultimately meet a goal of zero waste by 2030. Thus, the Proposed Project's contribution to cumulative impacts will continue to decrease as it increases waste diversion rates in accordance with City goals. Therefore, the Project's contribution to cumulative solid waste impacts will be less than cumulatively considerable, and cumulative impacts with respect to solid waste would be less than significant.

<sup>52</sup> *City of Los Angeles Department of Public Works Bureau of Sanitation, Overview of Services for FY 2005/06, updated June 14 2005.*

---

**XVIII. MANDATORY FINDINGS OF SIGNIFICANCE**

- a) **Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?**

**No Impact.** A significant impact may occur only if the Proposed Project would have an identified potentially significant impact for any of the above issues. The Proposed Project is located in a densely populated urban area and would have no unmitigated significant impacts with respect to biological resources and less-than-significant cultural resource impacts provided the mitigation measures listed above are implemented. The Proposed Project would not degrade the quality of the environment, reduce or threaten any fish or wildlife species (endangered or otherwise), or eliminate important examples of the major periods of California history or pre-history. Therefore, no impact would occur.

- b) **Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?**

**Less Than Significant Impact.** A significant impact may occur if the Proposed Project, in conjunction with the nine related projects in the area of the Project Site, would result in impacts that would be less than significant when viewed separately, but would be significant when viewed together. As concluded in this analysis, the Proposed Project’s incremental contribution to cumulative impacts related to aesthetics, agriculture and forestry resources, air quality, biological resources, cultural quality, geology and soils, greenhouse gases, hazards and hazardous materials, hydrology and water quality, land use/planning, mineral resources, noise, population/housing, public services, recreation, transportation/traffic, and utilities would be less than significant. As such, the Proposed Project’s contribution to cumulative impacts would be less than significant.

- c) **Does the project have environmental effects, which would cause substantial adverse effects on human beings, either directly or indirectly?**

**Potentially Significant Unless Mitigation Incorporated.** A significant impact may occur if the Proposed Project has the potential to result in significant impacts, as discussed in the preceding sections. Based on the preceding environmental analysis, the Proposed Project would not have significant environmental effects on human beings, either directly or indirectly. Any potentially significant impacts would be reduced to less-than-significant levels through the implementation of the mitigation measures identified within this analysis.

**APPENDIX F: ENERGY CONSERVATION**

Appendix F: Energy Conservation of the State CEQA Guidelines states the goal of conserving energy implies the wise and efficient use of energy. The State CEQA Guidelines outlines three means to achieve

this goal: (1) Decreasing overall per capita energy consumption, (2) Decreasing reliance on fossil fuels such as coal, natural gas and oil, and (3) Increasing reliance on renewable energy sources.

The Proposed Project includes the development of a mixed-use development on an infill site, which would contribute to the revitalization of the Northridge Community Plan area. The Proposed Project would be required to comply with the energy conservation standards established in Title 24 of the California Administrative Code. California's Energy Efficiency Standards for Residential and Nonresidential Buildings, located at Title 24, Part 6 of the California Code of Regulations and commonly referred to as "Title 24," were established in 1978 in response to a legislative mandate to reduce California's energy consumption. The standards are updated periodically to allow consideration and possible incorporation of new energy efficiency technologies and methods.

California's Building Energy Efficiency Standards are updated on an approximately three-year cycle. The California Energy Commission adopted 2013 Standards on May 31, 2012, and the California Building Standards Commission approved them on January 23, 2013. The 2013 Standards will continue to improve upon the 2008 Standards for new construction of, and additions and alterations to, residential and nonresidential buildings. The 2013 Standards became effective on July 1, 2014, and were a specific response to the mandates of AB 32 and to pursue California energy policy that energy efficiency is the resource of first choice for meeting California's energy needs. The Proposed Project includes energy efficiency components to conserve energy, which are detailed below.

#### *Energy Consumption*

In addition to the Title 24 Energy Efficiency Standards discussed above, the Proposed Project would be required to comply with the L.A. Green Building Code. The L.A. Green Building Code, effective January 1, 2014, requires the use of numerous conservation measures, beyond those required by Title 24 of the California Administrative Code. The L.A. Green Building Code contains both mandatory and voluntary green building measures to conserve energy. Among many requirements, the L.A. Green Building Code requires projects to achieve a 20 percent reduction in wastewater generation. Therefore, compliance with Title 24 of the California Administrative Code and the L.A. Green Building Code would reduce the Proposed Project's energy consumption.

Lastly, the Project would be expected to include energy conservation features. Specifically, the residential units would include low-flow water features and energy conservation appliances. Thus, the proposed residential units would incorporate energy conservation features.

#### *Fossil Fuels*

The Proposed Project would consume fossil fuels, including coal, natural gas and oil for the manufacturing of raw building materials such as steel, metal, aluminum, and concrete aggregate. The Project Site is located within walking distance to transit stops, including Metro bus line 240 located along Reseda Boulevard where it intersects with Chase Street and Roscoe Boulevard. Accessibility to public transportation is acknowledged to reduce vehicle miles traveled and thus would reduce the operational consumption of fossil fuels during the life of the project. The Proposed Project would also provide parking spaces capable of supporting future electric vehicle supply equipment for at least twenty (20) percent of

the total code-required parking spaces provided. The incorporation of EVSE into the project would also serve to reduce fossil fuel as electric vehicles and hybrid electric vehicles would be able to charge up on site.

Additionally, as an infill development, the Proposed Project will incorporate a mix of commercial and residential uses. Because of the Project Site's location near transit service, a number of trips would be expected to be transit or walk trips rather than vehicle trips. Some residents and/or visitors would take transit to their destinations, or would walk to destinations nearby. The reduction in vehicle trips would therefore decrease the Project's reliance on fossil fuels.

### *Renewable Energy*

With respect to renewable energy, the Proposed Project includes the following features:

- **In-Fill Smart Growth:** The Project Site is currently developed with 21,986 square feet of automobile services, a 5,425 square foot commercial office, and surface parking areas. The Project Site is located within a highly developed area within the City of Los Angeles. The Project Site is also located in an area that is adequately served by existing infrastructure.
- **Trip Reduction:** The Proposed Project would provide 163 on-site bicycle parking in bicycle storage spaces pursuant to the City of Los Angeles Bicycle Ordinance (Ord. 182,386).
- **Resource Conservation:** As mandated by the L.A. Green Building Code, the Project would be required to exceed Title 24 2013 standards and include ENERGY STAR appliances. The Project would incorporate energy conservation features in the proposed residential units such as low-flow water fixtures and energy conservation appliances.

With incorporation of the features identified above, the Proposed Project would reduce its environmental impacts with respect to renewable energy.

---

## IV. PREPARERS AND PERSONS CONSULTED

---

### PREPARERS OF THE INITIAL STUDY

#### Lead Agency

City of Los Angeles  
Department of City Planning  
200 North Spring Street, Room 763  
Los Angeles, California 90012

May Sirinopwongsagon

#### Project Applicant

Harridge Cadillac Group, LLC  
6363 Wilshire Boulevard, Suite 600  
Los Angeles, CA 90048

Marc Annotti

#### Environmental Consultants (CEQA)

Parker Environmental Consultants  
23822 Valencia Boulevard, Suite 301  
Santa Clarita, CA 91355

Shane E. Parker, President  
Leanna Williams, Project Manager  
Jennifer Kelley, Environmental Analyst  
Mariana Zimmermann, Associate Environmental Planner  
Elise Lorenzana, Assistant Environmental Planner

#### Architect

Van Tilburg, Banvard & Soderbergh, AIA  
1738 Berkeley Street  
Santa Monica, CA 90404

#### Civil Engineer

PSOMAS  
555 S. Flower Street, Suite 4300  
Los Angeles, CA 90071

John Chappe Jr., Professional Land Surveyor

**Historic Resources Consultant**

GPA Consulting  
231 California Street  
El Segundo, CA 90245

Allison M. Lyons, Associate Architectural Historian

**Geotechnical Engineers**

Feffer Geological Consulting  
1990 S. Bundy Drive, Suite 400  
Los Angeles, CA 90025

Joshua R. Feffer, Principal Engineering Geologist  
Dan Daneshfar, Principal Engineer

**Environmental Engineer**

California Environmental Geologists & Engineers, Inc.  
30423 Canwood Street, Suite 208  
Agoura Hills, CA 91301

Charles I. Buckley, Professional Geologist

**Traffic Consultant**

Overland Traffic Consultants, Inc.  
24325 Main Street #202  
Santa Clarita, CA 91321

**Arborist**

13351-D Riverside Drive #445  
Sherman Oaks, CA 91423

Paul A. Lewis, Landscape Architect

---

## V. REFERENCES AND ACRONYMS

---

### 1. REFERENCES

California Air Resources Board, Final Supplement to the AB 32 Scoping Plan Functional Equivalent Document (FED), Attachment D, August 19, 2011.

California Building Code, Ch. 10. 2013.

California Department of Conservation, Division of Land Resource Protection, Land Resource Protection – Home, Important Farmland Maps in PDF Format, Important Farmland in California, 2006, Map, website: <http://www.conservation.ca.gov/DLRP/Pages/Index.aspx>, accessed October 2015.

California Department of Transportation, Representative Environmental Noise Levels, 1998.

California Department of Transportation, Transportation- and Construction –Induced Vibration Guidance Manual, June 2004.

California Department of Transportation, Transportation and Construction Vibration Guidance Manual, September 2013.

California Environmental Protection Agency, Climate Action Team, Climate Action Team Report to Governor Schwarzenegger and the Legislature, March 2006.

California Environmental Protection Agency, State Water Resources Control Board, Storm Water Program, website: [http://www.swrcb.ca.gov/water\\_issues/programs/stormwater/construction.shtml](http://www.swrcb.ca.gov/water_issues/programs/stormwater/construction.shtml), accessed October 2015.

City of Los Angeles, Air Quality Element of the General Plan, November 24, 1992.

City of Los Angeles, Bureau of Engineering, Navigate LA, website: <http://navigatela.lacity.org>, accessed October 2015.

City of Los Angeles, CEQA Thresholds Guide, 2006.

City of Los Angeles Citywide General Plan Framework EIR, July 17, 1996.

City of Los Angeles Department of City Planning, Northridge Community Plan, 2009 Population Estimate, website: <http://cityplanning.lacity.org/DRU/Loc/LocFrame.cfm?geo=CP&loc=Nrd&sgo=ct&rpt=PnH&yrc=Y09>, accessed October 2015.

City of Los Angeles Department of City Planning, Northridge Community Plan Area Map, website: <http://planning.lacity.org/complan/valley/PDF/nrdplanmap.pdf>, accessed October 2015.

City of Los Angeles Department of City Planning, Northridge Community Plan, website: <http://planning.lacity.org/complan/pdf/nrdcptxt.pdf>, accessed October 2015.

- City of Los Angeles Department of City Planning, Demographic Research Unit, Statistical Information, Local Population and Housing Estimates, website: <http://cityplanning.lacity.org/DRU/HomeLocl.cfm>, accessed October 2015.
- City of Los Angeles Department of City Planning, Environmental and Public Facilities Maps, September 1996.
- City of Los Angeles Department of City Planning, Environmental and Public Facilities Maps, Areas Containing Significant Mineral Deposits in the City of Los Angeles, September 1996.
- City of Los Angeles Department of City Planning, Environmental and Public Facilities Maps, Critical Facilities & Lifeline Systems in the City of Los Angeles, September 1996.
- City of Los Angeles Department of City Planning, Environmental and Public Facilities Maps, Oil Field and Oil Drilling Areas in the City of Los Angeles, September 1996.
- City of Los Angeles Department of City Planning, Environmental and Public Facilities Maps: Prehistoric and Historic Archaeological Sites and Survey Areas in the City of Los Angeles, September 1996.
- City of Los Angeles Department of City Planning, Environmental and Public Facilities Maps: Vertebrate Paleontological Resources in the City of Los Angeles, September 1996.
- City of Los Angeles Department of City Planning, Parcel Profile Reports, Zoning Information and Map Access System (ZIMAS), <http://www.zimas.lacity.org>, accessed October 2015.
- City of Los Angeles Department of City Planning, The Citywide General Plan Framework, An Element of the City of Los Angeles General Plan, adopted December 11, 1996 and re-adopted August 8, 2001.
- City of Los Angeles, Downtown Design Guide, June 15, 2009.
- City of Los Angeles Department of Public Works, Bureau of Sanitation, Hyperion Treatment Plant, website: [http://san.lacity.org/lasewers/treatment\\_plants/hyperion/index.htm](http://san.lacity.org/lasewers/treatment_plants/hyperion/index.htm), accessed October 2015.
- City of Los Angeles Department of Public Works, Bureau of Sanitation, Wastewater, Facts & Figures, website: <http://www.lacitysan.org/wastewater/factsfigures.htm>, accessed October 2015.
- City of Los Angeles, Department of Public Works, Sanitation Department, Sewer System Management Plan, February 2015.
- City of Los Angeles Department of Recreation and Parks, Facility Locator, website: <http://www.laparks.org/>, accessed October 2015.
- City of Los Angeles Department of Water and Power, 2010 Urban Water Management Plan, LADWP Board of Water and Power Commissioners Resolution No. 011268, adopted May 3, 2011.

City of Los Angeles, Development Best Management Practices Handbook, Low Impact Development Manual, Part B Planning Activities. Fourth Edition, June 2011.

City of Los Angeles, Green Building Code (Ordinance No. 181,480).

City of Los Angeles Municipal Code.

City of Los Angeles, Noise Element of the General Plan, November 24, 1992.

City of Los Angeles Noise Ordinance (LAMC Section 112.05)

City of Los Angeles Ordinance 144331 and 161574.

City of Los Angeles Protected Tree Ordinance 177404.

City of Los Angeles Public Library, Hours and Locations, website: <http://www.lapl.org/branches>, accessed October 2015.

City of Los Angeles, Safety Element of the Los Angeles City General Plan, Exhibit G, Inundation & Tsunami Hazard Areas, March 1994.

County of Los Angeles Department of Public Works, *2013 Annual Report, Los Angeles Countywide Integrated Waste Management Plan*, May 2015.

Federal Emergency Management Agency (FEMA), Flood Insurance Rate Map, Los Angeles County, California and Incorporated Areas, Map number 06037C1620F, September 26, 2008.

Federal Emergency Management Agency, Flood Insurance Rate Maps, Search by Street Address, website: <https://msc.fema.gov/portal>, accessed October 2015.

Federal Transit Administration (Harris Miller Miller & Hanson), Transit Noise and Vibration Impact Assessment, May 2006.

Green LA: An Action Plan to Lead the Nation In Fighting Global Warming. City of Los Angeles, May 2007.

Institute of Transportation Engineers, Trip Generation Manual – 9<sup>th</sup> Edition, 2012.

Intergovernmental Panel on Climate Change, Fifth Assessment Report, 2013.

Los Angeles County Department of Public Work, Disaster Route Maps by City, City of Los Angeles – Valley Area Map, website: <http://dpw.lacounty.gov/dsg/DisasterRoutes/city.cfm>, accessed October 2015.

Los Angeles County Congestion Management Plan (CMP), 2010.

Los Angeles Department of Water and Power, Urban Water Management Plan, 2010.

Los Angeles Department of Water and Power, website: <http://wsoweb.ladwp.com/Aqueduct/historyoflaa/waterquality.htm>, accessed October 2015.

Los Angeles Fire Department, FireStatLA, City-wide and Station Response Metrics for 2015. Website: <http://www.lafd.org/fsla/stations-map>, accessed October 2015.

Los Angeles Police Department, COMPSTAT Unit, Devonshire Area Profile, accessed October 20, 2015.

Los Angeles Unified School District, Residential Development School Fee Justification Study, Table 5, February 25, 2008.

Los Angeles Unified School District, Resident School Identifier, website: <http://rsi.lausd.net/ResidentSchoolIdentifier/>, accessed October 2015.

National Cooperative Highway Research Program Report 117, Highway Noise: A Design Guide for Highway Engineers, 1971.

Senate Bill 97, August 2007.

Senate Bill 375, September 2008.

Senate Bill 743, September 2013.

South Coast Air Quality Management District, Final 2012 Air Quality Management Plan, February 2013.

South Coast Air Quality Management District, Air Quality Significance Thresholds, Revision March 2015, website: <http://www.aqmd.gov/docs/default-source/ceqa/handbook/scaqmd-air-quality-significance-thresholds.pdf?sfvrsn=2>, accessed October 2015.

South Coast Air Quality Management District, California Emissions Estimator Model (CalEEMod Version 2013.2.3), 2013.

South Coast Air Quality Management District, CEQA Air Quality Handbook, 1993, page 5-1.

South Coast Air Quality Management District, Final Localized Significance Threshold Methodology, June 2003, Revised July 2008.

Southern California Association of Governments, Regional Comprehensive Plan and Guide.

Southern California Association of Governments, 2012-2035 Regional Transportation Plan, Sustainable Communities Strategy, Towards a Sustainable Future. April 2012.

State of California Assembly Bill (AB 32), *the California Global Warming Solutions Act of 2006*, 2006.

State of California Department of Conservation, Division of Land Resource Protection, Farmland Mapping and Monitoring Program, Los Angeles County Important Farmland 2012, Map. <ftp://ftp.consrv.ca.gov/pub/dlrp/FMMP/pdf/2012/los12.pdf>, accessed October 2015.

State of California, Department of Conservation, Canoga Park Quadrangle Seismic Hazard Zones Map, February 1, 1998.

Stormwater LID Ordinance (No. 181899), 2011.

Title 24 of the California Code of Regulations.

United States Environmental Protection Agency, Noise from Construction Equipment and Operations, Building Equipment and Home Appliances, PB 206717, 1971.

USEPA Report No. EPA530-98-010. *Characterization of Building Related Construction and Demolition Debris in the United States*, June 1998, page A-1.

United States Census Bureau, 2010 Census Interactive Population Map, website: <http://www.census.gov/2010census/popmap/>, accessed October 2015.

Williamson Act Program, California Division of Land Resource Protection, website: [ftp://ftp.consrv.ca.gov/pub/dlrp/wa/2012\\_Statewide\\_Map/WA\\_2012\\_8x11.pdf](ftp://ftp.consrv.ca.gov/pub/dlrp/wa/2012_Statewide_Map/WA_2012_8x11.pdf), accessed October 2015.

## 2. ACRONYMS AND ABBREVIATIONS

AAM	Annual Arithmetic Mean
AB	Assembly Bill
ACM	Asbestos-containing materials
AEP	Association of Environmental Professionals
AFY	Acre-feet per year
APN	Assessor Parcel Number
AQMP	Air Quality Management Plan
ASTM	American Society of Testing and Materials
ASTs	above-ground storage tanks
ATCS	Adaptive Traffic Control System
Basin	South Coast Air Basin
BMPs	Best Management Practices
C/D	construction/demolition
CAA	Clean Air Act
CAAQS	California ambient air quality standards
Cal/EPA	California Environmental Protection Agency
CAPCOA	California Air Pollution Control Officers Association

---

CARB	California Air Resources Board
CAT	Climate Action Team
CBC	California Building Code (2007)
CCAA	California Clean Air Act
CCAR	California Climate Action Registry
CCR	California Code of Regulations
CDFG	California Department of Fish and Game
CDMG	California Division of Mines and Geology
CEC	California Energy Commission
CEQA	California Environmental Quality Act
CERCLIS	Comprehensive Environmental Response, Compensation, and Liability Information System
Cf	Cubic feet
CFC	Chlorofluorocarbons
CGS	California Geological Survey
CH <sub>4</sub>	Methane
CHMIRS	California Hazardous Material Incident Report System
CiSWMPP	City of Los Angeles Solid Waste Management Policy Plan
City Zoning Code	City of Los Angeles Planning and Zoning Code
CMP	Congestion Management Plan
CNEL	Community Noise Exposure Level
CO	carbon monoxide
CO <sub>2</sub>	carbon dioxide
CO <sub>2</sub> e	carbon dioxide equivalent
COHb	carboxyhemoglobin
COPC	Chemical of Potential Concern
CORRACTS	Corrective Action Treatment, Storage, and Disposal Facilities
CPA	Community Plan Area
CPT	cone penetrometer test
CPU	Crime Prevention Unit
CRA/LA	Community Redevelopment Agency of the City of Los Angeles
CWA	Clean Water Act
CWC	California Water Code
cy	cubic yards
dB	decibel
dBA	A-weighted decibel scale
d/D	flow level
DHS	California Department of Health and Services
DWP	Department of Water and Power
DWR	California Department of Water Resources
du	dwelling unit
EIR	Environmental Impact Report

---

EMS	Emergency Medical Service
EOO	Emergency Operations Organization
EPA	Environmental Protection Agency
ERNS	Emergency Response Notification System
EZ	Los Angeles State Enterprise Zone
FAR	Floor Area Ratio
FCAA	Federal Clean Air Act
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
GBCI	Green Building Certification Institute
GHG	greenhouse gas
gpd	gallons per day
gpm	gallons per minute
GWP	Global Warming Potential
HFC	hydrofluorocarbons
HSA	Hyperion Service Area
HTP	Hyperion Treatment Plant
HVAC	Heating, Ventilation and Air Conditioning
I-10	Santa Monica Freeway
I-101	Hollywood Freeway
ISO	Interim Control Ordinance
ITE	Institute of Transportation Engineers
km	kilometers
kV	kilovolt
kWh	kilowatt-hours
LAA	Los Angeles Aqueduct
LABS	Los Angeles Department of Public Works Bureau of Sanitation
LADBS	Los Angeles Department of Building and Safety
LADOT	Los Angeles Department of Transportation
LADRP	Los Angeles Department of Recreation and Parks
LADWP	Los Angeles Department of Water and Power
LAFD	Los Angeles Fire Department
LAMC	Los Angeles Municipal Code
LAPD	Los Angeles Police Department
LAPL	Los Angeles Public Library
LARWQCB	Los Angeles Regional Water Quality Control Board
LAUSD	Los Angeles Unified School District
LBP	Lead-based paint
lbs/day	pounds per day
LCFS	Low Carbon Fuel Standard
L <sub>dn</sub>	day-night average noise level
LEED	Leadership in Energy and Environmental Design

---

L <sub>eq</sub>	equivalent energy noise level/ambient noise level
LOS	Level of Service
LST	localized significance thresholds
LUST	leaking underground storage tank
LUTP	Land Use/Transportation Policy
MBTA	Migratory Bird Treaty Act
MCE	Maximum Considered Earthquake
MEP	maximum extent practicable
Metro	Los Angeles County Metropolitan Transit Authority
mgd	million gallons per day
mi	miles
MPO	Metropolitan Planning Organization
MS4	medium and large municipal separate storm sewer systems
msl	mean sea level
mm	millimeters
M <sub>max</sub>	maximum moment magnitude
MTA	Metropolitan Transportation Authority
MWD	Metropolitan Water District
MWh	Mega-Watt hours
N <sub>2</sub> O	nitrous oxide
NAAQS	National ambient air quality standards
NFRAP	No Further Remedial Action Planned Sites
NO <sub>2</sub>	nitrogen dioxide
NOP	Notice of Preparation
NOx	nitrogen oxides
NPDES	National Pollutant Discharge Elimination System
NPL	National Priorities List
O <sub>3</sub>	Ozone
OAL	California Office of Administrative Law
OPR	Office of Planning and Research
Pb	lead
PEC	Potential environmental concern
PFC	perfluorocarbons
PGA	peak horizontal ground acceleration
PM	particulate matter
PM <sub>10</sub>	respirable particulate matter
PM <sub>2.5</sub>	fine particulate matter
ppd	pounds per day
ppm	parts per million
PRC	Public Resources Code
PSI	pounds per square inch
PUC	Public Utilities Commission (also see CPUC)

---

PWS	Public water suppliers
RCP	Regional Comprehensive Plan
RCPG	Regional Comprehensive Plan and Guide
RCRA	Resource Conservation Recovery Act
RD	Reporting District
REC	Recognized Environmental Condition
ROG	Reactive Organic Gases
RTP	Regional Transportation Plan
RWQCB	Regional Water Quality Control Board
SB	Senate Bill
SCAB	South Coast Air Basin
SCAG	Southern California Association of Governments
SCAQMD	South Coast Air Quality Management District
SCG	Southern California Gas Company
SCH	State Clearinghouse
sf	square feet
SF <sub>6</sub>	sulfur hexafluoride
SIP	State Implementation Plan
SLIC	Spills, Leaks, Investigation and Cleanup
SO <sub>2</sub>	sulfur dioxide
SO <sub>4</sub>	sulfates
SO <sub>x</sub>	sulfur oxides
SOPA	Society of Professional Archeologist
SPT	Standard Penetration Test
SR-110	Harbor Freeway
SRA	source receptor area
SRRE	Source Reduction and Recycling Element
SWAT	Solid Waste Assessment Test
SWF/LF	Solid Waste Information System
SWFP	Solid Waste Facility Permit
SWMP	Stormwater Management Plan
SWP	State Water Project
SWPPP	Storm Water Pollution Prevention Plan
SWRCB	State Water Resource Control Board
TAC	Toxic Air Contaminants
TOD	Transit Oriented District
TPH	total petroleum hydrocarbons
TSD	Treatment, Storage, and Disposal
TSP	Transportation Specific Plan
ULSD	Ultra Low Sulfur Diesel
US-101	Hollywood Freeway
USEPA/ U.S. EPA	United States Environmental Protection Agency

USFWS	United States Fish and Wildlife Service
USGBC	United States Green Building Council
USGS	U.S. Geological Survey
UST	underground storage tank
UWMP	Urban Water Management Plan
V/C	Volume-to-Capacity
VCP	Voluntary Cleanup Plan
VdB	Vibration decibels
VMT	Vehicle Miles Traveled
VOC	Volatile Organic Compound
WMA	Watershed Management Area
WMUDS	Waste Management Unit Database System
WSA	Water Supply Assessment
µg/m <sup>3</sup>	micrograms per cubic meter
ZIMAS	Zoning Information and Map Access System